

CITY OF WEST TORRENS



Confidential Report Item 8.1

of the

COUNCIL ASSESSMENT PANEL

will be held in the George Robertson Room, Civic Centre
165 Sir Donald Bradman Drive, Hilton

on

TUESDAY, 14 FEBRUARY 2023
at 5.00pm

Pursuant to section 236(2) of the *Planning, Development and Infrastructure Act 2016* and clauses 16 & 17 of the *Assessment Panel Members – Code of Conduct*, it is an offence to disclose the information provided in confidence within this agenda except with prior approval of the Assessment Manager.

Hannah Bateman
Assessment Manager

City of West Torrens Disclaimer

Council Assessment Panel

Please note that the contents of this Council Assessment Panel Agenda have yet to be considered and deliberated by the Council Assessment Panel therefore the recommendations may be adjusted or changed by the Council Assessment Panel in the process of making the formal Council Assessment Panel decision.

Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.

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Released

8 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER

8.1 239, 241-243 Richmond Road, RICHMOND

Application No 21028599

Reason for Confidentiality

It is recommended that this Report be considered in CONFIDENCE in accordance with regulation 13(2)(a) (vii) of the *Planning, Development and Infrastructure (General) Regulations 2017*, which permits the meeting to be closed to the public for business relating to the following:

- (vii) matters that must be considered in confidence in order to ensure that the assessment panel, or any other entity, does not breach any law, or any order or direction of a court or tribunal constituted by law, any duty of confidence, or other legal obligation or duty;

as this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.

DEVELOPMENT APPLICATION DETAILS

APPLICATION NUMBER	21028599
APPLICANT	Rod Roach
ADDRESS	239, 241-243 Richmond Road, RICHMOND
NATURE OF DEVELOPMENT	Demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies (one bulky good) with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of regulated tree
ZONING INFORMATION	<p>Zones</p> <ul style="list-style-type: none"> • Employment <p>Overlays</p> <ul style="list-style-type: none"> • Aircraft Noise Exposure (ANEF 20) • Airport Building Heights (Regulated) • Building Near Airfields • Hazards (Flooding - Evidence Required) • Major Urban Transport Routes • Prescribed Wells Area • Regulated and Significant Tree • Traffic Generating Development <p>Technical Numeric Variations (TNVs)</p> <ul style="list-style-type: none"> • Maximum Building Height (Metres) (Maximum building height is 8.5m) • Maximum Building Height (Levels) (Maximum building height is 2 levels)
LODGEMENT DATE	1 October 2021
RELEVANT AUTHORITY	Council Assessment Panel
CODE VERSION	2021.14

CATEGORY OF DEVELOPMENT	Code Assessed - Performance Assessed
NOTIFICATION	Yes
REFERRALS	Internal <ul style="list-style-type: none"> City Assets - Traffic External <ul style="list-style-type: none"> Nil
DELEGATION	CAP <ul style="list-style-type: none"> A representor has lodged a valid representation and wishes to be heard
RECOMMENDING AUTHOR	Phil Smith
RECOMMENDATION	Support with conditions (as a result of a compromise)

BACKGROUND

The application was presented to the Council Assessment Panel at its meeting held on 11 October 2022 with a recommendation to support the application.

The CAP made the determination to refuse the application for the following reasons:

- Employment Zone, Desired Outcome 2
- Employment Zone, Performance Outcome 3.2
- Employment Zone, Designated Performance Feature 3.2
- Employment Zone, Performance Outcome 5.2
- Employment Zone, Designated Performance Feature 5.2
- Regulated and Significant Tree Overlay, Desired Outcome 1
- Regulated and Significant Tree Overlay, Performance Outcome 1.1 & 1.2 & 1.4
- General Development Policies, Design, Desired Outcome 1
- General Development Policies, Design, Landscaping, Performance Outcome 3.1 & 3.2
- General Development Policies, Design, Water Sensitive Design, Performance Outcome 5.1 & 5.2
- General Development Policies, Design, Car parking Appearance, Performance Outcome 7.1 & 7.6 & 7.7
- General Development Policies, Transport, Access and Parking, Vehicle Access, Performance Outcome 3.1

It should be noted that under General Development Policies, Design, Water Sensitive Design, Performance Outcome 5.2 does not exist in the Planning and Design Code.

An extract of the 11 October 2022 CAP meeting agenda report and minutes relating this application is included in **Attachment 2**.

AMENDMENTS

The compromise comprises the following:

- No changes are proposed to the built form or building envelope;
- An amended landscape plan has been prepared by Oxigen landscape architects that includes a more robust design including additional tree and plant species to be located throughout the site;
- The landscape plan comprises a total of 23 trees and 274 plant species includes amongst others, *Pistacia chinensis* (Chinese Pistache), *Syzygium austral* (creek lilly pilli), *Agapanthus*, *Acacias*, *Grevilleas*, *Lomandras* and *Viburnums*.

Details of the compromise, including a planning statement from Lou Fantasia planning consultant and a landscape plan from Oxigen Pty Ltd are contained in **Attachment 1**.

QUANTITATIVE STANDARDS

The amended proposal is assessed for consistency with the quantitative requirements of the Planning and Design Code as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
LANDSCAPING <i>Employment Zone</i> PO 5.1/DPF 5.1	DPF 5.1 10% of the site A minimum dimension of 1.5m	6.3% of site approx. Does not satisfy DPF Minimum dimension of 1.0m Does not satisfy DPF
BUILDING SETBACK <i>Employment Zone</i> PO 3.2/DPF 3.2	DPF 3.2 Building walls are no closer than 2m to the secondary street boundary	1m approx. Does not satisfy

ASSESSMENT

The Desired Outcomes (DO) and Performance Outcomes (PO) that comprise the grounds of refusal are expressed as follows:

Employment Zone

DO 2

Distinctive building, landscape and streetscape design to achieve high visual and environmental amenity particularly along arterial roads, zone boundaries and public open spaces.

PO 3.2

Buildings are set back from a secondary street boundary to accommodate the provision of landscaping between buildings and the street to enhance the appearance of land and buildings when viewed from the street.

DTS/DPF 3.2

Building walls are no closer than 2m to the secondary street boundary.

PO 5.2

Development incorporates areas for landscaping to enhance the overall amenity of the site and locality.

DTS/DPF 5.2

Landscape areas comprise:

- not less than 10 percent of the site;
- a dimension of at least 1.5m.

Regulated and Significant Tree Overlay**DO 1**

Conservation of regulated and significant trees to provide aesthetic and environmental benefits and mitigate tree loss.

PO 1.1

Regulated trees are retained where they:

- make an important visual contribution to local character and amenity;
- are indigenous to the local area and listed under the National Parks and Wildlife Act 1972 as a rare or endangered native species; and / or
- provide an important habitat for native fauna.

PO 1.2

Significant trees are retained where they:

- make an important contribution to the character or amenity of the local area;
- are indigenous to the local area and are listed under the National Parks and Wildlife Act 1972 as a rare or endangered native species;
- represent an important habitat for native fauna;
- are part of a wildlife corridor or a remnant area of native vegetation;
- are important to the maintenance of biodiversity in the local environment; and / or
- form a notable visual element to the landscape of the local area.

PO 1.4

A tree-damaging activity in connection with other development satisfies all the following:

- it accommodates the reasonable development of land in accordance with the relevant zone or subzone where such development might not otherwise be possible;
- in the case of a significant tree, all reasonable development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.

General Development Policies - Design

DO 1

Development is:

- contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributes to the character of the immediate area;
- durable - fit for purpose, adaptable and long lasting;
- inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access, and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors;
- sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

PO 3.1

Soft landscaping and tree planting is incorporated to:

- minimise heat absorption and reflection;
- maximise shade and shelter;
- maximise stormwater infiltration;
- enhance the appearance of land and streetscapes;
- contribute to biodiversity.

PO 3.2

Soft landscaping and tree planting maximises the use of locally indigenous plant species, incorporates plant species best suited to current and future climate conditions and avoids pest plant and weed species.

PO 5.1

Development is sited and designed to maintain natural hydrological systems without negatively impacting:

- the quantity and quality of surface water and groundwater;
- the depth and directional flow of surface water and groundwater;
- the quality and function of natural springs.

PO 7.1

Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on the streetscapes through techniques such as:

- limiting protrusion above finished ground level;
- screening through appropriate planting, fencing and mounding;
- limiting the width of openings and integrating them into the building structure.

PO 7.6

Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.

PO 7.7

Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.

General Development Policies - Transport, Access and Parking

PO 3.1

Safe and convenient access minimises impact or interruption on the operation of public roads.

GROUNDINGS FOR REFUSAL ASSESSMENT

Effectively the CAP refusal revolves around the following components:

- Trees/Landscaping;
- Building setbacks;
- Traffic management; and
- Waste Management.

Each will be dealt with as they relate to the grounds of refusal and how the compromise satisfies or does not satisfy the relevant PO's.

It is noted that the applicant has chosen not to address General Development Policies - Design PO 5.1 and PO7.7 relating to stormwater. Therefore thus the original stormwater reserved matter has been converted to a condition in the recommendation, as it is understood to be ERD Court practice to condition these matters.

Also, it is noted that General Development Policies - Design PO7.1 was included in the reasons for refusal, however this PO specifically relates to development involving semi-basement and undercroft car parking and therefore is not applicable to this proposal.

Trees/Landscaping

With respect to tree retention, it is noted that the easternmost, protected tree as shown in the Lou Fantasia planning statement, has a Tree Protection Zone (TPZ) radius of approximately 9.4m or circumference of approximately 18.8m. The TPZ thus takes up a considerable amount the site in a location on the site that would be reasonably expected to be developed. In other words, it would not be possible to develop Warehouse 1 without the removal of the tree. Mr Fantasia has carried out a measurement of this tree and has determined that the tree is regulated and not significant, therefore PO 1.2 no longer applies. The Regulated and Significant Tree Overlay DO 1, PO 1.1 and PO 1.4 are considered to be reasonably satisfied.

Furthermore, it is considered that the landscape design prepared by Oxygen Pty Ltd will enhance the streetscape and the buildings in general. With 23 trees planted and 274 plants incorporated into the site, the impact of the built form will be appropriately softened so that it does not appear as imposing or unattractive. Given this section of Richmond Road is not aesthetically pleasing from a landscape perspective or built form perspective, the proposed landscaping of the site will form a notable visual element to local area and result in significant improvements to the streetscape appearance of the land.

While the percentage of landscaping is less than the 10% noted in Employment Zone DPF 5.2, this is not considered to be fatal to the application. It is also acknowledged that the minimum dimension of 1.5m is not achieved, also not considered to be fatal. The reason being, the landscaping currently on site is not orderly and appears to be in a poor and unkempt state. The clearing of the site and starting anew will allow for a greater number of species to be planted and a far superior and sustainable streetscape appearance to be achieved. It is further noted the planting of a number of trees throughout the site shall minimise heat absorption and reflection, contribute to shade and shelter, provide for stormwater infiltration and biodiversity and enhance the appearance of land and streetscapes. The fact that the two above criteria are not met is considered to be outweighed by an end result that proposes a far superior landscape outcome than the current state of the site as sought by Employment Zone PO 5.2. On balance, an organised and robust landscape plan is preferable to the current state of the site.

As such, these satisfy the requirements of Employment Zone DO 2 and PO 5.2, and General Development Policies - Design DO 1, PO 3.1 and PO 3.2.

Building setbacks

It is noted no change to the built form has occurred with the compromise. The application was partly refused as a result of the built form being unable to achieve a 2m setback from the site's western boundary, thus not satisfying Employment Zone DPF 3.2. It is noted that City Assets have raised concern with pedestrian sightlines requirements of AS/NZS 2890.1:2004 from the exit point of the car park to Chambers Avenue due to the building wall of Warehouse 3.

Nonetheless, while DPF 3.2 is not satisfied, it could be argued that PO 3.2 is satisfied. PO 3.2 states that:

Buildings are set back from a secondary street boundary to accommodate the provision of landscaping between buildings and the street to enhance the appearance of land and buildings when viewed from the street.

When assessing applications in proximity to angled boundaries where it becomes a challenge to achieve the required setback, some dispensation is considered appropriate where the impact would effectively be no different had the required setback been achieved in the first instance. In this case, the application seeks to consider the intent of PO 3.2 by staggering the setback facing Chambers Avenue. In most instances, the shortfall is approximately 1m. Re-positioning the building an additional 1m would provide minimal, tangible relief, which may be considered inconsequential to the overall design.

As illustrated on the landscape plan prepared by Oxygen Pty Ltd, there is sufficient space for landscaping along this elevation. The PO nor DPF do not specify the type or height of landscaping required and it is considered that the landscaping proposed will enhance the appearance of the building. Furthermore, Part 1 of the Planning and Design Code - "Rules of Interpretation" states that a DPF is not the only way a PO can be achieved, thus it is considered that Employment Zone DO 2 and PO 3.2 is in fact satisfied.

Traffic Management

With respect to the compromise, no changes are proposed by the applicant in relation to traffic management. Council's Traffic Engineer has reviewed the attached documentation and is supportive of the development noting that it has been designed in accordance with the referral response prepared by the Commissioner of Highways. Council's Traffic Engineer has recommended that the front car park exit crossover be shifted slightly northwards to align with Car Park 20 to maximise the separation distance from the tangent point of the intersection, however noting that the 6m separation requirement would still not be achieved. The shift of the exit crossover has been included as a condition in the recommendation, if required and deemed appropriate by the Panel.

With respect to the rear car park, the traffic engineer has recommended that a condition restricting maximum vehicle size to an SRV size be included which is included in the recommendation.

It is considered that the access continues to be appropriate and will serve the site in a safe and efficient manner in accordance with the General Development Policy, Traffic, Access and Parking, PO 3.1.

PO 7.6 is considered to be satisfied as vehicle parking areas and associated driveways will be landscaped to provide shade and positively contribute to amenity. As noted previously, 23 trees will be planted and 274 plants incorporated throughout the site to provide shade and improve the aesthetics of the site.

The updated referral response from City Assets is contained in **Attachment 3**.

Waste Management

With respect to the compromise proposal, no changes are proposed by the applicant in relation to traffic, nonetheless two new conditions have been proposed in the recommendation to ensure on-site waste management is suitably addressed. The City Assets suggestion that the servicing of waste bins to occur outside business hours has not been included as a condition in the recommendation due to the potential adverse noise amenity impact on neighbouring residential properties.

SUMMARY

The revised proposal has sought to address the reasons for refusal through a number of changes, while maintaining the fundamental nature of the development.

On balance, the proposed development sufficiently accords with the relevant provisions contained within the Planning and Design Code Version 2021.14 and has satisfactorily addressed the relevant grounds of refusal.

To summarise, the proposal establishes a landscape plan which incorporates a number of positive features such as canopy trees, soft landscaping and a number of plant species. Reduced building setbacks will be softened by plantings around the perimeter of the site. Traffic management remains consistent with traffic advice provided by the Commissioner of Highways and is supported by the City Assets Traffic Engineer.

Overall, it is recommended that the development be supported and the Environment, Resources and Development Court be advised accordingly.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. The Council Assessment Panel, having considered the application for consent to carry out development of land (as amended) and pursuant to the provisions of the *Planning Development and Infrastructure Act 2016* finds the proposal to be not seriously at variance with the Planning and Design Code and resolves to advise the Environment Resources and Development Court that it does SUPPORT Planning Consent for Application No. 21028599 by Rod Roach to carry out demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies (one bulky good) with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of regulated tree at 239, 241-243 Richmond Road, RICHMOND (CT-5823/131, CT-6157/995) subject to the following conditions of consent:

Planning Consent Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application except where varied by any conditions listed below:

The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application except where varied by any conditions listed below:

- Existing/Demolition Site Plan, Sheet DA 1, prepared by Rod Roach Architecture, dated July 2021;
- Site and Floor Plan - Sheet DA 2A, prepared by Rod Roach Architecture, dated March 2022;
- Elevations, Sheet DA 4A, prepared by Rod Roach Architecture, dated March 2022;

- Page 11 of the Development Applicant Report - Schedule of Materials and Finishes prepared Rod Roach, dated August 2021;
 - Sections, Sheet DA 5, prepared by Rod Roach Architecture, dated July 2015;
 - Roof Plan, Sheet DA 3, prepared by Rod Roach Architecture, dated July 2021;
 - Landscape Plan, Sheet 101, prepared by Oxigen Pty Ltd, dated 14 December 2022.
2. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
 - a) Result in the entry of water into a building;
 - b) Affect the stability of a building;
 - c) Create unhealthy or dangerous conditions on the site or within the building;
 - d) Flow or discharge onto the land of an adjoining owner;
 - e) Flow across footpaths or public ways; or
 - f) Discharge to the adjacent creek.
 3. All driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) properly drained, and shall be maintained in a reasonable condition at all times.
 4. All car parking spaces shall be linemarked, in accordance with the approved plans and in accordance with Australian Standards Association Code AS 2890.1, 2004 Parking Facilities, Part 1, Off Street Car Parking, prior to the occupation of the proposed development. Linemarking and directional arrows shall be clearly visible at all times.
 5. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for storage or display of materials or goods including waste products and refuse.
 6. The stormwater connection through the road verge area is to be constructed of shape and material to satisfy Council's standard requirements:
 - 100 x 50 x 2mm RHS Galvanised Steel or
 - 125 x 75 x 2mm RHS Galvanised Steel or
 - Multiples of the above.
 7. Hours of operation for the site shall be as follows:
 - Shops - Monday to Friday - 7am to 6pm, Saturday 7am to 5pm, Sundays and Public Holidays - Closed
 - Warehouses - Monday to Friday - 7am to 6pm, Saturday 7am to 1pm, Sundays and Public Holidays - Closed
 8. That the maximum service vehicle, excluding refuse collection vehicle, shall be limited to a SRV as specified in Australian Standard 2890.2 - 2002 Parking Facilities, Part 2, Off-Street Commercial Vehicle Facilities.
 9. All planting and landscaping, with the exception of tree planting, shall be completed within six (6) months of occupation or the next available planting season, incorporate an appropriate irrigation system and be maintained in a reasonable condition at all times. Any plants that become diseased or die will be replaced with a suitable species to the reasonable satisfaction of the relevant authority.
 10. Landscaping shall provide clear sightlines at the property boundary to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath, in accordance with Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004.

11. All waste and other rubbish shall be stored in a manner so that it does not, in the reasonable opinion of the Council, create:
 - insanitary conditions on or off the site;
 - an unreasonable nuisance off the site; or
 - pollution to the environment (including by pollution caused by substances, materials or things entering the stormwater system either by wind or water).
12. All waste and other rubbish shall be stored in the designated areas and screened from public view in accordance with the approved plans.
13. A Stormwater Management Plan that demonstrates how stormwater is to be detained, discharged, collected and re-used in accordance with the relevant authority's requirements and to the satisfaction and approval of Council administration. A Finished Floor Level of 12.86 is required.
14. A revised site plan shall be provided to Council prior to Building Rules Consent being granted, identifying the relocation of the exit crossover from Chambers Avenue being shifted north in order to align with Car Park 20.

Commissioner of Highways Conditions

15. An amended plan shall be provided to Council prior to the issue of Development Approval showing vehicular access via Richmond Road limited to an ingress only access in reference to plan DA 2A dated March 2022, the access point angled at 70 degrees to the road and suitably signed and linemarked to reinforce the desired traffic flow.
16. All vehicles shall enter and exit the site in a forward direction and all vehicle manoeuvring areas shall be kept clear of impediments to manoeuvring.
17. The redundant crossover on Richmond Road shall be closed and reinstated to Council's kerb and gutter standards at the applicant's expense.
18. Stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

FURTHER

1. Pursuant to regulation 13(2) of the *Planning, Development and Infrastructure (General) Regulations 2017*, Item 8.1 - 239, 241-243 Richmond Road, RICHMOND, including the report, attachments and any discussions (excluding the decision), having been dealt with in confidence under regulation 13(2)(a)(vii) of the *Planning, Development and Infrastructure (General) Regulations 2017* and in accordance with regulation 14(4) of the *Planning, Development and Infrastructure (General) Regulations 2017*, be kept confidential until a decision of the Environment, Resources and Development Court relevant to the item is made, on the basis that it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.
2. The Council Assessment Panel gives authority to the Assessment Manager to review, but not extend, the confidential order on a monthly basis.

Attachments

1. **Proposed Plans**
2. **Previous CAP Minutes and Agenda report**
3. **Referral Response from City Assets**

Lou Fantasia PLANNING

10 January 2022

Phil Smith
Senior Development Officer - Planning
City of West Torrens
165 Sir Donald Bradman Drive
Hilton SA 5033

Dear Mr Smith

**Planning Appeal – Olympic Developments Pty Ltd and City of West Torrens
ERD-22-000166 - Demolition of the existing dwellings and associated structures,
and construction of three (3) warehouses with associated office and storage space,
two retail tenancies with associated offices along with associated carparking,
landscaping and freestanding pylon signage at 239-243 Richmond Road,
Richmond (DA ID 21028599)**

Lou Fantasia Planning has been engaged by Olympic Developments Pty Ltd and consider the reasons for refusal attached to Decision Notification 21028599 dated 13 October 2022 to the abovementioned development at 239-243 Richmond Road, Richmond.

Olympic Developments Pty Ltd lodged an appeal to the Environment Resources and Development Court and the preliminary conference was held on Friday 16 December 2022 where the matter was adjourned to enable the presentation of a submission and additional information for the consideration of the Council Assessment Panel.

We have considered the reasons for refusal and the key issues arising from the refusal and respond as follows:

1. The subject land is located within an Employment Zone where office, shop and warehouse are desired and envisaged uses thereby accords with Desired Outcome 1 which seeks a *'diverse range of low-impact light industrial, commercial and business activities that complement the role of other zones accommodating significant industrial, shopping and business activities.'*
2. The subject land is located within a commercial zone which anticipates a built form of a scale, design and appearance with materials and finishes compatible with commercial development along main arterial roads.

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The proposed building has been architecturally designed to achieve a distractive build form that addresses both Richmond Road (arterial road), and Chambers Avenue being the interface between commercial and residential development. The proposed building presents a narrow articulated profile to Chambers Avenue setback over 10.0 metres from the rear residential neighbour to the north and over 18.0 metres from Richmond Road.

The western wall articulation includes the stepped setback reflecting the angled street boundary to Chambers Avenue and decorative horizontal and vertical grooves in the precast concrete panels.

Unfortunately the retention of the existing trees adjacent to Richmond Road cannot be retained due to their location and also their appearance has been severely compromised by the extent of limb cutting by South Australia Power Networks to clear vegetation from the high voltage powered lines in front of the property.

Oxygen Pty Ltd – landscape architects and urban designers have been engaged to review the proposal in order to further improve the visual and environmental amenity of the development.

James Hayter from Oxygen has prepared a landscaping plan for the development which will introduce meaningful plantings and tree canopy on the site. The Landscape Plan and plant schedule drawing is attached.

3. The Regulated Tree (Lemon Scented Gum) is located approx. 7.5 metres from the eastern boundary and approx. 14.0metres from the main section of the northern boundary. The tree has a canopy of some 22.0 metre with a circumference of 2.45 metres measured at 1.0 metres above ground (DBH). The tree will have a Tree Protection Zone (TPZ) radius of approx. 9.40 metres.

The Figure 1 below shows the location of the Regulated Tree in relation to the eastern and northern boundaries, and the proposed warehouses. The Red circle is the approximate extent of the TPZ.

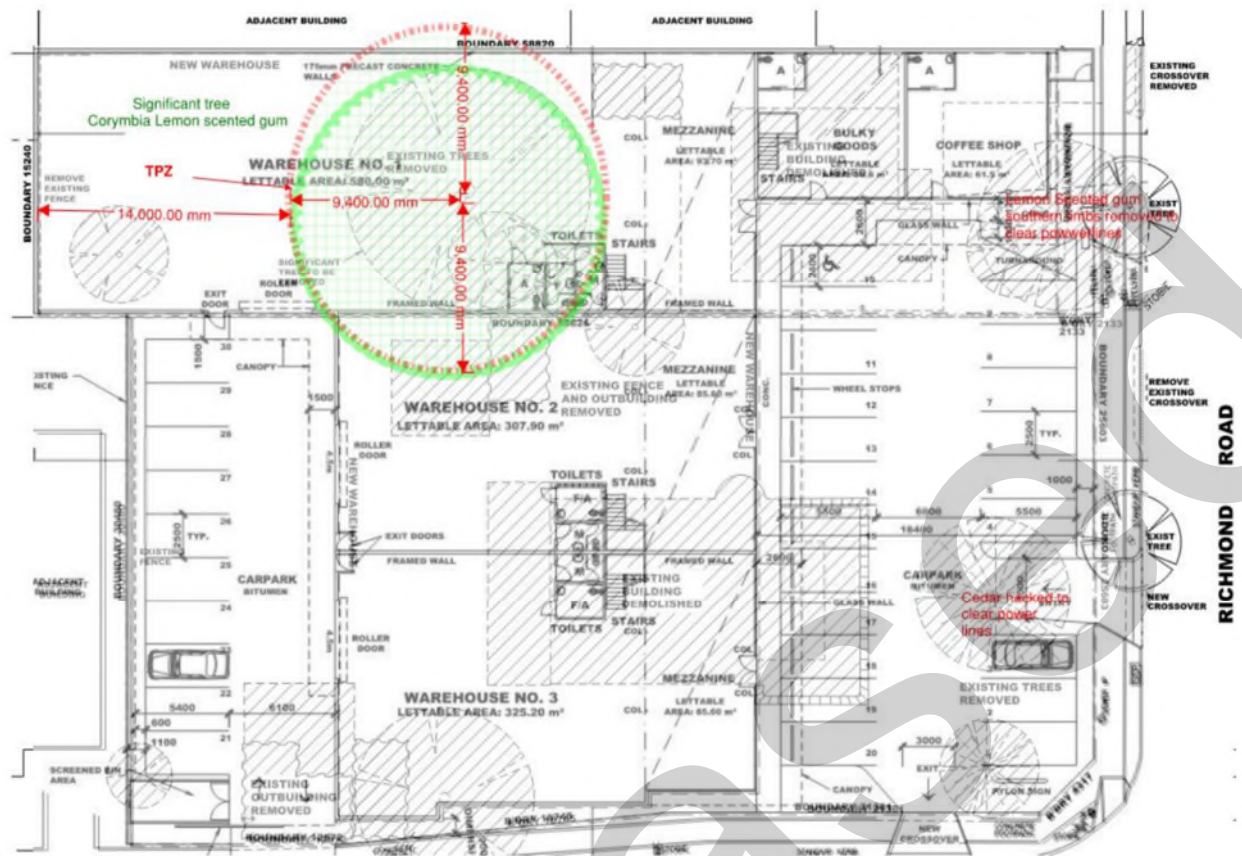


Figure 1

The tree has a lopsided canopy as a result of the removal of several large branches on the eastern side overhanging the adjoining commercial property.

The photographs below show the location of the remove limbs and appearance of the tree as view from the the south.



Figure 2



Figure 3

While Desired Outcome 1 and Performance Outcomes 1.1 and 1.2 seeks the retention of significant trees, Performance Outcomes 1.3 and 1.4 recognise there are circumstances where tree damaging activity ie tree removal may occur.

The land is within the Employment Zone where large commercial buildings are clearly envisaged including retailing and warehousing. Buildings associated with such development are large and require paved and sealed parking and trafficable areas to accommodate small to medium commercial delivery and pickup vehicles.

The designer considered various options for the reasonable development of the land taking into account the nature of the intended use of the building, the presentation of the building to both Richmond Road and Chambers Avenue, the location and design of parking areas and access for delivery and pickup vehicles.

The proposal, on balance represents a reasonable development of the land that more than adequately addresses the other relevant provisions of the Planning and Design Code.

The most appropriate outcome is to clear the site and allow the redevelopment with the implementation of the landscaping Plan prepared by Oxygen Pty Ltd that will introduce meaningful tree canopy to the development.

4. A stormwater management system has been designed for the proposed development incorporating retention and detention measures and has been endorsed by Council's Civil Engineer.
5. The Commissioner of Highways generally supports the proposed access and carparking arrangements subject to the change of the two way Richmond Road access to an ingress only. Drawing DA 2A Site & Floor Plans (23.02.2022) reflects the Commissioner's condition.

The proposal with ingress only Richmond Road access will provide for safe and convenient access into the front carpark and will minimise impact and interruption to the operation of Richmond Road.

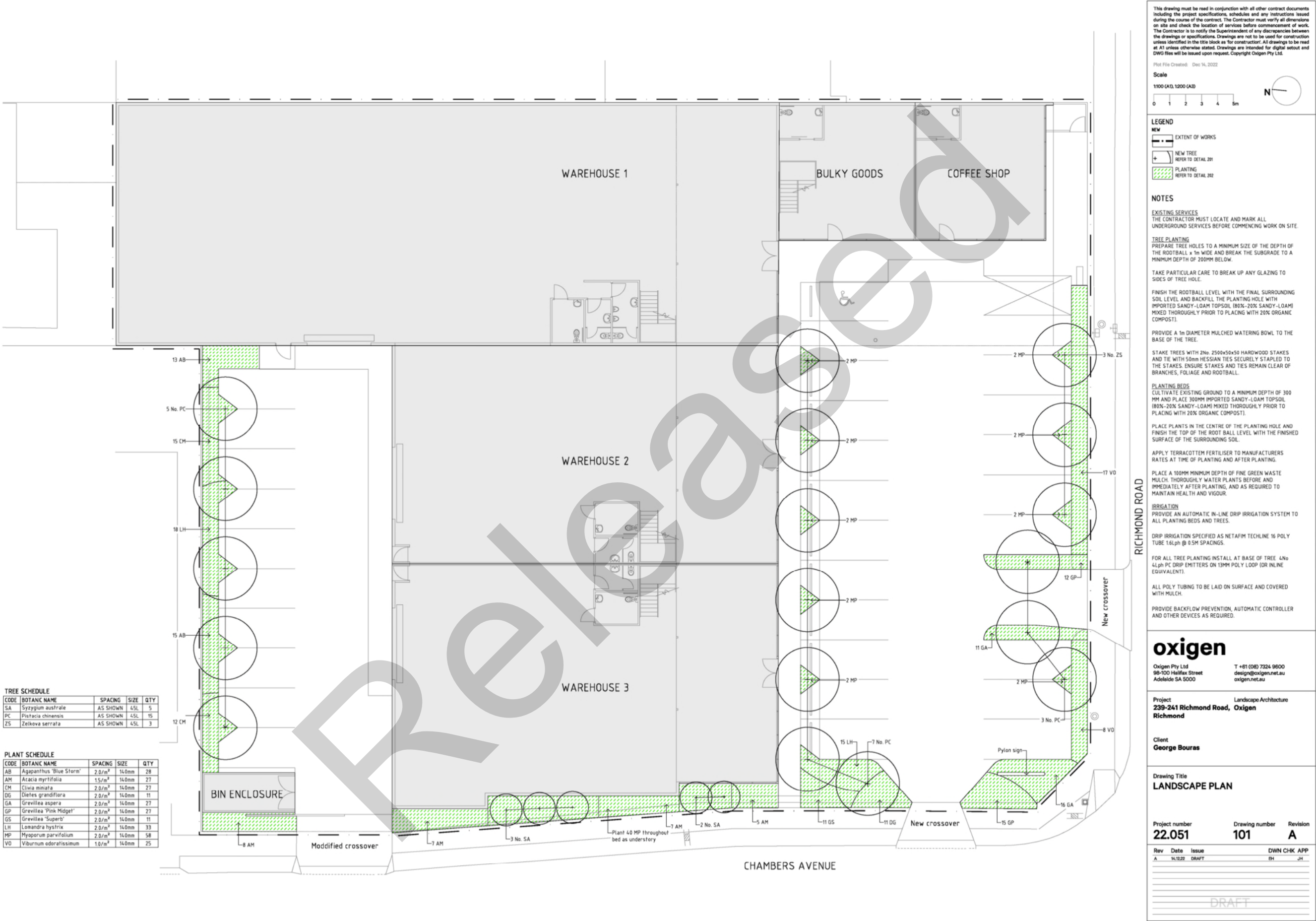
My client seeks the Panel's favourable consideration of the amended proposal to the granting of Development Plan consent.

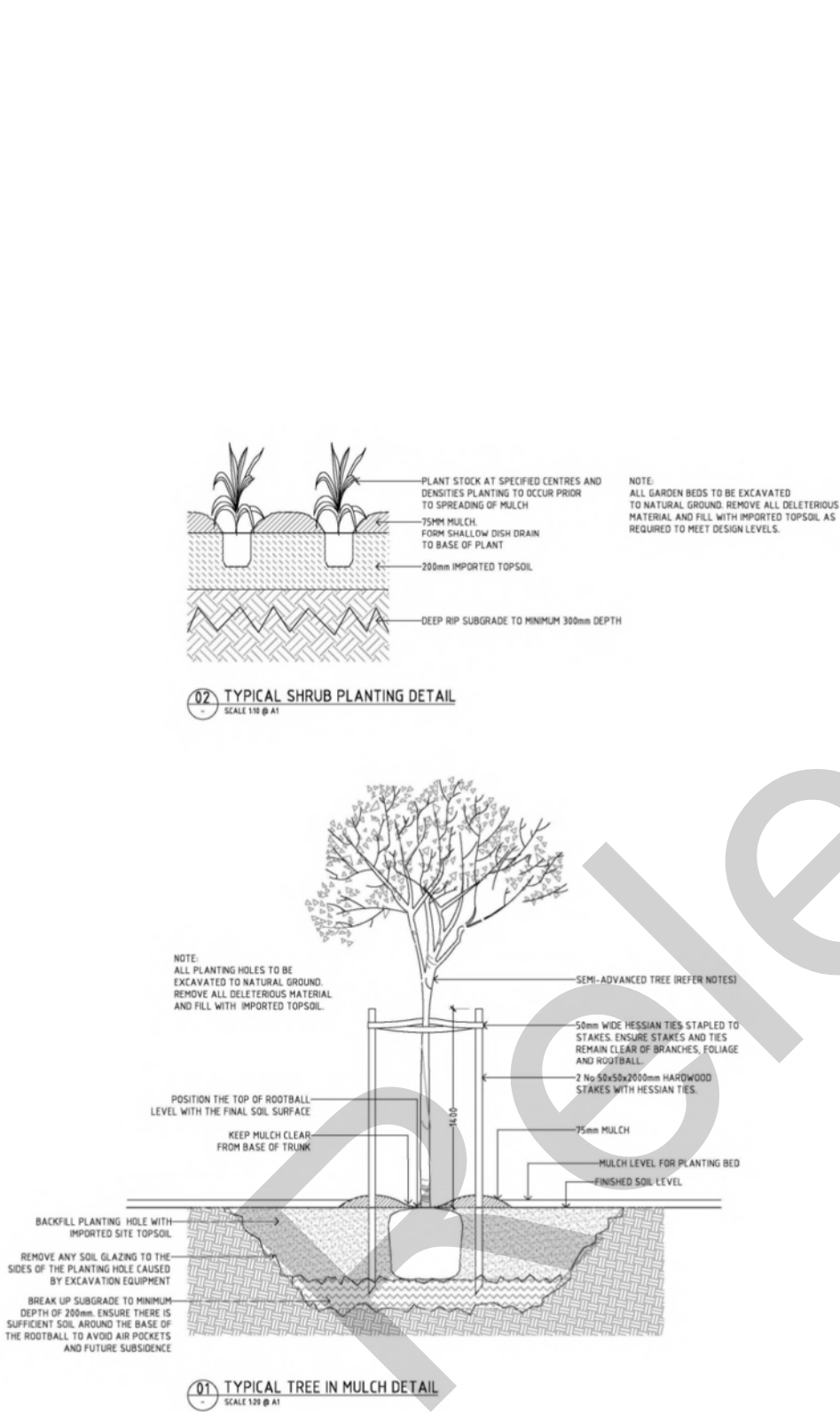
Please do not hesitate to call me on 0413 743 405 should you wish to discuss this matter or require further clarification on any aspect of this proposal.














Yours faithfully



Lou Fantasia MPiA KHS
Certified Practising Planner





TREE	
	
Platanus chinensis Chinese platane	Zelkova serrata Japanese zelkova
Height: 8m Width: 6m	Height: 14m Width: 10m
	
Syzygium australe Lilly Pilly	
Height: 4-5m Width: 2-3m	
PLANTING	
	
Acacia myrtilloides Myrtle Wattle	Clivia miniata Bush Lily
Height: 1-3m Width: 2-3m	Height: 0.4-0.5m Width: 0.8m
	
Agapanthus 'Blue Storm' Lily of the Nile	Iris grandiflora Wild Iris
Height: 0.7m Width: 0.5m	Height: 0.8-1m Width: 0.8-1m
	
Grevillea aspera Rough Grevillea	Grevillea 'Pink Midger' Grevillea
Height: 0.6-2.5m Width:	Height: 0.4m Width: 0.5m
	
Grevillea 'Superb' Grevillea	Lomandra hystrix Mat-Rush
Height: 1.5-2m Width: 1.5-2m	Height: 1.3m Width: 1m
	
Myoporum parviflorum Creeping Boobilla	Viburnum odoratissimum Sweet viburnum
Height: 0.3-0.3m Width: 1-3m	Height: 2-4m Width: 3m

This drawing must be read in conjunction with all other contract documents including the project specifications, schedules and any instructions issued during the course of the contract. The Contractor must verify all dimensions on site and check the location of services before commencement of work. The Contractor is to notify the Superintendent of any discrepancies between the drawings or specifications. Drawings are not to be used for construction unless identified in the title block as 'for construction'. All drawings to be read at A1 unless otherwise stated. Drawings are intended for digital setout and DWG files will be issued upon request. Copyright Oxygen Pty Ltd.

Plot File Created: Dec 14, 2022

Scale
VARIES

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Project: 239-241 Richmond Road, Oxigen Richmond

Landscape Architecture

Client: George Bouras

Drawing Title: DETAILS

Project number	Drawing number	Revision
22.051	201	A

Rev	Date	Issue	DWN	CHK	APP
A	14/12/22	DRAFT	BH	JH	

DRAFT

4 CONFIRMATION OF MINUTES

RECOMMENDATION

That the Minutes of the meeting of the Council Assessment Panel held on 09 August 2022 be confirmed as a true and correct record.

COUNCIL ASSESSMENT PANEL DECISION

The Panel resolved that the recommendation be adopted.

5 DISCLOSURE STATEMENTS

Nil

6 REPORTS OF THE ASSESSMENT MANAGER

6.1 Transitional Applications

Nil

6.2 PDI Act Applications

6.2.1 239, 241-243 Richmond Road, RICHMOND

Application No 21028599

Appearing before the Panel were:

Representor: **David Stevens** of 26B Lucas Street, Richmond appeared in support of the representation.

Applicant: **Rod Roach** (Architect) and **Paul Morris** (Traffic Consultant) appeared in response to the representation.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107 (2)(c) of the *Planning Development and Infrastructure Act 2016*, and having undertaken an assessment of the applicant against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code Version 2022.6.
2. Application No. 21028599 by Rod Roach to carry out demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies (one bulky good) with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of significant tree at 239, 241-243 Richmond Road, RICHMOND (CT-5823/131, CT-6157/995) is GRANTED Planning Consent subject to the following reserved matters and conditions of consent:

Reserved Matters:

The following information shall be submitted for further assessment and approval by the relevant authority as reserved matters under Section 102(3) of the *Planning, Development and Infrastructure Act 2016*:

1. A comprehensive Landscape Plan to be provided for the site to the satisfaction and approval of Council administration. Such planting shall include canopy trees, shrubs and groundcovers that are complimentary to the character sought for this Zone and to assist in the softening of the built form and hard paved surfaces.
2. A Stormwater Management Plan that demonstrates how stormwater is to be detained, discharged, collected and re-used in accordance with the relevant authority's requirements and to the satisfaction and approval of Council administration. A Finished Floor Level of 12.86 is required.

Pursuant to Section 127(1) of the *Planning, Development and Infrastructure Act 2016*, the relevant authority reserves its decision on the form and substance of any further conditions of consent that it considers appropriate to impose in respect of the reserved matters outlined above.

Planning Consent Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application except where varied by any conditions listed below:
 - Existing/Demolition Site Plan, Sheet DA 1, prepared by Rod Roach Architecture, dated July 2021;
 - Site and Floor Plan - Sheet DA 2A, prepared by Rod Roach Architecture, dated March 2022;
 - Elevations, Sheet DA 4A, prepared by Rod Roach Architecture, dated March 2022;
 - Page 11 of the Development Applicant Report - Schedule of Materials and Finishes prepared Rod Roach, dated August 2021;
 - Sections, Sheet DA 5, prepared by Rod Roach Architecture, dated July 2015;
 - Roof Plan, Sheet DA 3, prepared by Rod Roach Architecture, dated July 2021;
2. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
 - a) Result in the entry of water into a building;
 - b) Affect the stability of a building;
 - c) Create unhealthy or dangerous conditions on the site or within the building;
 - d) Flow or discharge onto the land of an adjoining owner;
 - e) Flow across footpaths or public ways; or
 - f) Discharge to the adjacent creek.
3. All driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) properly drained, and shall be maintained in a reasonable condition at all times.

4. All car parking spaces shall be linemarked, in accordance with the approved plans and in accordance with Australian Standards Association Code AS 2890.1, 2004 Parking Facilities, Part 1, Off Street Car Parking, prior to the occupation of the proposed development. Linemarking and directional arrows shall be clearly visible at all times.
5. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for storage or display of materials or goods including waste products and refuse.
6. The stormwater connection through the road verge area is to be constructed of shape and material to satisfy Council's standard requirements:
 - 100 x 50 x 2mm RHS Galvanised Steel or
 - 125 x 75 x 2mm RHS Galvanised Steel or
 - Multiples of the above.
7. Hours of operation for the site shall be as follows:
 - Shops - Monday to Friday - 7am to 6pm, Saturday 7am to 5pm, Sundays and Public Holidays - Closed
 - Warehouses - Monday to Friday - 7am to 6pm, Saturday 7am to 1pm, Sundays and Public Holidays - Closed
8. Vehicles accessing the site are limited to no larger than an SRV type.

Commissioner of Highways Conditions

9. An amended plan shall be provided to Council prior to the issue of Development Approval showing vehicular access via Richmond Road limited to an ingress only access in reference to plan DA 2A dated March 2022, the access point angled at 70 degrees to the road and suitably signed and linemarked to reinforce the desired traffic flow.
10. All vehicles shall enter and exit the site in a forward direction and all vehicle manoeuvring areas shall be kept clear of impediments to manoeuvring.
11. The redundant crossover on Richmond Road shall be closed and reinstated to Council's kerb and gutter standards at the applicant's expense.
12. Stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

COUNCIL ASSESSMENT PANEL DECISION

The Council Assessment Panel, having considered all aspects of the report and the application for consent to carry out development of land resolved to REFUSE Planning Consent, for Application No. 21028599 by Rod Roach for Demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies (one bulky goods) with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of significant tree at 239, 241-243 Richmond Road, RICHMOND (CT-5823/131, CT-6157/995) as the proposed development is contrary to the following provisions of the Planning and Design Code (Version 2021.14):

- Employment Zone, Desired Outcome 2
- Employment Zone, Performance Outcome 3.2
- Employment Zone, Designated Performance Feature 3.2
- Employment Zone, Performance Outcome 5.2
- Employment Zone, Designated Performance Feature 5.2
- Regulated and Significant Tree Overlay, Desired Outcome 1
- Regulated and Significant Tree Overlay, Performance Outcome 1.1 & 1.2 & 1.4
- General Development Policies, Design, Desired Outcome 1
- General Development Policies, Design, Landscaping, Performance Outcome 3.1 & 3.2
- General Development Policies, Design, Water Sensitive Design, Performance Outcome 5.1 & 5.2
- General Development Policies, Design, Carparking Appearance, Performance Outcome 7.1 & 7.6 & 7.7
- General Development Policies, Transport, Access and Parking, Vehicle Access, Performance Outcome 3.1

7 REVIEW OF ASSESSMENT MANAGER DECISION

Nil

8 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER

Nil

6.2 PDI ACT APPLICATIONS

6.2.1 239, 241-243 Richmond Road, RICHMOND

Application No 21028599

Appearing before the Panel will be:

Representors: **David Stevens** of 26B Lucas Street, Richmond wishes to appear in support of the representation.

Applicant: **Rod Roach** (Architect) wishes to appear in response to the representation.

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT NUMBER	21028599
APPLICANT	Rod Roach
ADDRESS	239, 241-243 Richmond Road, RICHMOND
NATURE OF DEVELOPMENT	Demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies (one bulky goods) with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of significant tree
ZONING INFORMATION	<p>Zones</p> <ul style="list-style-type: none"> • Employment <p>Overlays</p> <ul style="list-style-type: none"> • Aircraft Noise Exposure (ANEF 20) • Airport Building Heights (Regulated) • Building Near Airfields • Hazards (Flooding - Evidence Required) • Major Urban Transport Routes • Prescribed Wells Area • Regulated and Significant Tree • Traffic Generating Development <p>Technical Numeric Variations (TNVs)</p> <ul style="list-style-type: none"> • Maximum Building Height (Metres) (Maximum building height is 8.5m) • Maximum Building Height (Levels) (Maximum building height is 2 levels)
LODGEMENT DATE	1 October 2021
RELEVANT AUTHORITY	Council Assessment Panel
PLANNING & DESIGN CODE VERSION	2021.14
CATEGORY OF DEVELOPMENT	Code Assessed - Performance Assessed
NOTIFICATION	Yes
REFERRALS STATUTORY	<ul style="list-style-type: none"> • Commissioner of Highways
REFERRALS NON-STATUTORY	<ul style="list-style-type: none"> • City Assets (Engineering) • City Assets (Traffic)

DELEGATION	CAP <ul style="list-style-type: none">A representor has lodged a valid representation and wishes to be heard
RECOMMENDING OFFICER	Phil Smith
RECOMMENDATION	Approval with conditions

Subject land and Locality

The subject land is formally described as Allotments 21 and 22 of Filed Plans 144749 and 144750 in the area named Richmond, Hundred of Adelaide, Volume 5823 Folio 131 and Volume 6157 Folio 995, more commonly known as 239, 241-243 Richmond Road, Richmond. The subject site comprises two allotments and both are mostly rectangular in shape, with a combined primary frontage to Richmond Road of approximately 42 metres and a depth of 52-56m, resulting in an overall site area of approximately 2588m².

The allotments are currently developed with two dwellings in fair to poor condition. Sporadic landscaping is located throughout the site. There are a number of existing trees on the site. One Eucalypt tree is considered to be significant. There are two other trees which meet the size requirements to be regulated trees but are exempt due to their species. Council's City Operations (Horticulture) representatives they advise they are cedar and elm trees.

There are no easements, encumbrances or Land Management Agreements on the Certificates of Title.

The locality is commercial/industrial in nature with commercial/industrial land uses located on both the north and south sides of Richmond Road. These uses include a car rental agency, pet shop, an air conditioner company, a picture framer and a water tank retailer, amongst others.

The wider locality is residential directly to the north of the subject site in the General Neighbourhood Zone, with commercial/industrial land uses found in all other directions. Adelaide Airport is slightly further to the west.

The amenity within the immediate locality is considered to be heavily commercial/industrial, given the wide range of business type land uses found nearby and interspersed with some pockets of residential development.

The subject land and locality are shown on the aerial imagery and maps below.





Figure 1 - Subject Site, viewed towards the north (from Richmond Road)



Figure 2 - Subject Site, western boundary, viewed towards the north (from Chambers Avenue)



Figure 3 - Subject Site, viewed towards the east (from Richmond Road)



Figure 4 - Subject Site, viewed towards the northwest (from Richmond Road)



Figure 5 - Adjacent property, 237 Richmond Road at the eastern boundary, viewed towards the northeast (from Richmond Road)



Figure 6 - Adjacent property, 51 Chambers Avenue at northern boundary, viewed towards the northeast (from Chambers Avenue)

PROPOSAL

The proposal comprises demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of significant trees.

The application is further detailed as per the report prepared by Rod Roach as follows:

- The proposal comprises five elements namely, three warehouses (Element 1), a shop - bulky goods (Element 2), a shop (Element 3), an advertisement (Element 4) and removal of significant trees (Element 5);
- Element 1 works involve the construction of three warehouses that present to Richmond Road. The warehouses range from 393.5m² to 683.7m²;
- Each warehouse contains associated office space at mezzanine level and amenities at ground floor;
- 10 parking spaces are provided to the rear of Warehouses 2 and 3, accessed from Chambers Avenue;
- Materials and finishes include precast concrete panels, metal roof cladding, metal wall cladding and floor to ceiling glass showroom windows;
- Elements 2 and 3 involve the construction of two shops, comprising 118.3m² and 61.5m² respectively;
- The development maintains a consistent approach with respect to materials and finishes, thus the shops are clad similar to the warehouses;
- 20 parking spaces are provided to the front of the shops and warehouses and will be accessed via a new crossover from Chambers Avenue;
- Element 4 comprises a freestanding sign measuring 6m in height by 3m wide and will display the site's address and tenants;
- The sign is to be located in the southwest corner of the site adjacent to the new crossover;
- Element 5 includes the removal of a significant eucalyptus tree on the 239 Richmond Road allotment, which is necessary to facilitate the development.

For the purposes of an assessment the proposal is broken down into elements. Each element will have an assessment pathway as set out in the Planning and Design Code.

Elements	Application Category
1 - Warehouses (3)	Performance Assessed
2 - Shop - bulky goods	Performance Assessed
3 - Shop	Performance Assessed
4 - Advertisement	Performance Assessed
5 - Removal of significant tree	Performance Assessed

The relevant plans and documents are contained in **Attachment 1**.

PUBLIC NOTIFICATION

The application required public notification because it was performance assessed and not exempt from notification by *Table 5 - Procedural Matters* of the Employment Zone in the Planning and Design Code (The Code).

Properties notified	88 properties were notified during the public notification process.
Representations	Two (2) representations were received.
Persons wishing to be heard	One (1) representor wishes to be heard. <ul style="list-style-type: none"> David Stevens of 26B Lucas Street, Richmond
Summary of representations	Topics were raised regarding the following matters: <ul style="list-style-type: none"> The building will back onto one representor's backyard The proposed development will exacerbate flooding in the representor's backyard Removal of a significant tree One representor supported the development Improve and add to the quality of commercial development in locality
Applicant's response to representations	Summary of applicant's response: <ul style="list-style-type: none"> The applicant proposes to construct a large box gutter behind the boundary wall which would collect all roof water and discharged into a new stormwater disposal system. A proposed swale on the northern side of the site should also assist with this. The tree is some 25m away from the representor's property so there should be limited impact. Representor's support for the removal of the significant tree is noted.

A copy of the representations and the applicant's response is contained in **Attachment 2**.

INTERNAL REFERRALS

Department	Comments
City Assets (Engineering)	<ul style="list-style-type: none"> The application is supported from a stormwater standpoint. A minimum Finished Floor Level of 12.86 is required. Other City Assets standard requirements have been listed as conditions.
City Assets (Traffic)	<ul style="list-style-type: none"> The application is supported from a traffic standpoint

EXTERNAL REFERRALS

Agency	Comments
Commissioner of Highways	<ul style="list-style-type: none"> There is no objection to the development subject to standard requirements. The recommendation includes conditions provided by the referral agency to be placed in the approval (if granted).

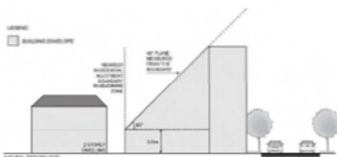
A copy of the internal and external referral responses are contained in **Attachment 3**.

RELEVANT PLANNING & DESIGN CODE PROVISIONS

The subject land is located within the Employment Zone as described in the Code. The subject land is also affected by a series of Overlays and Technical Numeric Variations (TNVs).

ASSESSMENT

The proposal is assessed for consistency with the quantitative requirements of the Planning and Design Code as outlined in the table below:

CODE PROVISIONS	STANDARD	ASSESSMENT
Building Height <i>Employment Zone</i> <i>Local Variation (TNV)</i> PO 3.5	Maximum Building Height (Metres) (Maximum building height is 8.5m, 2 levels)	8m proposed Satisfies
Interface Height <i>Employment Zone</i> PO 3.6	Buildings are constructed within a building envelope provided by a 45 degree plane, measured from a height of 3m above natural ground level at the boundary of an allotment used for residential purposes in a neighbourhood-type zone as shown in the following diagram, except where the relevant boundary is a southern boundary or where this boundary is the primary street boundary. 	3m on boundary with the roof following a 45 degree plane Satisfies

Setbacks (front) <i>Employment Zone</i> PO 3.1	The average setback to the building line of existing buildings on adjoining sites which face the same primary street	Does not satisfy (further discussion later in report)
Setbacks (secondary street) <i>Employment Zone</i> PO 3.2	Building walls are no closer than 2m to the secondary street boundary.	<2m Does not satisfy (further discussion later in report)
Car Parking <i>Table 1 - General Off-Street Car Parking Requirements</i>	Warehouse 0.5 spaces per 100m ² total floor area. Shop 5 spaces per 100m ² of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared. Shop (Bulky Goods) 2.5 spaces per 100m ² of gross leasable floor area.	30 spaces provided Satisfies
Landscaping <i>Employment Zone</i> PO 5.2	Landscape areas comprise: <ul style="list-style-type: none"> • not less than 10 percent of the site • a dimension of at least 1.5m. 	244m ² required 146m ² provided Does not satisfy (further discussion later in report)

In assessing the merits or otherwise of the application, only those quantitative requirements that do not meet the Code requirements will be discussed along with the respective qualitative provisions and the representors concerns. The proposed development is therefore discussed under the following sub headings:

Land Use

The land uses proposed, warehouses with ancillary offices and shops, including a bulky goods shop are envisaged land uses within the Employment Zone. These are noted at PO/DPF 1.1 of the Employment Zone within the Code.

Built Form/Building Height

The representor raised concern regarding the building height of the new building (Element 1) and the 45 degree pitch of the roof form located on the boundary. It is noted that the 3m high wall on the boundary and then angling at 45 degrees complies with the interface height provision set out in PO 3.6.

Construction methods of this particular design tend to have a box gutter hidden behind a parapet to contain any overflow of stormwater off the roof. Box gutters are typically large to cater for the large roof area found with warehouses.

As the warehouses, being the tallest buildings proposed, are located to the south of any adjacent dwellings, no overshadowing of these residential properties will occur.

Stormwater affecting neighbouring properties

The representor to be heard raised a concern that stormwater generated by the development will exacerbate flooding in their backyard. Given the measures to be put in place in the design of the warehouses, it is not clear how this will occur.

A solid wall on the boundary will prevent any stormwater from escaping the site. In addition, a large box gutter on the boundary wall will be able to contain large flows of roof water, detain and re-use it in accordance with Council's requirements. The roof water will be contained within the subject site. This accords with PO/DPF 1.1 of the Hazards (Flooding - Evidence Required) Overlay.

A Reserved Matter has been placed in the recommendation to ensure that the stormwater matters are satisfactorily resolved given the broad requirements of City Assets and the concerns raised by the representor are appropriately resolved. It is also noted that conditions have been placed in the recommendation to ensure that stormwater is appropriately dealt with on an on-going basis.

On this basis, stormwater matters, as they potentially affect adjacent landowners, are considered to be satisfactorily resolved.

Amenity

Although not raised as a concern, noise potentially emanating from the warehouses will be appropriately dealt with by acoustic measures incorporated into the building closest to the adjoining residences, i.e. the concrete precast panels, in addition to appropriate hours of operation thus satisfying PO/DPF 1.2 of the Interface between Land Uses General Development Policy.

Significant Trees/Tree Removal/Landscaping

The representor also raised the removal of the significant tree on the site as an issue as it is healthy. While regrettable, the removal of this tree and the others on the site, is not necessarily fatal to the application.

From a purely practical standpoint, the significant Eucalypt tree is considered to unreasonably interfere with the reasonable expectation to develop the site as afforded by the Code. The setbacks required to avoid damaging the root zone of the tree would be extensive, making approximately 25% of the 238 Richmond Road allotment undevelopable. This accords with PO/DPF 1.4 of the Regulated and Significant Tree Overlay which states that a tree-damaging activity in connection with other development satisfies all the following:

1. it accommodates the reasonable development of land in accordance with the relevant zone or subzone where such development might not otherwise be possible
2. in the case of a significant tree, all reasonable development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.

It is considered that in this instance, the removal of this tree satisfies these two requirements.

The applicant has not provided evidence from an arborist as to the condition of this tree from a health perspective. Typically applications for tree damaging activity (i.e. removal) would be referred to Council's consulting arborist for advice, however in this instance, the assessment put more weight on the Code provisions relating to the reasonable development of the site and the interference of the tree, rather than the Code provisions relating to the health of the tree.

There are other large trees located on the site, some with a trunk circumference greater than 2m. These trees are located within the front setback of the western existing dwelling and on the side setback of this dwelling where it faces Chambers Avenue. Their species have been identified by City Operations as a cedar tree and elm tree and therefore exempt from the regulated tree regulations and can be removed without requiring Development Approval. The trees are not considered to be fine specimens that are worthy of retention. One other Eucalypt in the front setback of the easternmost dwelling has a trunk circumference of 1.9m, thus is not regulated and can be removed.

The landscaping proposed does not satisfy the requirements set out in the Code under PO/DPF 5.2 of the Employment Zone. While the shortfall is sizeable (98m²), the outcome of a complete redevelopment of a largely unkempt site is considered to be more desirable and more aesthetically pleasing. Spread across a large site and with the amount of landscaping proposed, the shortfall does not appear to be as substantial. A comprehensive Landscape Plan has been requested as a Reserved Matter to improve the outcome given the shortfall in landscaping proposed.

On balance, it is considered that a more appropriate outcome would be to clear the subject site and allow for a redevelopment that would provide a more appropriate and structured landscape regime that beautifies the streetscape and locality.

Setbacks

The front and side setbacks have been noted as not satisfying relevant provisions of the Code. The Code requires front setbacks consistent with adjoining building setbacks. Having said that, the property and building adjacent provides a considerable setback in comparison to other buildings in the immediate locality. Thus while not strictly in accordance with PO 3.1 of the Employment Zone, the front setback proposed is considered to be more consistent with the prevailing character of the immediate locality. It should be noted that the development seeks to overcome this variance by having graduated building heights starting at single storey adjacent to Richmond Road. This is considered to be an appropriate design alternative.

On this basis, PO 3.1 is considered to be satisfactorily addressed.

Traffic Management

The application was referred to the Commissioner of Highways for comment. No objections were raised with the proposal. Slight alterations will be made to the entry adjacent to Richmond Road, however these changes are not substantial.

Council's Traffic Engineer has reviewed the application and is satisfied that a sufficient number of parking spaces have been provided. On this basis, *Table 1 – General Off-Street Car parking Requirements* of the Planning and Design Code is satisfied.

A condition has been included in the recommendation to ensure vehicles are limited to no larger than a small rigid vehicle (SRV) type.

The Traffic Engineer has raised some possible amendment that are considered unnecessary. While it is agreed that the disabled space is required to meet the relevant Australian Standard, the widened turn bay is considered unnecessary as any SRV is likely to either reverse inside the warehouses and exit in a forward direction if they need to enter the site, or park on the street. For the frequency this is likely to occur, it is considered more prudent to keep the two parking spaces.

The amount that Warehouse 3 would need to be cut back in order to achieve sightline requirements is considered to be trifling. The benefit would be outweighed by the construction costs associated with truncating the corner of the building to make it not worthwhile.

With the front car park, the writer disagrees that the Chambers Avenue crossover should be deleted. Based on the commentary provided by the Commissioner of Highways, its removal is not required and adds to better vehicle flow within the site. The crossover from Richmond Road will be modified as per the Commissioner's requirements and will be ingress only.

On this basis, it is considered that the traffic management matters are appropriately resolved.

SUMMARY

Having considered all the relevant provisions of the Planning and Design Code, the proposal is considered to be not seriously at variance with the Planning and Design Code Version 2021.14 dated 23 September 2021.

The development satisfactorily accords with the relevant provisions of the Planning and Design Code, in particular, the policies found with the Employment Zone, more specifically relating to interface height, building height and traffic management. In addition, a comprehensive landscape plan is required as a reserved matter to soften views of the proposed structures to the streetscape.

The hours of operation, as recommended, are considered to be reasonable, given the proposed land uses and the locality, and a condition has been included in the recommendation.

On balance, the proposal reasonably satisfies the relevant provisions of the Planning and Design Code Version 2021.14 and therefore the application warrants the granting of Planning Consent, subject to the specified reserved matters and conditions.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107 (2)(c) of the *Planning Development and Infrastructure Act 2016*, and having undertaken an assessment of the applicant against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code Version 2022.6.
2. Application No. 21028599 by Rod Roach to carry out demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies (one bulky goods) with associated offices along with associated car parking landscaping, freestanding pylon signage and removal of significant tree at 239, 241-243 Richmond Road, RICHMOND (CT-5823/131, CT-6157/995) is GRANTED Planning Consent subject to the following reserved matters and conditions of consent:

Reserved Matters:

The following information shall be submitted for further assessment and approval by the relevant authority as reserved matters under Section 102(3) of the *Planning, Development and Infrastructure Act 2016*:

1. A comprehensive Landscape Plan to be provided for the site to the satisfaction and approval of Council administration. Such planting shall include canopy trees, shrubs and groundcovers that are complimentary to the character sought for this Zone and to assist in the softening of the built form and hard paved surfaces.
2. A Stormwater Management Plan that demonstrates how stormwater is to be detained, discharged, collected and re-used in accordance with the relevant authority's requirements and to the satisfaction and approval of Council administration. A Finished Floor Level of 12.86 is required.

Pursuant to Section 127(1) of the *Planning, Development and Infrastructure Act 2016*, the relevant authority reserves its decision on the form and substance of any further conditions of consent that it considers appropriate to impose in respect of the reserved matters outlined above.

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1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application except where varied by any conditions listed below:
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 - Page 11 of the Development Applicant Report - Schedule of Materials and Finishes prepared Rod Roach, dated August 2021;
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 - a) Result in the entry of water into a building;
 - b) Affect the stability of a building;
 - c) Create unhealthy or dangerous conditions on the site or within the building;
 - d) Flow or discharge onto the land of an adjoining owner;
 - e) Flow across footpaths or public ways; or
 - f) Discharge to the adjacent creek.
3. All driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) properly drained, and shall be maintained in a reasonable condition at all times.
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5. Driveways, car parking spaces, manoeuvring areas and landscaping areas shall not be used for storage or display of materials or goods including waste products and refuse.
6. The stormwater connection through the road verge area is to be constructed of shape and material to satisfy Council's standard requirements:
 - 100 x 50 x 2mm RHS Galvanised Steel or
 - 125 x 75 x 2mm RHS Galvanised Steel or
 - Multiples of the above.
7. Hours of operation for the site shall be as follows:
 - Shops - Monday to Friday - 7am to 6pm, Saturday - 7am to 5pm, Sundays and Public Holidays - Closed
 - Warehouses - Monday to Friday - 7am to 6pm, Saturday - 7am to 1pm, Sundays and Public Holidays - Closed
8. Vehicles accessing the site are limited to no larger than an SRV type.

Commissioner of Highways Conditions

9. An amended plan shall be provided to Council prior to the issue of Development Approval showing vehicular access via Richmond Road limited to an ingress only access in reference to plan DA 2A dated March 2022, the access point angled at 70 degrees to the road and suitably signed and linemarked to reinforce the desired traffic flow.
10. All vehicles shall enter and exit the site in a forward direction and all vehicle manoeuvring areas shall be kept clear of impediments to manoeuvring.
11. The redundant crossover on Richmond Road shall be closed and reinstated to Council's kerb and gutter standards at the applicant's expense.
12. Stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

Attachments

1. **Application Documents**
2. **Representations and Response to Representations**
3. **Referral Responses**



Product
Date/Time
Customer Reference
Order ID

Register Search (CT 5823/131)
30/06/2021 01:03PM
Rod Roach
20210630004956

REAL PROPERTY ACT, 1899



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 5823 Folio 131

Parent Title(s) CT 2608/173
Creating Dealing(s) CONVERTED TITLE
Title Issued 09/11/2000 Edition 6 Edition Issued 17/05/2017

Estate Type

FEE SIMPLE

Registered Proprietor

AUSCELL PTY. LTD. (ACN: 007 762 987)
OF 84 RIVERSIDE DRIVE FULHAM SA 5024
1 / 2 SHARE

OLYMPIC SUPER STORES PTY. LTD. (ACN: 114 684 338)
OF 84 RIVERSIDE DRIVE FULHAM SA 5024
1 / 2 SHARE

239 RICHMOND.

Description of Land

ALLOTMENT 21 FILED PLAN 144749
IN THE AREA NAMED RICHMOND
HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

NIL

Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL

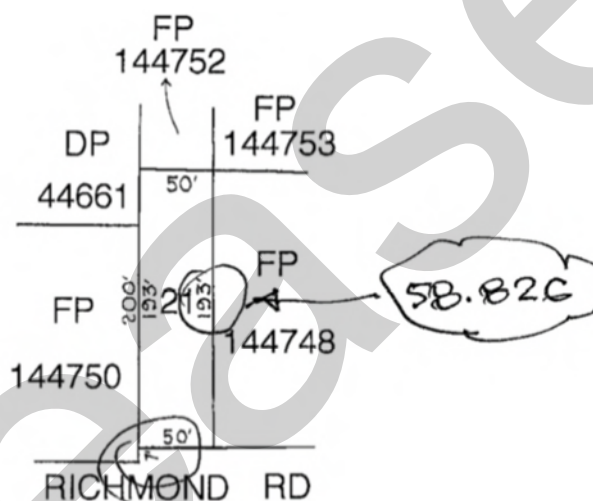


Product
Date/Time
Customer Reference
Order ID

Register Search (CT 5823/131)
30/06/2021 01:03PM
Rod Roach
20210630004956

THIS PLAN IS SCANNED FOR CERTIFICATE OF TITLE 2608/173

LAST PLAN REF: DP 2051



100 50 0 100 FT

DISTANCES ARE IN FEET AND INCHES
FOR METRIC CONVERSION

1 FOOT = 0.3048 METRES
1 INCH = 0.0254 METRES

NOTE: SUBJECT TO ALL LAWFULLY EXISTING PLANS OF DIVISION

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Page 2 of 2



Product
Date/Time
Customer Reference
Order ID

Register Search (CT 6157/995)
30/06/2021 01:13PM
20210630005064

REAL PROPERTY ACT, 1986



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 6157 Folio 995

Parent Title(s) CT 2225/153

Creating Dealing(s) RT 12334788

Title Issued 12/06/2015

Edition 4

Edition Issued

17/05/2017

Estate Type

FEE SIMPLE

Registered Proprietor

OLYMPIC SOLAR ENERGY PTY. LTD. (ACN: 126 384 110)
OF 84 RIVERSIDE DRIVE FULHAM SA 5024

Description of Land

ALLOTMENT 22 FILED PLAN 144750
IN THE AREA NAMED RICHMOND
HUNDRED OF ADELAIDE

24/
~~22-9~~ RICHMOND

Easements

NIL

Schedule of Dealings

NIL

Notations

Dealings Affecting Title NIL

Priority Notices NIL

Notations on Plan NIL

Registrar-General's Notes NIL

Administrative Interests NIL

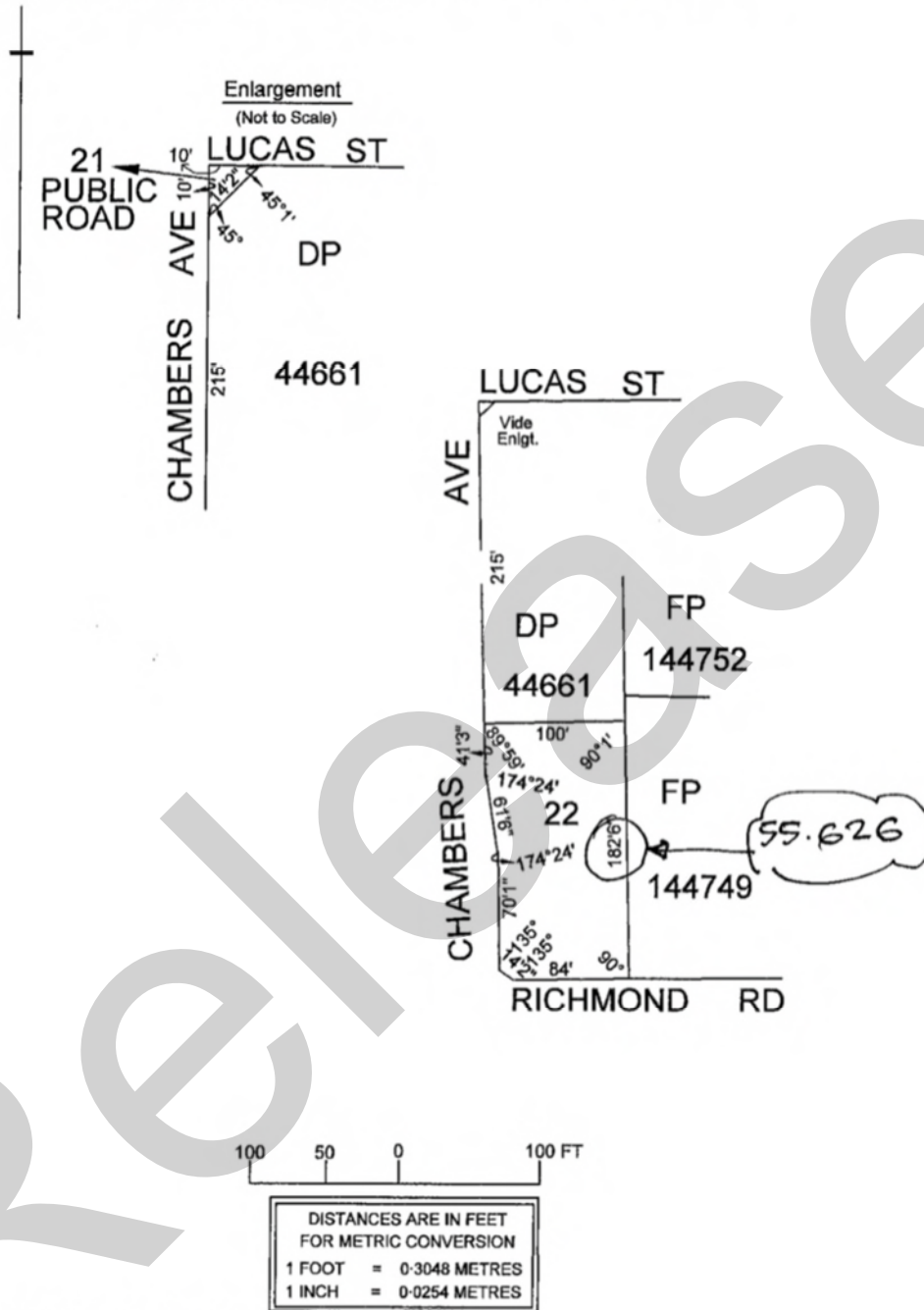


Product
Date/Time
Customer Reference
Order ID

Register Search (CT 6157/995)
30/06/2021 01:13PM
20210630005064

THIS PLAN IS SCANNED FOR CERTIFICATE OF TITLE 2225/153
ALLOTMENT 21 IS PUBLIC ROAD VIDE DKT 513/1952

LAST PLAN REF: DP 2051



NOTE: SUBJECT TO ALL LAWFULLY EXISTING PLANS OF DIVISION

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Page 2 of 2

OLYMPIC DEVELOPMENTS PTY LTD

COMMERCIAL DEVELOPMENT

239-241 RICHMOND ROAD, RICHMOND

DEVELOPMENT APPLICATION REPORT

ROD ROACH ARCHITECTURE

AUG 2021

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond**Development Application Report****INTRODUCTION**

The Olympic Group has purchased the property at 239-241 Richmond Road, Richmond, with a view to constructing a mixed use development consisting of three small scale warehouses, bulky goods and café.

THE APPLICANT

The Applicant for the proposed development is Olympic Developments Pty Ltd.

The registered address of the Applicant is – Corner Grand Junction Road and South Road, Wingfield.

Currently the Applicant has ownership of the following significant commercial properties:

- 263-271 Grange Road, Findon (immediately adjacent to subject site)
- Olympic Developments headquarters - corner Grand Junction Road & South Road, Wingfield
- Beachpoint Apartments - Beach Road, Christies Beach (ground floor retail with blue chip tenants including Headspace, Aussie Home Loans)
- Commercial properties in Sydney and Brisbane with many blue chip tenants including Woolworths, Telstra, Shell, Burson, MM Electrical and others.

The Applicant's parent company, The Olympic Group, consists of two main entities – Olympic Batteries Pty Ltd and Olympic Developments Pty Ltd.

Olympic Batteries Pty Ltd is a national wholesaler of sealed lead acid batteries with its head office in Adelaide and branches in Sydney and Brisbane. It is the main provider of batteries in the fire and security industries in Australia, currently importing 120-130 containers per year.

Olympic Developments Pty Ltd is the property development and investment arm in the group, established by Peter Bouras almost 35 years ago. The company has many targeted development sites across the greater Adelaide area, to grow the property investment portfolio on completion of the Grange Road, Findon site.

THE DESIGN BRIEF

The Applicant wishes to develop the well positioned corner site with three warehouses having front presentation access from Richmond Road as well as service access from Chambers Avenue at the rear. They are to be used by occupiers who do not require large vehicle access to the warehouse facilities.

The warehouses are to be self contained with a small mezzanine floor in each for storage as well as associated office use.

In addition to the warehouses, the Applicant is proposing a small café, bulky goods space and office above.

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond**Development Application Report**

The design of the built form is to be functional and classically simple, using precast concrete panels and wall claddings which will be patterned and textured. External colours will be generally understated.

A pylon sign for site identification and tenant naming will be required.

SITE DESCRIPTION

The land consists of two separate titles, each with an existing detached dwelling and associated shed structures.

Lot 22 at 241 Richmond Road has a frontage of 25.6 metres plus corner cut off to Richmond Road and 52.68 metres plus corner cut off to Chambers Avenue.

Lot 21 at 239 Richmond Road has a frontage of 15.24 metres to Richmond Road and a depth of 58.82 metres. The south boundary to Richmond Road is set back 2.13 metres possibly for future road widening, but is currently fenced and used as part of the front garden.

The total size area is some 2438 square metres.

The total site has a fall of some 300 mm from east to west.

SITE IMPROVEMENTS**241 Richmond Road**

Double storey dwelling, with sheds and lean-to structures. Access to the site is from Chambers Avenue on the west side.

239 Richmond Road

Single storey dwelling, with shed and carport structure. Access to the site is from Richmond Road on the south side.

PROPOSED SITE USES

The site falls within the "Employment Zone of the West Torrens Council where a variety of commercial uses are currently encouraged.

PREVIOUS DEVELOPMENT APPLICATION

The Olympic Group prepared a Development Application in 2015 but elected not to lodge it with the Council. At that stage, a Physical Features Survey, Stormwater Plan and Report was prepared and have been included as a part of the current Development Application.

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond**Development Application Report****CERTIFICATES OF TITLE**

The following CTs are attached to this Report. These were obtained on June 30, 2021.

241 Richmond Road CT Vol 6157 Fol 995

239 Richmond Road CT Vol 5823 Fol 131

ADJOINING PROPERTIES

The improvements to the site on the east side of the property at 239 Richmond Road consists of a showroom and warehouse with car parking facing Richmond Road. The existing buildings are positioned on the site boundaries.

On the north, there is a detached single storey dwelling facing Chambers Avenue with its side boundary abutting the subject site.

Also on the north, there is a three unit complex facing Lucas Street with its rear boundary abutting the subject site.

SITE SURVEY

A survey of existing physical features has been prepared by Specialty Surveys in 2015 and is attached with the Development Application drawings.

Note: the boundary dimensions are approximate only and have not been certified.

DEVELOPMENT APPLICATION DRAWINGS

The following drawings are a part of this Application.

DA1	Demolition Plan
DA2	Site Plan – Floor Plan
DA3	Roof Plan
DA4	Elevations
DA5	Sections
DA6	Landscape Plan
ES	Engineering Survey & Civil Design
	Stormwater Concept

TOWN PLANNING

The West Torrens Council Development Requirements for the site are as follows.

Council Zone

Employment Zone

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond

Development Application Report

Permitted Uses	Commercial Uses including Light Industry, Service Industry, Warehouse and Office Ancillary to Main Uses.	
Permitted Site Coverage	60% of site area.	
Car Parking	Warehouse	0.67 cars/100 sq m
	Office	3.3 cars/100 sq m
	Café	7 cars/100 sq m
Building Set Backs	3 metres from front and one side boundary.	
Landscaping	6% of site area.	

DEMOLITION OF EXISTING SITE FEATURES & VEGETATION

The existing dwellings, shed structures, trees, vegetation, paving and fence structures to Richmond Road and Chambers Street will be demolished and removed from the site.

Note: the existing eucalypt on 239 Richmond Road is a major tree and approval is sought to remove it. The retention of the tree would make the site development not viable.

The existing fence on the north boundary will be retained for the duration of the construction and replaced with a new fence upon completion.

CONSTRUCTION

To minimise on site construction time and disruption to adjoining property owners, the building envelope will be constructed with precast concrete panels and steel wall cladding.

The internal framework will be with structural steel.

A total on site construction period of 30 weeks is intended.

AREA SUMMATION

Site Area:	Lot 21	896 sq m
	Lot 22	1629 sq m
Total		2525 sq m
Building Area		1490 sq m
Site Coverage		59%

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Commercial Development – Richmond

Development Application Report

CAR PARKING CALCULATION

Building	Floor Area	Warehouse	Offices	Total Cars
Warehouse 1	683	590 4	93.7 4	8
Warehouse 2	394	308 2	85.6 3	5
Warehouse 3	411	325 2	85.6 3	5
Bulky Goods		56 1	62.5 2	3
Cafe	62 4			4
Total Required				25 cars
Achieved				30 cars

SITE PLANNING

The Applicant seeks to have a maximum presentation to Richmond Road and Chambers Avenue, with visitor and service access clearly visible from the two roadways.

Building setbacks are provided to achieve car parking and landscaping for the development.

Warehouse 1 and the café are built on the east boundary and abut existing buildings and car park.

CAR PARKING ACCESS

There are currently three vehicle crossovers providing access to the site, two in Richmond Road and one in Chambers Avenue. It is proposed to delete the east crossover to Richmond Road and to provide a six metre wide two-way access in the location of the existing crossover. In addition there will be provision for two new crossovers in Chambers Avenue, each six metres wide with two-way vehicle access.

The location of the crossovers are shown to provide vehicle access to the south and north car parking areas.

CAR PARKING LAYOUT

Car bays are 2500 mm wide and 5500 mm deep.

Vehicles will overhang edge garden kerbs and wheel stops by 500-800 mm.

Driveways are 6000 mm wide between car parking bays.

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond**Development Application Report**

One access vehicle park complying with AS/NZS 2890.1 is positioned in the south car parking area.

All car parking bays will be line marked with 100 mm wide painted delineation and access car park symbols.

Directional arrows will be painted to show vehicle access at crossovers to comply with Australian Standards.

Vehicle access signs will be positioned at each entry point.

POWER LINE SETBACK REQUIREMENTS

The original Development Application Drawings were shown to Saidus Sefat, Energy Project Officer from the Office of the Technical Regulator and the building clearances from the existing power lines were agreed generally. These set backs are now shown on the current DA drawings in accordance with the following table.

Power Rating	Distances from Power Lines	
66 kV	Dimension B	5.5 metres
1 kV and above	Dimension C	3.1 metres
1 kV bare line	Dimension C	1.5 metres

The Applicant will sign the Electricity Act Declaration Form for the proposed building work.

ARCHITECTURE & BUILT FORM

To maximise the commercial viability of the proposed development, the building is constructed on the east boundary.

The car parking areas will permit articulation to the building mass.

Projecting roof canopies will provide weather protection to the large glass windows facing east and west, and to the roller doors. These will have PFC steel fascias.

The precast concrete panels will have recessed line marking to provide façade texture as shown on the elevations.

The glass facades to the south of the warehouses and the west and south of the restaurant/café will be 3000 mm high and constructed with 100 mm x 50 mm commercial aluminium box sections with an external pocket glazing system.

Metal wall cladding to the mezzanine floors will be Colorbond pan profile with concealed fixings.

The roof elements will be concealed by parapets and will be accessible for roof mounted air conditioning equipment.

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond**Development Application Report**

Large box gutters will be provided, with substantial internal downpipes. External downpipes will have rainwater heads to enhance water disposal.

OVERSHADOWING

Because of the orientation of the site, there will be minimal overshadowing to the dwellings on the north of the site.

ENERGY EFFICIENCY

The design of the buildings will comply with the requirements of the Building Code of Australia Volume 1 Section J.

During the preparation of building construction documents, the following energy issues will be considered with the objective of achieving compliance.

Building fabric.
External glazings.
Building sealing.
Air movement.
Building services.

SOLAR ENERGY

The Applicant does not intend to provide photovoltaic energy capture but provision will be made in the electrical infrastructure for tenants to easily connect to their individual switchboards.

Solar panels would be roof mounted.

HEATING & COOLING

Reverse cycle split system air conditioning packages with necessary ducting and fresh air to each office area. The fan coil units will be located in the internal floor spaces of the tenancies to minimise visual impediments and noise to the adjoining dwellings and street frontages.

ROOF ACCESS

Roof access to service the roof mounted air conditioning units and to service box gutters and flashings will be via an internal ladder from the bulky goods tenancy, with roof access hatch.

Metal walkways will be provided from the access hatch to each AC unit.

Roof safety anchor points will be installed to comply with Work Safe requirements.

**OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond****Development Application Report****BUILDING IDENTIFICATION & SIGNAGE**

It is proposed to construct illuminated sign panels for each tenant mounted on a steel structure so that there is minimal visual obstruction to the car park. Refer to building elevations for proposed signage.

In addition there will be a requirement for sign panels attached to the building to identify with logo or lettering the tenant's corporate image. Details of these will be submitted to the Council for approval once the tenants provide this information. The size and detail of the tenant's signage will be controlled by the Applicant to achieve design harmony and to comply with the Council's subsequent approval.

SITE LIGHTING

The south car park will be illuminated with bollard lights positioned in landscape areas as well as lighting recessed into building canopies.

The north car park will have building mounted security lights, daylight switch operated.

The design of these and mounting height will minimise light spill to the adjacent dwellings.

BUILDING SECURITY

An automatic sliding gate with manual over-ride switching will be provided to secure the north car park. This will be time switched to close automatically for night time security. Fire egress access will be addressed.

ROOF CLADDING & DRAINAGE

The roof areas will be clad with zinclaume roof decking concealed behind external building parapets and fascias.

Roofing will have a three degree fall towards box gutters.

Downpipes will be positioned internally behind perimeter walls.

All roof water collection will be with underground pipework directed to the engineered detention system (refer to Ginos Engineers drawings).

STORMWATER CAPTURE, DETENTION & DISCHARGE

Refer to attached engineer's civil drawing for details of roof and site stormwater collection, detention and discharge including car park pollutant arresting measures.

Note: This was originally prepared in 2015

OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond**Development Application Report****WASTE MANAGEMENT**

A screened waste bin enclosure is provided to the north car park for warehouse tenant's use and an enclosure on the south side of the restaurant/café for the tenant's use.

PAVEMENTS

The pavements to the car parks will be bitumen. Walkways as shown will be concrete with line marking and broomed finish.

FENCES

The fences abutting the north boundary 2.4 metres high will be Colorbond steel with powdercoated steel columns.

LETTER BOXES

A group of four letter boxes will be mounted on a steel base and integrated with the landscaping on the Richmond Road frontage in the position shown.

LANDSCAPING

The area of site currently devoted to landscaping is 112 square metres. The requirement is 6% of site area 2438 sq metres which is 146 sq metres. There is a 34 sq metre shortfall.

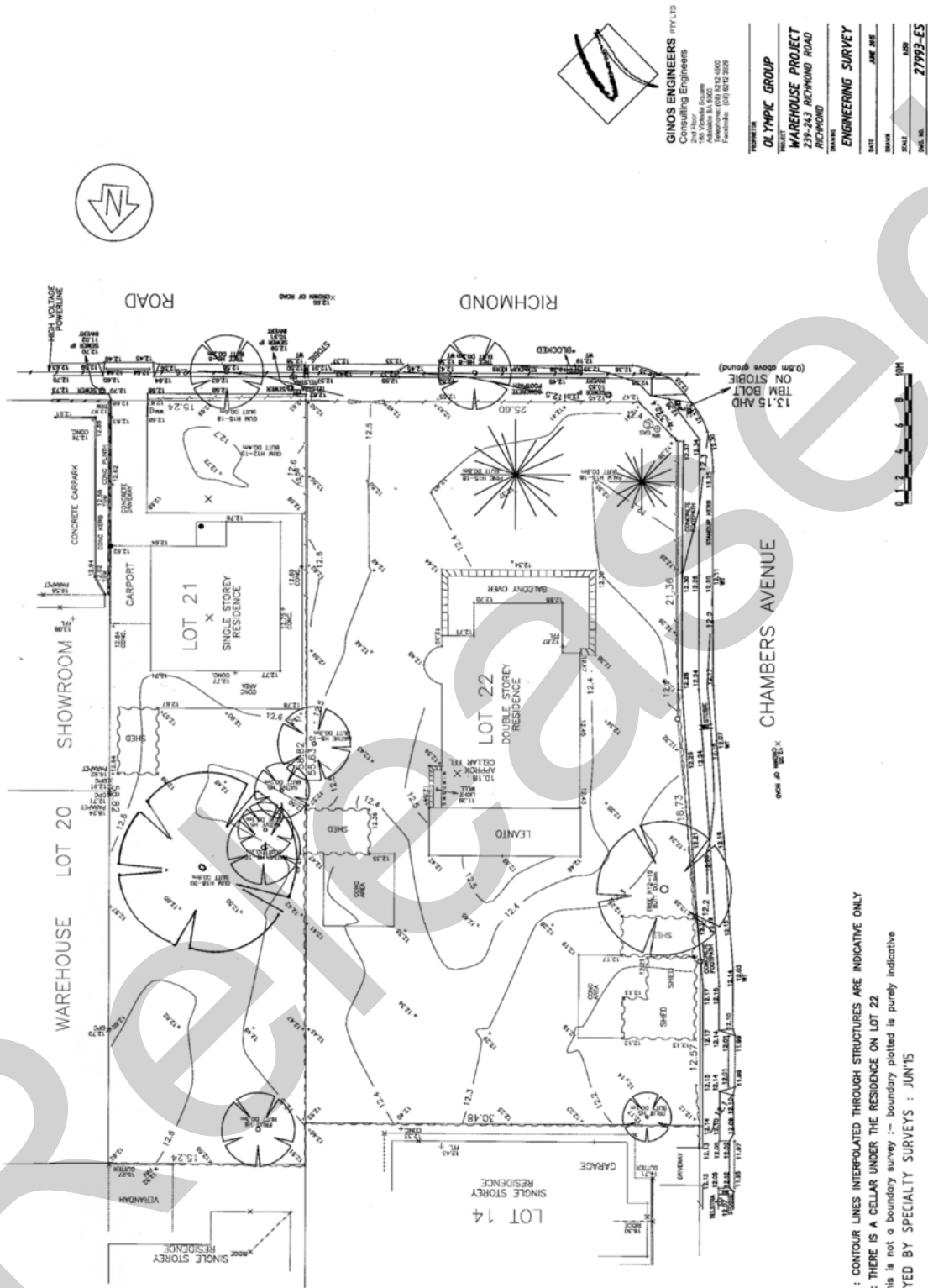
The planting will be low scale native species plants and ground covers and automatically irrigated. All landscape areas will have bark mulching.

PYLON SIGN

The pylon sign is intended to identify the site address and to provide tenant's identification signage. The dimensions will be 6 metres high by 3.0 metres wide and located as shown on the drawings.

Item 6.2.1 - Attachment 1

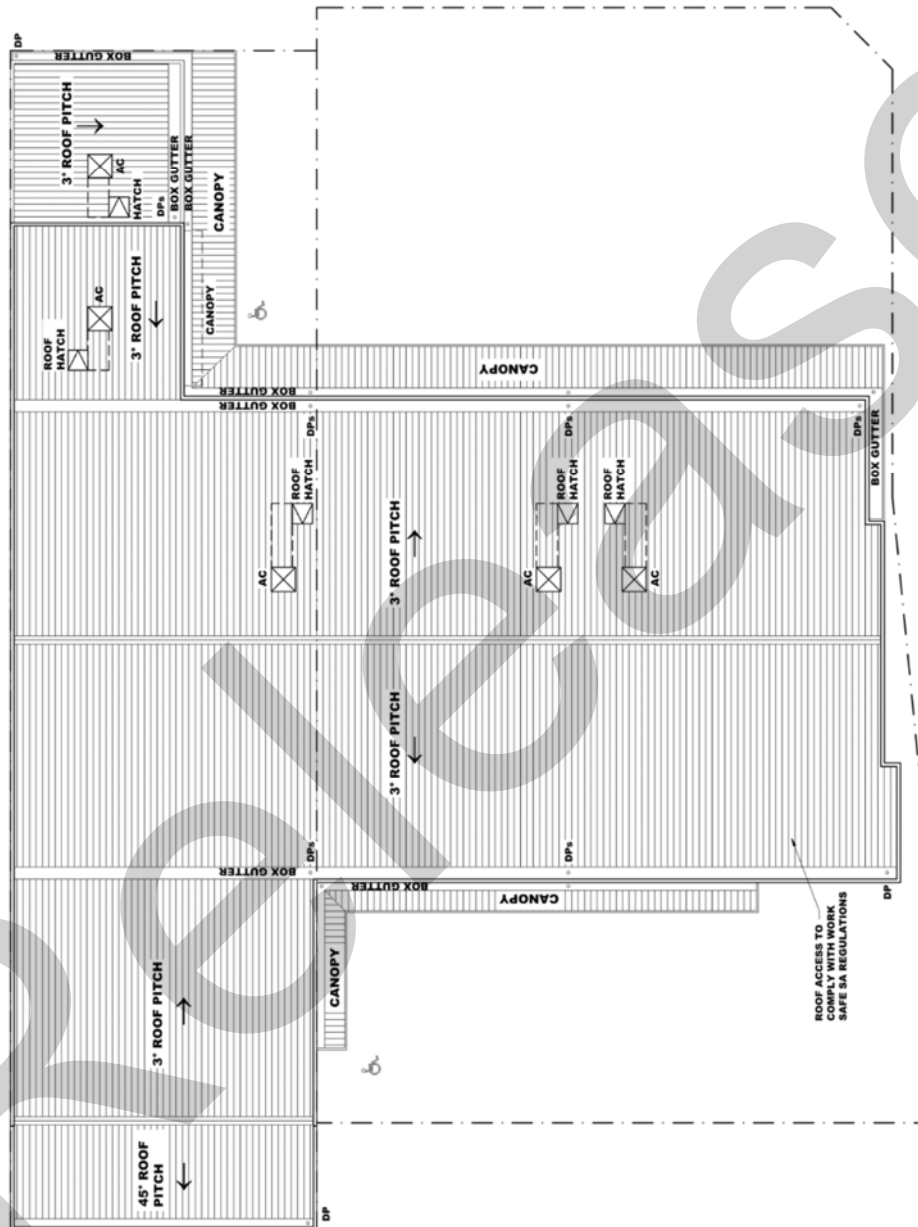
Council Assessment Panel



*NOTE : CONTOUR LINES INTERPOLATED THROUGH STRUCTURES ARE INDICATIVE ONLY
: THERE IS A CELLAR UNDER THE RESIDENCE ON LOT 22
note: this is not a boundary survey :- boundary plotted is purely indicative
SURVEYED BY SPECIALITY SURVEYS : JUN15

11 October 2022

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PROPOSED DEVELOPMENTS
 PROPOSED WAREHOUSE
 210-241 RICHMOND ROAD
 RICHMOND

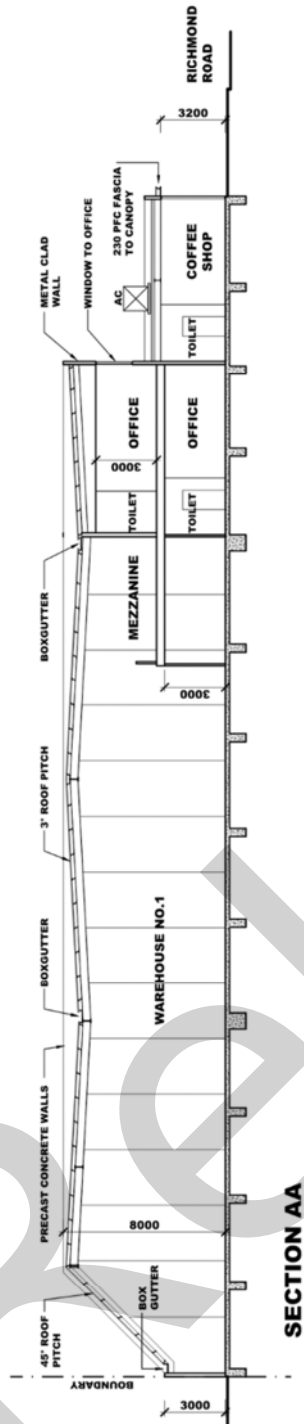
ROOF PLAN

DATE	DATE 2021
DESIGNED BY	DP
CHECKED BY	DP
DATE 2021	DA 3
DATE 2021	DA 3

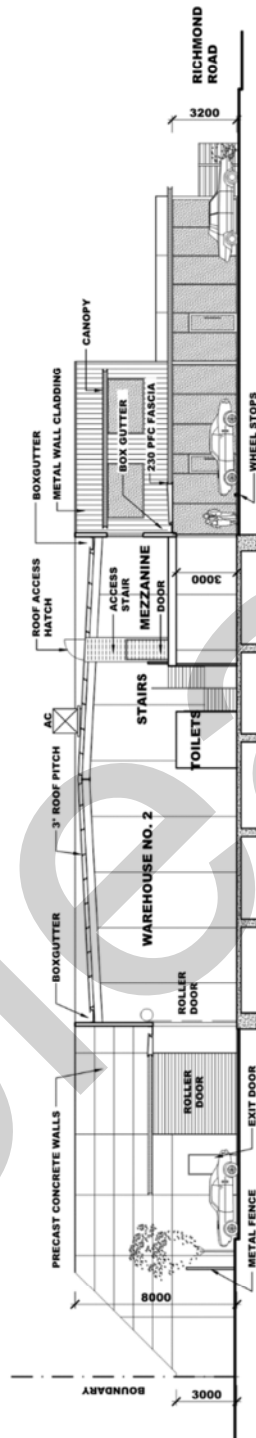
Roof Roach Architecture
 Tel: 011 811 8111 E: info@roofroach.co.uk

Item 6.2.1 - Attachment 1

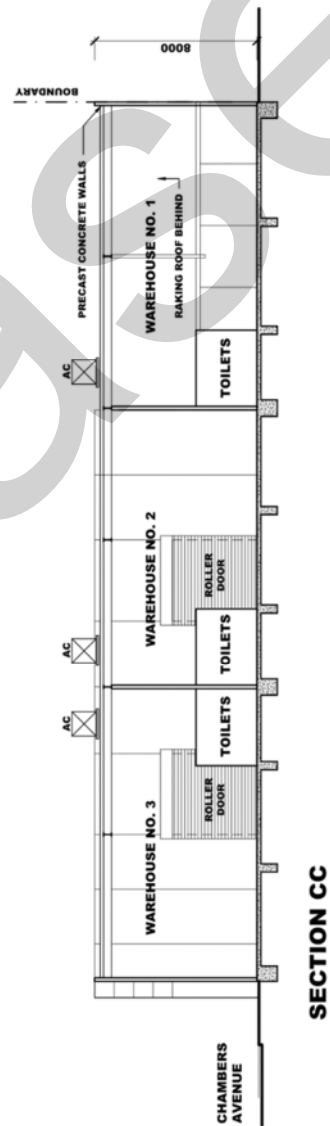
Council Assessment Panel



SECTION AA



SECTION BB



SECTION CC

OLYMPIC DEVELOPMENTS
 PROPOSED WAREHOUSE
 230-241 RICHMOND ROAD
 RICHMOND
 (1:1000)
 SECTIONS

DATE	REV	BY	NO
2022.10.11	DA 5		

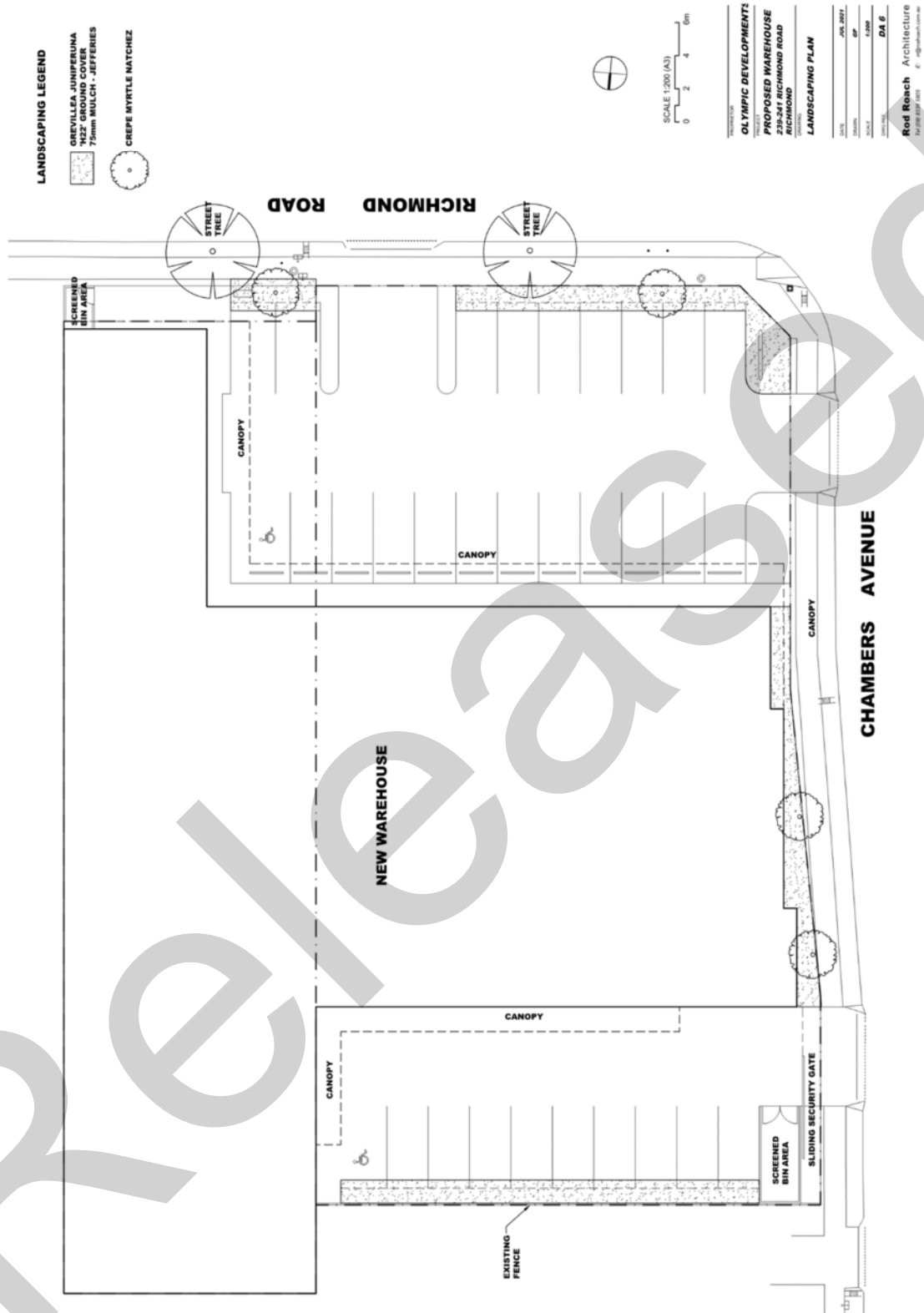
Red Reach Architecture
 Tel: 08 638 5500
 E: info@redreach.com.au

11 October 2022

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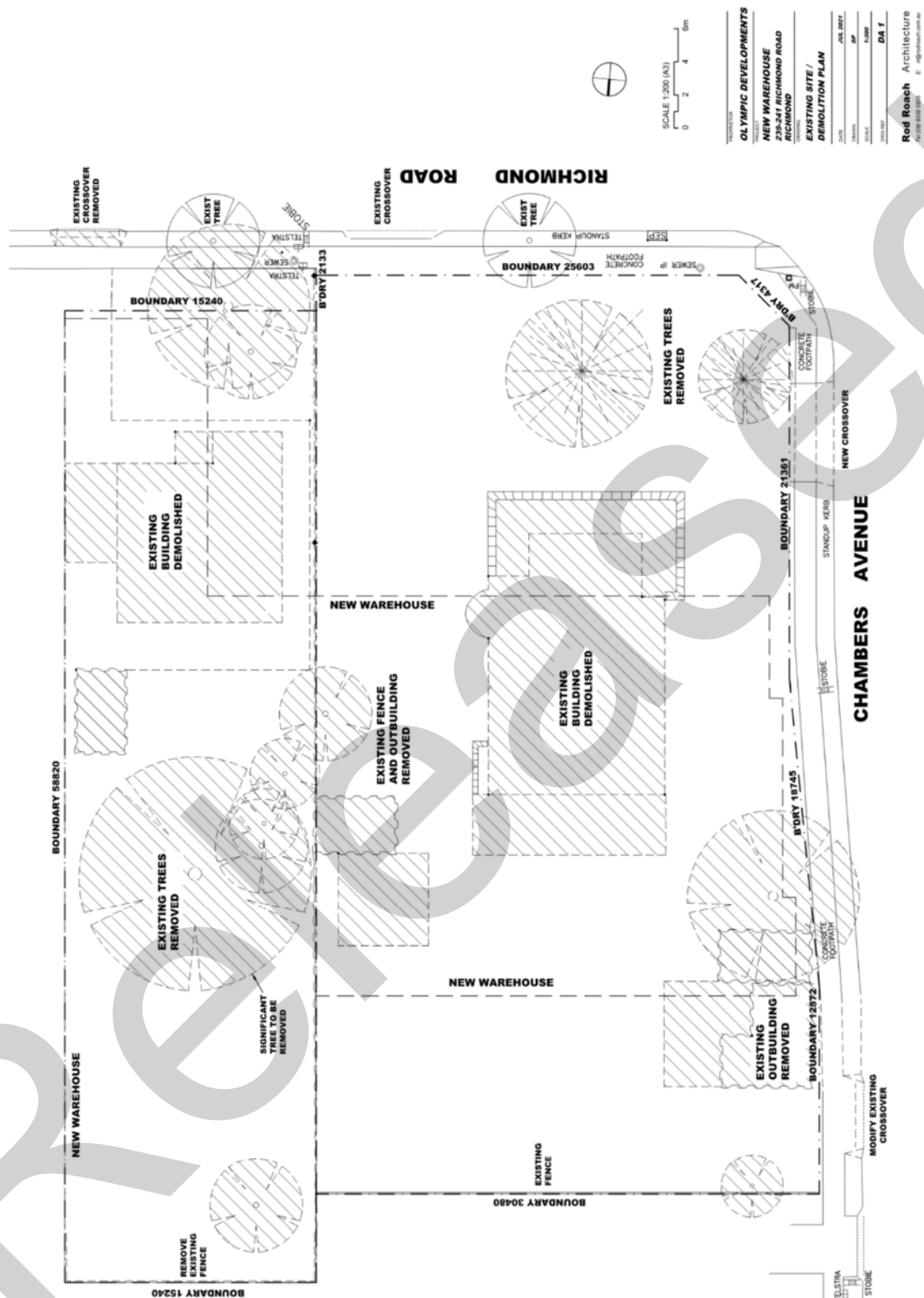
Item 6.2.1 - Attachment 1

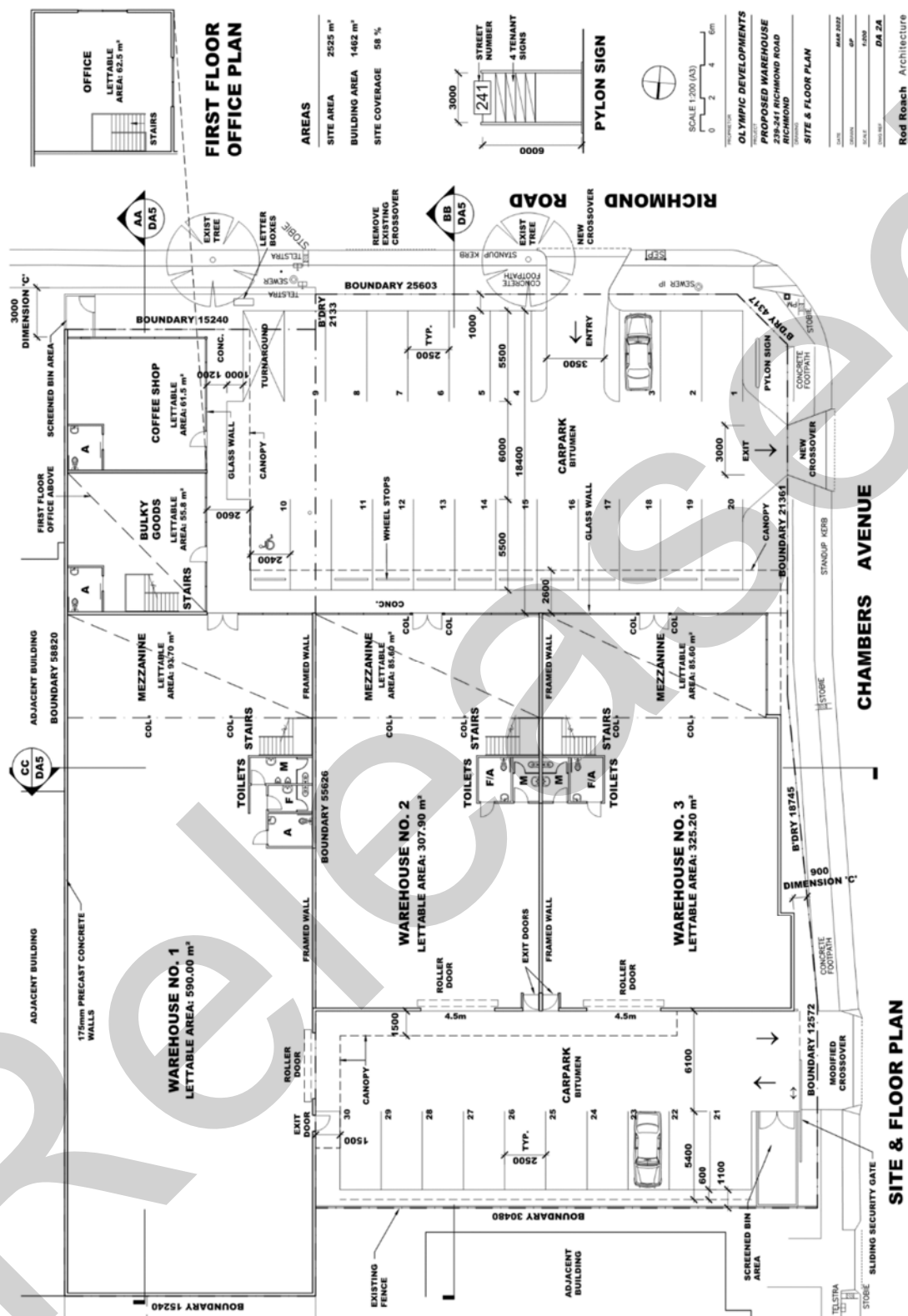
Council Assessment Panel



11 October 2022

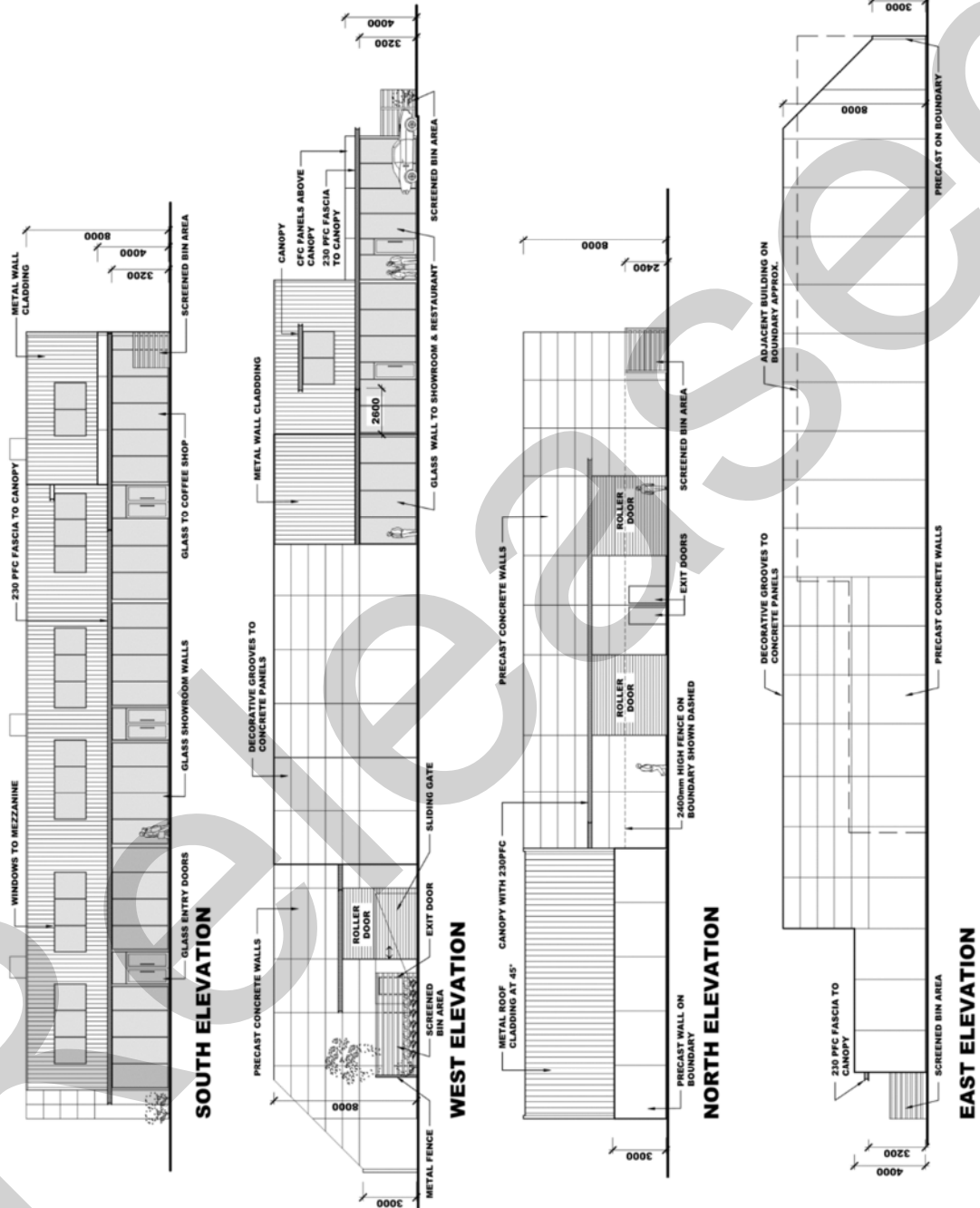
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Item 6.2.1 - Attachment 1

Council Assessment Panel



11 October 2022

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Stantec Australia Pty Ltd
Level 5, 75 Hindmarsh Square
Adelaide SA 5000

7 April 2022

Project/File: 300303484

Rod Roach Architecture
1/424 Portrush Road
Linden Park SA 5065

Dear Rod,

Reference: Dev No. 21028599 – 239-243 Richmond Road, Richmond – Proposed Warehouse Development – Response to DIT RFI

I refer to the proposed development located at 239-241 Richmond Road, Richmond for a proposed warehouse, office and shop development with regards to Development No. 21028599. A Schedule 9 – Further Information Request dated 7 February 2022 has been received from Department for Infrastructure and Transport for the proposed development. The FIR included the following:

1. *Details of the expected traffic generation from the development for all uses on the site including light vehicles and heavy vehicles for daily, AM and PM peak movements.*
2. *Details of the largest vehicle expected to require access to the site as per Major Urban Transport Routes Overlay DTS/DPF 1.1, c), (i.e. largest delivery/refuse vehicle) and predominant direction of travel to/from the site.*
3. *Consistency of the proposed access arrangements with Major Urban Transport Routes Overlay with DTS/DPF 4.1 - Access - Location (Spacing) - New Access Points, particularly turning movements into the new access adjacent the Richmond Road/Chambers Avenue junction and assess the potential for vehicles to queue back onto Richmond Road.*
4. *Review the right turn movements to/from the proposed access (existing crossover) on Richmond Road that is located within the Aldridge Terrace median opening and consistency with Major Urban Transport Routes Overlay with DTS/DPF 4.1 - Access - Location (Spacing) - New Access Points.*
5. *Provide vehicle swept path turn diagrams demonstrating that vehicle movements will be achieved for the largest vehicle expected to access the development and illustrating that two-way passenger car (B99) movements can via the proposed access points occur in accordance with the relevant Australian Standard and in accordance with the Major Urban Transport Routes Overlay.*
6. *A revised site plan demonstrating the above can be achieved.*

This letter provides a response to the above matters in the FIR from DIT.

Existing Situation

The subject site is located on allotments at 239 and 241 Richmond Road in Richmond, on the eastern side of Chambers Street. The site is also located opposite Aldridge Terrace on the southern side of Richmond Road. The existing site currently comprises residential dwellings with existing driveways on Richmond Road and Chambers Street.

The site is located in an Employment zone, with light industrial and commercial properties to the east and west on Richmond Road. Residential uses exist to the north of the site on Chambers Street.

Design with community in mind



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Page 2 of 8

Richmond Road is an arterial road under the care and control of Department for Infrastructure and Transport. It comprises dual carriageways with a raised central median, with two lanes in each direction. Richmond Road carries approximately 18,600 vehicles per day (DIT, 2021).

Chambers Street is a local road under the care and control of City of West Torrens. It comprises a single 9.5 metre wide (approx..) carriageway within a 15 metre road reserve (approx..). Footpaths are provided on each side of the road adjacent the site. Traffic surveys undertaken in March 2022 indicates Chambers Street carries approximately 500 vehicles per day near Richmond Road.

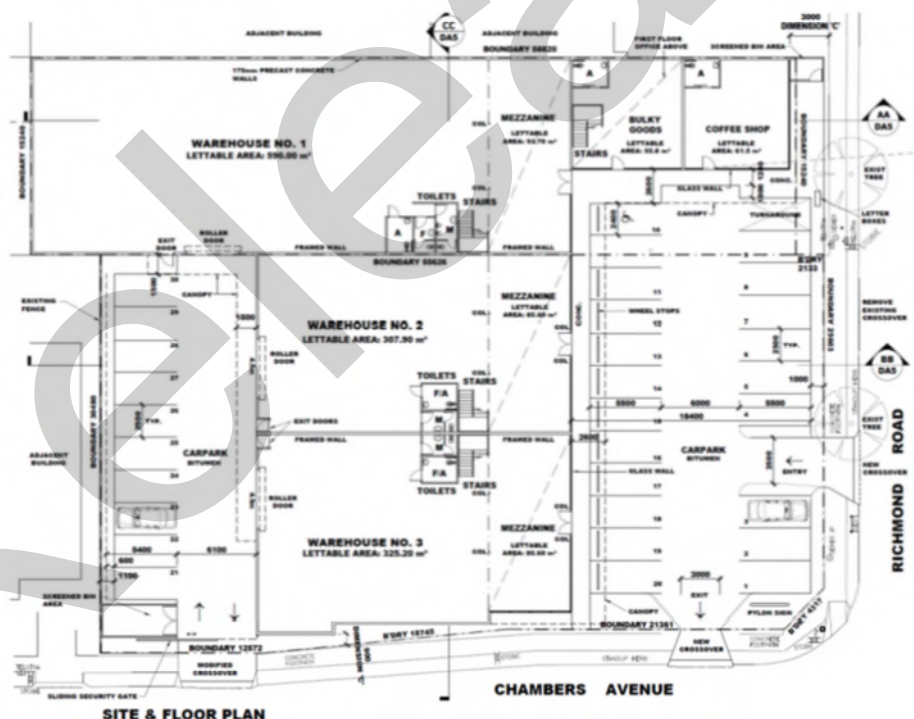
Proposed Development

The proposed development will comprise a mix of uses including:

- Warehouse (1,448sq.m)
- Office (56sq.m)
- Bulky Goods (56sq.m)
- Shop (61.5sq.m)
- Two car parking areas with access from Chambers Street and Richmond Road, to accommodate 30 car parking spaces

The proposed amended layout is shown in Figure 1.

Figure 1 Proposed Layout



Design with community in mind

20220407_300303484_239richmond_traffic_v01.docx
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Traffic Assessment

Traffic generation estimates for the proposed development have been sourced from the Transport for New South Wales 'Guide to Traffic Generating Developments' (formerly RTA NSW, 2002, henceforth referred to as the TfNSW Guide).

Estimates of peak hour and daily traffic volumes resulting from the proposed land use floor areas are set out in Table 3.

Table 1: Traffic Generation Estimates

Use	Size (sq. m)	Traffic Generation Rate (Trips/100 sq. m)		Total Vehicle Movements (vehicles)	
		Daily	Peak Hour	Daily	Peak Hour
Warehouse	1,488	4	0.5	60	8
Office (level 1)	56	10	2	6	1
Bulky Goods (ground)	56	17	2.7	9	2
Shop	62	100	10	100	6
		TOTAL		175	17

Table 1 indicates that if the peak periods for all the uses were to coincide, the proposed development would generate 17 vehicles per hour and 175 vehicles per day. When considering the existing use generation of 16 vehicles per day for the subject site, this would be a net increase of 159 vehicles per day. Trips associated with vehicle loading at the rear of the property are expected to typically occur outside of the traffic peak periods.

A survey of turning movements during the AM and PM peak hours identified a total of 45 and 42 vehicles per hour entering and exiting the Chambers Avenue/Richmond Road intersection. Richmond Road operates with significant gaps between platoons of vehicles providing a high level of capacity for the intersection with no significant queueing or delays for vehicles entering or exiting Chambers Avenue observed. The additional traffic identified for the proposed development will not significantly impact the operation of the existing intersection.

Against existing traffic volumes in the vicinity of the site, the additional traffic generated by the proposed development would not be expected to impact on the safety or efficiency of the surrounding road network.

Parking

A review of parking for the proposed development against the Planning and Design Code as referenced in Table 1 - General Off-Street Car Parking Requirements in Part 4 - General Development Policies - Transport, Access and Parking is shown in Table 2.

The assessment has identified 15 spaces are required for the proposed development. A total of 30 spaces will be provided which will more than cater for the anticipated parking demand of the land uses.

Design with community in mind

20220407_300303484_239richmond_traffic_v01.docx
Page 4 of 8

Table 2: Parking Requirement

Use	Size (sq. m)	P&D Code Requirement (spaces per 100sq.m)	Parking Spaces Required
Warehouse	1,488	0.5	8
Office (level 1)	56	4	2
Bulky Goods (ground)	56	2.5	2
Shop	62	5	3
TOTAL			15

Car Park Layout

The parking layout has been designed in accordance with Australian Standard/New Zealand Standard for Off Street Car parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009).

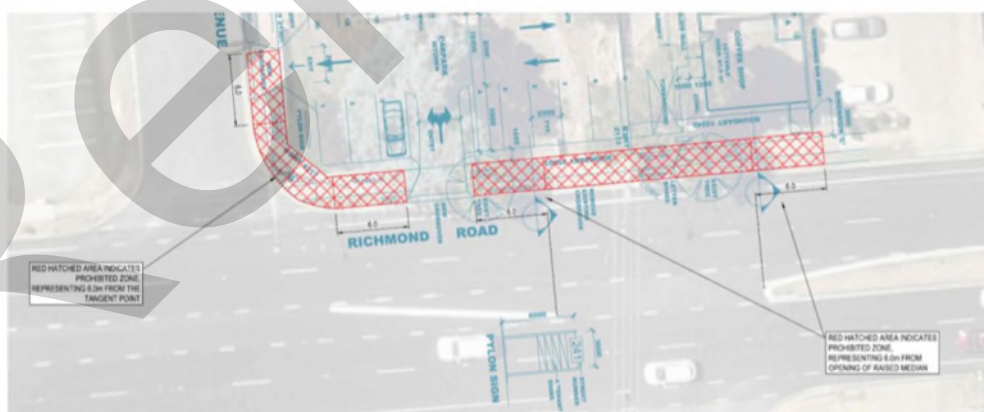
Key features of the car park design include:

- 90 degree parking spaces have been provided with a width of 2.5m and a length of 5.5m and set within a 6m wide aisle;
- Parking space for people with disabilities and associated shared spaces have been provided in accordance with the AS/NZS2890.6:2009 in the front car park;
- 1m blind aisle extensions have been provided at each end of the car park and a turning bay has been provided at each end of the car park in accordance with AS/NZS2890.1:2004;

Access

The proposed development will have an access point on Richmond Road and two access points on Chambers Street. The Richmond Road access point has been identified by DIT as not meeting the requirements of Major Transport Routes Overlay DTS/DPF 4.1 – Access – Location (Spacing) – New Access Points. A review of the site with regards to the requirements of the P&D Code has found there is limited opportunity for an access point on Richmond Road.

Figure 2 Access Prohibited Zones



Design with community in mind

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In order to comply with the P&D Code, it is proposed that an entry only driveway be provided on Richmond Road, and an exit only to Chambers Avenue. This arrangement will provide safe and convenient access given the small size of the car park and low traffic volumes estimated for the site. It will ensure that the existing junction of Aldridge Terrace will not be impacted by the proposed development.

Whilst the exit driveway proposed to Chambers Avenue will technically be within a prohibited zone, the exit movement can operate safely as it will be required to give-way to all traffic on Chambers Avenue. There is good sight distance for exiting vehicles to view other vehicles on Chambers Avenue and at the intersection with Richmond Road.

Vehicle Access

The proposed development has been designed to generally accommodate vehicles up to Medium Rigid Vehicle at the rear access point on Chambers Street. This would require this size vehicle to reverse into or out of the site from Chambers Street. Considering this is likely to be occasional service (i.e. less than 1 per day), this arrangement is considered acceptable given low volumes on Chambers Street and good sight distance for these movements. The turn path for a Medium Rigid Vehicle entering and exiting the site with a reverse manoeuvre is shown in

Figure 3.

Figure 3 MRV access to rear car park



The warehouse layout will support Small Rigid Vehicles to enter and exit in a forward direction, with access into the proposed warehouse doors from the rear car park. Larger vehicles may access the site

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and unloading the car park, as typically occurs in smaller warehouse developments. The size of the warehouses would only require small to medium sized trucks to access the site. Turn path diagram for SRV accessing the warehouses is shown in Figure 4 and Figure 5.

Figure 4 SRV Entry Movements

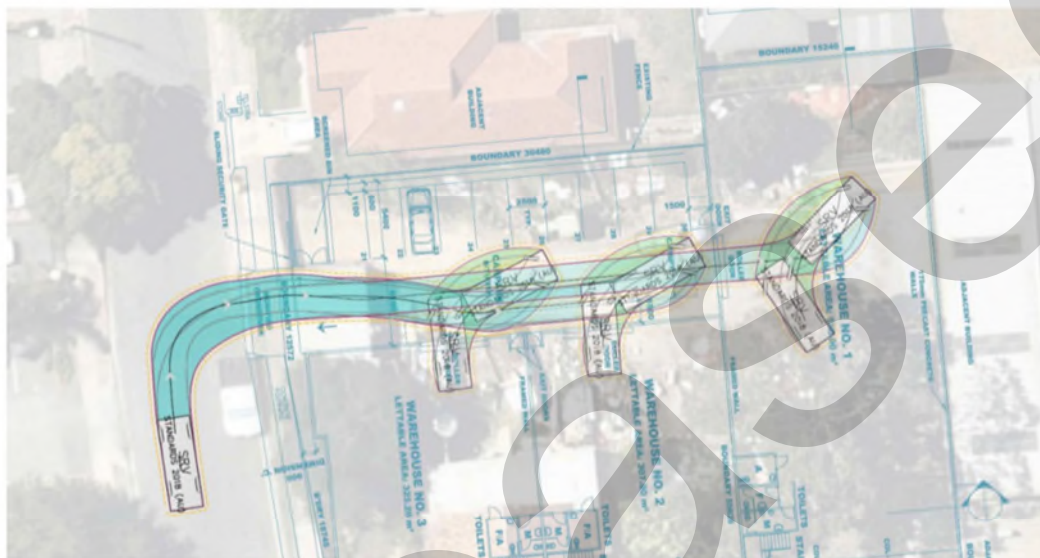


Figure 5 SRV Exit Movements

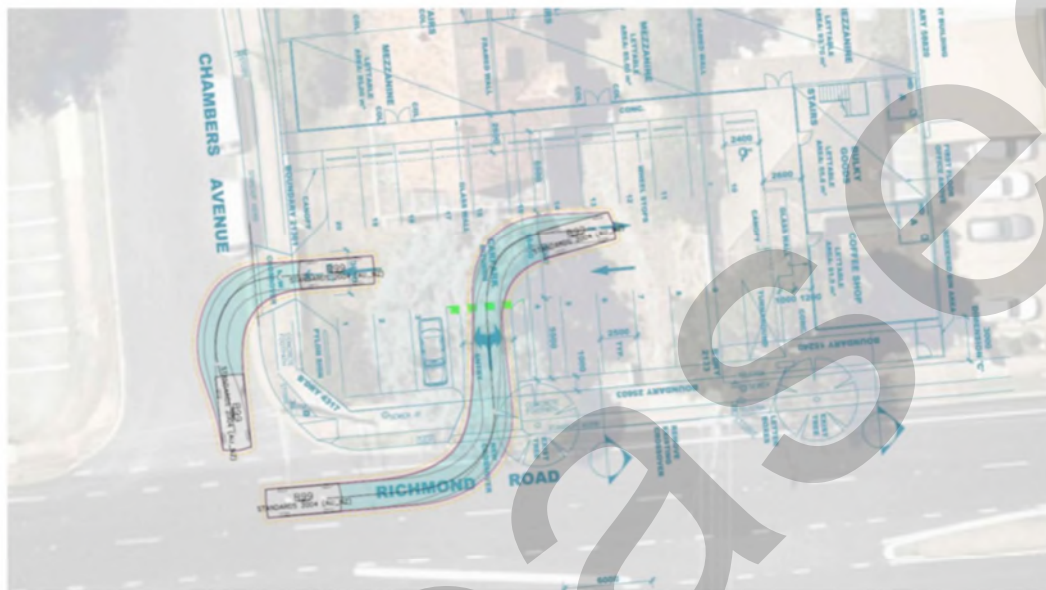


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Light vehicles will be able to access the front car park with the proposed access points with separate entry and exit movements. These are shown in Figure 6.

Figure 6 Light Vehicle Car Park Access



Summary

The traffic assessment of the proposed development has been undertaken and has the following in repose to the matters raised in the Department for Infrastructure and Transport Request for Further Information:

1. Details of the expected traffic generation from the development for all uses on the site including light vehicles and heavy vehicles for daily, AM and PM peak movements.

The traffic assessment has found the proposed development would generate 17 vehicles per hour and 175 vehicles per day. When considering the existing use generation of 16 vehicles per day, this would be a net increase of 159 vehicles per day. The overall increase in traffic is minor.

2. Details of the largest vehicle expected to require access to the site as per Major Urban Transport Routes Overlay DTS/DPF 1.1, c), (ie. largest delivery/refuse vehicle) and predominant direction of travel to/from the site.

The largest vehicle expected to access the site is a waste collection vehicle based on a typical 8.8 metre size Medium Rigid Vehicle. Other vehicles accessing the site would typically be smaller rigid trucks (SRV class) and light vehicles. These vehicles are proposed to access the site via the rear access point on Chambers Street.

It is expected that these vehicles would arrive and depart the site via Richmond Road to the east and west of the site.

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3. *Consistency of the proposed access arrangements with Major Urban Transport Routes Overlay with DTS/DPF 4.1 - Access - Location (Spacing) - New Access Points, particularly turning movements into the new access adjacent the Richmond Road/Chambers Avenue junction and assess the potential for vehicles to queue back onto Richmond Road.*

The proposed access points have been revised to meet the requirements of Performance Outcome 4.1 of the Major Urban Transport Routes Overlay in the planning and Design Code.

4. *Review the right turn movements to/from the proposed access (existing crossover) on Richmond Road that is located within the Aldridge Terrace median opening and consistency with Major Urban Transport Routes Overlay with DTS/DPF 4.1 - Access - Location (Spacing) - New Access Points.*

The relocation of the access point on Richmond Road will not permit right turn movements from Richmond Road into the site.

5. *Provide vehicle swept path turn diagrams demonstrating that vehicle movements will be achieved for the largest vehicle expected to access the development and illustrating that two-way passenger car (B99) movements can via the proposed access points occur in accordance with the relevant Australian Standard and in accordance with the Major Urban Transport Routes Overlay.*

Swept path diagrams have been provided for vehicles in the site in this report.

6. *A revised site plan demonstrating the above can be achieved.*

A revised site plan has been developed as per the comments in this report.

Naturally, should you have any questions or require any further information, please do not hesitate to contact me on (08) 8334 3600.

Yours sincerely,

STANTEC AUSTRALIA PTY LTD



Paul Morris
Group Leader - Transportation SA
M.Trans&Traff, MAITPM
paul.morris2@stantec.com

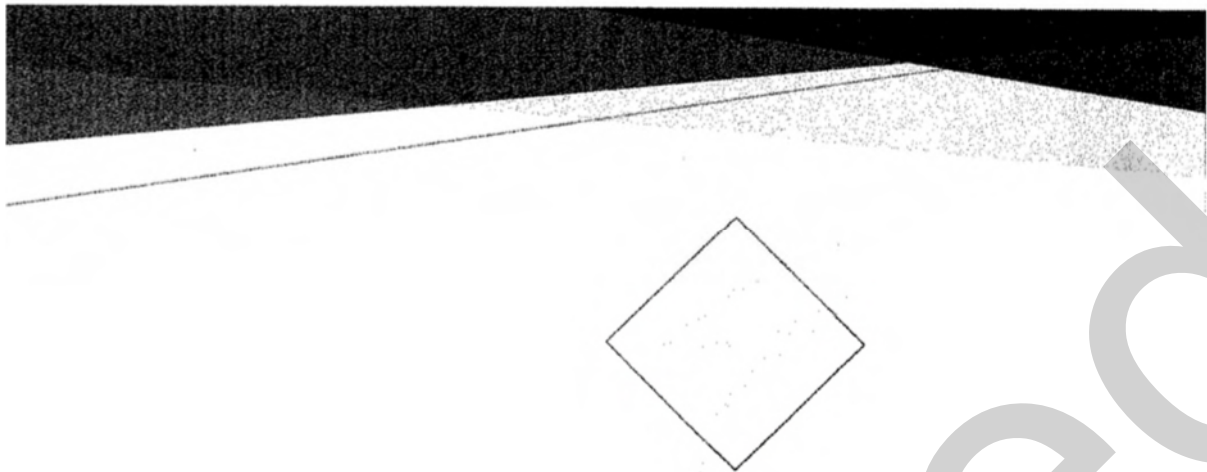
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OLYMPIC DEVELOPMENTS PTY LTD
Commercial Development – Richmond

Development Application Report

SCHEDULE OF MATERIALS & FINISHES

Element	Material	Finish	Colour
Structural Walls	Precast concrete	Painted	Dulux "Chinchilla White"
Wall Cladding	Steel panel	Colorbond	"Wallaby"
Window & Door Frames	Aluminium	Powder coated	To match "Wallaby"
Glass Shop Fronts	Laminated	Clear	
Mezzanine Windows	Laminated	Grey tinted	
Roller Doors	Steel	Powder coated	To match "Wallaby"
Exit Doors	Steel clad	Powder coated	To match "Wallaby"
Roof	Steel roof deck	Colorbond	"Surf Mist"
Fascias	Steel PFC	Painted	To match "Wallaby"
Canopy Ceilings	Fibre cement	Painted	Dulux "Chinchilla White"
Bin Screens	Perforated steel	Powder coated	To match "Wallaby"
Fencing	Steel	Colorbond	"Wallaby"



GINOS ENGINEERS PTY LTD
Consulting Engineers

STORMWATER CONCEPT

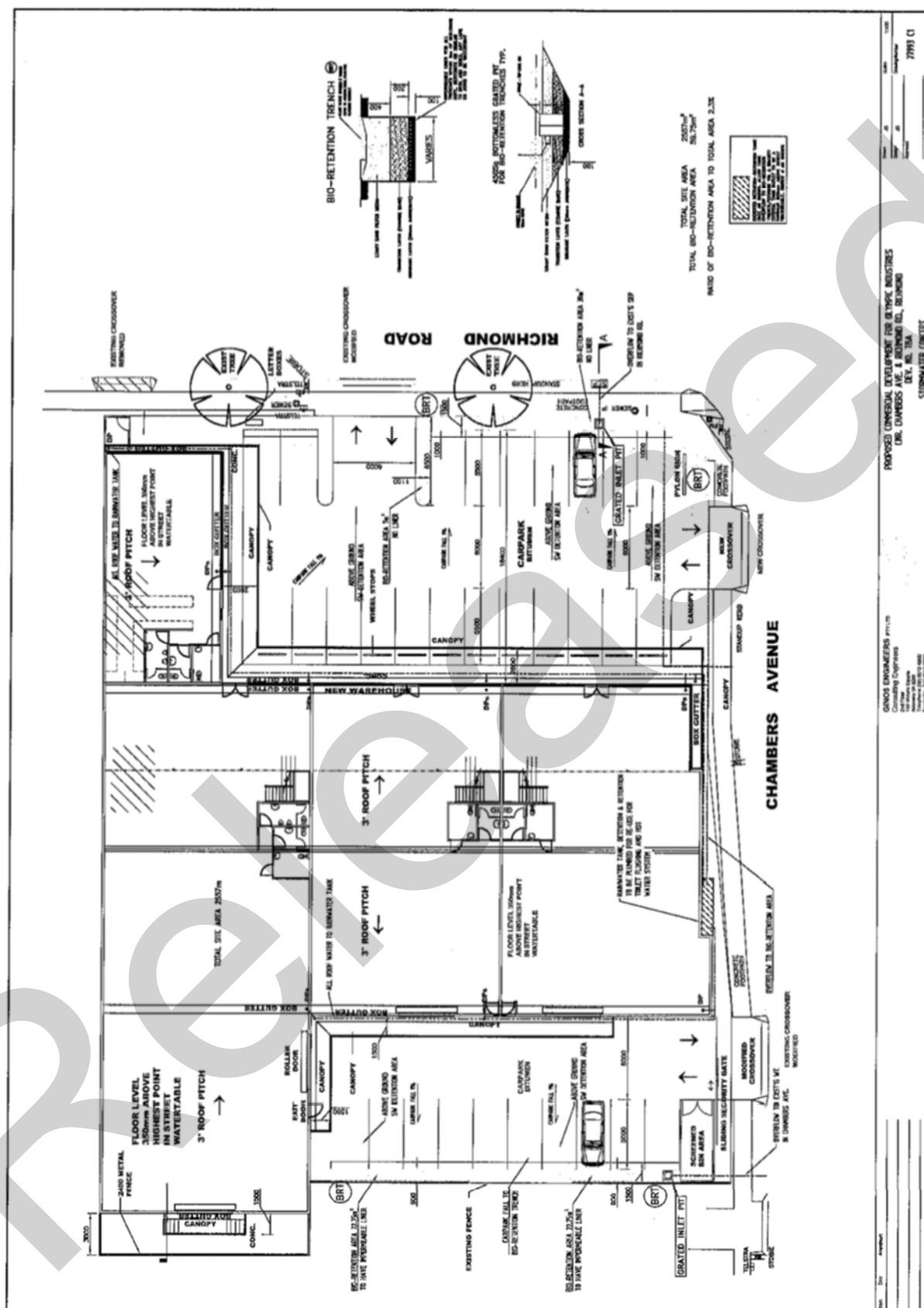
PROPOSED COMMERCIAL DEVELOPMENT
CNR. CHAMBERS AVE. AND RICHMOND RD. RICHMOND

FOR: OLYMPIC INDUSTRIES

JOB NO. 27993

DATE: 16th JUL 15

Contact: John Savic
Ph. 8212 4900



Job no.27993

COMMERCIAL DEVELOPMENT – RICHMOND
Stormwater Concept

Introduction

The proposed development consists of new warehousing, restaurant and car parking at the corner of Chambers ave. and Richmond rd. Richmond.

The site is currently made up of two existing residential properties.

Proposed Components:

- Bitumen car parking area
- Building roofs
- Underground sw pipes, pits
- Above ground detention
- WSUD – Water sensitive urban design, bio-retention filtration

Stormwater Proposal

Council generally requires that post development flows are restricted. Attached is correspondence from council which outlines stormwater requirements for the proposed development.

The general approach for this development will be in line with councils requirements which can be broadly summarised as follows, improve water quality prior to stormwater exiting the site, limiting post-development flows from the site to 20l/s and ensuring that people and property are safe from inundation.

Grassed areas where they can be, will be used to provide water quality improvement, carparks may be used for detention if practical, above ground tanks will be used for retention and detention as required.

If grassed areas/ bio-retention is not possible, then a proprietary filtration system may be required.

*Job no.27993***Council Preliminary requirements****1.0 Stormwater Detention (Commercial-Industrial) (Less than 4000m²)**

As the allotment(s) being affected by the proposed development total between 1000 and 4000 square metres, stormwater detention measures will be required to be undertaken to restrict the total discharge from the total development site to a maximum of 20 litres per second for the site critical 20 year ARI storm event.

In calculating the stormwater detention requirements, runoff from any existing structures and buildings to be maintained must be taken into consideration.

An indication of how the storage is to be provided and calculations supporting the nominated volume are to be provided.**2.0 Detention Volume Trade-off for Rainwater Re-use Provision**

To encourage the conservation of water in commercial developments, the applicant should be requested to provide a roof stormwater collection and re-use system which delivers harvested water back into the building for use in at least the toilet and potentially also the hot water system.

The ability for such a system to deliver the majority of the non-potable water supply for a development of this scale is relatively simple and easy to achieve.

Upon the provision of information demonstrating a considered efficiency between the supply and demand associated with an active re-use scheme, Council is typically willing to enable an offset to any required detention storage by an amount equivalent to stormwater re-use storage.

Stormwater Quality Treatment**3.0 Stormwater Quality – Commercial Sites**

Council typically requests that stormwater quality measures within carpark and manoeuvring area to address the collection of oils, grease and sediment from the stormwater flow.

Although these measures are often addressed through the installation of proprietary devices, Council encourages the use of better Water Sensitive Urban Design measures to improve the quality of site discharge flows. An example of which may be to drain the site to appropriately designed vegetated swales and bio-filtration trenches. It would appear that the proposed layout may lend itself to this style of treatment.

2 ***Cnr. Chambers ave. & Richmond rd. Richmond***

Job no.27993

An indication of how the water quality requirements are to be met should be provided on revised site plans prior to the finalisation of the planning assessment for this development.

4.0 Stormwater Quality Targets

It is considered within the stormwater industry that the following table outlines current best practice for the targeted improvement of stormwater quality from new developments of scale.

The targets being;

Parameter	Target
Litter/Gross Pollutant	Retention of litter greater than 50mm for flows up to the 3 month ARI peak flow.
Oil and Grease	No visible oils for flows up to the 3 month ARI peak flow.
Reduction in Average Annual Total Suspended Solids (TSS)	80% *1
Reduction in Average Annual Total Phosphorous (TP)	60% *1
Reduction in Average Annual Total Nitrogen (TN)	45% *1

*1 - Reduction as compared to an equivalent catchment with no water quality management controls.

Industry standard computer modelling would be anticipated (as a minimum) to be utilised to demonstrate the suitability of the proposed measures to achieve the above nominated targets.

Details of Representations

Application Summary

Application ID	21028599
Proposal	Demolition of existing dwellings and associated structures, and construction of three (3) warehouses with associated office and storage space, two retail tenancies with associated offices along with associated carparking landscaping and freestanding pylon signage
Location	239 RICHMOND RD RICHMOND SA 5033, 241-243 RICHMOND RD RICHMOND SA 5033

Representations

Representor 1 - Liana Castellan

Name	Liana Castellan
Address	Unit 1, 10 Clifton Street HAWTHORN SA, 5062 Australia
Submission Date	13/07/2022 06:42 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development
Reasons	I approve of the development as it will improve and add to the quality of commercial properties on Richmond Road. Also I encourage removal of the significant tree close to the boundary of my property at 237 Richmond Road. The tree has, over a number of years, caused damage to my property with the consistent blocking of gutters with leaves resulting in flooding into my warehouse and damage to roof panels.

Attached Documents

Representations

Representor 2 - David Stevens

Name	David Stevens
Address	26b Lucas St RICHMOND SA, 5033 Australia
Submission Date	02/08/2022 09:12 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons Plans indicate a) removal of my fence, meaning the building will neighbour my residence, and b) a 45 degree roof pitch for the part of the building backing on to my backyard. My concern is in heavy rain, the water will spill into my back yard. My backyard floods now, so this will only get worse. I also have a concern regarding the removal of the large native tree that. It is a healthy tree.	

Attached Documents

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OLYMPIC GROUP
239 Richmond Rd, Richmond DA21028599

Responses to 2 Representations.

Representation 1- "Does not" support the Development

Submitted by David Stevens, Owner of 26b Lucas Street Richmond. Received 2 August 2022
Contact details, Email, [Mobile](#).

I have spoken to Mr Stephens on the 30 August 2022 to address his concerns, the principle one being that the proposed sloping roof to the South of his property would direct roof water onto the rear yard of his property. It was discussed that a large box gutter behind the boundary wall would collect all roof water which would be discharged into the proposed new stormwater disposal system. In addition the wall capping flashing would slope into the box gutter to eliminate any wall drips from falling onto his land.

From the photos submitted it can be seen that the rear yard is inundated with surface water. I discussed this with Mr Stevens, who said that there was insufficient drainage to the yard. This matter should be given attention before likely damage may result with the existing dwelling. Should the proposed development proceed this issue could be eliminated with overflow into the swale proposed along the north boundary side.

In addition Mr Stevens is concerned that the significant Eucalypt in East side of the property is to be removed.

As this tree is some 25 metres from Mr Stevens property if approval is given by the Council, it would not have significant impact on him.

I have discussed these matters with Mr Stevens, but he will not withdraw his objection and still wishes to be heard at the Council Assessment.

Representation 2 – "Does" support the Development

Submitted by Liana Castellan, Owner of property on the East side of the proposed Development in Richmond Road.

Contact details, [Email](#), [Mobile](#).

I have spoke to Liana Castellan on the 30 August 2022 to address the development proposal lodged and her support for same.

Liana Castellan has advised me that she and her husband have owned their property for some 35 years which was used as an office-warehouse for their building and plumbing businesses. She supports the proposed Development as it will reinforce the commercial activities in Richmond Road when it is occupied. The existing Eucalypt has been a constant irritant for her as the leaf litter from the tree has cost her considerable sums of money for roof maintenance, repair and replacement. She advises that the tree was only about 3 metres when they purchased the property 35 years ago, but has grown to the height and spread it is currently. She strongly supports the removal of the tree.

NOTIFICATION TO REPRESENTORS

These Responses will be emailed to David Stevens and Liana Castellan for their information.

COUNCIL ASSESSMENT PANEL

As Mr Stevens wishes to be heard at the Council Assessment Panel meeting, I wish to attend same with the Olympic Development Group to put their case before the Panel.

Memo

From Richard Tan
Date 24/01/2022
Subject 21028599, 239 - 243 RICHMOND RD RICHMOND SA 5033

Rachel/Phil,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

Note: Civil plan has not been provided.

1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

- 1.1 Based on the survey information provided on 'Engineering Survey' (GE, Ref: 27993-ES, dated 06/2015) a minimum FFL of 12.86 would be required.

It is recommended that revised plans indicating the required minimum FFL be provided to Council.

The applicant should also confirm that the provided survey plan is reflecting current site and verge condition, as the survey was done in year 2015.

2.0 Verge Interaction

- 2.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically desired to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram ramps, etc. (as measured at the kerb line, except for driveway separation which will be measured from property boundary). An absolute minimum offset of 0.5m from new crossovers and stormwater connections to other existing road verge elements is acceptable in cases where space is limited.

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council's Technical Officer (Arboriculture).

- 2.1.1** Civil plan should be provided with all existing and proposed verge features indicated.

It is recommended that civil plan indicating satisfaction to the above requirements should be provided to Council.

- 2.1.2** The proposed new crossover at Chambers Avenue must be 6.0m away from the tangent point of the kerb of the intersection of Richmond Road and Chambers Avenue. This is a requirement under the Australian Standards (AS2890.1-2004).

It is recommended that revised plans indicating satisfaction to the above requirement should be provided to Council.

- 2.2** It should also be nominated for the stormwater connection through the road verge area to be constructed of shape and material to satisfy Council's standard requirements

- 100 x 50 x 2mm RHS Galvanised Steel or
- 125 x 75 x 2mm RHS Galvanised Steel or
- Multiples of the above.

It is recommended that revised plans clearly and accurately indicating satisfaction of the above criteria be provided to Council.

- 2.3** It should be noted that any portion of the existing crossover that will be made redundant should be reinstated to vertical kerb prior to the completion of any building works at the applicant's expense. It should also be indicated on revised plans that any redundant crossovers will be reinstated.

It is recommended that revised plans showing the reinstatement of redundant crossovers be provided to Council.

3.0 Traffic Requirements

The following comments have been provided by Council's Traffic Consultant, Mr Frank Siow:

I refer to the above development which comprises of 3 warehouses, a small bulky goods tenancy or office, a coffee shop and associated car parking. I note that the proposal plan identifies a small office tenancy instead of a 'bulky goods' tenancy as described in the application. The proposal plans show car parking of 31 spaces, comprising of a 21-space car park at the Richmond Road frontage and a 10-space car park at the Chambers Avenue frontage.

While I understand that there was a similar proposal for the subject site in 2015 (assessed under the previous Council's Development Plan), this would be irrelevant to the current proposal which has been lodged under the Planning and Design Code.

The subject site is located within the Employment Zone of the Planning and Design Code.

3.1 Parking Layout

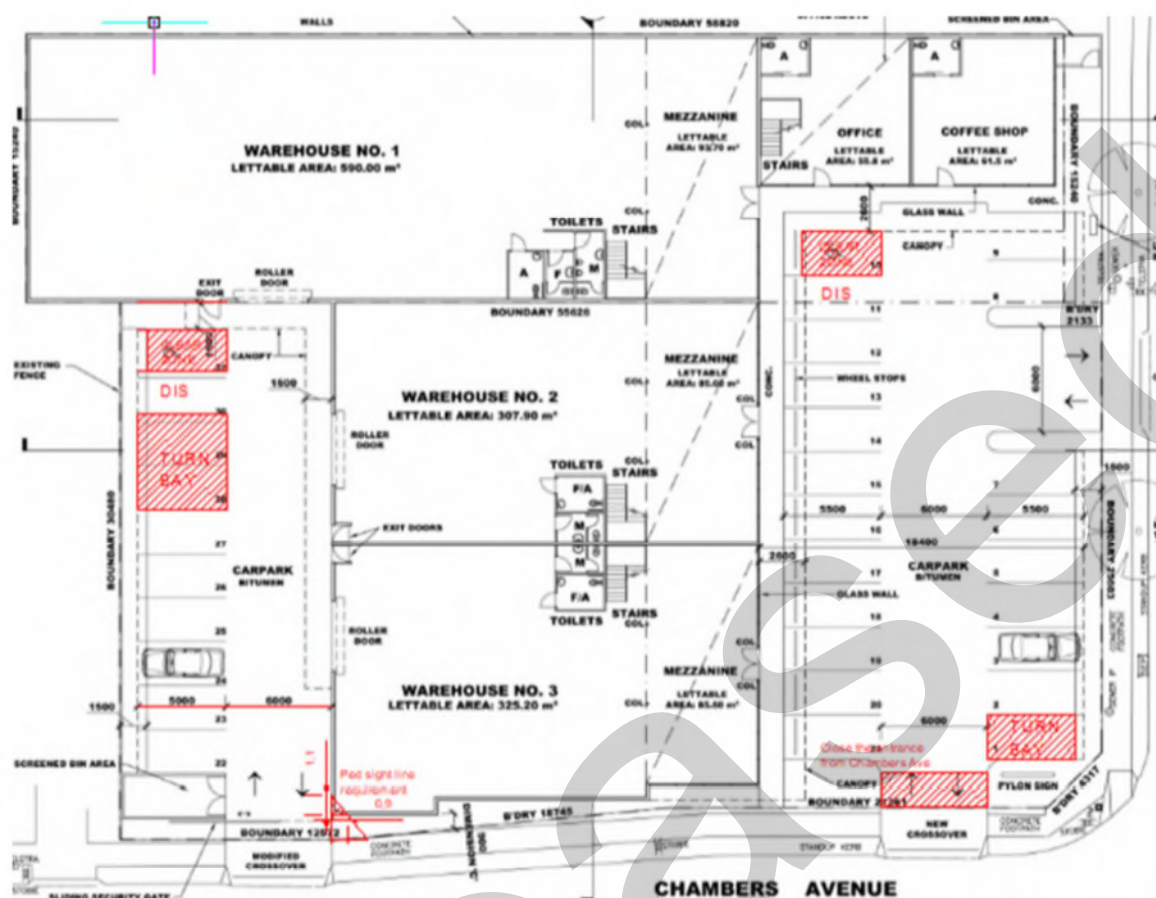
There are a number of amendments required to the parking layout to enable the layout to meet the requirements of the current parking standards and also to deal with the servicing aspects of the proposal.

I note that there is limited information provided regarding service vehicles that may require access to the warehouses other than the comment that the warehouses would be 'used by occupiers who do not require large vehicle access to the warehouse facilities' (see Rod Roach Architecture Report August 2021 Page 2). Given the constrained nature of the rear car park (Chambers Avenue) where the roller doors of the warehouses are located, my assessment indicates that the 6.4m long SRV truck (as per AS 2890.2-2018) would be able to access the warehouses and rear car parking layout. Vehicles larger than the SRV would not be able to access the roller doors. A condition restricting service vehicles to SRV size for the proposed 3 warehouses would be appropriate.

A screened refuse bin area is proposed at the entrance to the rear car park. The constrained nature of the rear car park would not allow a typical refuse truck to enter the site and turnaround. Given that refuse collection would likely be of an infrequent nature, it would be acceptable for a refuse truck to reverse into the site from Chambers Avenue for the waste collection. However, the waste collection should occur after business hours so as to minimise the impact on other users of the car park and users of Chambers Avenue. A condition restricting servicing of the bins to outside of business hours would be appropriate.

I note that the two tenancies at the Richmond Road frontage (bulky goods or office and coffee shop) have relatively small floor areas. Servicing for the tenancies would likely to be of an infrequent nature and by small vehicles. A condition restricting service vehicles to passenger vehicle size for these two tenancies would be appropriate.

The following amendments to the car park layouts would be required:



Rear Car Park

1. The disabled parking space would require an adjacent clear zone space to comply with AS/NZS 2890.6-2009. One parking space would be lost as a result.
2. A widened turn bay space would be required for the dead end car park and also to accommodate the turnaround for the SRV truck access to Warehouse 1. Two parking spaces would be lost as a result.
3. To comply with the pedestrian sight line requirement in AS/NZS 2890.1-2004, the corner of the Warehouse 3 building would need to cut-back.

Front Car Park

1. The disabled parking space would require an adjacent clear zone space to comply with AS/NZS 2890.6-2009. One parking space would be lost as a result.
2. The proposed location of the Chambers Avenue crossover does not comply with AS/NZS 2890.1-2004 or Overlay: *Major Urban Transport Routes PO 4.1 and DTS/DPF 4.1 (a)*. The close proximity of the entry from Richmond Road may create adverse impact on the Chambers Avenue/Richmond Road junction, if an entering vehicle is blocked by

vehicles queued to exit Chambers Avenue to Richmond Road. The proposed access point should be deleted. One parking space would be lost as a result (to provide a turnaround bay for the dead end car park).

3. While the proposed car park access point in Richmond Road would be located opposite Aldridge Road, it would be possible to provide a 'seagull' type island at the median opening to restrict right turns into and out of the subject site in Richmond Road. I leave it to DIT to specify its requirement for the central median arrangement as Richmond Road is managed and controlled by DIT.
4. To encourage cycling as a mode of transport, I recommend that 2 bicycle rails be provided at the front car park.

Recommendations:

- **That the amendments (1) to (5) as discussed above be incorporated into the proposed car park layouts.**
- **A condition is required to restrict service vehicles to SRV size for the proposed 3 warehouses.**
- **A condition is required to restrict servicing of the bins to outside of business hours.**
- **A condition is required to restrict service vehicles to passenger vehicle size for the office/bulky goods and coffee shop tenancies.**
- **Two bicycle rails be provided at the front car park.**

The above amendments would reduce the on-site parking provision from 31 spaces to 26 spaces.

3.2 Traffic and Parking Assessment

Table 1 – General Off-Street Car Parking Requirements of the Planning and Design Code would be relevant to the parking assessment of the subject development.

The relevant parking rates are:

- Warehouse – 0.5 spaces per 100m² total floor area.
- Shop (in the form of a restaurant or involving a commercial kitchen) – 0.4 spaces per seat.
- Shop (in the form of a bulky goods outlet) – 2.5 spaces per 100m² GLFA.
- Office – 4 spaces per 100m² GLFA.

The amended car park shown above would have 26 parking spaces.

The proposed floor areas as identified in the Development Application Report are:

Tenancy	Warehouse	Office
Warehouse 1	590m ²	93.7m ²
Warehouse 2	308m ²	85.6m ²
Warehouse 3	325m ²	85.6m ²
	Floor area	

Bulky goods or office tenancy	119m2	
Coffee shop (unknown number of seats)	62m2	

If considered as 'warehouse' land uses, the parking required for Warehouse 1, 2 and 3 would be 7.4 spaces and the office tenancy would require 4.8 spaces, ie total of 12.2 spaces. As 26 parking spaces would be available on-site, the surplus parking of 13.8 spaces would be more than adequate to accommodate the parking requirement of the small coffee shop, ie the surplus parking would be equivalent to a rate of 22 spaces per 100m2.

If the warehouse tenancies were to be considered separately as 'warehouse' and 'office' land uses, the parking requirement for Warehouse 1, 2 and 3 would be 16.6 spaces and the office tenancy would require 4.8 spaces, ie total of 21.4 spaces. The surplus parking availability for the small coffee shop would be equivalent to a rate of 7.4 spaces per 100m2.

Given the site's reasonable proximity to public transport services and peak hour bicycle lanes in Richmond Road, it would not be unreasonable to discount the above parking requirement for the subject site. If say a 10% discount were to be adopted, the parking requirement estimated above would be reduced by say 2 spaces.

Based on the above assessment, I am satisfied that adequate parking would be provided for the development.

Based on the amended parking layout shown above, I do not have concern with the traffic impact of the development on the adjacent road network.

4.0 Waste Management

- 4.1 Due to the nature of this application being a commercial development, it is recommended that further assessment from Council's Waste Management Team is required.

It is recommended that further assessment from Council's Waste Management Team is required.

5.0 Stormwater Management

5.1 Stormwater Harvest and Re-use

City Assets typically strongly encourages the inclusion of stormwater collection and active re-use, particularly with function with possible high demand of water reuse and hence a high volume of reduction of stormwater runoff from the site can be simply achieved.

Collection and active re-use of stormwater in developments of this nature can go a long way towards the achieving the other stormwater management

measures if water quality and detention, as well as the sustainability benefits which area achieve through water conservation considerations.

It is strongly encourage that the applicant explore the stormwater collection and re-use option as above.

5.2 Stormwater Detention

Stormwater detention measures will be required to be undertaken to limit the peak discharge rate for the site critical 20 year ARI storm event to equivalent to a predevelopment arrangement with a 0.25 runoff coefficient.

In calculating the stormwater detention requirements, runoff from any existing structures and buildings to be maintained must be taken into consideration.

It is recommended that an indication of how the storage is to be provided and calculations supporting the nominated volume be submitted to Council.

5.3 Stormwater Quality

Council typically requests the implementation of stormwater quality measures for development of this nature to address the removal of stormwater pollutants from the stormwater flow exiting the site.

The following table outlines current recommended practice for the targeted improvement of stormwater quality from new developments of scale, as outlined in the State Government's Water Sensitive Urban Design Policy - 2013. The targets being;

Parameter	Target
Reduction Litter/Gross Pollutant	90%*
Reduction in Average Annual Total Suspended Solids (TSS)	80%*
Reduction in Average Annual Total Phosphorous (TP)	60%*
Reduction in Average Annual Total Nitrogen (TN)	45%*

* Reduction as compared to an equivalent catchment with no water quality management controls.

Although these measures are often addressed through the installation of proprietary devices, Council encourages the use of Water Sensitive Urban Design measures to improve the quality of site discharge flows which may also provide other added benefits to the development, such as permeable pavers or raingardens.

An indication of how the water quality requirements are to be met should be provided on revised site plans prior to the finalisation of the planning assessment for this development.

Regards
Richard Tan
Civil Engineer

OFFICIAL

In reply please quote 21028599
 Enquiries to Mr Martin Elsworthy
 Telephone (08) 7322 9038
 E-mail dit.landusecoordination@sa.gov.au



Government of South Australia

Department for Infrastructure
and TransportTRANSPORT PLANNING AND
PROGRAM DEVELOPMENT

Transport Assessment

GPO Box 1533
ADELAIDE SA 5001

ABN 92 366 288 135

24 June 2022

Mr Phil Smith
 City of West Torrens
 165 Sir Donald Bradman Drive
 Hilton

Dear Mr Smith

SCHEDULE 9 - REFERRAL RESPONSE

Development No.	21028599
Applicant	Rod Roach
Location	241-243 Richmond Road, Richmond
Proposal	Commercial development with three warehouses, small bulky goods, coffee shop with associated car parking and site works

The above application has been referred to the Commissioner of Highways (CoH) in accordance with Section 122 of the *Planning, Development and Infrastructure Act 2016*, as the prescribed body listed in Schedule 9 of the *Planning, Development and Infrastructure (General) Regulations 2017*.

CONSIDERATION

The subject development abuts Richmond Road, an arterial road under the care, control and management of the CoH and Chambers Avenue, a local road under the control of Council. Richmond Road is identified as a Freight Route under DIT's 'A Functional Hierarchy for South Australia's Land Transport Network'. Richmond Road carries approximately 18,600 vehicles per day (6.5% commercial vehicles) and has a posted speed limit of 60km/h.

The site is currently comprises a dwelling with an existing access to Richmond Road at the south eastern corner of the site and an existing access on Chambers Avenue at the north western corner of the site.

The subject proposal comprises a commercial development with the main car park being located adjacent to Richmond Road. A small car park and loading area is located to the rear of the development. Access to the Richmond Road car park is proposed via a single ingress only access on Richmond Road and an egress only access on Chambers Avenue.

A Traffic Impact Assessment (TIA) has been undertaken by Stantec identifying that the development is expected to generate 17 vehicles per hour and 175 vehicles per day which can be accommodated by the surrounding road network. A turn path assessment has been undertaken demonstrating that vehicles can safely enter and exit the proposed development.

The Richmond Road access is located between a staggered T-junction which has back to back right turn lanes serving both Chambers Avenue and Aldridge Terrace. This TIA notes that this is the only location along the Richmond Road frontage that achieves the necessary

#18788849

separation from the adjacent junctions. The Chambers Avenue access is located in close proximity to the Richmond Road/Chambers Avenue junction and is non-compliant with Figure 3.1 in AS/NZS2890.1:2004.

It is understood that the access arrangements to the Richmond Road car park are largely as a result of the development footprint and adjacent junction locations. However, the access arrangements are unusual and may not result in ideal circulation through the car park. Having the Richmond Road access at the existing location adjacent the eastern property boundary, although non-compliant with the Australian Standard, may have provided an improved flow. However, given the development layout, the proposed location is acceptable.

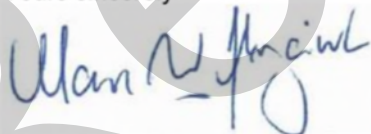
With respect to the Chambers Avenue access to the Richmond Road car park, it is recommended that this be located slightly further away from the junction to achieve the objective of Figure 3.1 of AS/NZS2890.1:2004 and made two-way to improve the operation of the car park. This arrangement will avoid the potential for vehicles approaching from the east to enter the site via the egress or being required to undertake a combined u-turn and left turn manoeuvre to access the site. It is understood that this will require modification to the car parking adjacent to Chambers Avenue.

ADVICE

DIT supports the proposed development and directs the planning authority to attach the following conditions to any approval:

1. Vehicular access to serve the site via Richmond Road shall be limited to an ingress only access located in accordance with the Site Plan by Olympic Developments, reference DA 2A, dated March 2022. The access point shall be angled at 70 degrees to the road and suitably signed and line-marked to reinforce the desired traffic flow.
2. All vehicles shall enter and exit the site in a forward direction and all vehicle manoeuvring areas shall be kept clear of impediments to manoeuvring.
3. The redundant crossover on Richmond Road shall be closed and reinstated to Council's kerb and gutter standards at the applicant's expense.
4. Stormwater run-off shall be collected on-site and discharged without impacting the safety and integrity of the adjacent road network. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

Yours sincerely



/MANAGER, TRANSPORT ASSESSMENT
for COMMISSIONER OF HIGHWAYS

#18788849

From: Frank Siow
Sent: Thursday, 2 February 2023 4:20 PM
To: Phil Smith
Cc: Joseph Ielasi ; Richard Tan
Subject: 239-243 Richmond Road Richmond - Development ID 21028599 - Review of the revised plans

Hi Phil,

I refer to our discussions concerning the above appeal matter. You have asked for my opinions about the proposed access points and car park layout as shown in the revised plans (more particularly Drawing DA 2A dated March 2022).

The above drawing shows an ingress-only access point from Richmond Road into the proposed front car park of the development, with an egress-only access point to Chambers Avenue. At the rear of the proposed building, a car park with two-way access to Chambers Avenue is proposed.

I have previously provided traffic assessment advice for the above development on 24/1/2022. At the time, the front car park of the development was proposed to have a two-way access point on Richmond Road (opposite Aldridge Road), a two-way access point on Chambers Avenue and the rear car park was proposed to have a two-way access point on Chambers Avenue. At the time of my assessment, I expressed a number of concerns regarding the proposed parking layout, the location of the Richmond Road access point opposite Aldridge Road and the lack of information regarding the service vehicles that would access the rear of the proposed tenancies.

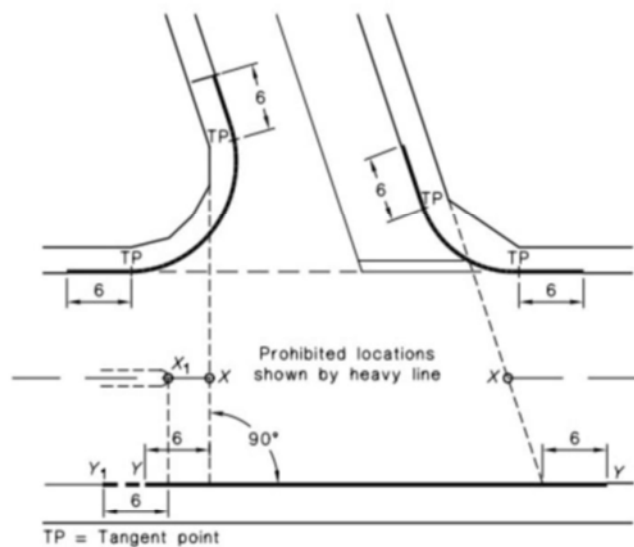
Since my assessment on 24/1/2022, I note that Stantec traffic consultants was engaged by the Applicant to address the concerns raised by the Department for Infrastructure and Transport (DIT). I note that, based on Stantec's assessment of the proposal (see letter 7/4/2022), having regard to the Major Urban Transport Routes Overlay DTS/DPF 4.1, the proposed access points for the front car park were subsequently amended to provide an ingress-only access point on Richmond Road and an egress-only access point on Chambers Avenue. I understand that the revised plans were also forwarded to DIT as part of the referral process.

On 24/6/2022, DIT provided their response to the referral. In conclusion, DIT accepted the proposed ingress-only access point on Richmond Road for the front car park of the development with a direction to attached 4 conditions to any approval for the site (see DIT letter 24/6/2022).

In that same letter of 24/6/2022, DIT also commented that the proposed egress-only access point for the front car park on Chambers Avenue should be made two-way and its location adjusted to meet the 6m separation distance measured from the tangent point of the kerb with specific reference to Figure 3.1 of A/NZS 2890.1:2004 (see excerpt below).

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AS/NZS 2890.1:2004



NOTES:

1. Accesses to domestic driveways are excluded from the prohibition in respect of the kerb section marked Y-Y (see Clause 3.2.3(a)).
2. The points marked X_1 and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension Y-Y extends to Point Y_1 .

DIMENSIONS IN METRES

FIGURE 3.1 PROHIBITED LOCATIONS OF ACCESS DRIVEWAYS

1.0 Proposed access points for the front car park

I provide the following opinions regarding the proposed access points for the front car park:

1. **Proposed ingress-only access point on Richmond Road** - Having regard to the Stantec assessment, DIT's acceptance of the ingress-only arrangement and the requirements of DTS/DPF 4.1 of the Major Urban Transport Routes Overlay, I consider the proposed ingress-only arrangement to be acceptable.
2. **Proposed egress-only access point on Chambers Avenue** – Having regard to the Stantec assessment, DIT's alternative suggestion to provide a two-way access point at this location and Figure 3.1 of AS/NZS 2890.1:2004, on balance, I consider the proposed egress-only arrangement to be acceptable. Instead of the egress driveway being located centrally as shown in the revised drawing, I recommend that the alignment be shifted slightly northwards so that the driveway alignment is in line with Space 20, so as to maximise the separation distance from the tangent point, albeit the 6m separation requirement would still not be satisfied.

While I have noted the DIT suggestion to provide a two-way access point on Chambers Avenue to facilitate entry movements from the east direction, I am concerned that an entering vehicle from Chambers Avenue that is delayed from entering the car park may have a detrimental impact on turning traffic at the junction of Chambers Avenue/Richmond Road. The proposed land uses are not significant traffic generators. I take the view that customers from the east direction should be able to find alternative routes to the site without too much difficulty in order to use the ingress-only access point on Richmond Road.

2.0 Proposed rear car park

My comments about the revised drawing, with respect to the rear car park layout, are as follows:

1. I note that the SRV truck (reference AS 2890.2:2018) is the nominated service vehicle for the 3 proposed warehouse tenancies. A condition restricting service vehicles to SRV size should be included in any approval for the site.
2. As shown in the Stantec report dated 7/4/2022, and more particularly as shown in Stantec's Figures 4 and 5 of their report, the SRV turnaround for Warehouse 1 would occur inside the building. The SRV for Warehouse 1 would not be able to turnaround within the car park aisleway given the spatial constraints. The area shown inside the building required for the turnaround manoeuvres would require this area to be kept clear (not used for storage etc). A condition requiring the turnaround area inside the warehouse to be kept clear for vehicle manoeuvres only should be included in any approval for the site.
3. Given the infrequent nature of waste collection, I have previously accepted that a waste collection truck, equivalent to the size of an MRV truck (reference AS 2890.2:2018), would be able to reverse in from Chambers Avenue. A condition requiring servicing of the waste bins to outside of business hours should be included in any approval for the site.
4. I have previously advised that at the exit point of the car park to Chambers Avenue, the pedestrian sight line requirement of AS/NZS 2890.1:2004 is not met, due to the building wall of Warehouse 3. I note that this issue has not been addressed in the revised plan.

3.0 Other matters

I do not have concerns with the parking provision for the development. I also do not have concerns with the traffic impact of the development on the adjacent road network.

Kind regards,

Frank Siow
Principal Consultant

Frank Siow & Associates



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