

CITY OF WEST TORRENS



Notice of Panel Meeting

Notice is Hereby Given that a Meeting of the
COUNCIL ASSESSMENT PANEL

will be held in the George Robertson Room, Civic Centre
165 Sir Donald Bradman Drive, Hilton

on

TUESDAY, 14 JUNE 2022
at 5.00pm

Public access to the meeting will also be available via livestream at:
www.westtorrens.sa.gov.au/livestream

CAP member, applicant and representor attendance via livestream only available by prior arrangement with the Assessment Manager.

Hannah Bateman
Assessment Manager

City of West Torrens Disclaimer

Council Assessment Panel

Please note that the contents of this Council Assessment Panel Agenda have yet to be considered and deliberated by the Council Assessment Panel therefore the recommendations may be adjusted or changed by the Council Assessment Panel in the process of making the formal Council Assessment Panel decision.

Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.

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1 MEETING OPENED**1.1 Acknowledgement of Country****1.2 Evacuation Procedures****1.3 Electronic Platform Meeting****2 PRESENT****3 APOLOGIES****4 CONFIRMATION OF MINUTES****RECOMMENDATION**

That the Minutes of the meeting of the Council Assessment Panel held on 10 May 2022 be confirmed as a true and correct record.

5 DISCLOSURE STATEMENTS

In accordance with section 7 of the *Assessment Panel Members – Code of Conduct* the following information should be considered by Council Assessment Panel members prior to a meeting:

A member of a Council Assessment Panel who has a direct or indirect personal or pecuniary interest in a matter before the Council Assessment Panel (other than an indirect interest that exists in common with a substantial class of persons) –

- a. must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the panel; and
- b. must not take part in any hearings conducted by the panel, or in any deliberations or decision of the panel, on the matter and must be absent from the meeting when any deliberations are taking place or decision is being made.

If an interest has been declared by any member of the panel, the Assessment Manager will record the nature of the interest in the minutes of meeting.

6 REPORTS OF THE ASSESSMENT MANAGER

6.1 TRANSITIONAL APPLICATIONS

Nil

6.2 PDI ACT APPLICATIONS

6.2.1 90-94 Richmond Road & 1 & 3 Farnham Road, KESWICK

Application No 21039722

Appearing before the Panel will be:

Representors: **Alex Boehm** of 22/40 Chatham Road, Keswick wishes to appear in support of his representation.

Rebecca Ristic of 7 Farnham Road, Keswick wished to appear in support of her representation via an electronic platform

Applicant: **Greg Vincent and Daniel McKenna** on behalf of Smart Property Solutions wishes to appear in response to the representations.

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT NUMBER	21039722
APPLICANT	Smart Property Solutions
ADDRESS	90-94 Richmond Road & 1 & 3 Farnham Road, KESWICK
NATURE OF DEVELOPMENT	Alterations and extension to existing carpark (including fencing) in association with an existing shop (bulky goods outlet), canopies (retrospective) and replacement of air conditioning units
ZONING INFORMATION	<p>Zones</p> <ul style="list-style-type: none"> • Employment Zone (90- 94 Richmond Road) • Housing Diversity Neighbourhood (1 & 3 Farnham Road) <p>Overlays</p> <ul style="list-style-type: none"> • Airport Building Heights (Regulated) (All structures over 15 metres) • Affordable Housing • Building Near Airfields • Future Road Widening • Hazards (Flooding) • Hazards (Flooding - Evidence Required) • Prescribed Wells Area • Regulated and Significant Tree • Stormwater Management • Traffic Generating Development • Urban Tree Canopy • Water Resources

	Technical Numeric Variations (TNVs) <ul style="list-style-type: none"> Maximum Building Height - 3 levels & 12.5 metres (1 & 3 Farnham Road); 2 levels & 8.5 metres (94 Richmond Road)
LODGEMENT DATE	18 January 2022
RELEVANT AUTHORITY	Council Assessment Panel
PLANNING & DESIGN CODE VERSION	Version 2021.17; 16 December 2021
CATEGORY OF DEVELOPMENT	Code Assessed - Performance Assessed
NOTIFICATION	Yes
REFERRALS STATUTORY	Nil
REFERRALS NON-STATUTORY	City Assets
RECOMMENDING OFFICER	Zoe Delmenico
RECOMMENDATION	Support with reserved matters & conditions

SUBJECT LAND AND LOCALITY

The subject land comprises four (4) contiguous allotments that are commonly known as 90-94 Richmond Road (2 allotments), 1 Farnham Road and 3 Farnham Road Keswick. Together, the land is regular in shape with a frontage to Richmond Road of 36.98 metres, a frontage to Farnham Road of 76.2 metres, resulting in an overall site area of 3037m².

The portion of the site comprising 90-94 Richmond Road, located on the corner of Richmond Road and Farnham Road, contains an existing shop (bulky goods outlet) currently tenanted by Snowys Outdoors, which has been a long term tenant of the site. An external on-site storage / service area and designated on-street loading area exist to the side of the existing building along Farnham Road.

The current operating hours of Snowys Outdoors are:

- Monday to Friday 8.30am to 5pm
- Saturday 9am to 5pm
- Sunday 11am to 5pm

The existing carpark is located at No. 1 Farnham Road and is provided with direct vehicle access from Farnham Road, and currently accommodates seventeen (17) car parking spaces with unauthorised waste storage and collection areas in the north-east corner. Canopies (unauthorised) defining the rear entrance to Snowys Outdoors and a storage area are also existing adjacent the existing carpark to the rear of the existing building.

The portion of the carpark proposed to be extended is located at No. 3 Farnham Road and has a frontage width to Farnham Road of 15.24 metres and an overall area of 697m². This site currently contains a single storey detached dwelling and outbuilding located in the north-eastern corner of the site. A single width crossover is located at the northern end of the site, whilst a semi-mature street tree is located centrally within the verge.

Richmond Road contains a range of commercial uses including the RAA complex and Trans Adelaide Bus Depot (northern side) and a service trade premises, motor repair station, offices and vehicle repair centre in addition to Snowys Outdoors (the subject site) on the southern side.

The amenity of the locality is moderate to the northern end of Farnham Road due to service areas, car parking, loading areas and vehicle access to a vehicle repair centre all presenting to Farnham Road. As is common throughout the broader locality, car parking, vehicle access and loading areas to support existing long term commercial uses fronting Richmond Road are located on side streets adjacent to the residential interface.

Some of the surrounding residential streets, such as Farnham Road, are affected by overflow parking from existing commercial uses, which has resulted in the introduction of on-street parking controls over time of which impacts the amenity of the area.

The character of Farnham Road to the south is of low to medium density residential development comprising detached dwellings, group dwellings and residential flat buildings. Overflow on-street car parking currently extends into the existing residential area that is located in the Housing Diversity Neighbourhood Zone, where future anticipated medium density housing is sought.

The site is affected by flooding, with the Keswick/Brown Hill Creek drainage channel being located within the eastern boundary of the carpark portion of the site.

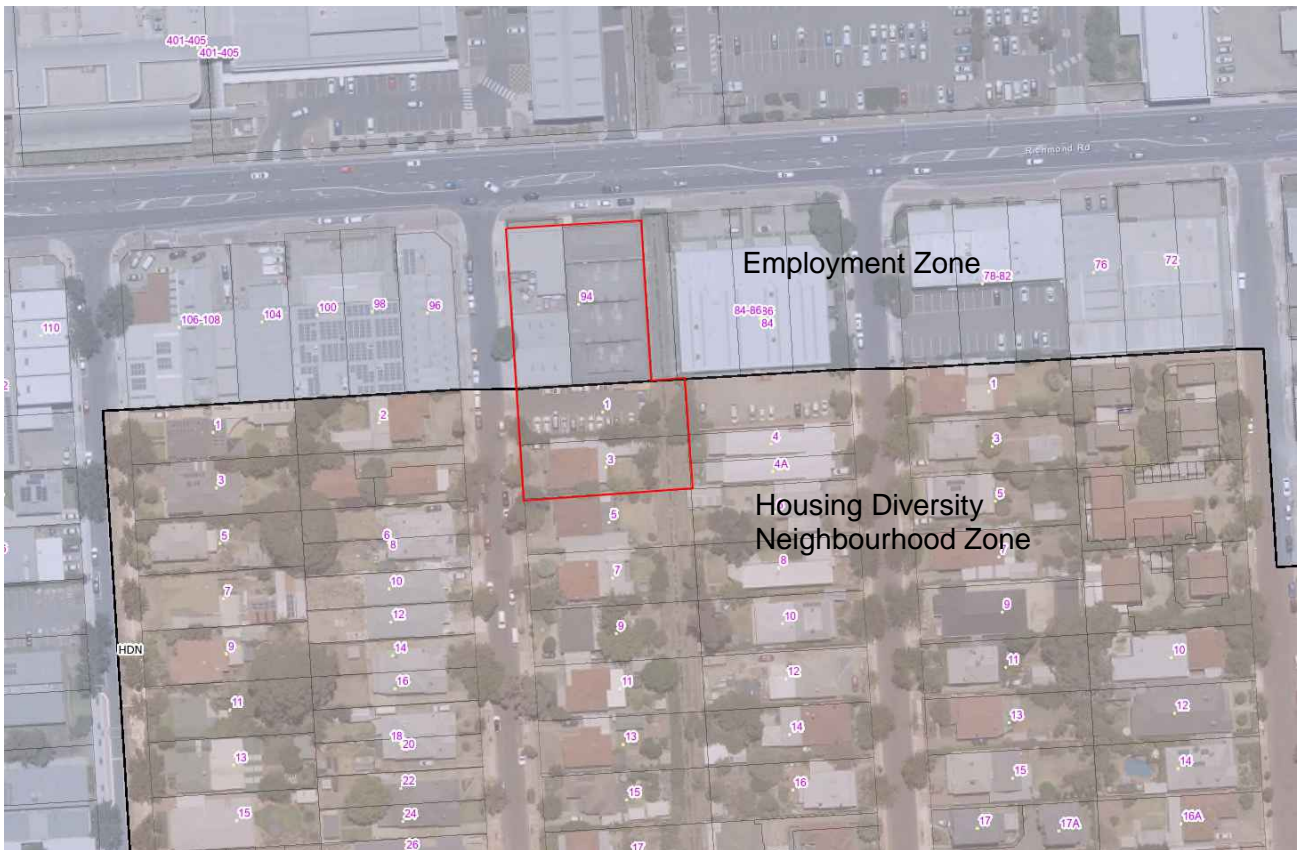
There are no significant or regulated trees located on, or adjoining, the site.

There are no easements, encumbrances or Land Management Agreements on the relevant Certificates of Title.

Photographs of the subject site and locality are contained in **Attachment 1**.

The subject land and locality are shown on the aerial imagery and maps below.





RELEVANT APPLICATIONS

DA Number	Description of Development	Decision	Decision Date
211/588/2007	Demolition of existing dwelling	Approved	29 June 2007
211/1431/2006	Carpark associated with adjacent Retail Showroom to the north	Approved	28 September 2007
211/143/1991	Alterations to retail showroom, office & warehouse	Approved	4 June 1991
211/299/1990	Extend office/ showroom / warehouse	Approved	4 December 1990

PROPOSAL

It is proposed to demolish all buildings at No. 3 Farnham Road, reconfigure the existing carpark at No. 1 Farnham Road and extend the carpark on to No. 3 Farnham Road along with the provision of perimeter landscaping.

The additional on-site parking is proposed as it has become evident to the operator of the site that the existing provision of seventeen (17) parking spaces is not sufficient to cater for existing staff and visitors to the site, of whom are often forced to park off site, creating overflow on to the local street network, including in the immediate locality of Farnham Road. The expansion of the car park would enable the site to cater for thirty-two (32) on site car parking spaces, by providing an additional fifteen (15) spaces on the site including one (1) accessible space.

As part of the proposal the applicant also initially sought to use part of the carpark for the storage of waste and undertake waste collection activities. This is currently being undertaken on the site but is unauthorised, not forming part of the previous car park approval. The storage and collection of waste has been removed from the proposal to minimise off-site amenity impacts, in particular noise to the residential interface. Waste management will continue as historically approved, being undertaken from the service yard and loading area located further towards Richmond Road, accessed from Farnham Road.

Minor works are also proposed in relation to the existing Snowys Outdoors building, including the addition of canopies located at the rear entrance, adjacent the existing carpark. It was identified during the assessment these works were unauthorised. The applicant is seeking their formalisation as part of the assessment of the application in addition to the replacement of existing air conditioning units, proposed to be located centrally on the easternmost building on the site (8.8 metres from the site's eastern boundary). The overall height of the replacement air conditioning will be 8.48 metres, whilst the overall height of the existing canopies is 3.6 metres.

New 2.1m and 1.9m high good neighbour fences are proposed to be constructed along the site's eastern boundary with the creek and southern boundary of the site respectively. The existing tubular fence will be extended across the frontage of the proposed car park and provided with gates for security and to ensure the carpark is not used during evening hours. The existing tubular fence adjacent the creek is proposed to be retained.

Landscaping will be provided to the site's boundaries and will comprise a mix of small to medium canopy trees and large and small shrubs.

There is no additional lighting proposed as a result of the extension of the carpark.

For the purposes of an assessment the proposal is broken down into elements. Each element will have an assessment pathway as set out in the Planning and Design Code

Elements	Application Category
Shop (bulky goods outlet) - Bulky good outlet is excluded from being a restricted form of development in the Employment Zone, including canopies and air conditioning.	Performance Assessed
Carpark - Given the shop (bulky goods outlet) has been an longstanding existing use exceeding 1,000m ² and its floor area is not proposed to be extended within the Housing Diversity Neighbourhood Zone, the extension of the proposed carpark ancillary to the shop is considered to be an element in its own right, defaulting to a performance assessed form of development.	Performance Assessed
Fence	Performance Assessed

The relevant plans and documents are contained in **Attachment 2**

PUBLIC NOTIFICATION

The application required public notification because it was performance assessed and not exempt from notification by *Table 5 - Procedural Matters* of the Housing Diversity Neighbourhood Zone in the Planning and Design Code (The Code).

Properties notified	73 properties were notified during the public notification process.
Representations	Four (4) representations were received from three (3) representors.
Persons wishing to be heard	<ul style="list-style-type: none"> • Alex Boehm of 22/40 Chatham Road Keswick • Rebecca Ristic of 7 Farnham Road Keswick
Summary of representations	<p>Concerns were raised regarding the following matters:</p> <ul style="list-style-type: none"> • Noise impacts to 4 & 4A Chatham Road Keswick. Request for boundary fence between No. 3 Farnham Road and the creek to be of solid construction. • Carpark use is inappropriate and No. 3 Farnham Road should be retained for residential land uses to support residential densification given location of site and Housing Diversity Neighbourhood Zone intent of providing medium density housing. • Residential land use should be maintained at No. 3 Farnham Road to maintain aesthetic value and street appeal • Additional car park and access increases traffic risk to Farnham Road and pedestrians • The extension of the carpark is not required as the existing Snowys Outdoors carpark is not to capacity and overflow car parking in Farnham Road is from Snowys Outdoors staff and other businesses in the locality
Applicant's response to representations	<p>Summary of applicant's response:</p> <ul style="list-style-type: none"> • A new 2.1m high fence and landscaping is proposed between No. 3 Farnham Road and the creek as per the representor's request to minimise noise impacts. • The Planning & Design Code is the relevant tool for development assessment of the application. The extension of the carpark supports a land use specifically envisaged in the Housing Diversity Neighbourhood Zone. • The alleviation of on-street overflow parking will improve residential amenity in accordance with the intent of the Housing Diversity Neighbourhood Zone. • Both staff and customers will utilise the extended car parking area. • Snowys Outdoors has limited ability to further intensify their operations and the proposed parking spaces will simply satisfy the parking demands being experienced by the existing business. • The proposed carpark is a positive response to an identified issue that is negatively impacting the existing Farnham Road streetscape amenity.

A copy of the representations and the applicant's response is contained in **Attachment 3**.

Following public notification, the applicant provided amended plans that included alterations to the existing building, including existing canopies located at the rear of the existing building and accessed from the existing carpark which are currently unauthorised, and replacing air conditioning units.

The amended application was not required to be publically notified as the external alterations to the existing building did not trigger public notification in their own right. More specifically, the proposed works met the listed exceptions specified in Table 5 of the Employment Zone and Housing Diversity Neighbourhood Zone, including not exceeding the maximum building heights specified of 12.5 metres / 3 levels and 8.5 metres / 2 levels respectively.

INTERNAL REFERRALS

The application was referred to Council's City Assets department who advised of no objection to the proposal subject to specified matters being addressed. The plans being considered by the CAP have satisfactorily addressed all matters previously raised in relevant referral responses relating to the watercourse, likely flood impacts, verge interaction, and parking and traffic requirements.

The applicant's engineer has advised the carpark construction and vehicle traversal within the car park will not adversely impact the creek banks, as existing ground levels are not being altered and therefore no earth retaining structures are required. Similarly, the proposed new fence and pad footings will not impact on the existing drain construction. Council's City Assets engineer is satisfied on this basis that retaining walls are not required.

A copy of the relevant referral responses are attached in **Attachment 4**.

EXTERNAL REFERRALS

Nil

RELEVANT PLANNING & DESIGN CODE PROVISIONS

The portion of the site comprising No. 90 - 94 Richmond Road is located within the Employment Zone whilst No.'s 1 & 3 Farnham Road are located within the Housing Diversity Neighbourhood Zone as described in the Code. The subject land is also affected a series of Overlays and Technical Numeric Variations (TNVs) including the Hazards (Flooding) Overlay.

ASSESSMENT

In assessing the merits or otherwise of the application, only those quantitative requirements that do not meet the Code requirements will be discussed along with the respective qualitative provisions. The proposed development is therefore discussed under the following sub headings:

Land Use and Intensity

Although a carpark is not expressly envisaged in the Housing Diversity Neighbourhood Zone, a shop is envisaged providing it is consistent with Housing Diversity Neighbourhood Zone PO 1.2, which states that '*Commercial Activities improve community access to services are of a scale and type to maintain residential amenity.*'

The application represents an incursion of a commercial land use into the Housing Diversity Neighbourhood Zone, albeit in a support capacity to an existing use (bulky goods outlet) located in the Employment Zone to the north.

Whilst a car park is not expressly envisaged in the Housing Diversity Neighbourhood Zone, it is also not expressly discouraged providing it maintains residential amenity.

In this instance, it is recognised that the existing Snowys Outdoors parking demand being generated is greater than what it can currently accommodate in its existing carpark for both staff and customers. This is evident from the extent of overflow parking in Farnham Road.

The amenity of the residential area along Farnham Road is currently reduced due to the impact of overflow parking from Snowys Outdoors (and other existing longstanding commercial uses in the locality). It is considered that the transference of on-street parking to vehicles being parked on site as a result of the alterations and extension of the carpark would positively contribute to improved amenity within Farnham Road.

Additionally, vehicles would enter the carpark at the northern end of Farnham Road, rather than being directed adjacent to, or beyond the site to find a car park further to the south, which may result in further amenity improvements to the residential section of Farnham Road through a reduction in traffic movements.

The scale of the extension of the carpark is considered small in context, in that it does not involve the construction of buildings that would be highly visible in the streetscape.

The submitted plans indicate a reasonable level of landscaping (including canopy trees and shrubs) located in garden beds of a minimum of 1 metre (adjacent to, and consistent with, the existing carpark at No. 1 Farnham Road) and up to 2.8 metres along the frontage of the site to the south. Holistically, with the landscaping provided in the north-west corner of the existing carpark and residential interfaces to the south and east, this will ensure the car park will not display a high level of visual prominence in the streetscape.

On this basis, it is considered the proposed carpark addition will not be highly visible and will not adversely affect the residential character or amenity of the existing residential area.

It is considered the proposed alterations and extension to the existing carpark will not reduce the overall capacity of the Housing Diversity Neighbourhood Zone to the extent that it will limit its ability to deliver densification of dwellings as a whole.

It is also considered unlikely future expansion of the carpark or Snowys Outdoors would be required beyond that sought in this application.

In order to ensure the carpark is used in association with the existing bulky goods outlet, it is considered appropriate, should the Panel be minded to support the application, for the land owner to enter into a Land Management Agreement (LMA) to reflect this arrangement. The applicant has agreed to this, however as the agreement has not been finalised to date, it is considered appropriate to require this to be prepared and registered on title as a reserved matter.

Hazards (Flooding) Overlay

The proposed carpark and fencing has been designed to ensure they minimise exposure of people and property to unacceptable flood risk, doesn't impede the flow of flood waters or cause unacceptable risks to adjoining properties.

The carpark is provided with a stormwater system that includes adequate on-site stormwater detention as reviewed appropriate by Council's City Assets Development Engineer. Council's City Assets Engineer is also satisfied flood flow corridors are maintained through the pervious nature of the existing tubular fencing located at the rear of the site of the existing carpark and new tubular fencing proposed at the front of the site.

The location of proposed fencing is consistent with the existing conditions of the site with a solid fence located adjacent to the creek and southern property boundary.

Design in Urban Areas

Soft landscaping (including tree planting) has been included in the carpark design in accordance with PO 3.1 in order to:

- (a) minimise heat absorption and reflection
- (b) maximise shade and shelter
- (c) maximise stormwater infiltration
- (d) enhance the appearance of land and streetscapes.

The proposed vehicle parking areas have been appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through being attractively landscaped (including small and large shrubs and canopy trees in suitably sized landscape beds) and provided with 2.1 metre and 1.9 metre screen fencing to the east and south respectively.

The building works proposed in relation to the existing Snowys Outdoors building, including the canopies located at the rear entrance, and replacement of existing air conditioning units, are considered to have limited impacts on the amenity of the area. These structures don't exceed the maximum heights specified in the Employment Zone of 8.5 metres and 12.5 metres in the Housing Diversity Neighbourhood Zone and are set back well within the site to ensure they are not highly visible in comparison to the existing building.

Interface between Land Uses

The Desired Outcome of this general policy is *'Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.'*

It is considered the alterations and additions to the carpark will not result in any adverse impacts to existing residential land uses in the locality.

The hours of operation of the proposed carpark are:

- Monday to Friday 8.30am to 5pm;
- Saturday 9am to 5pm and
- Sunday 11am to 5pm

And are within the range of hours for commercial activities nominated as being acceptable as specified in Interface between Land Uses DPF 2.1.

Given the proposed carpark will operate in accordance with these hours, and the carpark will accommodate passenger vehicles only, it is not considered that there will be any unreasonable noise impacts as a result of the proposed extension to the carpark.

Additionally, it is considered there won't be any adverse off-site amenity impacts through air conditioning noise given the existing air conditioning units exist in a similar location on the site, the substantial separation to the residential interface proposed, and no changes to the operating hours of the air conditioning system are proposed.

There is no additional lighting proposed as a result of the extension to the carpark. The hours of operation will not be during evening hours and as the car park will be secured at night, there is no requirement for additional illumination.

No waste storage/collection or deliveries will occur from the carpark, with these activities to continue to be undertaken as originally approved from the on-street loading area and on-site service area located further towards Richmond Road and away from the site's residential interface.

Access and Carparking

The proposed car park layout and access design has been assessed by Council's City Assets engineer as satisfying Australian Standard AS/NZS 2890.1 - 2004 - Off Street Parking - Car Parking Facilities. No additional vehicle access points are proposed as a result of the proposal, although existing access points will be suitably reconfigured.

The car park has been designed to ensure all vehicles can enter and exit the site conveniently and safely in a forward direction.

SUMMARY

Having considered all the relevant provisions of the Planning and Design Code, the proposal is considered to be not seriously at variance with the Planning and Design Code Version 2021.17 dated 16 December 2021.

The proposed alterations and extension to the existing carpark will not reduce the overall capacity of the Housing Diversity Neighbourhood Zone to the extent that it will limit its ability to deliver densification of dwellings.

The increase in on-site parking associated with Snowys Outdoors will be an improvement to the amenity of the locality through the reduction of on-street parking within Farnham Road.

The carpark addition is provided with appropriate landscaping and fencing to ensure it is not highly visible and does not adversely affect the residential character or amenity of the existing residential area or neighbouring properties.

On balance, the proposal reasonably satisfies the relevant provisions of the Planning and Design Code Version 2021.17 and therefore the application warrants the granting of Planning Consent, subject to specified conditions and reserved matters.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107 (2)(c) of the *Planning Development and Infrastructure Act 2016*, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code Version 2021.17.
2. Application No. 21039722 by Smart Property Group for alterations and extension to existing carpark (including fencing) in association with an existing shop (bulky goods outlet), canopies (retrospective) and replacement of air conditioning units at 90-94 Richmond Road & 1 & 3 Farnham Road Keswick (CT6135/26, 6135/27, 5231/962 & 5215/624) is GRANTED Planning Consent subject to the following reserved matter and conditions of consent:

Reserved Matters:

The following information shall be submitted for further assessment and approval by the relevant authority as reserved matters under Section 102(3) of the *Planning, Development and Infrastructure Act 2016*:

1. The applicant / owner must enter into and register a Land Management Agreement on CT 5215/624 (No. 3 Farnham Road Keswick) which associates the proposed carpark at No. 3 Farnham Road Keswick with the existing shop (bulky goods) retail use at No. 90 - 94 Richmond Road Keswick.
2. A Construction Environmental Management Plan (CEMP) shall be prepared and submitted to the relevant authority for endorsement. The plan shall provide for but not be limited to:
 - Management of vehicles within Farnham Road with respect to parking, loading and unloading of materials, contractors, sub-contractors and the like.
 - Alternative arrangements for the management of vehicles associated with staff and customers of Snowy's Outdoors during construction of the proposed carpark to ensure overflow parking on Farnham Road is reasonably minimised to the satisfaction of Council.

Pursuant to Section 127(1) of the *Planning, Development and Infrastructure Act 2016*, the Council reserves its decision on the form and substance of any further conditions of consent that it considers appropriate to impose in respect of the reserved matters outlined above.

Planning Consent Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this application, specifically plans as listed below except where varied by any condition(s).
 - The plans prepared by Studio Nine Architects including:
 - Snowys Outdoor Site plan / Carpark Plan Drawing No. 107-103-SK01 Revision G dated 27.05.22
 - Snowys Outdoor Roof Plan / Sections Drawing No. 907-103-SK02 Revision A
 - Snowys Outdoor Fence Boundary Elevation, Drawing No. 107-103-SK02, Revision F dated 25.05.22
 - The plans and documents prepared by TMK Consulting Engineers including:
 - Civil Plan Drawing No. 2103129-C1/E dated 25.05.22
 - Civil Details & Notes, Drawing No. 2103129 - C2/E dated 25.05.22

- Planning Report Carpark Expansion for Snowys Outdoors prepared by MasterPlan SA Pty Ltd dated December 2021
 - Response to Council Request for DA21039722 letter prepared by MasterPlan SA Pty Ltd dated 03.05.22.
 - Email from TMK engineers dated 25 May 2022 confirming retaining walls not required and no adverse impact to creek bed.
2. The hours of operation of the carpark shall not exceed those of the existing shop (bulky goods outlet) as follows:
 - Monday to Friday 8.30am to 5pm
 - Saturday 9am to 5pm
 - Sunday 11am to 5pm
 3. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
 - a) Result in the entry of water into a building; or
 - b) Affect the stability of a building; or
 - c) Create unhealthy or dangerous conditions on the site or within a building; or
 - d) Flow or discharge onto the land of an adjoining owner;
 - e) Flow across footpaths or public ways; or
 - f) Discharge to the adjacent creek.
 4. All proposed canopy trees nominated shall be at an advanced planting height of not less than 2 metres at time of planting. All landscaping shall be established in the landscaping areas nominated on the plans and provided with an automatic watering system, with all landscaping completed within 3 months of the commencement of the use of the car park. All landscaping shall be tended to and nurtured at all times, with any plantings that are dead or diseased being replaced to the reasonable satisfaction of the relevant authority.
 5. The car parking area shall be set out and line-marked in accordance with the approved plans and Australian Standard AS/NZS 2890.1 - 2004 - Off Street Parking - Car Parking Facilities. Appropriate directional signage shall be installed to reinforce the desired flow of traffic to, from and through the site.
 6. All landscaped areas shall be separated from adjacent driveways and parking areas by a suitable kerb or non-mountable device to prevent vehicle movement thereon, prior to occupation of the development.
 7. The driveway and car-parking areas are to be surfaced, drained and completed to the reasonable satisfaction of the relevant authority prior to the commencement of the use of the carpark.
 8. New or amended crossovers and the stormwater connections shall be constructed in accordance with Council's minimum technical standards.
 9. The car parking areas shall be maintained at all times to the reasonable satisfaction of the relevant authority.
 10. Driveways, carparking spaces, manoeuvring areas and landscaping areas shall not be used for storage or display of materials or goods including waste products and refuse.

Attachments

1. **Subject site and locality photographs**
2. **Relevant Plans and Documents**
3. **Representations and applicant response**
4. **Referral responses**



Figure 1: Existing carpark at No. 1 Farnham Road Keswick



Figure 2: Existing carpark at No. 1 Farnham Road Keswick

(Note: taken outside of normal operating hours)



Figure 3: Existing unapproved rear canopies in existing carpark



Figure 4: No. 3 Farnham Road Keswick (site of proposed carpark extension)

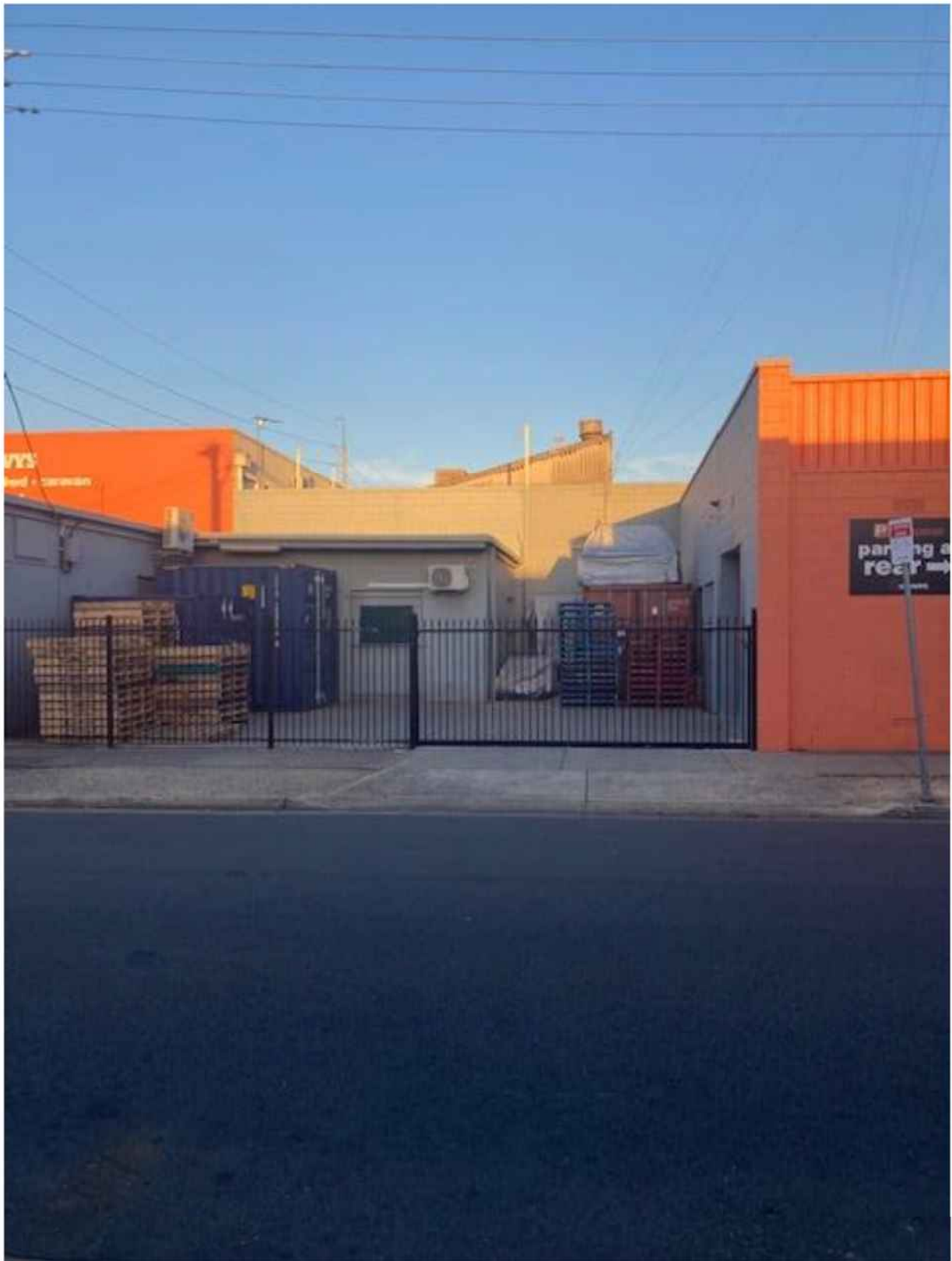


Figure 5: No. 90-94 Richmond Road existing outdoor storage / service area
(Farnham Road frontage)



Figure 6: Existing on-street loading area at 90-94 Richmond Road
(Farnham Road street frontage)



Figure 7: Farnham Road - southerly direction 9am weekday

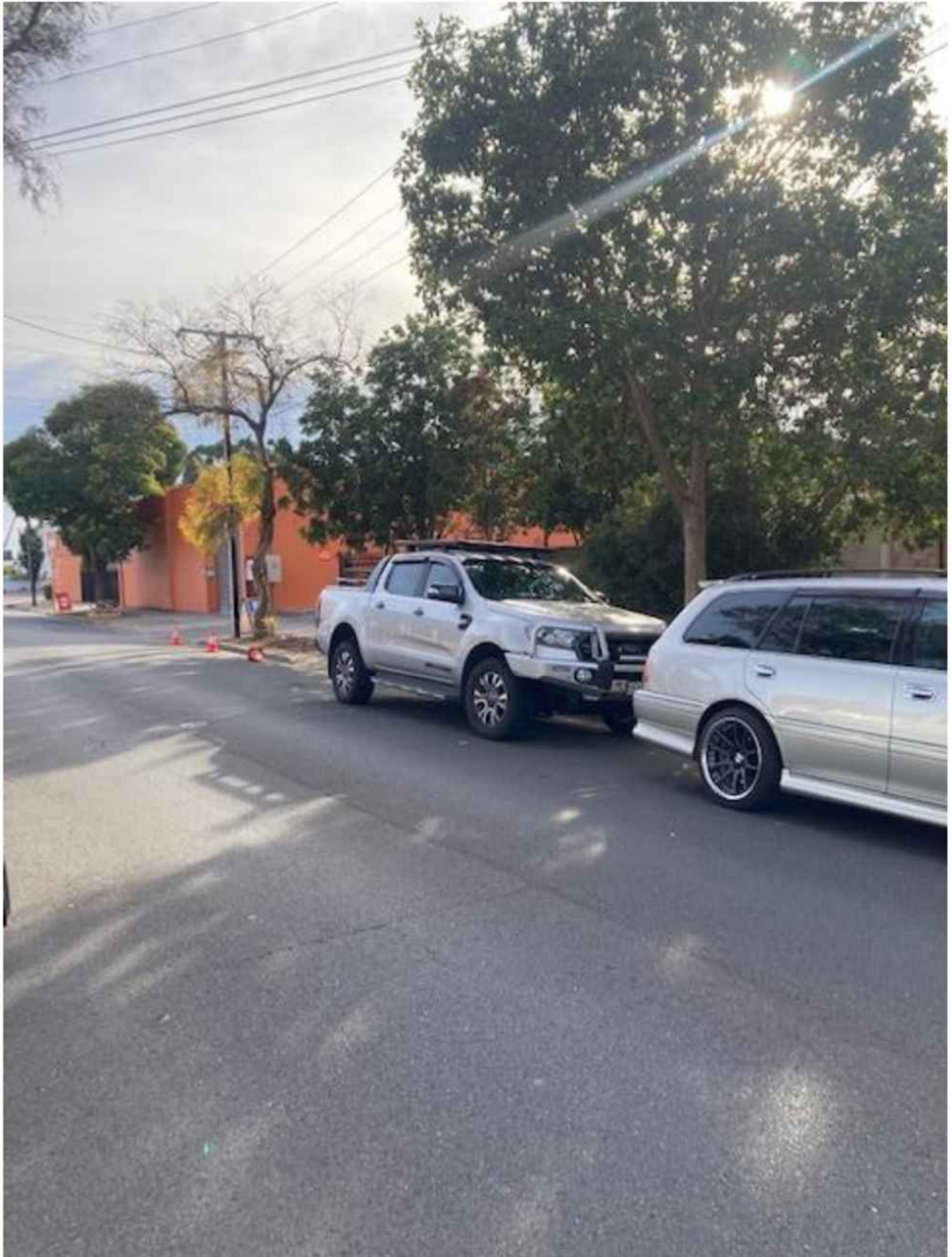


Figure 8: Farnham Road northerly direction



Figure 9: Interface between No.'s 3 & 5 Farnham Road



Product Register Search (CT 6135/26)
 Date/Time 25/05/2022 03:06PM
 Customer Reference 52318
 Order ID 20220525007591



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 6135 Folio 26

Parent Title(s) CT 5464/953
 Creating Dealing(s) DDA 12097077
 Title Issued 28/03/2014 Edition 2 Edition Issued 29/11/2015

Estate Type

FEE SIMPLE

Registered Proprietor

TRIPLE A RICHMOND RD PTY. LTD. (ACN: 115 534 195)
 OF CARE RDC PARTNERS LEVEL 1 193 WAKEFIELD STREET ADELAIDE SA 5000

Description of Land

ALLOTMENT 2 FILED PLAN 19511
 IN THE AREA NAMED KESWICK
 HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
10340041	MORTGAGE TO WESTPAC BANKING CORPORATION
12621830	LEASE TO SNOWY'S OUTDOORS PTY. LTD. (ACN: 079 380 180) COMMENCING ON 01/09/2015 AND EXPIRING ON 31/08/2022

Notations

Dealings Affecting Title NIL
 Priority Notices NIL
 Notations on Plan NIL

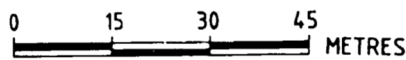
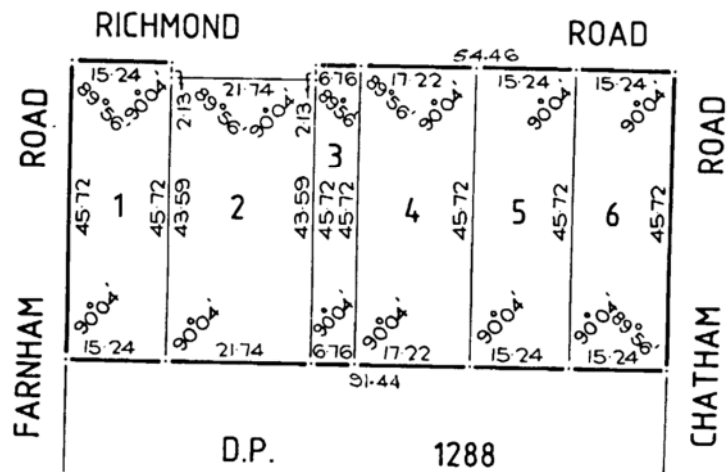
Registrar-General's Notes

NEW EDITION CREATED DUE TO EXPIRATION OF LEASE

Administrative Interests NIL



Product Register Search (CT 6135/26)
Date/Time 25/05/2022 03:06PM
Customer Reference 52318
Order ID 20220525007591





Product Register Search (CT 6135/27)
 Date/Time 25/05/2022 03:06PM
 Customer Reference 52318
 Order ID 20220525007597



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 6135 Folio 27

Parent Title(s) CT 5465/212
 Creating Dealing(s) DDA 12097077
 Title Issued 28/03/2014 Edition 2 Edition Issued 29/11/2015

Estate Type

FEE SIMPLE

Registered Proprietor

TRIPLE A RICHMOND RD PTY. LTD. (ACN: 115 534 195)
 OF CARE RDC PARTNERS LEVEL 1 193 WAKEFIELD STREET ADELAIDE SA 5000

Description of Land

ALLOTMENT 1 FILED PLAN 19511
 IN THE AREA NAMED KESWICK
 HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
10340041	MORTGAGE TO WESTPAC BANKING CORPORATION
12621830	LEASE TO SNOWY'S OUTDOORS PTY. LTD. (ACN: 079 380 180) COMMENCING ON 01/09/2015 AND EXPIRING ON 31/08/2022

Notations

Dealings Affecting Title NIL
 Priority Notices NIL
 Notations on Plan NIL

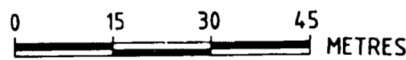
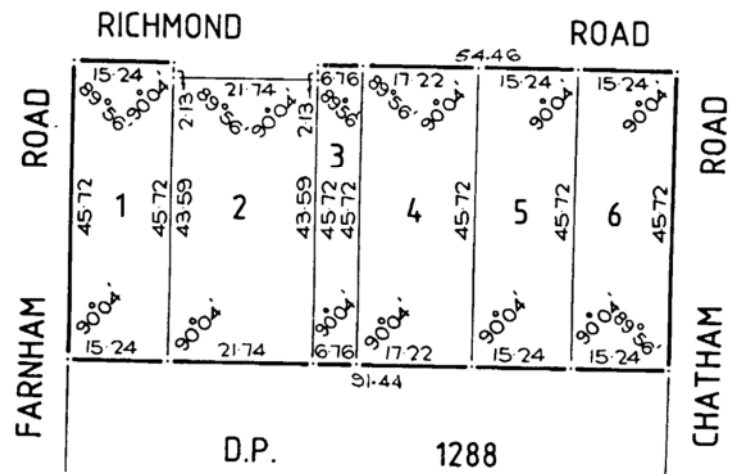
Registrar-General's Notes

NEW EDITION CREATED DUE TO EXPIRATION OF LEASE

Administrative Interests NIL



Product Register Search (CT 6135/27)
Date/Time 25/05/2022 03:06PM
Customer Reference 52318
Order ID 20220525007597



DocuSign Envelope ID: 7EEFAE6E-1392-4380-B3F5-4AA974DE0AEF



Product
Date/Time
Customer Reference
Order ID

Annexure A
 Register Search (CT 5215/624)
 12/07/2021 10:23AM
 20210712002264



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 5215 Folio 624

Parent Title(s) CT 1340/198
Creating Dealing(s) CONVERTED TITLE
Title Issued 20/09/1994 **Edition** 6 **Edition Issued** 25/07/2012
Diagram Reference 1340198

Estate Type

FEE SIMPLE

Registered Proprietor

WEN JIE LIAO
 YING XU
 OF 15 LAUREL AVENUE LINDEN PARK SA 5065
 AS JOINT TENANTS

Description of Land

ALLOTMENT 351 DEPOSITED PLAN 1288
 IN THE AREA NAMED KESWICK
 HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

Dealing Number	Description
11789380	MORTGAGE TO AUSTRALIA & NEW ZEALAND BANKING GROUP LTD.

Notations

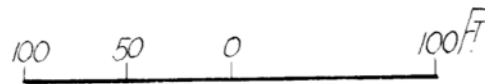
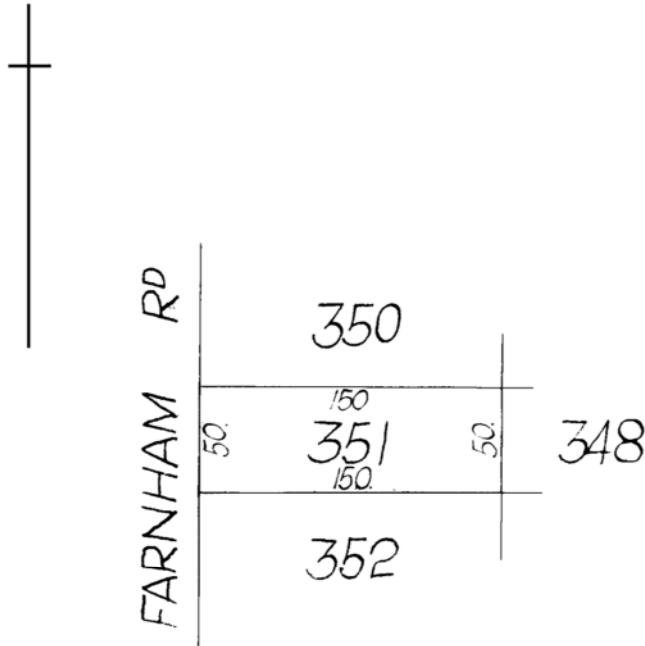
Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL

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Product
Date/Time
Customer Reference
Order ID

Register Search (CT 5215/624)
12/07/2021 10:23AM
20210712002264



DISTANCES ARE IN FEET AND INCHES	
FOR METRIC CONVERSION	
1 FOOT	= 0.3048 metres
1 INCH	= 0.0254 metres



Product	Register Search (CT 5231/962)
Date/Time	10/01/2019 03:30PM
Customer Reference	Keswick
Order ID	20190110009503
Cost	\$28.75



The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



Certificate of Title - Volume 5231 Folio 962

Parent Title(s)	CT 4260/819		
Creating Dealing(s)	CONVERTED TITLE		
Title Issued	01/12/1994	Edition	4
		Edition Issued	30/06/2006

Estate Type

FEE SIMPLE

Registered Proprietor

TRIPLE A RICHMOND ROAD PTY. LTD. (ACN: 115 534 195)
OF CARE RDC PARTNERS LEVEL 1 193 WAKEFIELD STREET ADELAIDE SA 5000

Description of Land

ALLOTMENT 350 DEPOSITED PLAN 1288
IN THE AREA NAMED KESWICK
HUNDRED OF ADELAIDE

Easements

NIL

Schedule of Dealings

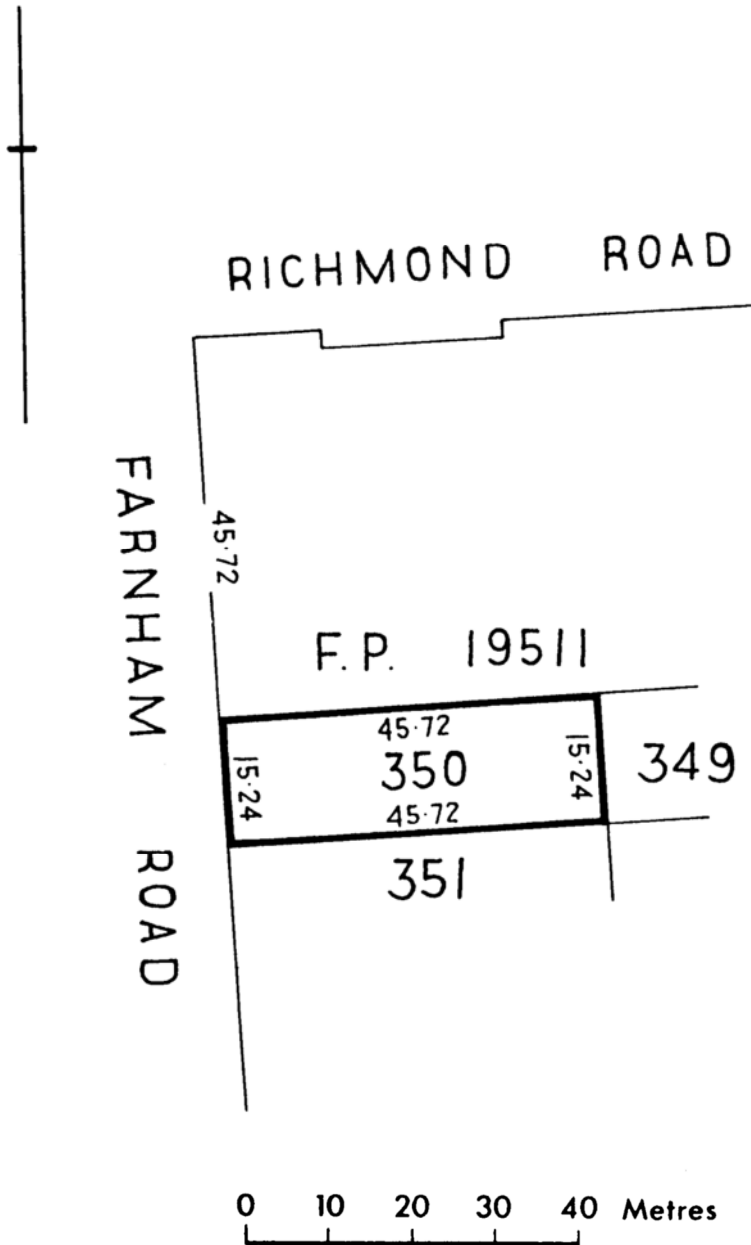
Dealing Number	Description
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12621831	LEASE TO SNOWY'S OUTDOORS PTY. LTD. (ACN: 079 380 180) COMMENCING ON 01/09/2015 AND EXPIRING ON 31/08/2022

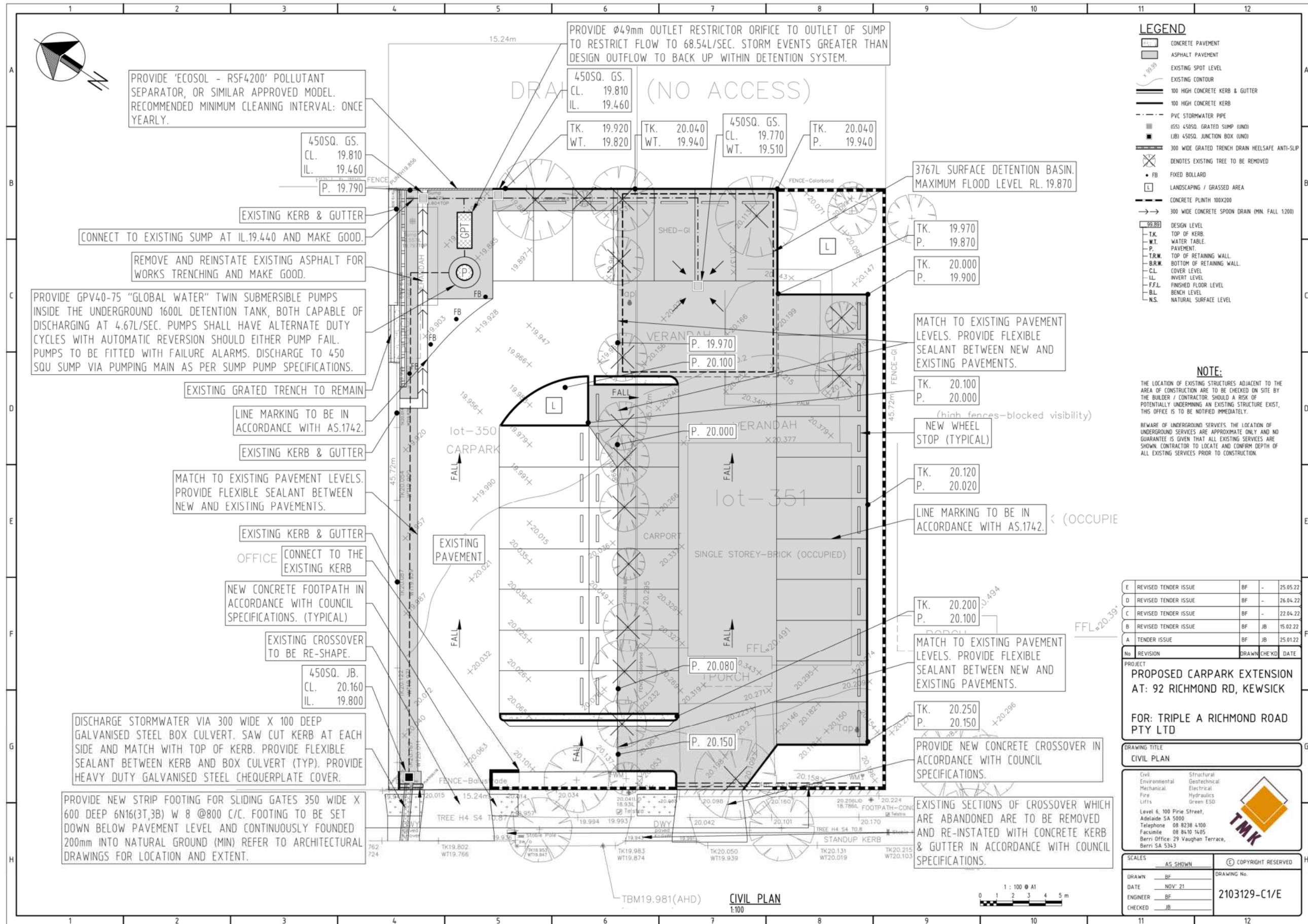
Notations

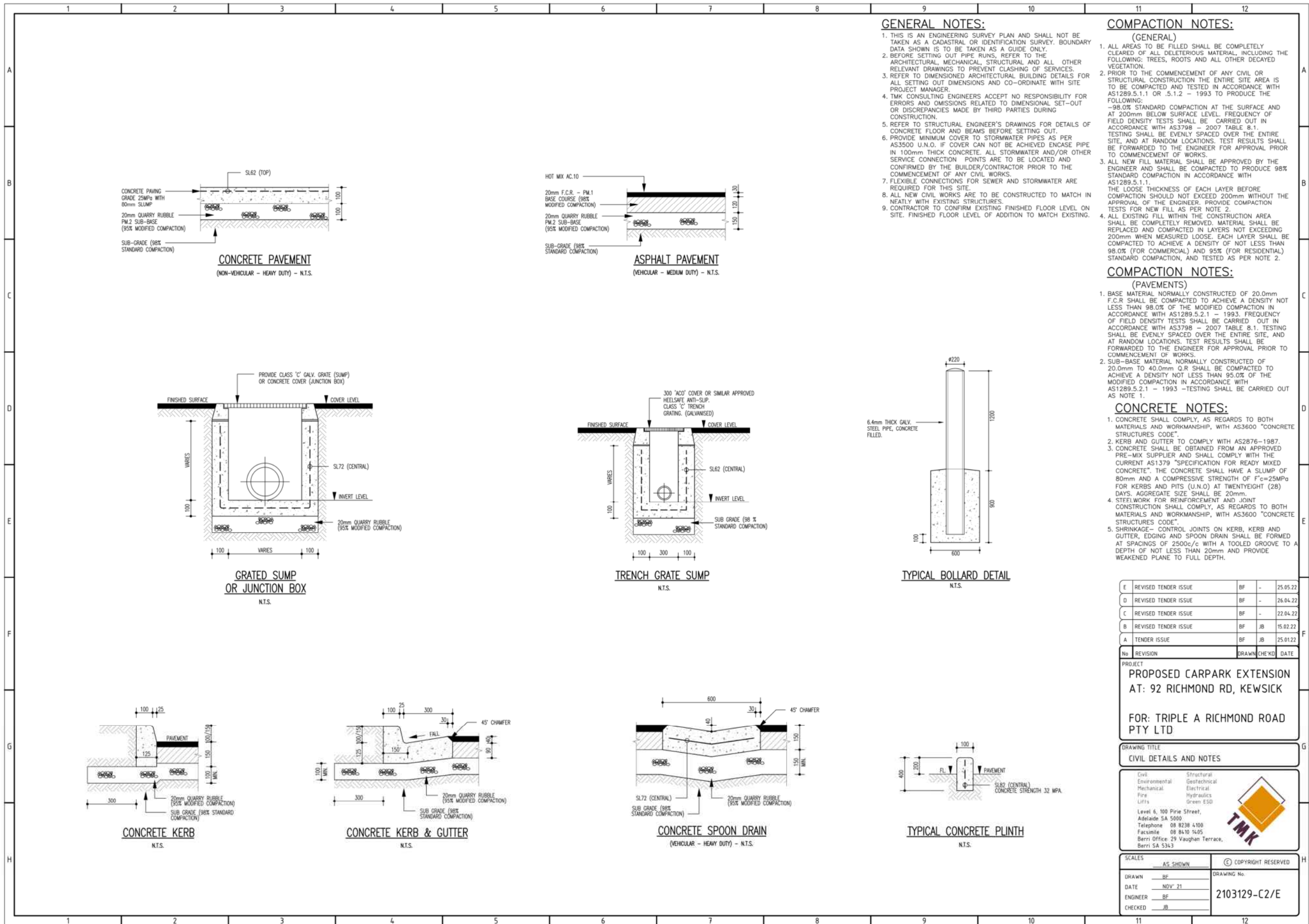
Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL



Product Register Search (CT 5231/962)
Date/Time 10/01/2019 03:30PM
Customer Reference Keswick
Order ID 20190110009503
Cost \$28.75







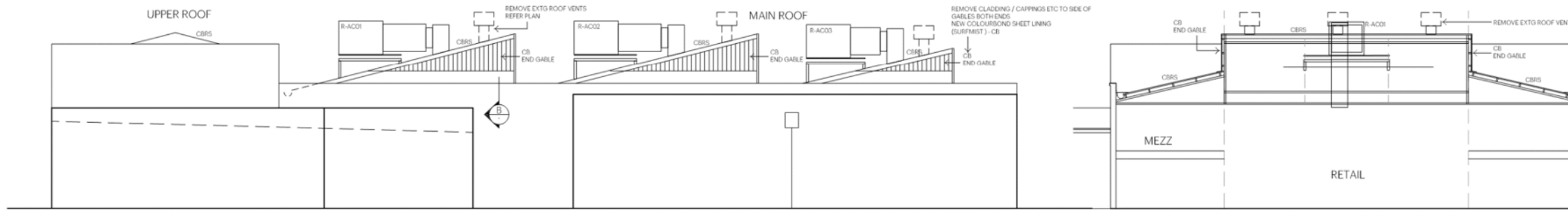


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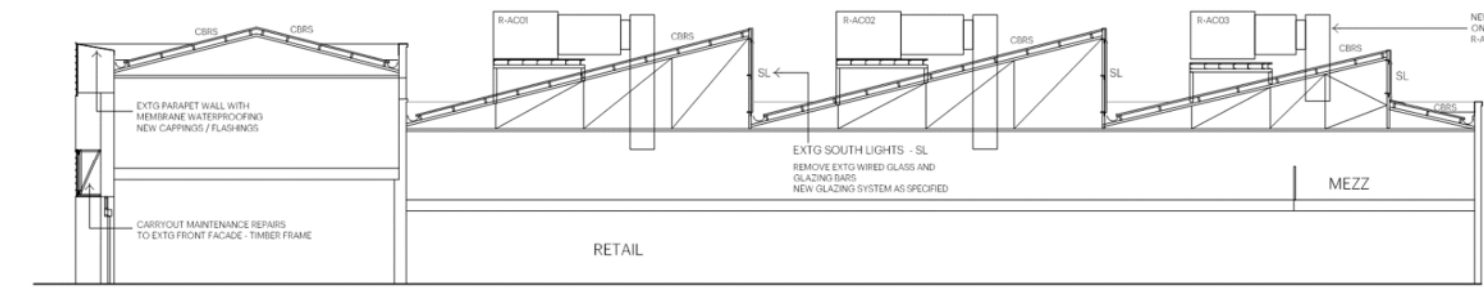
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FIRST ISSUED:	20.02.18	CONS:	-
DATE ISSUED:	20.02.18	DRAWN:	GS
		CHECKED:	JG

DRAWING AMENDMENTS		
Rev	Date	Amendment
A	22/04/28	EXTG ROOF PLAN SHOWN

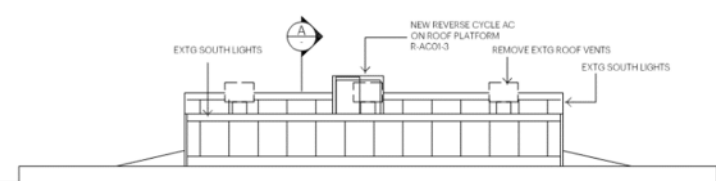


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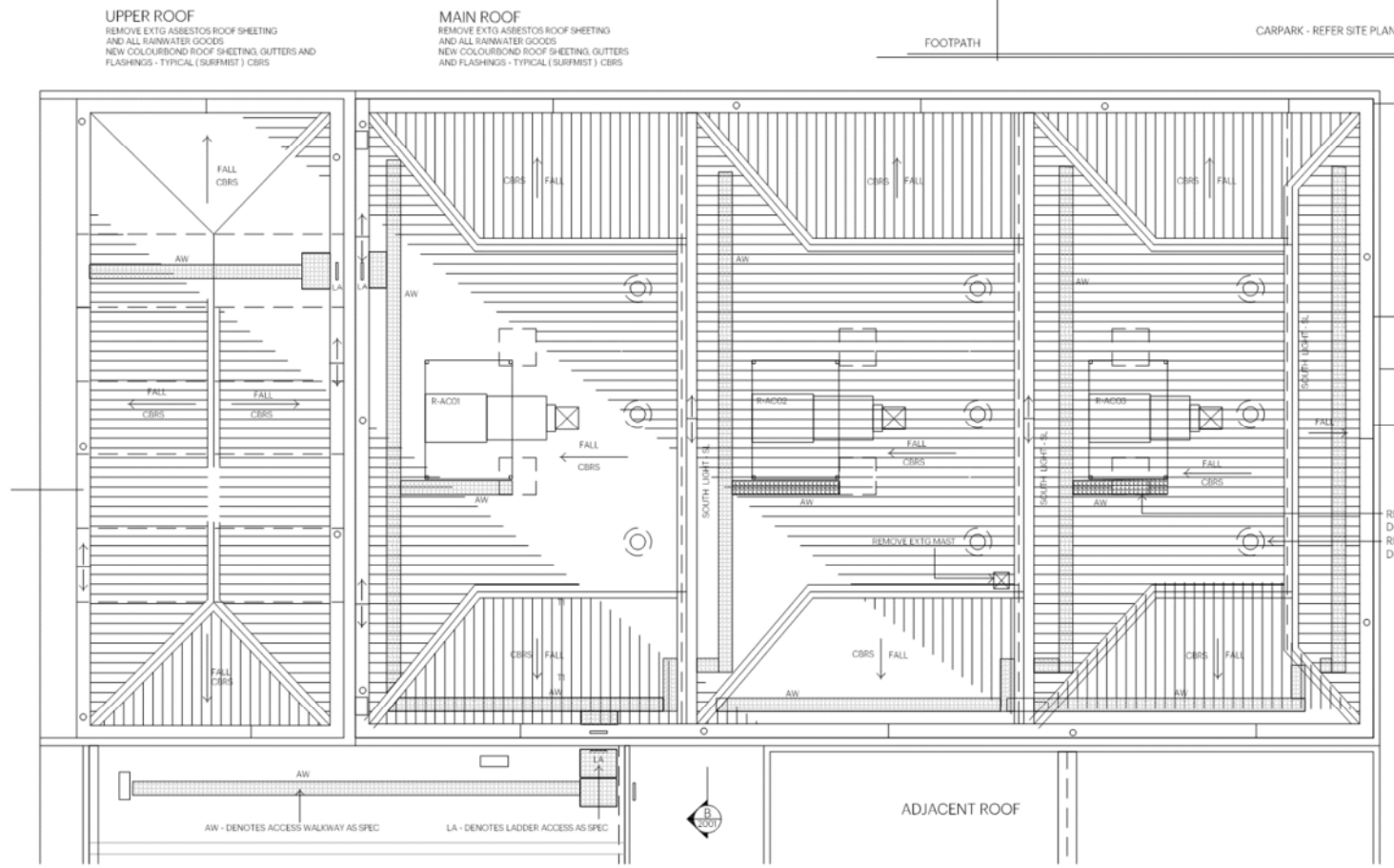
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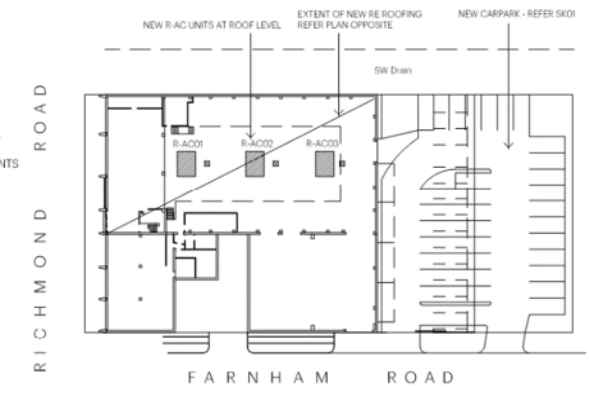
SECTION
SCALE 1:100



SOUTH ELEVATION
SCALE 1:100



RE - ROOFING PLAN / MECH AC PLANT
SCALE 1:100 @ A1



SITE LOCATION PLAN
SCALE 1:500 @ A4

PROJECT
SNOWYS OUTDOOR
92 RICHMOND ROAD
KESWICK SA 5035

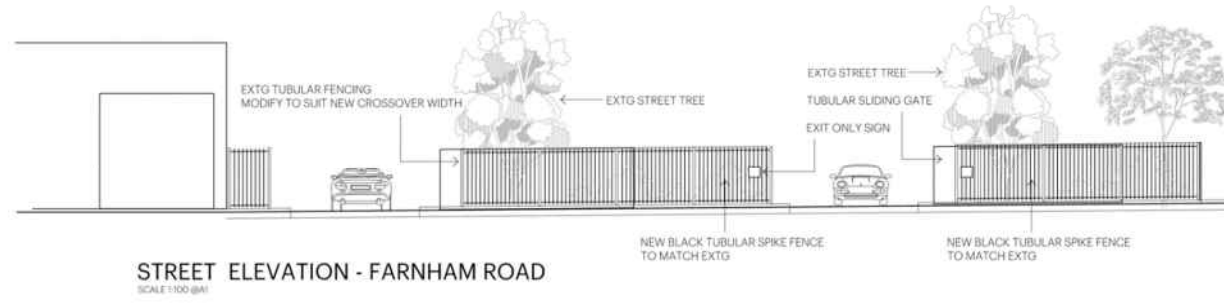
CLIENT
ROC PARTNERS
LEVEL 1
193 WALKERFIELD ST ADELAIDE SA 5000

DRAWING TITLE
ROOF PLAN / SECTIONS

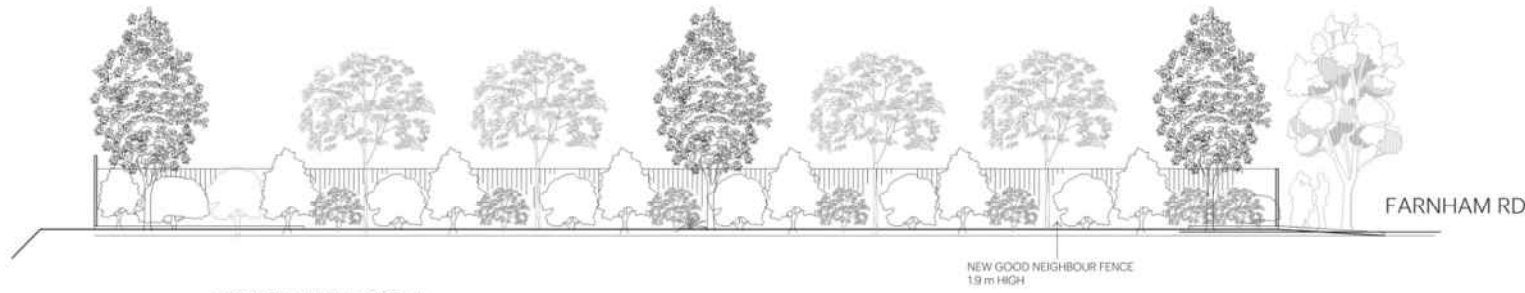
DRAWING NUMBER
907-103-SK02

REVISION
- A

Revised: 26.04.2022 File: Y:\Snowys\Projects\907-103-Snowys Outdoor\Roof\Drawings\220428\907-103-021-roof-plan DA.18486528X



STREET ELEVATION - FARNHAM ROAD
SCALE 1:100 @A1



NORTH ELEVATION
SCALE 1:100 @A1



WEST ELEVATION
SCALE 1:100 @A1

P:\Projects\2022\107-103-Snowys\107-103-Snowys-103-Snowys-Richmond-Road-Drawing\220401-SK02-Carpark-Plan-DIA-SUBMISSION



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ISSUE	DATE ISSUED	DESCRIPTION	DIA PLAN
0000			00 00 00
0001	XX OF YY	DRAWN	00 00 00
0002	A1 1999	CHECKED	00 00 00
0003		TENDER	00 00 00
0004	DD/M/YY		00 00 00

REV	DATE	AMENDMENT
F	220525	FENCE ELEVATIONS ISSUED

PROJECT
SNOWYS OUTDOOR
92 RICHMOND ROAD
KESWICK SA 5035
CLIENT
SITE CARPARK PLAN
LEVEL 103 WAKEFIELD ST ADELAIDE 5000

DRAWING TITLE
FENCE BOUNDARY ELEV
DRAWING NUMBER
107-103-SK02
REVISION
F

PLANNING REPORT

Carpark expansion for Snowys Outdoors

1-3 Farnham Road Keswick
for Smart Property Solutions



Prepared by
MasterPlan SA Pty Ltd
ABN 30 007 755 277, ISO 9001:2015 Certified
33 Carrington Street, Adelaide SA 5000
Telephone: 8193 5600, masterplan.com.au

December 2021



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1.0 INTRODUCTION

This report has been prepared in collaboration with the applicant (Smart Property Solutions) and the project team identified in **Table 1** below and contains a description of the subject site, the locality and the proposed development, as well as our assessment of the proposed development against the relevant provisions of the Planning and Design Code.

Table 1: Project Team

PROJECT TEAM	
Applicant	Smart Property Solutions
Planning Consultant	MasterPlan SA Pty Ltd
Contact Person	Greg Vincent of MasterPlan SA Pty Ltd
List of Other Contributors	
- Architects / Designer	Studio Nine Architects
- Civil/Stormwater Engineer	TMK

In preparing this assessment of the proposed development we have had regard to the *Planning, Development & Infrastructure Act 2016* ('the Act'), *Planning, Development & Infrastructure Regulations 2017* ('the Regulations') and the Planning and Design Code.

This planning report has been informed by and should be read in conjunction with the following documentation:

- the Certificates of Title;
- Site Plan prepared by Studio Nine Architects; and
- Civil Plan prepared by TMK.

We have formed the view that the proposal reasonably satisfies the intent of the policies as set out in the Planning and Design Code's Overlays, Zone and General Development Policies most relevant to the subject site. We therefore conclude that the proposal is an appropriate development and warrants favourable consideration.

2.0 SUBJECT SITE

The subject site (the site) is identified in **Image 1** below, and **Table 2** which provides a summary description, being the whole of the site.



Image 1: Subject Site; 1-3 Farnham Road Keswick (Source – SA Property and Planning Atlas)

We note that the existing carpark it utilised by Snowys Outdoors on the northern adjacent land.

Table 2: Project Site

PROJECT SITE	
Address	1-3 Farnham Road Keswick
Local Government Area	West Torrens Council
Certificates of Title or Land Parcels	Allotment 351, D1288, Hundred of Adelaide, Volume 5215 & Folio 624 Allotment 350, D1288, Hundred of Adelaide, Volume 5231 & Folio 962
Existing Use / Built form	Lot 351 comprises single storey detached dwelling located adjacent the carpark to the south on 3 Farnham Road Keswick. Lot 350 is used as a car park with landscaping is located behind this building at 1 Farnham Road Keswick. Whilst not part of the site, a two-storey building used as a shop (Bulky Goods Retail Outlet) by Snowys Outdoors is located at 94 Richmond Road. The carpark located within the site is used by Snowys Outdoors, however no change to the physical building or operational management of Snowys Outdoors is proposed, and as such, is not included as part of the site.

In review of the site’s land titles there are no encumbrances or easements that would be impacted by the proposed development.



The site is largely flat, and does not comprise building with heritage significant, or landscaping/trees that are protected under the *Planning, Development Infrastructure Act 2016*.

An existing double width crossover is located at the northwest corner of the property at 1 Farnham Road Keswick, whilst a single width crossover is located at the northwest corner of the property at 3 Farnham Road Keswick. Street infrastructure fronting the properties at 1-3 Farnham Road includes two (2) stobie poles and two (2) medium sized street trees.

The existing carpark comprises 17 parking spaces with infrastructure and waste bins associated with the shop located at the north east corner of the car parking area.

A stormwater channel runs along the east portion of 1-3 Farnham Road Keswick in a north/south direction.

The parcel of land comprising the shop fronting Richmond Road is located in the Employment Zone, whilst the carpark and dwelling at 1-3 Farnham Road (the site) are located in the Housing Diversity Neighbourhood Zone.

3.0 LOCALITY

Richmond Road is characterised by multi storey commercial development located in the Employment Zone. Properties located on the north side of Richmond Road comprise large parking areas at the front and rear portions of the land. Properties on the south side of Richmond Road comprise a mix of both street front and rear located parking areas, all of which are lower in scale to the parking areas on the north side of Richmond Road.

Land to the southeast, south and southwest of the site comprises of residential development, all of which is located in the Housing Diversity Neighbourhood Zone.

Street parking along Farnham Road comprises the following:

- East side of road between 94 Richmond Road, 1 Farnham Road Keswick: Loading Zone between 9.00 am-5.00 pm Monday to Friday.
- East side of the road between 3 to 7 Farnham Road Keswick: unrestricted parking.
- East side of the road onwards from 7 Farnham Road Keswick: 2-hour parking limited between 8.00 am-5.00 pm Monday to Friday.
- West side of road between 96 Richmond Road to 2 Farnham Road Keswick: unrestricted.
- West side of the road onwards from 4 Farnham Road Keswick: 2-hour parking limited between 8.00 am-5.00 pm Monday to Friday.



4.0 DESCRIPTION OF PROPOSED DEVELOPMENT

The proposed development is summarised in **Table 3** below and with a more detailed description of the proposed works outlined in the following sections.

Table 3: Proposal

PROPOSED DEVELOPMENT SUMMARY	
Summary Description	Expansion of existing carpark and associated landscaping and fencing.
Development Elements	<ul style="list-style-type: none"> • Change of use from detached dwelling to car park. • Construction of 32 car parking spaces (including one (1) accessible parking space)

The intent of the proposed development is to provide additional on-site parking to serve the existing commercial activities associated with Snowys Outdoors. At present, the existing provision for 17 parking spaces is not sufficient to cater for demand, and staff and visitors are forced to park off site through the local streets. The applicant has identified this issue and proposes the expansion of the existing carpark to accommodate an additional 15 parking spaces resulting in a total of 32 parking spaces.

The proposed changes to Council’s verge are modest, with the existing crossover at 3 Farnham Road being altered and widened. These alterations ensure sufficient separation between crossovers and Council street infrastructure is achieved.

Directional and wayfinding signage is to be attached to the entrance fencing/gates in order to comply with relevant Australian Standards. These forms of signage and fencing are not development in their own right.

The extent of the proposed changes is better depicted on the Site Plan prepared by Studio Nine and the Civil Plan prepared by TMK (**enclosed**). These documents depict both the extent of landscaping being removal, as well as the extent of proposed landscaping, largely focussed along the street boundary and southern boundary which abuts residential development.

5.0 PLANNING AND DESIGN CODE

Table 4 below, provides a summary of the applicable Zone, Sub Zone, Overlays and General Development Policies sections that have been identified as applying to the subject site.

**Table 4: Planning and Design Code Summary**

PLANNING AND DESIGN CODE SUMMARY	
Version and Date	2021.16 gazetted 4 November 2021
Zone	Housing Diversity Neighbourhood
Sub Zone	N/A
Local Variation (TNV)	<p>Maximum Building Height (Metres) (Maximum building height is 12.5m)</p> <p>Minimum Frontage (Minimum frontage for a detached dwelling is 9m; semi-detached dwelling is 9m; row dwelling is 5m; group dwelling is 7m; residential flat building is 15m)</p> <p>Minimum Site Area (Minimum site area for a detached dwelling is 250 sqm; semi-detached dwelling is 200 sqm; row dwelling is 150 sqm; group dwelling is 150 sqm; residential flat building is 150 sqm)</p> <p>Maximum Building Height (Levels) (Maximum building height is 3 levels)</p>
Overlays for 1-3 Farnham Road	<p>Airport Building Heights (Regulated) (All structures over 15 metres)</p> <p>Affordable Housing</p> <p>Building Near Airfields</p> <p>Hazards (Flooding)</p> <p>Hazards (Flooding - Evidence Required)</p> <p>Prescribed Wells Area</p> <p>Regulated and Significant Tree</p> <p>Stormwater Management</p> <p>Traffic Generating Development</p> <p>Urban Tree Canopy</p> <p>Water Resources</p>

6.0 PROCEDURAL MATTERS

Table 5 below, provide a summary of the procedural matters associated with the proposed expansion of existing carpark and associated landscaping and fencing.

Table 5: Procedural Matters

PROCEDURAL MATTERS SUMMARY	
Relevant Authority	West Torrens Council
Assessment Pathway	Code Assessed – Performance Assessment
Statutory Referrals	N/A
Public Notification	Notification Required



6.1 Assessment Pathway

Pursuant to the Act and Regulations, the proposed demolition of the dwelling at 3 Farnham Street is exempt from requiring approval.

The proposed expansion to the site's existing car park is not expressly listed in Tables 1-2 or 4 of the Housing Diversity Neighbourhood Zone. As such, we have formed the view that the proposal defaults to Table 3 and is a **code assessed development** requiring a **performance assessment**.

We note that a Shop is listed as restricted development in Table 4 of the Housing Diversity Neighbourhood Zone, unless there is a gross leasable floor area less than 1000 square metres, or the shop is for a restaurant. For determining the procedural nature of the development, we do not consider the proposal to be a Shop even though the carpark is used by Snowys Outdoors.

The development element is limited to the change of use to a car park (albeit ancillary). In this respect we note that Section 102 (7) of the *Planning Development and Infrastructure Act 2016* allows for each element that will require planning consent to be assessed separately and granted a planning consent with respect to that particular element.

The proposal only comprises physical changes to the existing carpark which is already located in the Housing Diversity Neighbourhood Zone. No physical change the shops building, internal layout or operational management is proposed. As such, we do not consider the shop's land parcel to form part of the site, nor do we consider the intent of Table 4 to capture other associated elements of a shop, such as carparking/landscaping/fencing (and the like), for the purposes of determining the procedural nature of the development.

Further, the above is intended to make a clear distinction between determining the procedural nature of a development and determining the merits of a proposed development. As such, Section 7 of this report addresses the merits of the proposal in context with existing shop and surrounding locality with reference to the most relevant policies of the Planning and Design Code.

6.2 Statutory Referrals

In review of the Regulations and the Planning and Design Code, we have not identified the need for any statutory referrals.

6.3 Public Notification

Performance Assessed development require notification unless they are expressly listed as exempt in Table 5 of the Housing Diversity Neighbourhood Zone. The nature of the proposed development is not listed as exempt and accordingly requires public notification.



7.0 PLANNING ASSESSMENT

Our assessment of the policy identified in the Planning and Design Code is detailed **below** with the following being our assessment of the proposal against the most relevant Desired Outcomes (DO), Performance Outcomes (PO) and Designated Performance Features (DPF) prescribed within the Planning and Design Code.

7.1 Land Use

The relevant land use and intensity DO, PO and DPFs set out in the Housing Diversity Neighbourhood Zone related to land use are provided below with our assessment following.

Housing Diversity Neighbourhood Zone	
DO 1	
Medium density housing supports a range of needs and lifestyles, located within easy reach of a diversity of services and facilities. Employment and community service uses contribute to making the neighbourhood a convenient place to live without compromising residential amenity.	
PO 1.1	DTS/DPF 1.1
Diverse range of medium density housing and accommodation complemented by a range of compatible non-residential uses supporting an active, convenient, and walkable neighbourhood.	Development comprises one or more of the following: <ul style="list-style-type: none"> a) Ancillary accommodation b) Consulting room c) Community facility d) Dwelling e) Educational establishment f) Office g) Place of Worship h) Pre-school i) Recreation area j) Residential flat building k) Retirement facility l) Shop m) Supported accommodation.
PO 1.2	DTS/DPF 1.2
Commercial activities improve community access to services are of a scale and type to maintain residential amenity.	A shop, consulting room or office (or any combination thereof) satisfies any one of the following: <ul style="list-style-type: none"> ... c) is located more than 500m from an Activity Centre and satisfies one of the following:



	<ul style="list-style-type: none"> i) does not exceed 100m² gross leasable floor area (individually or combined, in a single building) where the site does not have a frontage to a State Maintained Road ii) does not exceed 200m² gross leasable floor area (individually or combined, in a single building) where the site has a frontage to a State Maintained Road
PO 1.3	DTS/DPF 1.3
<p>Non-residential development located and designed to improve community accessibility to services, primarily in the form of:</p> <ul style="list-style-type: none"> a) small-scale commercial uses such as offices, shops and consulting rooms b) community services such as educational establishments, community centres, places of worship, pre-schools and other health and welfare services c) services and facilities ancillary to the function or operation of supported accommodation or retirement facilities d) open space and recreation facilities. 	None are applicable.
PO 1.5	DTS/DPF 1.5
Non-residential development sited and designed to complement the residential character and amenity of the neighbourhood.	None are applicable.

The proposed expansion of the carpark is not a shop; however, it will continue to service the Snowys Outdoors shop (Bulky Goods Outlet), of which is a land use that is expressly envisaged in DTS/DPF 1.1 of the Housing Diversity Neighbourhood Zone, on the basis that it supports *'an active, convenient, and walkable neighbourhood'* (PO 1.1) and *'contributes to making the neighbourhood a convenient place to live without compromising residential amenity'* (DO 1). Further, PO 1.2, 1.3 and 1.5 support non-residential land uses that are compatible with and preserve residential character and amenity.

The applicant has identified a need for additional parking as the current overflow is being spread throughout surrounding residential streets. This existing arrangement is not considered to be compatible with the above provisions as the parking activities are negatively impacting surrounding streets and residential amenity. This overflow is creating clusters of cars being parked throughout the surrounding residential streets, thereby limiting the convenient use and movement of local residents and/or visitors to the area. The application proposes an additional 15 parking spaces at the rear of Snowys Outdoors to alleviate demands on surrounding streets and residential amenity, thereby according with the intent of the land use provisions prescribed within the Housing Diversity Neighbourhood Zone.



The applicant acknowledges that the removal of a residential dwelling in a neighbourhood type zone may be perceived as being at odds with the intent of the Housing Diversity Neighbourhood Zone. As demonstrated above, the Housing Diversity Neighbourhood Zone contemplates non-residential uses that reasonably maintain residential amenity. The benefits associated with the proposal (detailed above) are considered to reasonably satisfy the intent of the Housing Diversity Neighbourhood Zone. We note further that only one (1) residential dwelling currently abuts the site's existing car park (to the south). As such, the application proposes a neutral arrangement, being that only one (1) dwelling will continue to directly abut the carpark associated with the existing shop. As such, we consider the proposal to provide a broader range of benefits to the wider community and residential area, particularly to those dwellings situated at the periphery of the Employment and Housing Diversity Neighbourhood Zone.

We note that mitigating measures to limit impact on the abutting dwelling to the south are detailed below in subsequent sections of this report.

Given the above, we consider the proposal reasonably satisfies the land use intent of the Housing Diversity Neighbourhood Zone.

7.2 Transport, Access and Parking

The relevant Transport, Access and Parking DO, PO and DPFs set out in the Planning and Design Code include:

General Development Policies – Transport, Access and Parking	
DO 1	
A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.	
PO 1.2	DTS/DPF 1.2
Development is designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive receivers.	None are applicable.
PO 2.1	DTS/DPF 2.1
Sightlines at intersections, pedestrian and cycle crossings, and crossovers to allotments for motorists, cyclists and pedestrians are maintained or enhanced to ensure safety for all road users and pedestrians.	None are applicable.
PO 3.1	DTS/DPF 3.1
Safe and convenient access minimises impact or interruption on the operation of public roads.	The access is: <ul style="list-style-type: none"> a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land



	<p>or</p> <p>b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.</p>
PO 3.5	DTS/DPF 3.5
<p>Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.</p>	<p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <p>a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land</p> <p>b) where newly proposed, is set back:</p> <p>i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner</p> <p>ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance</p> <p>iii) 6m or more from the tangent point of an intersection of 2 or more roads</p> <p>iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.</p>
PO 3.6	DTS/DPF 3.6
<p>Driveways and access points are separated and minimised in number to optimise the provision of on-street visitor parking (where on-street parking is appropriate)</p>	<p>Driveways and access points:</p> <p>a) for sites with a frontage to a public road of 20m or less, one access point no greater than 3.5m in width is provided</p> <p>b) for sites with a frontage to a public road greater than 20m:</p> <p>i) a single access point no greater than 6m in width is provided</p> <p>or</p> <p>ii) not more than two access points with a width of 3.5m each are provided.</p>
PO 6.6	DTS/DPF 6.6
<p>Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.</p>	<p>Loading areas and designated parking spaces are wholly located within the site.</p>



We have formed the view that the proposal accords with the above provisions as follows:

- the intent of the proposal is to alleviate parking demand and impacts on surrounding residential streets, thereby supporting a safer transport network for the locality (DO 1);
- the extent of vehicle movements, parking and manoeuvring along Farnham Street will ideally be minimised (PO 1.2);
- each crossover will maintain appropriate site lines for vehicle and pedestrian safety to ensure the design of each remains in accordance with Council and Australian Standards (PO 2.1 & 3.1);
- each crossover will ensure a separation distance to street infrastructure of one (1) metre, of which is generally accepted in neighbourhood areas (PO 3.5);
- whilst the proposal seeks to increase the width of one (1) existing crossover, the outcome of the development will vastly improve accessibility for street parking to the wider community as the shop’s staff will not be reliant on these spaces (PO 3.6); and
- loading areas for service vehicles associated with the shop will remain within the boundaries of the site (PO 3.6).

Whilst an assessment of the existing shop’s provision for parking is not considered necessary, Table 1 - General Off-Street Car Parking Requirements stipulates a parking demand for Shop (in the form of a bulky goods outlet) at 2.5 space per 100 square metres of gross leasable floor area. The existing shop has a Development Approval to operate with the existing provision of 17 parks. Notwithstanding this, the extent of the shops gross leasable floor area (approximately 1400 square metres) results in the current provision for parking falling 18 spaces short of the requirement expressed in Table 1. Whilst an assessment of the existing shops gross leasable floor area is not considered relevant for the purposes of this proposal, the development will create a net improvement in parking and provide a provision of spaces that more closely aligns with the demand rate stipulated Table 1.

Given the above, the proposal offers substantial benefits to the wider locality and significantly improves the provision for parking within the boundaries of the site, thus achieving the intent of the relevant provisions outlined in the Planning and Design Code for Transport, Access and Parking.

7.3 Design and Appearance

The relevant design DO, PO and DPFs set out in the Planning and Design Code include:

General Development Policies - Design in Urban Areas	
DO 1	
Development is:	
a)	contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality
b)	durable - fit for purpose, adaptable and long lasting



General Development Policies - Design in Urban Areas	
c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.	
PO 7.2	DTS/DPF 7.2
Vehicle parking areas appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.	None are applicable.
PO 7.4	DTS/DPF 7.4
Street-level vehicle parking areas incorporate tree planting to provide shade, reduce solar heat absorption and reflection.	Vehicle parking areas that are open to the sky and comprise 10 or more car parking spaces include a shade tree with a mature canopy of 4m diameter spaced for each 10 car parking spaces provided and a landscaped strip on any road frontage of a minimum dimension of 1m.
PO 7.5	DTS/DPF 7.5
Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.	Vehicle parking areas comprising 10 or more car parking spaces include soft landscaping with a minimum dimension of: <ul style="list-style-type: none"> a) 1m along all public road frontages and allotment boundaries b) 1m between double rows of car parking spaces.
PO 7.7	DTS/DPF 7.7
Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.	None are applicable.
PO 9.1	DTS/DPF 9.1
Fences, walls and retaining walls of sufficient height maintain privacy and security without unreasonably impacting visual amenity and adjoining land's access to sunlight or the amenity of public places.	None are applicable.



The proposed development seeks to expand the site’s existing car park by utilising the adjoining residential property to the south. As demonstrated in **Section 7.1**, non-residential development is supported on the basis that the proposal does not jeopardise residential amenity of the wider locality. As such, the design and scale of the proposal provides an appearance we consider to be generally expected, particularly given it will largely match the appearance of the existing carpark. As the proposal is intended to minimise the need for on-street parking, the existing clustered appearance of vehicles parked along Farnham Street will ideally be reduced. We argue that the appearance of the carpark expansion will be visually appropriate, particularly when viewed in context with the clustered arrangement of vehicles currently parking along Farnham Street.

We note that the existing carpark is located in the Housing Diversity Neighbourhood Zone, and directly abuts one (1) allotment with a single storey dwelling. The proposed expansion will maintain this arrangement, ensuring that only one (1) dwelling to the south directly abuts the proposed carpark. In order to mitigate the visual interface between the car park and southern dwelling, extensive landscaping is proposed at a width and dimension that accords with the relevant policy listed above for Design in Urban Areas.

Further, the proposal seeks to minimise potentially adverse visual impacts viewed from the street by providing extensive boundary landscaping at a width and dimension that largely satisfies the relevant policy listed above for Design in Urban Areas.

The proposed fencing and gates will match the site’s existing tubular design, thereby ensuring the proposed landscaping is highly visible when viewed from the street. As demonstrated within the proposed set of plans, the entire street boundary will be landscaped with exception of the two (2) crossover access points.

Given the above, we expect the proposal will improve the visual amenity of the surrounding streetscape, offer a contextually appropriate scale, and provide an extent of boundary landscaping that will adequately preserve residential amenity, as desired in the policy outlined above for Design in Urban Areas.

7.4 Interface between Land Uses

The relevant Interface between Land Uses DO, PO and DPFs set out in the Planning and Design Code include:

General Development Policies - Interface between Land Uses	
DO 1	
Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses. management, environmental performance, biodiversity and local amenity and to minimise energy consumption.	
PO 1.2	DTS/DPF 1.2



General Development Policies - Interface between Land Uses									
Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.	None are applicable.								
PO 2.1	DTS/DPF 2.1								
<p>Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:</p> <ul style="list-style-type: none"> a) the nature of the development b) measures to mitigate off-site impacts c) the extent to which the development is desired in the zone d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land. 	<p>Development operating within the following hours:</p> <table border="1"> <thead> <tr> <th>Class of Development</th> <th>Hours of operation</th> </tr> </thead> <tbody> <tr> <td>Consulting room</td> <td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td>Office</td> <td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td>Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</td> <td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday</td> </tr> </tbody> </table>	Class of Development	Hours of operation	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday
Class of Development	Hours of operation								
Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday								
PO 4.1	DTS/DPF 4.1								
Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).	Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.								

The proposal is simply a response to an identified need to alleviate overflow parking to the surrounding residential streets. As such, the type and nature of vehicles entering/exiting the site will continue to comprise largely of domestic sized vehicles.

As detailed in **Section 7.3** of this report, extensive landscaping is proposed along the southern boundary that will be shared with a residential dwelling. As such, any light spill directed towards the southern dwelling in the evening will be largely diffused by the proposed landscape screening and proposed good neighbour fence. As operating hours are to remain consistent with existing operations, there will be limited occasions where vehicles exit the site during darker hours of the day. As such, the impact of light spill directed towards the southern dwelling will be reasonable and consistent with the existing operations.

With regard to noise spill within the carpark, the southern abutting dwelling will be subject to similar levels of noise currently experienced by the existing dwelling at 3 Farnham Street. As such, the net impacts of noise on residential dwellings are anticipated to be neutral. As demonstrated above, the proposal will result in a reduced demand for on-street parking, thereby minimising the clustering of vehicle parking, noise spill, manoeuvring and light spill on the wider Farnham streetscape.



As such, the proposed interface impacts created by the proposal are considered to be reasonable when balanced with the anticipated benefits offered to the wider community, thereby satisfying the general intent of the policy listed above for Interface between Land Uses.

7.5 Infrastructure

The relevant infrastructure DO, PO and DPFs set out in the Planning and Design Code include:

General Development Policies – Infrastructure and Renewable Energy Facilities
DO 1
Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

A Civil Plan prepared by TMK is **enclosed** and details the proposed method for stormwater management. We expect the proposed civil design adequately satisfies Council standards for stormwater management, as prescribed in the above policy for Infrastructure and Renewable Energy Facilities.

8.0 CONCLUSION

The planning merits of the proposed development can be summarised as:

- the proposal is considered to be a reasonable response to the relevant policy set out in the site’s associated Overlays, zoning and General Policies of the Planning and Design Code;
- the proposal supports a land use expressly envisaged in the zone, largely given the minimal impacts expected on residential amenity;
- the proposal is designed with adequate infrastructure, as depicted by TMK;
- the proposal offers benefit to the wider residential community by alleviating use of the surrounds streets for parking; and
- interface between land uses will be managed through provision for adequate landscaping and fencing.

Having regard to the nature of the subject site and locality, the proposed development and relevant policy identified in the Planning and Design Code, we are of the opinion that the proposal demonstrates substantial planning merit to warrant Council’s favourable consideration.

Greg Vincent MPIA

B/A in Planning

6 December 2021



3 May 2022

165 Sir Donald Bradman Drive,
HILTON SA 5033

Attention: Zoe Delmenico

Dear Zoe

Re: Response to Council Request for DA 21039722

MasterPlan SA Pty Ltd has been engaged by Smart Property Solutions, the applicant, for DA 21039722 to act on their behalf for the proposed development for "*Extension to existing carpark ancillary to existing shop*".

We have had the benefit of reviewing the Council's request for additional information and **enclose** the following to address each item:

- amended 'SITE PLAN / CARPARK PLAN' prepared by Studio Nine;
- amended Stormwater Management Plan and calculations prepared by TMK; and
- a 'ROOF PLAN / SECTIONS' prepared by Studio Nine depicting replacement AC units on the existing building and minor alterations to the external cladding.
- TMK email correspondence regarding potential impact on channel.

Item 1

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) with a full schedule of landscaping and plantings proposed.

Item 2

The existing / proposed car park will not be used for deliveries and servicing. This is undertaken in the existing service yard (western portion of existing building) and will continue to do so.

Item 3

Bin storage exists in the carpark and will continue to do so in the location shown on the proposed plans. Out of hours collection is currently in place with collections occur between 2.00 - 5.00 am Tuesdays and Fridays.



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Adelaide SA 5000
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52318LET02

**Item 4**

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) indicating all existing and proposed fencing.

Item 5 – Street Tree

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) showing that the crossover has been amended to 4.0 metres in width and splayed to allow a 2.0-metre separation from the existing street tree.

Item 6 – Waste Truck

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) showing waste vehicle turning circles. As detailed above, waste pick up occurs between 2.00 am - 5.00am, thereby limiting any conflict with surrounding vehicle movements or residential vehicles.

Item 7 – Fencing

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) for all proposed fencing. Fencing along portions of the eastern boundary will comprise of Colorbond, of which was requested by a neighbouring representor to limit any potential perceived issues of noise. Fencing along the Farnham Road frontage is permeable to assist with flow of flood waters. The fencing along the shared residential boundary to the south comprises Colorbond, largely to limit potentially negative visual impacts on the neighbouring residential land. As tubular fencing is proposed along the Farnham Road frontage, we expect this will facilitate appropriate management and flow of flood water in accordance with relevant standards.

TMK have provided an amended Stormwater Management Plan and calculations (**enclosed**) which addresses this item.

Item 8 – Landscaping

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) with a full schedule of landscaping and plantings proposed. The plantings are predominantly located along the southern boundary that is shared with a residential dwelling. The selection of plantings ensures the species, height and variety are complimentary to the existing residential character of Farnham Street. The variety of plantings selected will provide both the site and adjoining land a necessary level of environmental amenity.

Item 9 – LMA

As Council is aware, an LMA is being prepared to associate the proposed carpark with the existing retail use at 90-94 Richmond Road. We await an update from Council's legal team on this item.



Item 10 – Unauthorised Development

Please see the amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) depicting these structures with an elevation provided also.

Item 11 – City Assets (Watercourse)

TMK have communicated (see **enclosed** email) that the carpark construction should not adversely impact the creek and its banks as the proposal is not altering the existing ground levels; therefore, no earth retaining structures are proposed. Additionally, TMK have communicated that the proposed new fence and pad footings do not impact on the existing drain construction.

Item 12 – City Assets (Verge)

We provide the following answer for each point:

- 2.1: The driveway access is shown at 90 degrees to the kerb alignment and the property.
- 2.1.1: The crossovers are now shown 2.0 metres from the existing street trees and utilises a 4.0-metre-wide crossover.
- 2.1.2: The existing stobie pole is a consumer mains supply located within the site and is not a SAPN stobie pole. The applicant intends on retaining the internal pole.
- 2.1.3: The amended 'SITE PLAN / CARPARK PLAN' (**enclosed**) shows that the crossover has been separated from the northern crossover by 1.0 metre as requested by council.
- 2.1.4: TMK have advised that stormwater discharge is only proposed via the existing channel, as demonstrated on their proposed plans.
- 2.2: Existing redundant crossovers are noted to be closed on all documentation and reinstated in accordance with Council standards.
- 3.1: Noted, albeit crossovers have been amended to 4 metres in wide and have been flared in accordance with Item 4.1 for vehicle access. Traffic signage has also been noted on the proposed drawings.
- 3.2: Existing structures have now been shown accurately on the proposed documents.
- 4.1: Waste is addressed above, and the crossovers have been amended to 4 metres in wide and flared as shown on the proposed documents.
- 5.1: TMK have amended the proposed stormwater management plan and calculations (**enclosed**) to ensure compliance with Council standards.
- 5.2: Noted.



Additional Item

Enclosed is a 'ROOF PLAN / SECTIONS' depicting replacement AC units on the existing building and minor alterations to the external cladding. We note that no screening devices around the new AC units are proposed, however the following images have been taken from surrounding streetscape view points to demonstrate the limited views currently available.



Image 1: East facing view opposite the site on Farnham Street

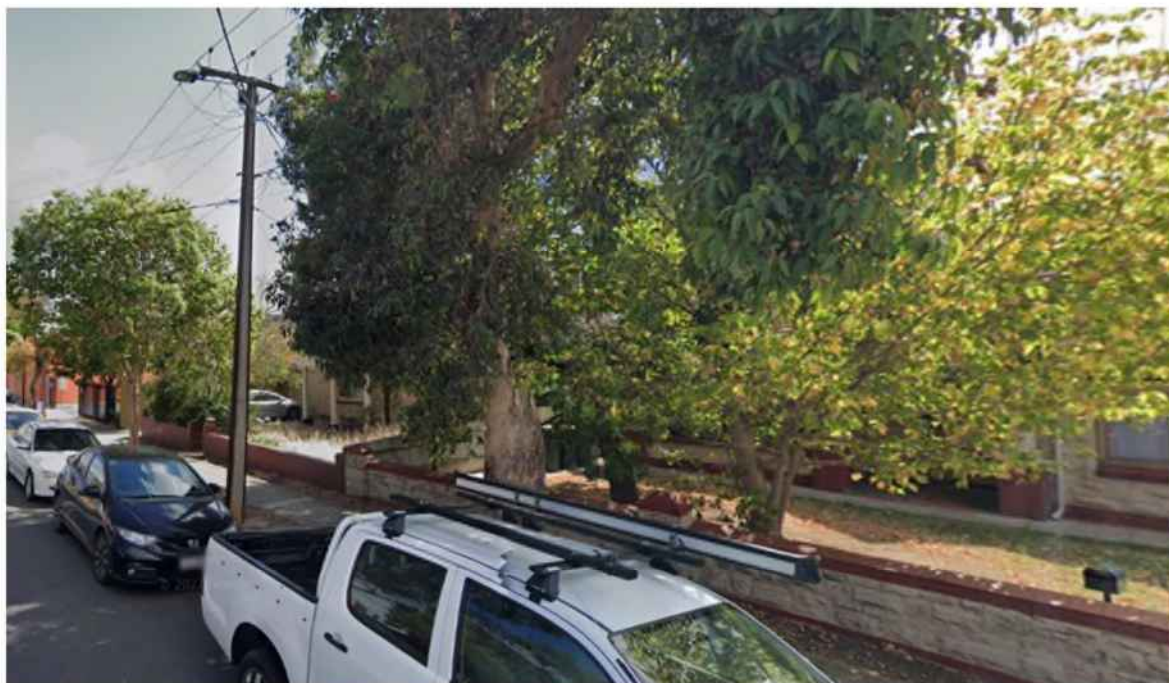


Image 2: North east facing view opposite from 5 Farnham Street



Image 3: View from Richmond Road

Based on the images above, the new AC Units will only be visible from a small number of residential view points along Farnham Street, largely to be obscured from view by existing buildings, street trees, landscaping and infrastructure. As such, we consider the new AC units to be appropriately located in place of the existing and will not negatively impact surrounding residential amenity.

We trust that the above satisfies Council's request, however if any further clarification is required please do not hesitate to contact our office directly.

Yours sincerely

Greg Vincent
MasterPlan SA Pty Ltd

enc: Amended 'SITE PLAN / CARPARK PLAN' prepared by Studio Nine.
Amended Stormwater Management Plan and calculations prepared by TMK.
'ROOF PLAN / SECTIONS' prepared by Studio Nine.
Email correspondence from TMK.

From: Daniel McKenna
Sent: Thursday, 26 May 2022 9:19 AM
To: Daniel McKenna
Subject: FW: 220525 DA TMK civil drawing amendments

From: Fotis Koklas <fkoklas@tmkeng.com.au>
Sent: Wednesday, 25 May 2022 4:40 PM
To: George Stubbs <george@studionine.net.au>
Cc: Bruno Frudeli <bfrudeli@tmkeng.com.au>; John Galluccio <john@studionine.net.au>
Subject: RE: 220523 DA TMK civil drawing amendments Issue F

Hello George,

Updated Civil documentation attached. Please also note the following in response to the below queries:

Dot Point 1

We believe the carpark construction should not adversely impact the creek and it's banks as we are not altering the existing ground levels - therefore no earth retaining structures are being proposed. Additionally the proposed new fence and pad footings do not impact on the existing drain construction. We also do not believe vehicular traversal within the proposed car park will impact stability of the creek bank.

Dot Points 2-4

Civil drawing updated to suit.

If you have any queries please do not hesitate to contact me.

Regards,

Fotis Koklas

BEng (Hons) (Civil)
Senior Engineer / Senior Associate

D (08) 8238 4130 | **M** 0413007505
E fkoklas@tmkeng.com.au
Level 6, 100 Pirie St, Adelaide, SA 5000
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TMK Consulting Engineers

Civil – Geotechnical – Environmental – Structural – Mechanical – Electrical – Fire – Hydraulics – Forensic – Construction Assist

Parking for Visitors to TMK Head Office

TMK Consulting Engineers have made car parking available for visitors to its Level 6, 100 Pirie Street office. To access this you should park in the UPark on Wyatt Street, located at 18 - 34 Wyatt Street, between Grenfell & Pirie Streets. Enter from either Wyatt St or Hyde St and obtain the normal parking ticket on entry. Prior to leaving our office, please ask one of our Reception staff for a UPark on Wyatt Street voucher to use when exiting the car park.

Details of Representations

Application Summary

Application ID	21039722
Proposal	Extension to existing carpark ancillary to existing shop
Location	1 FARNHAM RD KESWICK SA 5035, 3 FARNHAM RD KESWICK SA 5035, 94 RICHMOND RD KESWICK SA 5035, 94 RIC..

Representations

Representor 1 - Christopher Wright

Name	Christopher Wright
Address	13 De Haviland Ave HENDON SA, 5014 Australia
Phone Number	
Email Address	
Submission Date	09/03/2022 04:20 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
Reasons	I own the two properties directly across the Creek at 4 and 4A Chatham Road (opposite the house proposed for demolition/carpark conversion at 3 Farnham Road). A low/open fence and lack of trees/shrubs along the creek boundary will likely result in additional noise transfer across the open concrete channel to my properties. I therefore seek clarity on the height and construction type of the eastern boundary fence (adjacent to the Keswick Creek) and potential amendment to be higher/solid construction. I also seek amendments to the landscaping (additional tree planting to this boundary) to provide a noise buffer from the proposed expanded carpark which I understand will be open 7 days per week. Please contact me to discuss this on 0411 657 116

Attached Documents

Keswick_Snowys_development_feedback.jpg



Representations

Representor 2 - Christopher Wright

Name	Christopher Wright
Address	13 De Haviland Ave HENDON SA, 5014 Australia
Phone Number	
Email Address	
Submission Date	11/03/2022 01:32 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
Reasons	Please refer to my previous feedback submitted 9 March 2022. Following subsequent discussion with West Torrens City Council (Zoe Delmenico on 11 March 2022), I am making a further submission to request I am updated as to how my request is addressed including any amended plans. My feedback is as follows: I request that the eastern fence for the proposed Snowy's car park extension at 1-3 Farnham Road Keswick be of solid construction for at least the portion across 3 Farnham Road (same as the current fence) to reduce noise transfer from the car park (which will be operating 7 days per week) to the rear yard/house of my properties at 4 and 4A Chatham Road Keswick, located directly across the concrete drain/creek from 3 Farnham Road. Additional landscaping within the fence may also help prevent noise transfer. I've provided a map showing this, attached.

Attached Documents

Keswick_Snowys_development_feedback.jpg



Representations

Representor 3 - Alex Boehm

Name	Alex Boehm
Address	u 22/40 Chatham Road KESWICK SA, 5035 Australia
Phone Number	
Email Address	
Submission Date	20/03/2022 11:03 AM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons	This is an inner city suburb that is only a 15 minute bike ride from the CBD or 5 minute bus ride. Keswick is in an urban renewal process of densification to support greater Adelaide's growing population. Cost of living pressures are now higher than ever for residents and the price of housing is pushing more people to rent. Taking away existing residential land that could fit townhouses for at least four families and replacing it with asphalt that will be unused most of the time goes against everything the 30 year plan for greater Adelaide outlines. It actively discourages transit use, reduces housing density, increases CO2 emissions and decreases housing affordability in one of the most impoverished inner suburbs.

Attached Documents

Representations

Representor 4 - Rebecca Ristic

Name	Rebecca Ristic
Address	7 Farnham Road KESWICK SA, 5035 Australia
Phone Number	
Email Address	
Submission Date	30/03/2022 09:57 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons	

Attached Documents

SNOWYS_carpark_1.pdf

Rebecca and Dejan Ristic

7 Farnham Road

Keswick, SA, 5035

To Whom it may concern

Regarding Proposed Development at 3 Farnham Road, Keswick, SA, 5035

Application ID 21039722

We are writing with regard to the proposed development at number 3 Farnham Road, Keswick to remove the existing home and have a carpark in its place.

Keswick is a lovely suburb to live in and we have lived here for more than 20 years. It's close proximity to the city and an abundance of public transport makes it so convenient and accessible. Keswick and the surrounding suburbs have a diverse collection of people. It has a great mixture of large residential homes, flats, and a growing number of medium density homes which fits in with the new zoning of Housing Diversity Neighborhood Zone. This new zoning means the suburb is likely to attract more people to live here.

In the evenings the Kesmond Reserve on Farnham Road, Keswick is a buzz with activity. There are often up to 30 children and 20 adults all participating on various activities like using the playground, playing tennis, basketball, soccer, cricket and then there are the ladies who do yoga and the men who walk and talk. This level of activity demonstrates the growing number of families living in Keswick and the surrounding suburbs. It is a rich community.

There is increasing pressure on families who live in the area to find rental accommodation particularly with a lot of redevelopments. The loss of this rental accommodation in the area is of concern and with the proposed South Road redevelopment the surrounding suburbs of Ashford and Marleston are set to lose a considerable number of homes available for families in the area.

There are a couple of reasons why people like the area apart from the close proximity to the city of Adelaide. Firstly we have a great Primary School, Richmond Primary School in Surry Road. Just a short walk from our home. A school of 400 children and growing.

Secondly is Adelaide High School. We are still an affordable suburb for people to buy and rent in to secure a future at Adelaide High School for their children.

What are our concerns about the proposed demolition of a house and a carpark in it's place.

The loss of a home to the community. We are a growing community and want to encourage people to live here, we don't want the surrounding businesses and industries encroaching into the suburb and loosing families to carparks. There needs to be the balance and putting a carpark in the suburb is not maintaining the residential component of our suburb. The rental market is so stressful in the current climate and families are being driven out of the suburb due to the loss of homes and the redevelopment of affordable homes to smaller less affordable homes. We believe there is value in maintaining the aesthetic value and street appeal.

The opening up of a dual access carpark increases the danger to the community of safe passage past the carpark which is already fraught with danger. Teaching our child to ride her bike around the block becomes quite complex when we reach Snowy's. Trucks are often parked halfway on the footpath. There is an electric lift offloading goods from said trucks across the footpath on a street, where you have to expect humans, even little ones wanting desperately trying to learn to ride a bike, to be on the footpath. And then there is the multitude of various shapes and sizes of 4-wheel drives whose size make it more dangerous for a little person learning to ride a bike. Then there are the fumes she has to dodge her way through.

There are well established businesses along Richmond Road and it is just that a road. A road by definition is mainly for the passage of cars. A street is smaller and more likely to have homes along it and be used for cars and pedestrians. Although Farnham Road is called a "Road" it is indeed a street by definition. This was explained to us about 10 years ago by the West Torrens Council when the road was narrowed by 1.2 meters citing it was a street not a road. We voiced our concerns to the council regarding the impact this would have on the flow of traffic to our residential street as it was at that time very busy. Since that narrowing of the street our woes regarding moving around it have grown.

The street parking has been an issue we have learned to live with and accept there are businesses along Richmond Road and parking inadvertently spills into the suburb. Our observations are that most of the parking from Richmond Road to beyond our home at number 7 Farnham Road is actually workers from the surrounding businesses including Snowy's. There are untimed parking spots that are quickly snapped up by 7:30am on weekdays and then the workers who are later than that spend their day doing the 2 hourly car shuffle or take the risk of receiving a fine for overstaying their welcome.

The carpark that is proposed is for customer parking. During the week the Snowy's carpark is seldom full and our experience is it is not Snowy's customers parking in the street but workers from the surrounding businesses. On the weekends Snowy's certainly get busier and customers do park in the street. The difference is on the weekends there is far more street parking available as the workers from other businesses that are closed on weekends are not parking in the street. It really is just Snowy's workers from our experience.

We do not believe the parking issue is going to be solved by extending the carpark. On really busy weekends like the upcoming Snowy's Easter Long Weekend Sale we don't believe even an extended carpark could cope with the excitement of bagging a bargain at Snowy's. We accept the street will be busy on those weekends but do not agree that is a worthwhile substitute for a home to a carpark to accommodate traffic. That traffic should be managed with established means.

We can see the current carpark is difficult to maneuver for some vehicles, let's not forget those 4-wheel drives trying to navigate it. But is it any worse than what people endure at other businesses in similar circumstances?

If you take a short walk along Richmond Road there is an abundance of underutilized parking allotments. There is the RAA, they are closed on Sundays and open until 12pm Saturdays. Could there be a collaboration between businesses to optimize the use of these already existing established carparks. These could be used by workers to free up street parking. There is the Mile End Depot carpark that sits empty most days. Could there be collaboration between businesses to make use of these carparks. There are businesses next to Snowy's who again are closed on weekends, could there be collaboration with other business owners to utilize already established carparks rather than loose a house and encroach the business industry into our suburb.

On those busy weekends there could be negotiations with local businesses, "Snowy's Easter Long Weekend Parking Available here this Weekend only!!".

A walk along the shopping precinct on Anzac Highway Keswick will demonstrate there can be collaboration between businesses regarding carparking. These businesses have very limited parking outside their shop front however as a customer you are not expected to move your car to the front of each shop you want to browse. You park your car in front of one shop and walk along the row of shops.

We like Snowy's, we like shopping there, we like going there when it's too hot or too cold to go to the playground, we love trying out the various tents and camp beds. We really like the workers; they are very friendly in store and when we encounter them parking in the street. This objection to the extension is in no way an objection to them as a business or individuals. It is an objection for our suburb and maintaining the way it looks and functions as a place where we live that is safe from excessive movement of traffic for our children.

Now for some references to the very long document. Actually, I've done it on a work computer during lunch one day and have forgotten to send it home and now it's the day before this is due. I am sure I picked up on the areas of parking which we really don't think will be solved as our main problem with is the street parking and unless staff are offered to use the customer parking, I see little that solves our issue as residents in a busy street. I think I probably referenced the part relating to design, in particular that a carpark is not nice to look at and it makes little sense to replace a home with a carpark if we are thinking about good design. We do not want a carpark in our residential street.

Regards

Rebecca and Dejan Ristic

And some photo's I've taken with regard to parking in Keswick

This is the current carpark in the middle of a weekday, hardly bursting at the seams.



This is taken from Richmond Road along Farnham Road at 7:30am on my way to work, you can see the untimed street parking is already taken



These are the currently untimed parking outside number 3, the proposed site of the carpark, we assume the street loses these carpark spaces in the proposal and therefore spots for Snowy's staff. Will they be offered carparks on the new site? I have my doubts that will be the case, it is proposed customer parking.



More photos of the parking cogestion



This is a picture taken on a weekday of our car travelling down Farnham road towards Richmond Road, only 1 car can travel at a time due to the street parking and then there is a truck parked outside Snowy's



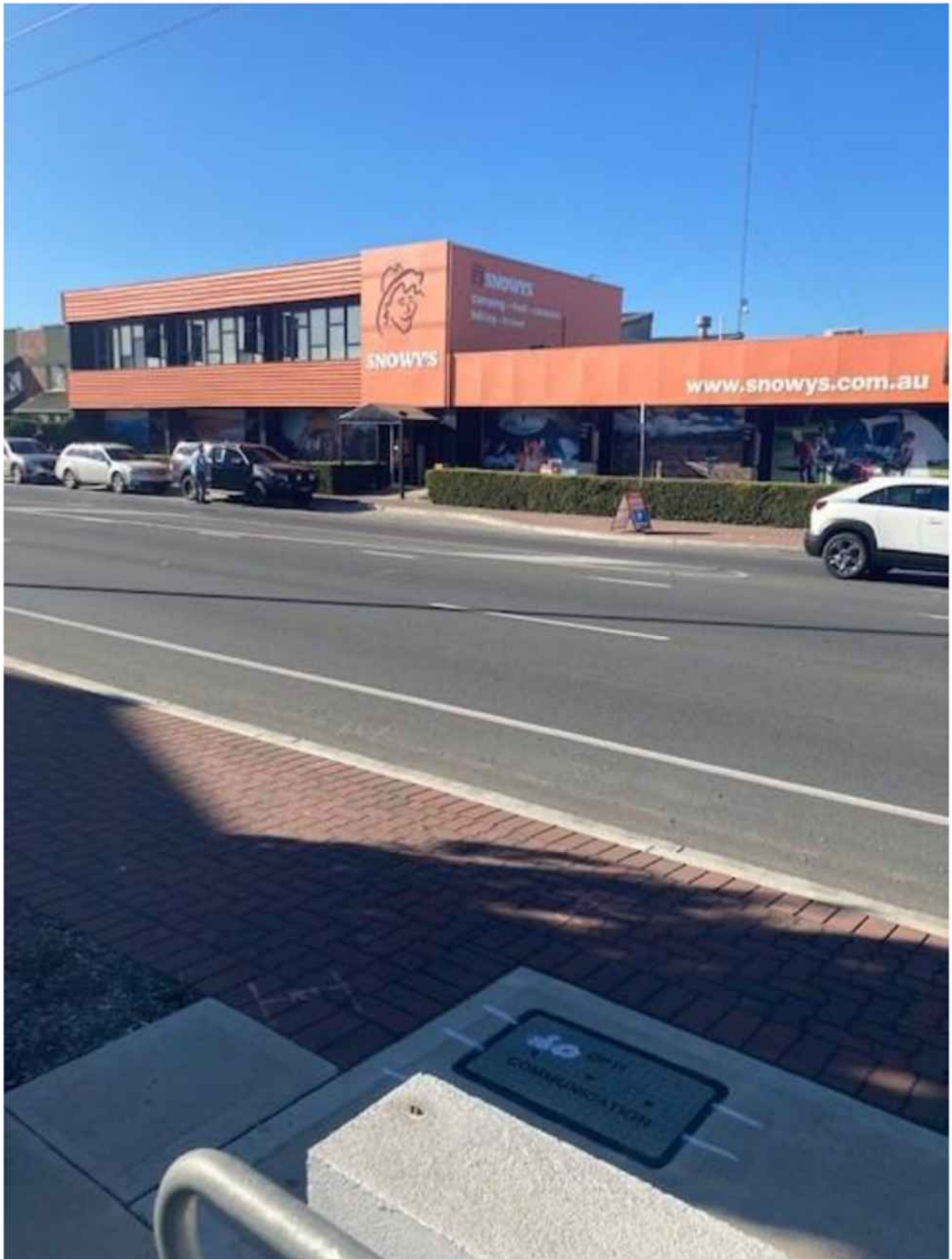
And some photos of the local carparks we think are underutilized and some collaboration between businesses could alleviate the street parking in Farnham Road and surrounding streets















3 May 2022

165 Sir Donald Bradman Drive,
HILTON SA 5033

Attention: Zoe Delmenico

Dear Zoe

Re: Response to Representations for DA 21039722

MasterPlan SA Pty Ltd has been engaged by Smart Property Solutions, the applicant, for DA 21039722 to act on their behalf for the proposed development for "*Extension to existing carpark ancillary to existing shop*".

We have had the benefit of reviewing the representations and note that there was one (1) supportive response with some concerns, and two (2) opposing representations, both of whom wish to be heard at the hearing of the matter before the Council's Assessment Panel.

In response to the representations, we **enclose** the following:

- Amended 'SITE PLAN / CARPARK PLAN' prepared by Studio Nine.

The matters raised in the favourable representation (with some concerns) can be summarised as follows with our response following.

Concern 1

Eastern fence along carpark be of solid construction for at least the portion across 3 Farnham Road (same as the current fence) to reduce noise transfer from the car park to the rear yard/house of properties at 4 and 4A Chatham Road Keswick. Additional landscaping within the fence may also help prevent noise transfer.

Response:

Please see **enclosed** an amended 'SITE PLAN / CARPARK PLAN' prepared by Studio Nine depicting both fencing and landscaping to address the representors concern.

The key points raised by the opposing representors are summarised **below** followed by our response.



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Adelaide SA 5000
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ISO 9001:2015 Certified
ABN 30 007 755 277
plan@masterplan.com.au

52318LET01



Concern 2

Taking away existing residential land that could fit townhouses for at least four families and replacing it with asphalt that will be unused most of the time goes against everything the 30-year plan for "Greater Adelaide" outlines. It actively discourages transit use, reduces housing density, increases CO² emissions and decreases housing affordability in one of the most impoverished inner suburbs.

Response:

The 30 Year Plan for Greater Adelaide is a State strategic document that is not specifically utilised by planning authorities to assess the merits of a given development application. The relevant planning tool for development assessment is the Planning and Design Code. Pursuant to the Planning and Design Code, the applicant maintains that the proposal accords with the intent of the Housing Neighbourhood Zone noting that the land is at the periphery of that zone adjacent the employment zone and abutting an existing ancillary car parking area.

As detailed in the planning report provided at lodgement, the proposed car park supports a land use (Snowys Outdoors) that is expressly envisaged in DTS/DPF 1.1 of the Housing Diversity Neighbourhood Zone, on the basis that it supports '*an active, convenient, and walkable neighbourhood*' (PO 1.1) and '*contributes to making the neighbourhood a convenient place to live without compromising residential amenity*' (DO 1).

The applicant has identified a need for additional parking as the current overflow is being spread throughout surrounding residential streets, and in doing so impacting negatively on the local amenity. The application proposes an additional 15 car parking spaces at the rear of Snowys Outdoors to alleviate demand on surrounding streets and residential amenity, of which we consider to accord with the intent of the Housing Diversity Neighbourhood Zone.

Concern 3

A carpark in the suburb is not maintaining the residential component of our suburb. The representor believes there is value in maintaining the aesthetic value and street appeal.

Response:

The proposal seeks to alleviate the high demand for parking created by Snowys, of which currently results in staff/customers parking their cars within the residential streets surrounding Richmond Road. The on-street parking demand is not considered to be a positive outcome for the business, nor the residential homes surrounding Richmond Road. In terms of expansion, Snowy's has limited ability to intensify their operations and the proposed parking spaces will simply satisfy the parking demands being experienced by the business. As such, there would be no future need to expand the parking area any further. We consider the proposal to be a positive response to an identified issue that is negatively impacting the existing Farnham streetscape amenity.

**Concern 4**

Dual access points into the site creates higher traffic risk to the community, and the section of road opposite Snowy's is commonly highly congested with poorly parked vehicles.

Response:

The property at 1 Farnham and 3 Farnham currently comprises one crossover each. The proposal does not increase the number of street crossovers that currently exist and thereby creates a neutral arrangement. The intent of the proposal is to alleviate the demand for on street parking, which is a concern acknowledged by the representor.

We note that the proposal does not intensify the existing operations of Snowys, in that all deliveries, truck movements and hours of operation will be maintained. Further, the additional parking is to cater for domestic vehicles only.

Concern 5

Traffic congestion on Farnham results largely from the workers from the surrounding businesses including Snowy's. The carpark that is proposed is for customer parking only, and the Snowy's carpark is not frequently full and it is more likely the workers from the surrounding businesses parking along Farnham.

Response:

The proposed car park is for both customer parking and staff parking. This is made clear in the planning report submitted with the development application. Whilst we understand the proposal will not resolve all of the existing parking congestion along Farnham, it will provide a significant increase in onsite parking for customers and staff of Snowy's. The proposal will at least assist in alleviating demand for on street parking throughout Farnham Street.

We note further that the image provided by the representor on Page 5 (suggested to be taken at 7.30 am on a weekday) indicates that the existing car park is essentially full for the exception of 3-4 spaces. Given the nature of activities occurring onsite (retail), the time in which customers park can be short with a high level of turn over. We do not consider a single image taken at one point in time to form sufficient evidence that the existing car park is infrequently used. The applicant has experienced high demand for onsite parking for many years by both staff and customers.



No intensification of existing activities is proposed; therefore, the applicant would have no need to construct additional parking if supply met demand. The car park will only be utilised by Snowy's customers and staff, of whom commonly use on-street parking. Given the above we maintain that the proposal is an appropriate response to an identify issue, of which is negatively impacting the business and the residential amenity of surrounding dwellings along Farnham Street.

Yours sincerely

Greg Vincent
MasterPlan SA Pty Ltd

enc: Amended 'SITE PLAN / CARPARK PLAN' prepared by Studio Nine.

Memo

From Richard Tan
Date 1/3/2022
Subject 21039722 - 1-3 FARNHAM RD KESWICK SA 5035, 94 RICHMOND RD KESWICK SA 5035

Zoe,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

1.0 Development within The 5% AEP Principal Flow Path

- 1.1 It is noted that a portion of the development is located within the 5% AEP (20 year ARI) flood footprint, with the overall site only impacted by the lowest category of anticipated flooding for the 1%AEP (100 year ARI) critical storm event. Under the new planning Code, the presence of the development within the 5% AEP flood footprint directly triggers provisions which strongly discourage any form of development within these areas. This new direct provision was not present or referenced in previous derivations of the Development Plan for our Council area. Council Administration are currently seeking further clarification in relation to the implementation of this new provision and the flexibilities around the consistent and appropriate implementation of it.

Based on the lower level of flood risk considered to be associated with this particular development site, and our current understanding of the planning provision, we would not consider it appropriate to recommend refusal of any development within the area of the site impacted by the 5% AEP flood footprint at this time.

2.0 Watercourse Impacts

- 2.1 For proposed developments adjacent to major watercourses, there are two typical considerations which Council's City Assets Department seek to be addressed within the planning assessment process.

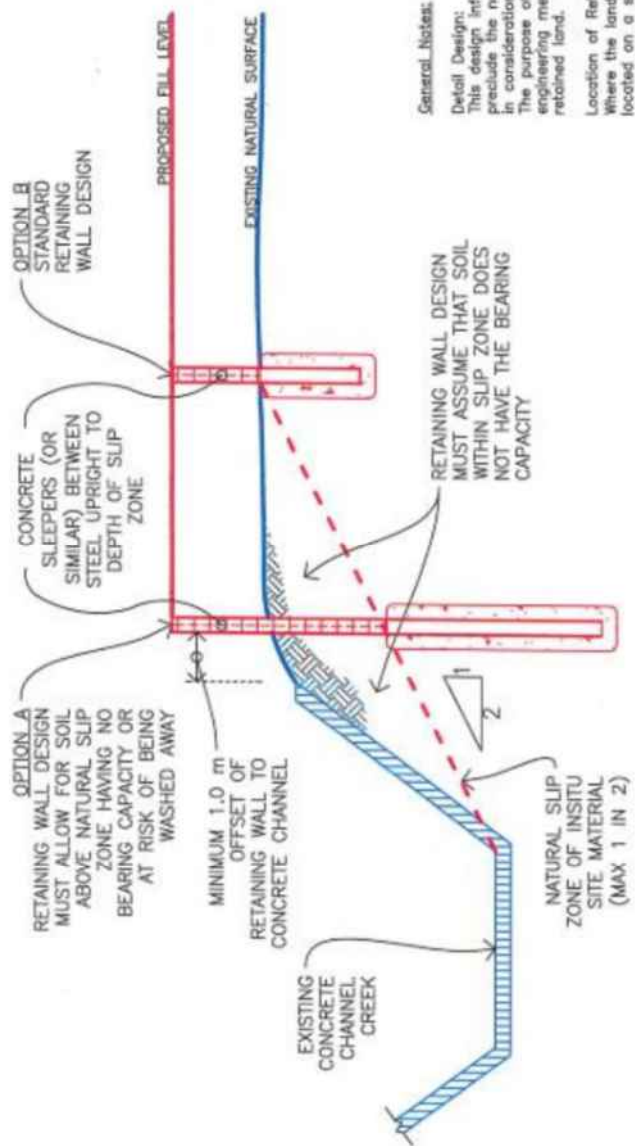
Firstly, the offset of any new substantial structure should be sufficiently separated from the creek banks to prevent the potential for creek erosion and movement from impacting the structures. In this location, the typical 10m offset from the centreline of the adjacent creek would be sought.

Given that this proposal is a parking area, hence the typical 10m offset from the centreline of the creek requirement will not be required, provided that a retaining wall as mentioned in the following has been provided.

Secondly, consideration needs to be given to the potential impacts and stabilised of any structures or retaining proposed to be constructed on or within proximity of the creek and its banks.

Due to the proximity of the proposed parking area to the creek, a retaining wall will be required. It is probable that these items could interfere with the existing banks and shaping of the creek, and as appropriate consideration needs to be given to the design and scale of these works.

The diagram below outlines the scale of retaining which typically need to be considered within proximity of a watercourse.



General Notes:

Detail Design:
 This design information is generic in nature and does not preclude the necessity for detailed design of retaining structures in consideration of the specific conditions of each installation. The purpose of this diagram is to demonstrate the scale of engineering measure necessary to secure stable and usable retained land.

Location of Retaining Wall in Relation to Property Boundary:
 Where the land to be developed, filled and/or stabilised is located on a separate land title from the creek, it must be ensured that the placement of the retaining wall structure enables all construction related works to be undertaken without encroachment onto the land title containing the creek. This is a minimum requirement, greater offset from creek may be required in complying with this diagram).

Type of Retaining Structure:
 This arrangement demonstrates the requirements for stabilising land and protecting existing watercourses utilising a basic retaining wall concept. Other forms of creek bank stabilisation may be appropriate for utilisation and may reduce the offset between the creek and structures. Detailed engineering design of such structures would be required to demonstrate usable stable land for Development Application purposes.

**BASIC RETAINING WALL
 LOCATION AND OFFSET TO
 CONCRETE LINED CREEK
 (SHALLOW)**

DRAWING "NOT TO SCALE"



8 JUNE 2016 - DWG 1516.002

It is recommended that, based on creek survey information, the applicant provide further detail on the proposed location, scale and construction technique for the proposed retaining wall elements to be located with proximity of the creek.

3.0 FFL Consideration – Finished Floor Level (FFL) Requirement

3.1 Not applicable

4.0 Verge Interaction

4.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically desired to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram ramps, etc. (as measured at the kerb line, except for driveway separation which will be measured from property boundary). An absolute minimum offset of 0.5m from new crossovers and stormwater connections to other existing road verge elements is acceptable in cases where space is limited.

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council's Technical Officer (Arboriculture).

4.1.1 The proposed southern crossover is less than 2m offset from an existing street tree

It is recommended that further assessment from Council's Arboriculture team is required.

4.1.2 A stobie pole within the property boundary is to be removed. It is unclear if this stobie pole belongs to SAPN or the owner of the property. If this stobie pole is own by SAPN, then evidence must be provided to Council indicating that SAPN has agreed to the removal of the pole and that any relevant expenses are borne by the developer.

The applicant should provide correspondence from the appropriate authority indicating that the existing stobie pole can be removed and should confirm that any relevant expenses would be borne by the developer.

- 4.1.3 The northern crossover should be separated from the existing crossover servicing the building. The offset between the crossovers is required to be a minimum of 1m (measured at the front property boundary). The 1m separation allows for a pedestrian refuge. The extension and separation of the driveway crossover shall be undertaken prior to the completion of any building works at the expense of the applicant.



It is recommended that revised plans indicating satisfaction to the above requirements should be provided to Council.

- 4.1.4 While discharging site stormwater for the additional parking area through existing stormwater system is supportable, it is important that the existing stormwater connection/system should be indicated on provided civil plan. Based on the civil plan (note: it is unclear if this is the final approved plan, as there is no record of other civil plan) as recorded in Objective for previous DA 211/1431/2006 for the site, the site's stormwater connection location (in red circle) does not seem to match the actual site situation.

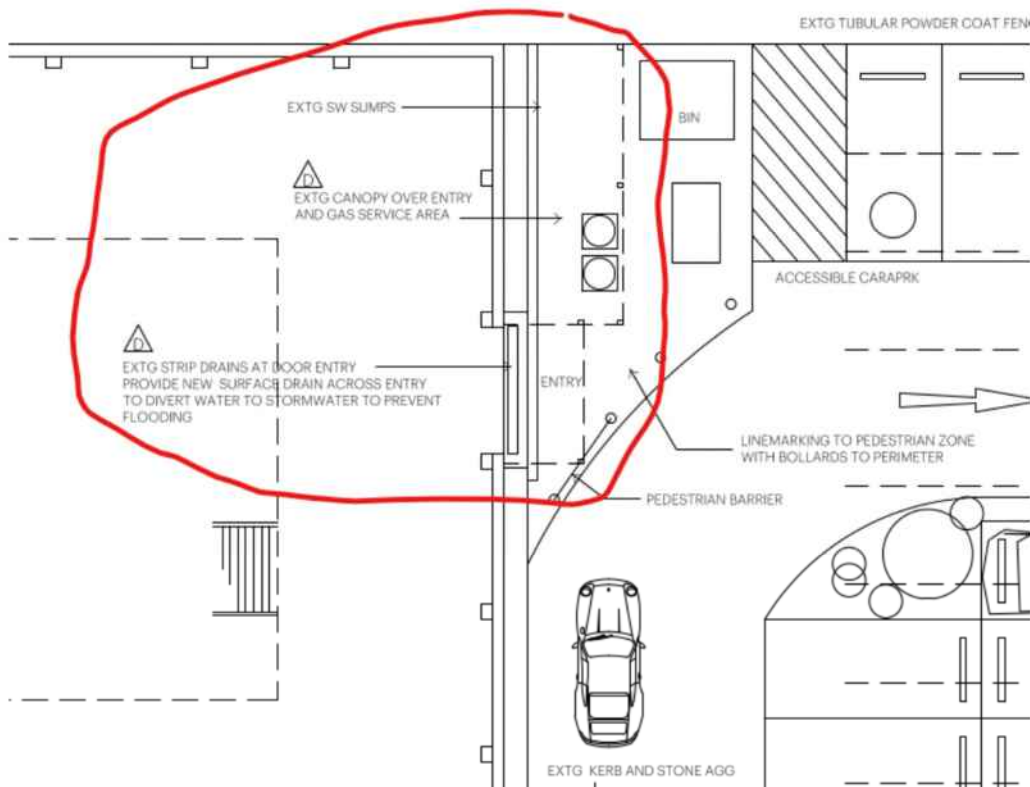
Based on the provided site plan/carpark plan, the proposed one way circulation car park layout is supportable. To further reinforce the one-way circulation, the following changes is recommended:

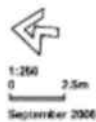
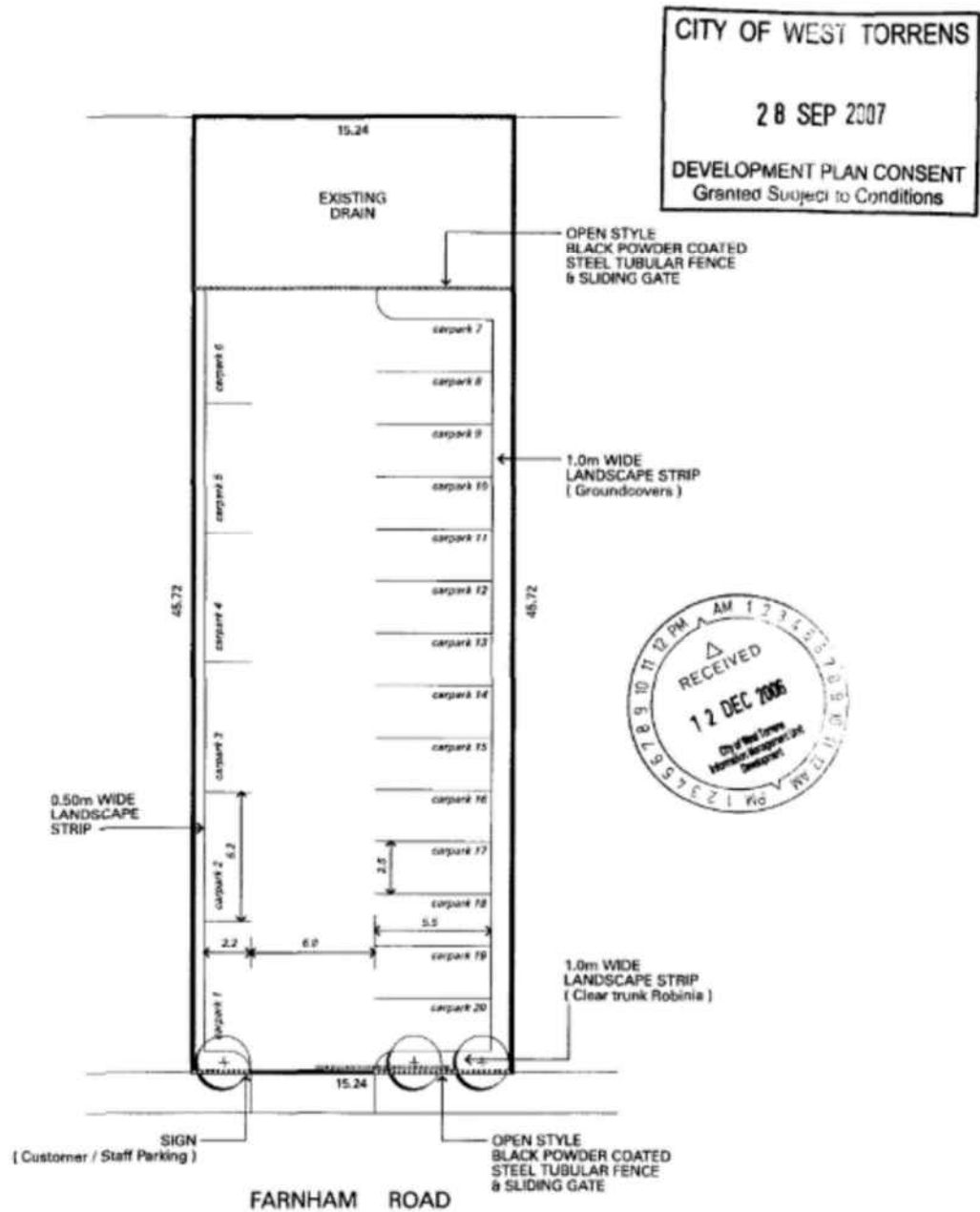
- The entry and exit crossover is to be 4m wide with no flaring; as the exit crossover has been reduced to a single width crossover, it can easily achieved a 2m offset from existing street tree.
- Traffic signage (ie: entry only, exit only, directional signage etc) and linemarking should be installed on site;

It should be conditioned/noted that the crossover should be constructed as per requirements which can be downloaded at <https://www.westtorrens.sa.gov.au/files/sharedassets/public/objective-digitalpublications/external-website/building-and-development/guidelines-for-driveway-crossing-places.pdf>

5.3 Other

While the provided plan has indicated that the area in red is existing structures, however, this does not match the approved plan in previous DA 211/1431/2006. I leave this to the planners consideration on whether the notation on provided plans should be amended and whether or not a compliance issue will arise if this DA is withdrawn or rejected.





PROPOSED SITE PLAN
 1 FARNHAM ROAD
 KESWICK

6.0 Waste Management

6.1 Due to the nature of this application, it is recommended that further assessment from Council's Waste Management Team is required. I also noted that based

on West Map, the site is service by Council's waste service. If private waste service is required for this site, then the crossover will be widened to 4m with 0.3m flaring on both sides, for MRV to be able to access to the site. Some parking restriction along Farnham Road may also be required.

It is recommended that further assessment from Council's Waste Management Team is required.

7.0 Stormwater Management

7.1 Stormwater Detention

Stormwater calculation has been provided. It is noted that the stormwater requirements adopted does not meets Council's standard.

Council requires all stormwater detention measures to be undertaken to limit the peak discharge rate for the site critical 20 year ARI storm event to equivalent to a predevelopment arrangement with a 0.25 runoff coefficient. In this case, the predevelopment peak discharged rate is calculated at $0.25 \times 121 \times 562 / 3600 = 4.7\text{L/s}$.

It should also be noted that while surface detention is supportable in a 20 year ARI critical storm event, further demonstration should be provided to indicate that surface ponding will not occur in a 5 year ARI critical storm events.

It is recommended that revised calculation indicating satisfaction to the above requirements should be provided to Council.

7.2 Stormwater Quality

Given that the carpark is relatively small, although the Ecosol RSF 4200 is unlikely able to assist in reducing the site's stormwater pollutant to Council's requirements (ie: nitrogen and phosphorus removal rate does not meet target), however, in this case, the proposed Ecosol RSF 4200 has been assessed as satisfying minimum requirements.

Regards
Richard Tan
Civil Engineer

Memo

From Richard Tan
Date 5/5/2022
Subject 21039722 - 1-3 FARNHAM RD KESWICK SA 5035, 94 RICHMOND RD KESWICK SA 5035

Zoe,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

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- 1.1 It is noted that a portion of the development is located within the 5% AEP (20 year ARI) flood footprint, with the overall site only impacted by the lowest category of anticipated flooding for the 1%AEP (100 year ARI) critical storm event. Under the new planning Code, the presence of the development within the 5% AEP flood footprint directly triggers provisions which strongly discourage any form of development within these areas. This new direct provision was not present or referenced in previous derivations of the Development Plan for our Council area. Council Administration are currently seeking further clarification in relation to the implementation of this new provision and the flexibilities around the consistent and appropriate implementation of it.

Based on the lower level of flood risk considered to be associated with this particular development site, and our current understanding of the planning provision, we would not consider it appropriate to recommend refusal of any development within the area of the site impacted by the 5% AEP flood footprint at this time.

2.0 Watercourse Impacts

- 2.1 No further assessment provided. Item still considered outstanding.

For proposed developments adjacent to major watercourses, there are two typical considerations which Council's City Assets Department seek to be addressed within the planning assessment process.

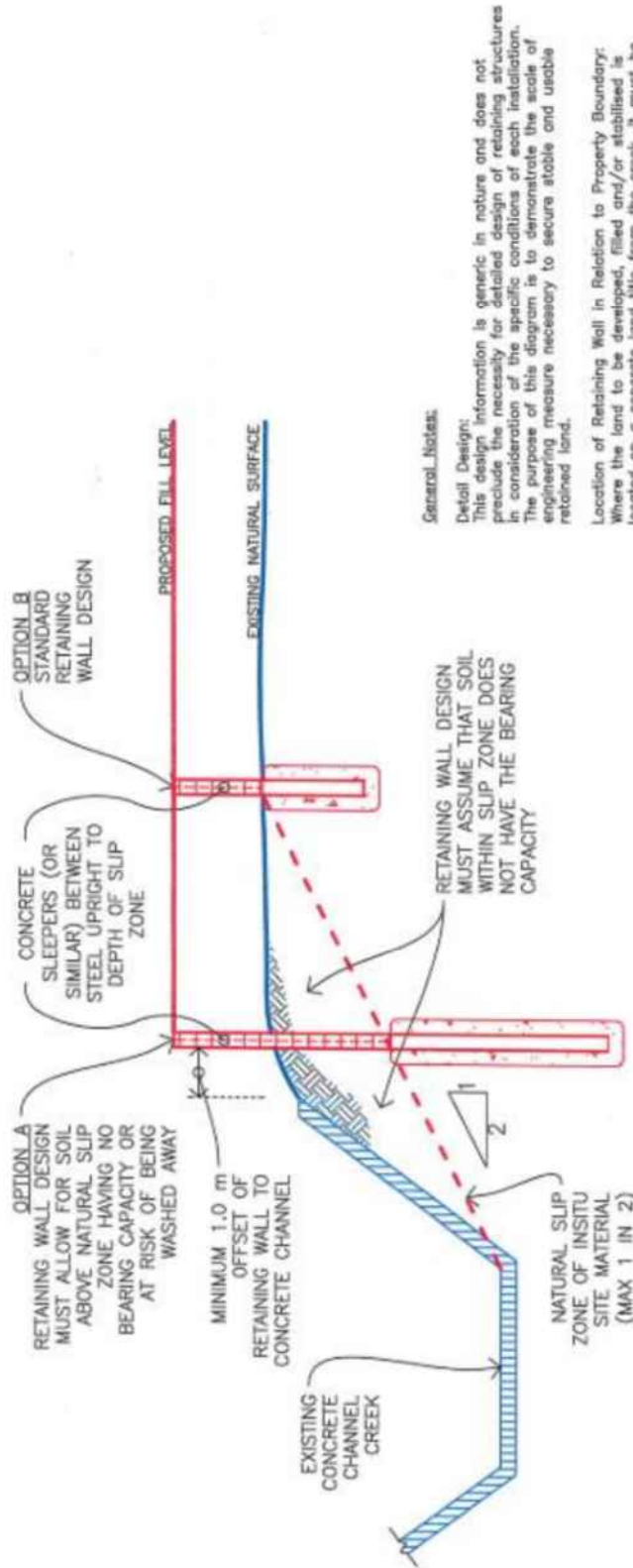
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**BASIC RETAINING WALL
LOCATION AND OFFSET TO
CONCRETE LINED CREEK
(SHALLOW)**

DRAWING 'NOT TO SCALE'



8 JUNE 2016 - DWG 1516.002

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4.1.1 Issue resolved

4.1.2 Stobie pole has been remained

4.1.3 Issue resolved

4.1.4 The planning response has indicated

2.1.4: TMK have advised that stormwater discharge is only proposed via the existing channel, as demonstrated on their proposed plans.

However, the provided civil plan has not indicated the above. Item still considered outstanding.

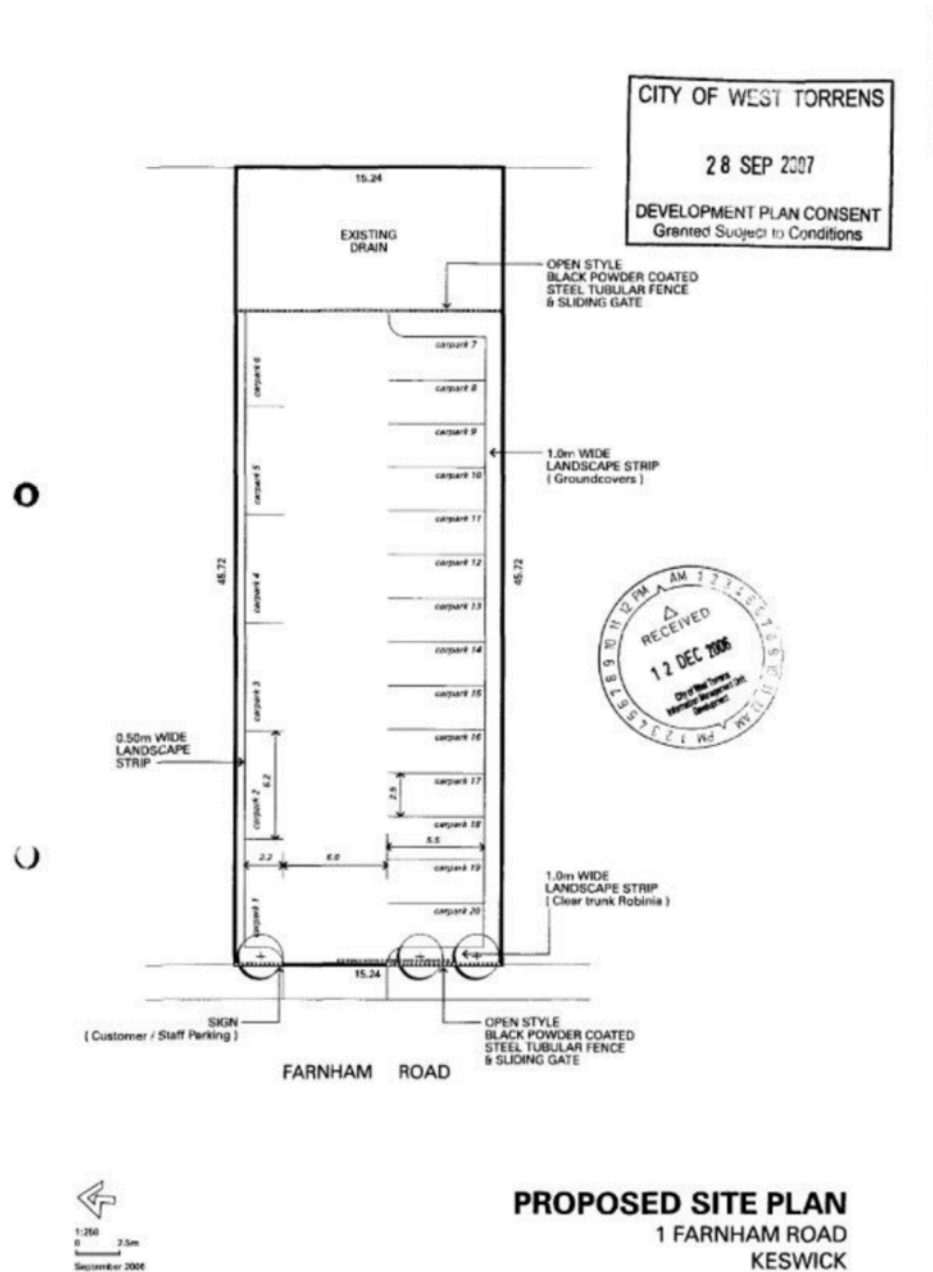
It is recommended that civil plan indicating satisfaction to the above requirements should be provided to Council.

4.2 The redundant kerb has been indicated to be reinstated on revised plans.

5.0 Traffic Requirements

5.1 Parking Assessment

Based on the approved stamped site plan provided in DA 211/1431/2006, there are 20 parking spaces in total.



The proposed development with 33 parking spaces without increasing the building floor area has been assessed as satisfying parking requirements.

5.2 Parking Layout

The planning letter has indicated that

- 3.1: Noted, albeit crossovers have been amended to 4 metres in wide and have been flared in accordance with Item 4.1 for vehicle access. Traffic signage has also been noted on the proposed drawings.

However, the civil plan has not demonstrated the above. Given that it is confirmed that the site will be serviced by a MRV truck, hence both the crossover will be supported to be 4m wide with 0.3m flaring on both sides.

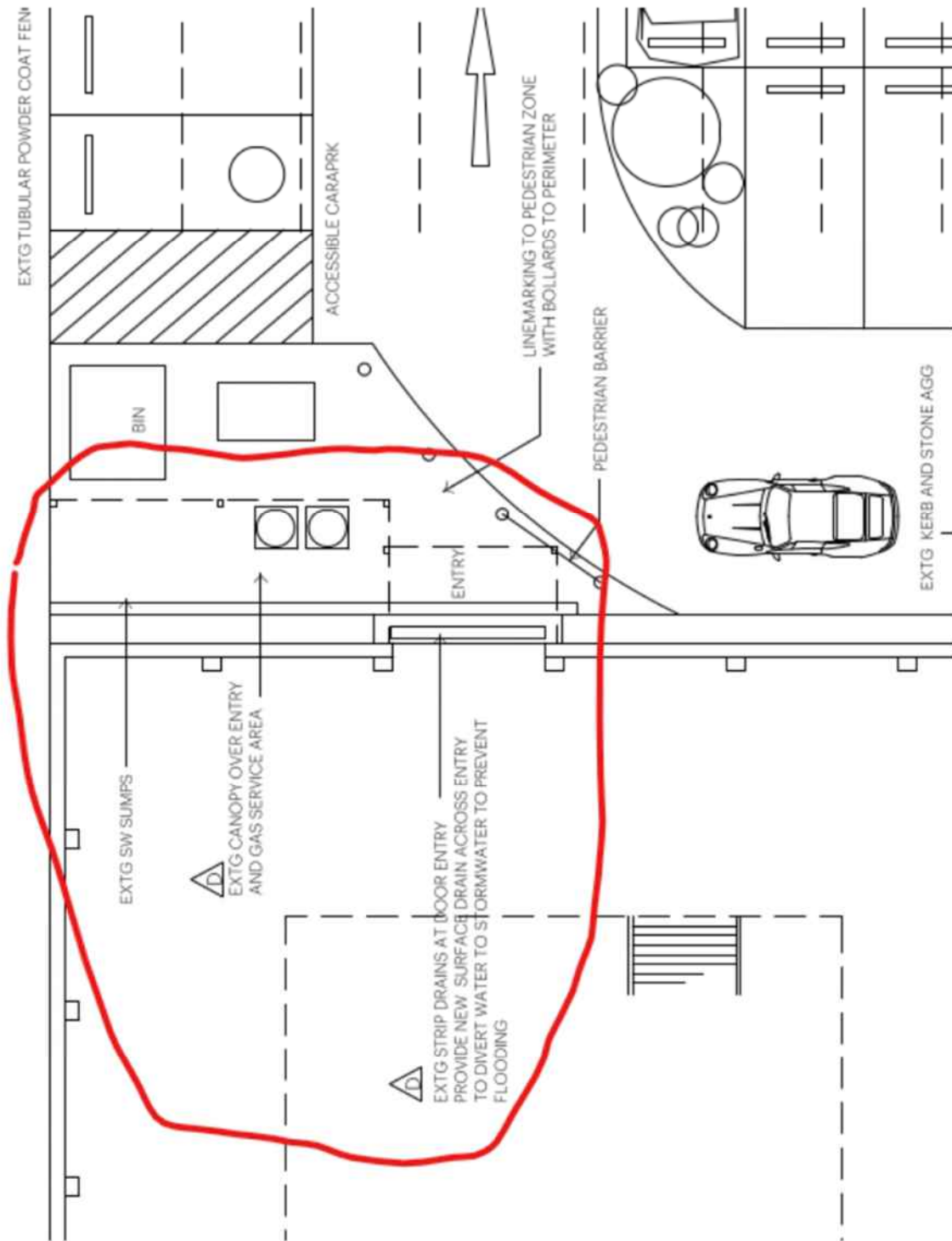
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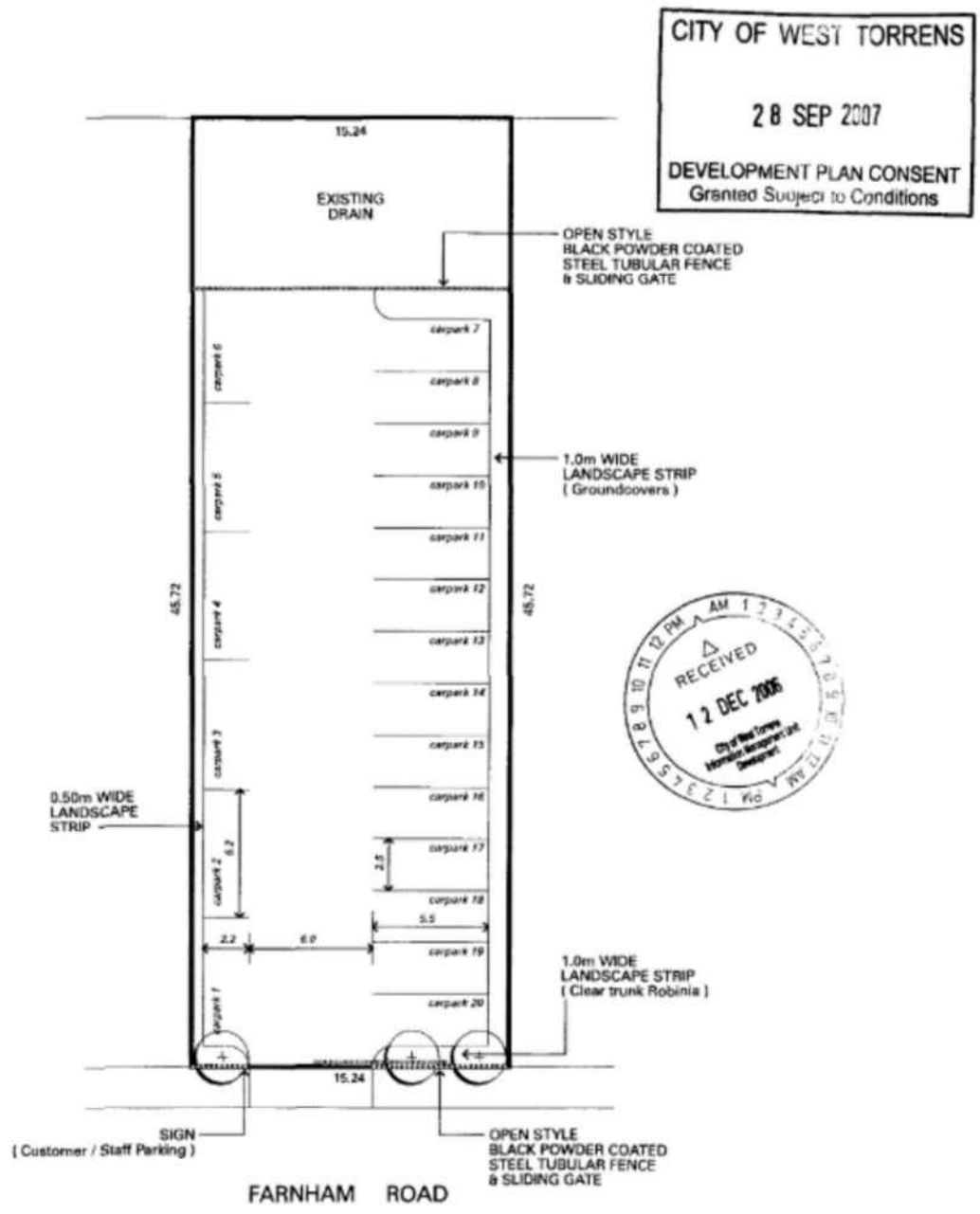
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It should be conditioned/noted that the crossover should be constructed as per requirements which can be downloaded at <https://www.westtorrens.sa.gov.au/files/sharedassets/public/objective-digitalpublications/external-website/building-and-development/guidelines-for-driveway-crossing-places.pdf>

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CITY OF WEST TORRENS
 28 SEP 2007
 DEVELOPMENT PLAN CONSENT
 Granted Subject to Conditions



PROPOSED SITE PLAN
 1 FARNHAM ROAD
 KESWICK

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7.0 Stormwater Management

7.1 Stormwater Detention

Stormwater detention has been assessed as satisfying minimum requirements.

7.2 Stormwater Quality

Given that the carpark is relatively small, although the Ecosol RSF 4200 is unlikely able to assist in reducing the site's stormwater pollutant to Council's requirements (ie: nitrogen and phosphorus removal rate does not meet target), however, in this case, the proposed Ecosol RSF 4200 has been assessed as satisfying minimum requirements.

Regards
Richard Tan
Civil Engineer

6.2.2 11-17 James Congdon Drive, MILE END

Application No 22000566

Appearing before the Panel will be:

Representor: **Andrew Mimidas** of 11 Rose Street, Mile End wishes to appear in support of the representation.

Applicant: **Andrew Raeburn** of Aecom Australia Pty Ltd wishes to appear in response to the representation.

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT NUMBER	22000566
APPLICANT	Kennards Self Storage
ADDRESS	11-17 James Congdon Drive, MILE END
NATURE OF DEVELOPMENT	Alterations and additions to existing self storage facility comprising 90 additional single storey self storage units, conversion of existing ground level office space to 26 storage units, variation to conditions 2, 3, and 6 in DA 211/984/2015 to allow for 24/7 access to the site, relocation of access, reconfiguration of on-site parking, associated landscaping and construction of a 3m high acoustic wall along portion of the western boundary
ZONING INFORMATION	<p>Zones</p> <ul style="list-style-type: none"> • Established Neighbourhood • Urban Corridor (Boulevard) <p>Overlays</p> <ul style="list-style-type: none"> • Aircraft Noise Exposure • Airport Building Heights (Regulated) • Affordable Housing • Building Near Airfields • Character Area • Design • Hazards (Flooding - Evidence Required) • Major Urban Transport Routes • Noise and Air Emissions • Prescribed Wells Area • Regulated and Significant Tree • Stormwater Management • Traffic Generating Development • Urban Tree Canopy <p>Technical Numeric Variations (TNVs)</p> <ul style="list-style-type: none"> • Maximum Building Height (Metres) (Maximum building height is 32.5m) • Minimum Site Area (Minimum site area for a detached dwelling is 270 sqm; semi-detached dwelling is 270 sqm; row dwelling is 270 sqm) • Maximum Building Height (Levels) (Maximum building height is 1 level)

	<ul style="list-style-type: none"> • Minimum Primary Street Setback (Minimum primary street setback is 0m) • Minimum Side Boundary Setback (Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher) • Interface Height (Development should be constructed within a building envelope provided by a 30 degree plane, measured 3m above natural ground at the boundary of an allotment)
LODGEMENT DATE	25 January 2022
RELEVANT AUTHORITY	Council Assessment Panel
PLANNING & DESIGN CODE VERSION	2022.1
CATEGORY OF DEVELOPMENT	Code Assessed – Performance Assessed
NOTIFICATION	Yes
REFERRALS STATUTORY	Nil
REFERRALS NON-STATUTORY	Internal <ul style="list-style-type: none"> • City Assets • Arboriculture Advisor
DELEGATION	CAP <ul style="list-style-type: none"> • A representor has lodged a valid representation and wishes to be heard
RECOMMENDING OFFICER	Rachel Knuckey
RECOMMENDATION	Grant consent with reserved matters and conditions

BACKGROUND

It is noted that the subject land was approved as a self-storage facility in 2016 via Development Application 211/984/2015 which included the following description:

- Establishment of a self-storage facility with associated car parking, plus minor building modifications including demolition of shed and construction of a free-standing storage building and fencing/gates.

Conditions 2 and 6 of Development Application 211/984/2015 restrict the use of the facility to the following hours:

- 24-hour access to the storage units on the ground floor of the main facility (as identified on the approved plans); with the remainder of the main facility being restricted to 8:30am – 5:00pm Monday to Friday and 8:30am – 2:00pm on Saturday.
- 7:00am and 7:00pm Monday to Friday for the free-standing external storage building.

The free-standing external storage building referred to in Condition 6 has not been constructed.

Additional approvals were granted for signage (DA 211/409/2016) and internal office fit-out (DA 211/1130/2019).

SUBJECT LAND AND LOCALITY

The subject land is commonly known as 11-17 James Congdon Drive, Mile End and is more formally described as:

- Allotment 1 in Deposited Plan 64577 – Certificate of Title Volume 5922 Folio 940
- Allotment 4 in Deposited Plan 64577 – Certificate of Title Volume 5922 Folio 943
- Allotment 5 in Deposited Plan 64577 – Certificate of Title Volume 5974 Folio 676
- Allotment 6 in Deposited Plan 64577 – Certificate of Title Volume 5974 Folio 677

A Land Management Agreement (LMA) is noted on the Certificate of Titles. In general terms, the LMA seeks to ensure that the four parcels which comprise the subject land are used in a coordinated manner including the provision of parking areas with unrestricted vehicular access from the surrounding streets.

With a total area of 7,270 square metres (m²), the subject land is irregular in shape and features three road frontages and access arrangements as follows:

- 67 metres to James Congdon Drive with a single two-way access point;
- 85 metres to Goodenough Street with a single entry point and single exit point; and
- 17.5 metres to Rose Street with a single access point (exit only).

The site currently contains a large two-storey commercial building which is located in the north-eastern corner of the site on the boundary of Goodenough Street and James Congdon Drive. The ground floor of the building contains a number of self storage units and associated office while the upper level is currently unoccupied, but was previously used as an office.

The site is relatively flat and is completely covered in impervious surfaces. Accordingly, there is no vegetation of significance on the subject site.

Of relevance, the subject site is located within two different Zones as per the Planning and Design Code. More specially, the majority of the site (Lots 4 and 5) are within the Urban Corridor (Boulevard) Zone which runs along James Congdon Drive while the western side of the site (Lots 1 and 6) are located in the Established Neighbourhood Zone. It is noted that, currently, the portion of the site that is within the Established Neighbourhood Zone is generally used for vehicle movements, car parking and for the external storage of machinery and equipment in association with the overall 'storage' use of the site. A corrugated iron fence with a height of approximately 1.5m to 1.8m is currently located on the western boundary of the subject land.

The locality is mixed in character and nature with a number of relatively large non-residential uses fronting James Congdon Drive and residential development generally in the form of single-storey detached dwellings and group dwellings to the west of the subject site. The non-residential land uses in the locality include the Sarah Homes Display Centre immediately adjoining the site to the south and Clinpath Pathology on the northern side of Goodenough Street. To the east, on the other side of the four-lane James Congdon Drive, is the Adelaide Park Lands.

The subject land and locality are shown on the aerial imagery and maps below.



Figure 1 - Subject Site, viewed from Goodenough Street



Figure 2 - Subject Site, viewed from Rose Street



Figure 3 – Existing interface with residential area viewed from Rose Street



Figure 4 – Existing interface with residential area viewed from Goodenough Street

PROPOSAL

The proposed development seeks a range of alterations and additions to the existing self-storage facility. More specifically, the proposed development involves:

- The construction of 90 self-storage units to be located around the subject site as follows:
 - 38 units with floor areas ranging from 9m² to 45m² and heights up to 3.5 metres will be placed 'back to back' on the existing hardstand area to the south of the main building and close to the main entrance from Goodenough Street;
 - 10 units with floor areas ranging from 9m² to 11.7m² and heights of 3.2 metres will be placed on the existing car parks along the western boundary of Lot 6;
 - 24 units with floor areas ranging from 16.5m² to 43m² and heights of 3.2 metres will be placed on both sides of the existing access to Rose Street on Lot 1;
 - 18 units with floor areas of 18m² and heights of 3.51 metres will be placed on the southern side of the existing building on the existing car parks;
- The installation of 26 storage units within the eastern portion of the ground floor of the existing building with floor areas ranging from 4m² to 18m²;
- Vehicular access arrangements will be as follows:
 - An entrance and exit with sliding gates from Goodenough Street (as per the existing crossovers) which will be separated by the proposed 'back to back' storage units;
 - An exit with sliding gate to Rose Street via a relocated crossover which will require the removal of a street tree;
 - An exit with sliding gate to James Congdon Drive as per the existing arrangements;

- A reduction in the number of onsite car parking spaces (to allow for the placement of the storage units) with the retention of 9 parking spaces in various locations around the site;
- Erection of a 3 metre high acoustic wall along portions of the western property boundary where storage units will not be located;
- Provision of landscaping beds alongside all of the site's access points; and
- Amendments to the approved operating hours of the facility, to allow 24-hour access to the entire site, seven days a week.

For the purposes of an assessment the proposal is broken down into elements. Each element will have an assessment pathway as set out in the Planning and Design Code.

Elements	Application Category
Alterations and additions to existing self storage facility, including change in operating hours	Performance Assessed
Acoustic Wall/Fence	Performance Assessed

The relevant plans and documents are contained in **Attachment 1**.

PUBLIC NOTIFICATION

The application required public notification because it was performance assessed and not exempt from notification by *Table 5 - Procedural Matters* of the Established Neighbourhood Zone and the Urban Corridor (Boulevard) Zone in the Planning and Design Code (The Code).

Properties notified	64 properties were notified during the public notification process.
Representations	Two representations were received.
Persons wishing to be heard	<p>One representor wishes to be heard.</p> <ul style="list-style-type: none"> • Andrew Mimidas of 11 Rose Street, Mile End •
Summary of representations	<p>Concerns were raised regarding the following matters:</p> <ul style="list-style-type: none"> • The existing use of the site for the storage and movement of heavy vehicles and other equipment is causing noise impacts and creates a land use conflict with the adjoining residential area. • The proposed development does not adequately address the appropriateness of storage units within the Established Neighbourhood Zone. • The proposed development does not adequately address potential noise impacts on adjoining land uses, namely residential dwellings. • The proposed development does not adequately address operational matters and their impact on adjoining land uses • The proposed development does not adequately detail mitigation measures relating to these impacts.

Applicant's response to representations	<p>Summary of applicant's response:</p> <ul style="list-style-type: none"> • The proposed development will address existing noise and disturbance issues by ending the storage of trade vehicles, small trucks and trade equipment on the site and by reducing the transmission of noise through the placement of storage units and acoustic fencing along the boundary. • The proposed development does not change the longstanding commercial use of the land • Noise impacts were assessed as acceptable by Resonate subject to the erection of a 3m high acoustic fence along the western boundary. • Operation of the facility will be carefully managed through controlled access gates, CCTV monitoring and short-term contracts with customers.
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A copy of the representations and the applicant's response is contained in **Attachment 2**.

INTERNAL REFERRALS

Nil

Department	Comments
City Assets	<ul style="list-style-type: none"> • Finished Floor Levels (FFL) should be provided to ensure that the development is a minimum of 350mm above the adjacent street water table. • An amended site plan should be provided showing all existing and proposed verge features. • Recommends that an additional two car parking space be shown on the plans to cater for the additional storage units within the main building • Swept path diagrams shown that a Medium Rigid Vehicle (MRV) can enter, exit and circulate around the site. • A Condition of Consent should be included to restrict service vehicles to no greater than a MRV • The additional traffic generated by the proposal will be relatively low and will be acceptable. • Further details are required in relation to the management of waste. • Roof runoff from the storage units should be directed to the surface and discharged via the existing stormwater system.
Arboriculture Advisor	<ul style="list-style-type: none"> • The existing street tree on Rose Street can be removed subject to the payment of \$1,500 to be invested in the Council's annual Greening Program. • The proposed landscape beds should include the planting of trees.

EXTERNAL REFERRALS

Nil

A copy of the relevant referral responses is contained in **Attachment 3**.

RELEVANT PLANNING & DESIGN CODE PROVISIONS

The subject land is located within the Urban Corridor (Boulevard) Zone and Established Neighbourhood Zone as described in the Code. The subject land is also affected a series of Overlays and Technical Numeric Variations (TNVs).

ASSESSMENT

In assessing the merits or otherwise of the application, only those quantitative requirements that do not meet the Code requirements will be discussed along with the respective qualitative provisions. The proposed development is therefore discussed under the following sub-headings:

Land Use

In relation to land use, the Urban Corridor (Boulevard) Zone seeks:

A vibrant mix of land uses adding to the vitality of the area and extend activities outside shop hours including restaurants, educational, community and cultural facilities and visitor and residential accommodation. (PO 1.1)

In contrast, the Established Neighbourhood Zone seeks:

Predominantly residential development with complementary non-residential activities compatible with the established development pattern of the neighbourhood. (PO 1.1)

While neither Zone specifically anticipates a storage facility, it is acknowledged that the subject site has been used for this purpose for a number of years. Given that the 'storage' use has already been established, it is important to consider whether or not the proposed intensification of the use is appropriate. With this in mind, it is noted that 34 of the proposed 90 new storage units will be placed on the portion of the site zoned Established Neighbourhood. It is also noted that the storage units will replace the existing car parks which currently run along the western boundary of the site and which appear to be creating a negative impact on the adjoining residential area in terms of noise.

While the placement of an additional 34 storage units within the portion of the site zoned Established Neighbourhood represents a significant intensification of the 'storage' use of the land, it is also noted that the potential impacts in terms of noise and visual appearance will be managed more effectively than the current situation. More specifically, the existing 1.5m to 1.8m high (approximately) corrugated iron fence along the western boundary will be replaced with a combination of 3m high acoustic fence and the rear walls of the storage units. This will create a more effective visual and noise barrier to the activities occurring on the subject site. On this basis, and noting that the existing traffic circulation arrangements will be retained, the further intensification of the 'storage' use of the subject site is unlikely to have a detrimental impact on the adjoining residential area to the west. Therefore, the proposed development is considered acceptable from a land use perspective.

Built Form and Character

In terms of built form, it is noted that some of the additional storage units will be visible from the surrounding streets and adjoining residential properties. However, the storage units will be modest in height (up to a maximum of 3.5m) and views from the streets will be softened by the proposed landscape beds and plantings. On this basis, the proposed additional storage units will not have a detrimental impact on the streetscape and will complement the existing substantial and prominent building on the subject site.

In terms of street setbacks, the proposed storage units on the primary frontage (James Congdon Drive) will be setback slightly from the existing building to allow for the introduction of a landscape bed. Similarly, the storage units fronting Goodenough Street and Rose Street will be setback from the streets to accommodate landscaping. While it is noted that the 1.56m (minimum) setback to Rose Street means that the storage units will sit forward of the adjoining dwelling to the west, the setbacks will be consistent with a number of existing buildings within the Sarah Homes Display Centre to the east. On this basis, and given that additional landscaping will be introduced to soften views of the storage units, the proposed development is considered acceptable from a built form and character perspective.

Boundary Development

As noted previously, an acoustic fence as well a number of the storage units will be located along the western boundary of the site. The acoustic fence will be 3 metres in height while the rear walls of the storage units will be 2.91 metres in height. Further, the elevations indicate that the fence will be 'Slate Grey' while the rear walls of the storage units will comprise metal sheeting coloured 'Colorbond Astro Metallic' (see screenshots of the western elevations below).

While the Established Neighbourhood Zone generally seeks buildings that are setback from side and rear boundaries (POs 7.2 and 8.1), the proposed siting of the storage units on the boundary is considered acceptable given that they will be of a similar height and will be constructed of a similar material as the acoustic fence. In other words, when viewed from the adjoining properties to the west, the storage units will appear as if they are a continuation of the acoustic fence.

It is also noted that the acoustic fence and the rear walls of the storage units will be slightly higher than a typical 2.1 metre high boundary fence. However, this additional height of approximately 0.8 metres is considered acceptable given the context of the existing non-residential land uses on the subject site and given that the fence and the storage units are intended to form an acoustic barrier to protect the amenity of the adjoining dwellings to the west.

Transport, Access and Parking

At the outset, it is noted that the proposed development seeks to retain nine dedicated (formal) car parking spaces around the subject site which will continue to cater for the existing storage activities occurring in the main building. It is also noted that parking for the additional storage units which will be located outside the main building will be provided via an informal arrangement whereby cars will 'pull up' in front of individual storage units rather than parking in dedicated spaces.

This arrangement will be facilitated by the relatively wide one-way internal driveways around the storage units which will allow a vehicle to safely pass another vehicle which may be parked in front of a storage unit.

With the above in mind, and given that the storage units require direct vehicular access, it is not considered necessary for the development to provide additional car parks for the new storage units located outside the main building. Further, Stantec Traffic Consultants have assessed the car parking requirements of the development and note that the proposed additional 1,750m² of storage would normally generate a need for 9 car parking spaces (based on the Planning and Design Code's requirements for 0.5 spaces per 100m² of floor area for a store). However, given that cars will park informally in front of the storage units for a limited amount of time, Stantec do not consider that additional dedicated parking spaces are required.

While the Council's Traffic Engineer generally supports Stantec's assessment, he also notes that an additional 26 storage units will be installed within the existing building where they will not be accessible to cars. Therefore, customers using these additional storage units will need to park elsewhere on the site and walk to the units. On this basis, the Council's Traffic Engineer has recommended that an additional two car parking spaces be provided on the site. While the applicant has indicated that they would prefer not to increase the number of car parks, they have also indicated that they would accept a condition requiring the inclusion of an additional car parking spaces. Given that there appears to be sufficient space near the centre of the site to include additional car parking, is considered appropriate to include a reserve matter requiring that these spaces be identified on the plans prior to Development Approval being issued.

In terms of traffic movements into and within the site, Stantec has provided swept paths which demonstrate that vehicles up to an 8.8 metre long Medium Rigid Vehicle (MRV) can access and circulate within the site appropriately. Further Stantec estimate that the additional storage units will only result in an additional 6 to 9 vehicle movements during the peak hour. While these additional vehicle movements will all enter via Goodenough Street, vehicles then have the option of leaving the site via three different exits. This means that additional vehicle movements leaving the site will be spread over three different exits, thereby reducing the impact on adjoining streets.

Interface between Land Uses

Of particular relevance to the assessment of the proposed development is the potential impact on the adjoining residential area in terms of appearance and noise. In this regard, it is noted that, while the development is not proposing to change the use of the land, the proposal represents a significant intensification of non-residential development within the Established Neighbourhood Zone. This intensification includes additional built-form as well as increased hours of operation which would enable the facility to operate 24 hours a day, seven days a week. However, it is also noted that the activities related to the storage units are likely to be relatively benign and will typically only involve infrequent car movements associated with customers delivering or picking-up goods and materials stored in the individual units. In effect, the impact associated with these car movements is likely to be less than the existing movements on the subject land which involve a range of heavy vehicles which are currently being stored on the land.

In addition, it is noted that the potential transmission of noise from the subject land to the adjoining residential area will be minimised through the incorporation of a 3 metre high acoustic wall as well as by the placement of a number of the storage units along the western boundary. In this way, an effective barrier will be created along the western boundary to minimise the transmission of noise.

The proposed acoustic treatment along the western boundary also sufficiently addresses the proposed increase in the hours of operation of the facility. More specifically, while there will be a minor increase in traffic movements relating to the storage facility during the night, the impacts associated with these movements will be minimised by the treatment along the western boundary.

Further, Resonate has prepared an Environmental Noise Assessment of the proposed development which concludes that, subject to the installation of the proposed acoustic fence, the storage units will operate within the relevant requirements of the Environment Protection (Noise) Policy.

For the reasons outlined above, the proposed development appropriately addresses the relevant Interface between Land Uses provisions of the Planning and Design Code and is likely to result in a reduction in the transmission of noise which is currently associated with the storage of heavy vehicles, machinery and equipment on the subject site.

Landscaping

Currently, the subject site features very little if any landscaping. The applicant submitted landscape plans following the public notice period, and subsequently amended landscape plans were submitted. Accordingly, the proposed landscape beds at the various access points to the site represents an improvement over the current situation. On this basis, the proposed landscaping is considered acceptable.

SUMMARY

Having considered all the relevant provisions of the Planning and Design Code, the proposal is considered to be not seriously at variance with the Planning and Design Code Version 2022.1 dated 20 January 2022.

Notwithstanding the proposed intensification of non-residential activities in the Established Neighbourhood Zone, the proposed development satisfactorily accords with the relevant provisions of the Planning and Design Code. In particular, the proposed development will incorporate a number of measures which are likely to reduce the issues raised by the representors in relation to the transmission of noise. In addition, the introduction of landscaped beds at the various access points to the site will improve the streetscape appearance while also softening views of the new storage units.

On balance, the proposal reasonably satisfies the relevant provisions of the Planning and Design Code Version 2022.1 and therefore the application warrants the granting of Planning Consent, subject to specified reserved matters and conditions.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107 (2)(c) of the *Planning Development and Infrastructure Act 2016*, and having undertaken an assessment of the applicant against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code Version 2021.1.
2. Application No. 22000566 by Kennards Self Storage to undertake Alterations and additions to existing self storage facility comprising 90 additional single storey self storage units, conversion of existing ground level office space to 26 storage units, variation to conditions 2, 3, and 6 in DA 211/984/2015 to allow for 24/7 access to the site, relocation of access, reconfiguration of on-site parking, associated landscaping and construction of a 3m high acoustic wall along portion of the western boundary at 11-17 James Congdon Drive, MILE END (CT-5922/940, CT-5922/943,CT-5974/676,CT-5974/677) is GRANTED subject to the following reserved matters and conditions of consent:

Reserved Matters:

The following information shall be submitted for further assessment and approval by the City of West Torrens as reserved matters under Section 102(3) of the *Planning, Development and Infrastructure Act 2016*:

1. The provision of Civil and Drainage Management Plan prepared by a suitably qualified and experienced Engineer which demonstrates that the proposed Finished Floor Levels of the storage units will be sufficient to protect the development from inundation.
2. An amended Site Plan which includes an additional 2 car parking spaces to cater for the needs of the additional internal storage units.

Pursuant to Section 127(1) of the *Planning, Development and Infrastructure Act 2016*, the Council reserves its decision on the form and substance of any further conditions of consent that it considers appropriate to impose in respect of the reserved matters outlined above.

Development Plan Consent Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application, specifically plans as listed below except where varied by any condition(s).
 - The plans prepared by Jacob Design and Visualisation including:
 - Site Location Plan, Project No. JDV0085, Drawing No. JDV0085_SK00, Revision P2, dated 09.05.2022
 - Existing Site Plan, Project No. JDV0085, Drawing No. JDV0085_SK01, Revision P2, dated 09.05.2022
 - Proposed General Layout Plan, Project No. JDV0085, Drawing No. JDV0085_SK02, Revision P10, dated 15.05.2022
 - Proposed External Elevations, Project No. JDV0085, Drawing No. JDV0085_SK03, Revision P2, dated 03.06.2021
 - Proposed External Elevations, Project No. JDV0085, Drawing No. JDV0085_SK04, Revision P2, dated 03.06.2021
 - The plans prepared by Oxigen including:
 - Planting plan, Project: Mile End DA, Project No. 22.015, Drawing No. 101, Revision B, Dated 23.05.22
 - Planting plan, Project: Mile End DA, Project No. 22.015, Drawing No. 201, Revision B, Dated 23.05.22
 - Planting plan, Project: Mile End DA, Project No. 22.015, Drawing No. 202, Revision B, Dated 23.05.22
 - Planting plan, Project: Mile End DA, Project No. 22.015, Drawing No. 203, Revision B, Dated 23.05.22
 - Letter from AECOM, titled Development Application - Kennards Self Storage Facility, 11 James Congdon Drive, dated 11 January 2022.
 - Traffic and Parking Assessment prepared by Stantec, titled Reference: Storage Facility Expansion, - James Congdon Drive, Mile End, File: 301401631, dated December 22 2021.
 - Environmental Noise Assessment prepared by Resonate, 11 James Congdon Drive, Mile, A210937RP1 Revision B, dated 11 January 2022.
 - Plan of Management, dated May 2022.
2. The maximum size of service vehicles accessing the site, including the refuse collection vehicle, shall be limited to an 8.8 long Medium Rigid Vehicle.
3. The car parking area shall be set out and line-marked in accordance with the approved plans and Australian Standard AS/NZS 2890.1 - 2004 - Off Street Parking - Car Parking Facilities. Appropriate directional signage shall be installed to reinforce the desired flow of traffic to, from and through the site.
4. The car parking areas shall be maintained at all times to the reasonable satisfaction of the relevant authority.
5. The amended crossover shall be constructed in accordance with Council's minimum technical standards.

6. The driveways, parking and vehicle manoeuvring areas shall not be used for the storage or display of materials or goods, including waste products and refuse.
7. The loading and unloading of goods and merchandise shall be carried out on the subject land and is not permitted to be carried out in the street.
8. Prior to the occupation or use of the development, all stormwater design and construction shall be to the satisfaction of Council to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
 - a) Result in the entry of water into a building; or
 - b) Affect the stability of a building; or
 - c) Create insanitary or dangerous conditions on the site or within the building; or
 - d) Flow or discharge onto the land of an adjoining owner; or
 - e) Flow across footpaths or public ways.
9. The waste enclosure area and disposal facilities shall be installed and made operative to the satisfaction of Council prior to occupation of the development. All waste and rubbish shall be stored in covered containers prior to removal and shall be screened from public view.

The collection of waste from the site shall occur only between the hours of:

Monday to Saturday 7am - 7pm

Sunday 9am - 7pm

10. The fence along the western boundary shall be a minimum of 3 metres in height and shall have a minimum Base Metal Thickness (BMT) of 0.48 steel or alternative solid material without gaps, with a minimum surface density of 10kg/m².
11. All external lights on the subject site shall be directed, screened and of such limited intensity that overspill of light into nearby premises is avoided and no nuisance or loss of amenity is caused to any person beyond the site, including passing motorists.
12. All proposed canopy trees nominated shall be at an advanced planting height of not less than 2 metres at time of planting. All landscaping shall be established in the landscaping areas nominated on the plans and provided with an automatic watering system, with all landscaping completed within 3 months of the commencement of the use of the car park. All landscaping shall be tended to and nurtured at all times, with any plantings that are dead or diseased being replaced to the reasonable satisfaction of the relevant authority.

NOTES

- City Operations has considered the health, structure, form, useful life expectancy and age of the street tree in Rose Street and in this instance will support the removal. Payment of a fee of \$1500.00 will be required prior to the commencement of any work.

Attachments

1. **Application documents**
2. **Representations and applicant's response**
3. **Referral responses**



11 EAST TERRACE, MILE END, SA

SCALE: 1:500 @ A1



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 FIGURED DIMENSIONS SHALL BE USED. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS ON SITE PRIOR TO THE COMMENCEMENT OF WORK.

ISSUE	DATE	REASON
P1	03.12.2021	PRELIMINARY ISSUE
P2	09.05.2022	EXISTING ROSE STREET CROSSOVER ADDED

CLIENT DETAILS	
COHEN GROUP PO BOX 145, GLENIDE SA 5065	
PROJECT DETAILS	
KENNARDS STORAGE 11 JAMES CONGDON DRIVE, MILE END SA	
PROJECT NUMBER	JDV0085
DRAWING NUMBER	JDV0085_SK00
DRAWING NAME	SITE LOCATION PLAN
REVISION	P2
DRAWN BY	JAA
PROJECT:	KENNARDS STORAGE
ADDRESS:	11 JAMES CONGDON DRIVE MILE END, SA
JACOB DESIGN & VISUALISATION 5 GLEN ROAD BRIDGEWATER, SA 5155 mobile: 0463 974 946 email: jacob@andv-visual.com.au	

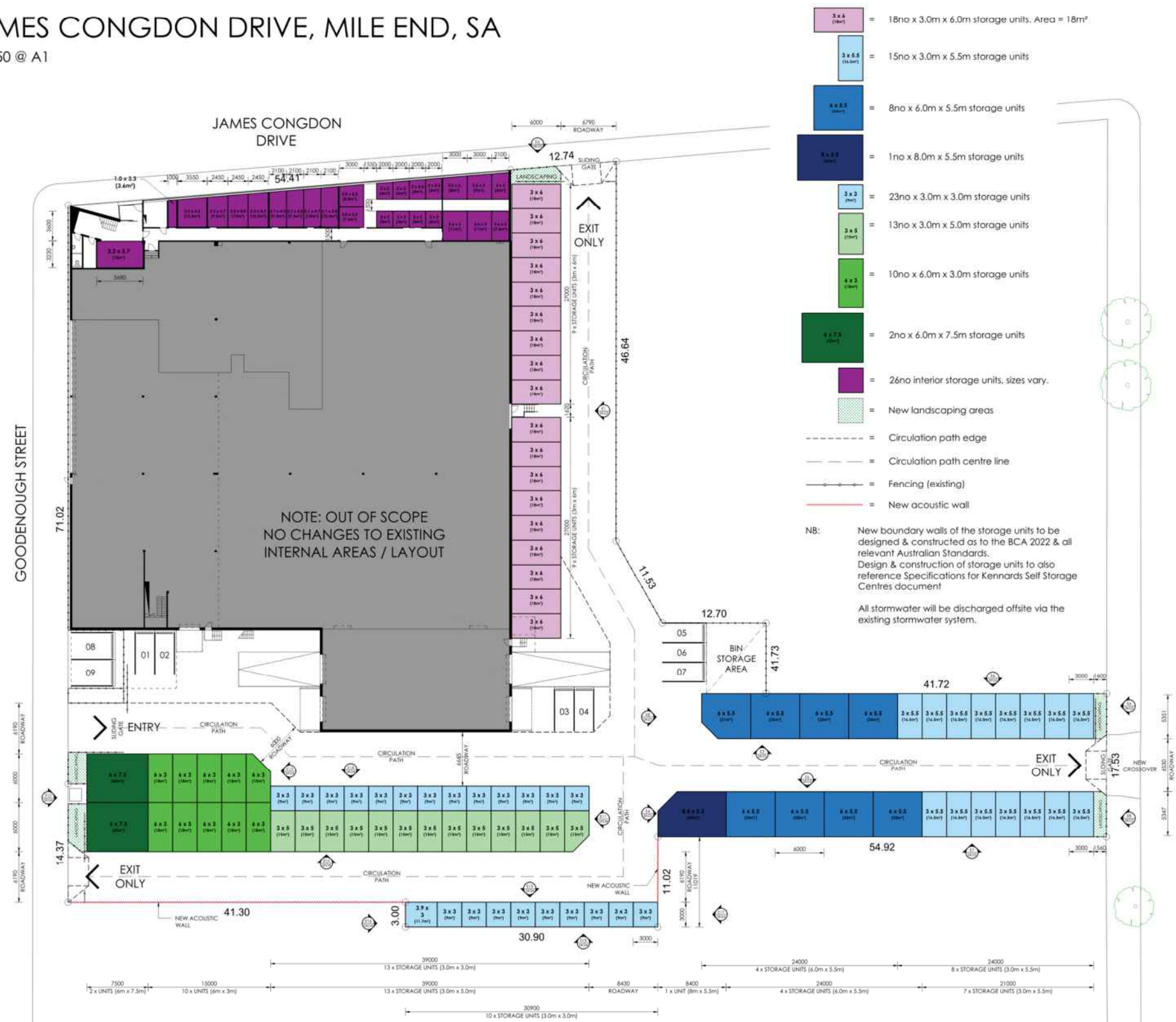


SITE LOCATION PLAN



11 JAMES CONGDON DRIVE, MILE END, SA

SCALE: 1:250 @ A1



- = 18no x 3.0m x 6.0m storage units, Area = 18m²
- = 15no x 3.0m x 5.5m storage units
- = 8no x 6.0m x 5.5m storage units
- = 1no x 8.0m x 5.5m storage units
- = 23no x 3.0m x 3.0m storage units
- = 13no x 3.0m x 5.0m storage units
- = 10no x 6.0m x 3.0m storage units
- = 2no x 6.0m x 7.5m storage units
- = 26no interior storage units, sizes vary.
- = New landscaping areas
- = Circulation path edge
- = Circulation path centre line
- = Fencing (existing)
- = New acoustic wall

NB: New boundary walls of the storage units to be designed & constructed as to the BCA 2022 & all relevant Australian Standards. Design & construction of storage units to also reference Specifications for Kennards Self Storage Centres document

All stormwater will be discharged offsite via the existing stormwater system.

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ISSUE	DATE	REASON
P1	03.12.2021	PRELIMINARY ISSUE
P2	04.12.2021	PRELIMINARY ISSUE - AMENDED
		FOLLOWING SKETCH PLAN BY CLIENT
P3	07.12.2021	PRELIMINARY ISSUE - AMENDED
P4	09.12.2021	PRELIMINARY ISSUE - AMENDED
		INCLUDE INTERIOR STORAGE UNIT AREA, ROSE STREET AREA CHANGE TO CENTRAL ROADWAY
P5	13.12.2021	PRELIMINARY ISSUE - AMENDED
		ADD IN STORAGE AREA BY BIN STORE
P6	14.12.2021	PRELIMINARY ISSUE - AMENDED
		REFER CLOUDS FOR CHANGES
P7	21.12.2021	PRELIMINARY ISSUE - AMENDED
		REMOVE INTERNAL LAYOUT AS OUT OF SCOPE. AMEND EXTENT & LOCATION OF ACoustic FENCING. ADD IN EXISTING CAR PARKS ON GOODENOUGH STREET.
P8	09.05.2022	PRELIMINARY ISSUE - AMENDED
		ADD IN ROSE STREET NEW CROSSOVER. ADD GENERAL NOTE RE BCA & STANDARDS COMPLIANCE
P9	13.05.2022	PRELIMINARY ISSUE - AMENDED
		ADJUST STORAGE UNITS ONTO BOUNDARY TO REPLACE FENCE LINE.
P10	15.05.2022	PRELIMINARY ISSUE - AMENDED
		ADD NOTE RE STORMWATER

CLIENT DETAILS	
COHEN GROUP	
PO BOX 145, GLENIDE SA 5065	
PROJECT DETAILS	
KENNARDS STORAGE	
11 JAMES CONGDON DRIVE, MILE END SA	
PROJECT NUMBER	JDV0085
DRAWING NUMBER	JDV0085_SK02
DRAWING NAME	PROPOSED GENERAL LAYOUT PLAN
REVISION	P10
DRAWN BY	JAA
PROJECT: KENNARDS STORAGE	
ADDRESS: 11 JAMES CONGDON DRIVE MILE END, SA	

JACOB DESIGN & VISUALISATION
 5 GLEN ROAD
 BRIDGEWATER, SA 5155
 mobile: 0403 974 946
 email: jacob@jacobdesign.com.au

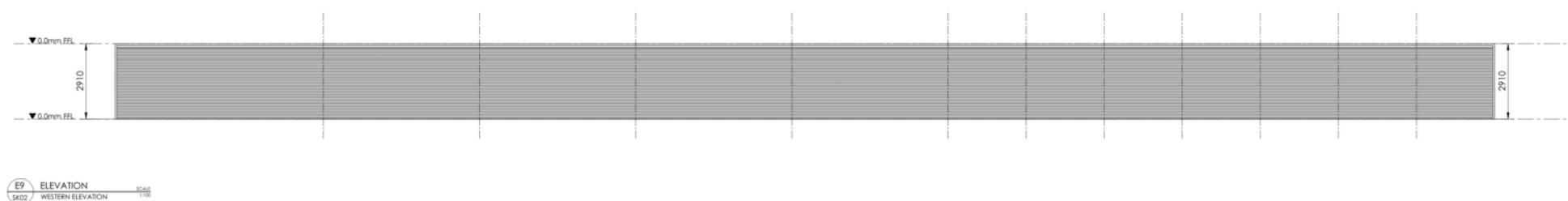
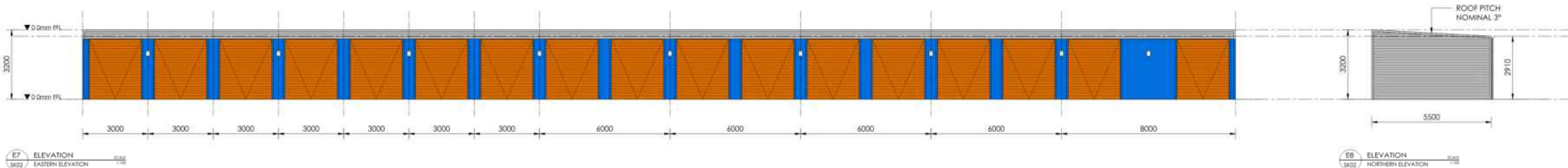
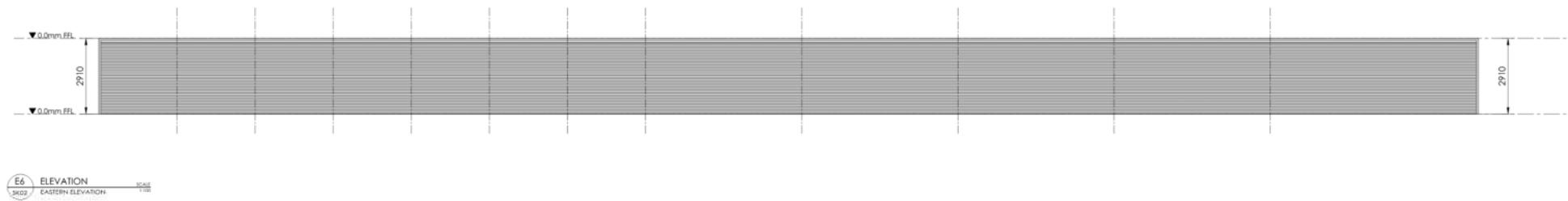
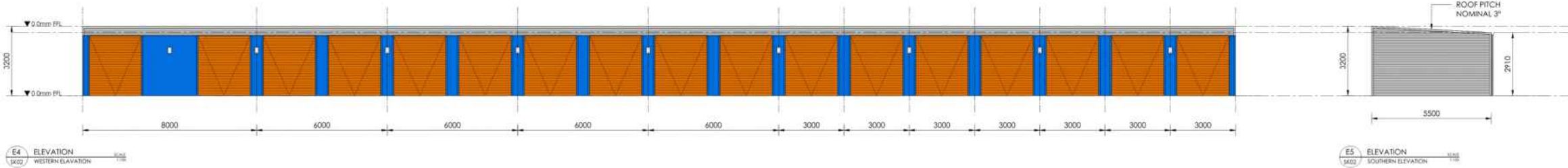
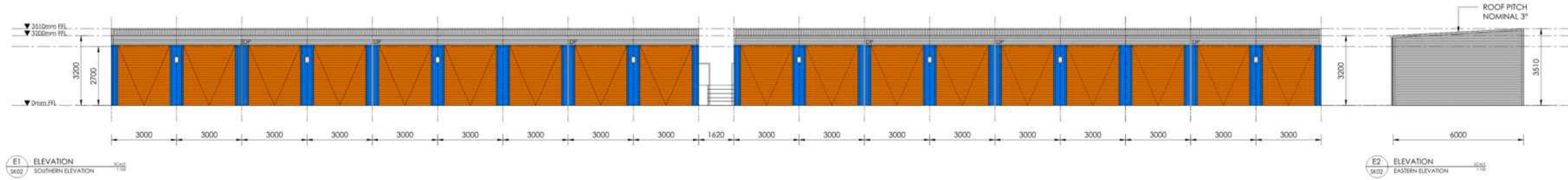
PROPOSED GENERAL LAYOUT PLAN



11 JAMES CONGDON DRIVE, MILE END, SA
SCALE: 1:100 @ A1

PROPOSED BUILDING FINISHES - as per kennards self storage specifications

ROOF TRIMDECK HI-TEN ZINCALUME FINISH	FACADE METAL SHEETING COLORBOND ASTRO METALLIC	ROLLER DOORS MANDARIN ORANGE	DOOR SURROUNDS LAZERITE BLUE	DOWNPIPES COLORBOND WINDSPRAY	PROMINENT FENCE INTERPON D2525 FRENCH BLUE	BOUNDARY FENCE SLATE GREY
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ISSUE	DATE	REASON
P1	13.12.2021	PRELIMINARY ISSUE
P2	03.06.2022	AMENDMENTS TO INCLUDE COLOUR SELECTIONS FOR BUILDINGS AS TO K35 SPECIFICATION DOCUMENT

CLIENT DETAILS	
COHEN GROUP PO BOX 145, GLENIDE SA 5045	
PROJECT DETAILS	
KENNARDS STORAGE 11 JAMES CONGDON DRIVE, MILE END SA	
PROJECT NUMBER	JDV0085
DRAWING NUMBER	JDV0085_SK03
DRAWING NAME	PROPOSED EXTERNAL ELEVATIONS
REVISION	P2
DRAWN BY	JAA
PROJECT:	KENNARDS STORAGE
ADDRESS:	11 JAMES CONGDON DRIVE MILE END, SA

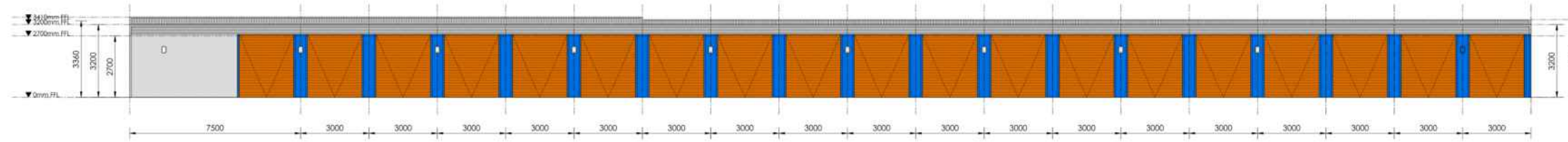
JACOB DESIGN & VISUALISATION
5 GLEN ROAD
BRIDGWATER, SA 5155
mobile: 0403 974 946
email: jacobdandv@outlook.com.au



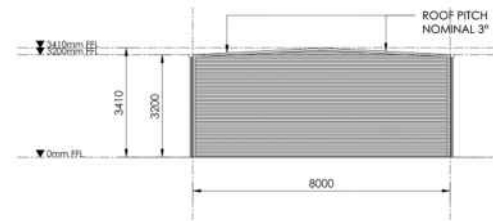
11 JAMES CONGDON DRIVE, MILE END, SA
SCALE: 1:100 @ A1

PROPOSED BUILDING FINISHES - as per kennards self storage specifications

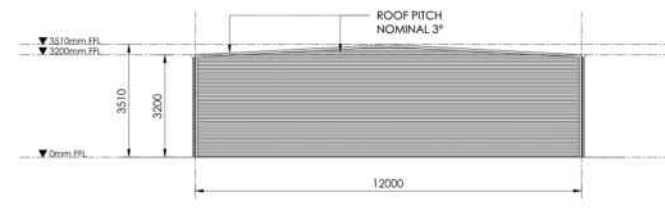
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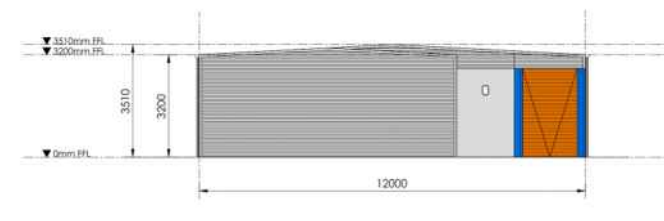
E10 ELEVATION
WESTERN ELEVATION



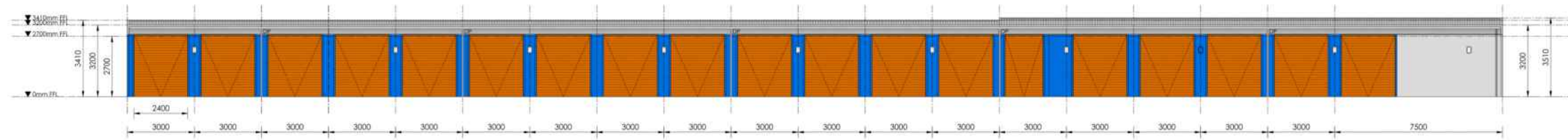
E11 ELEVATION
SOUTHERN ELEVATION



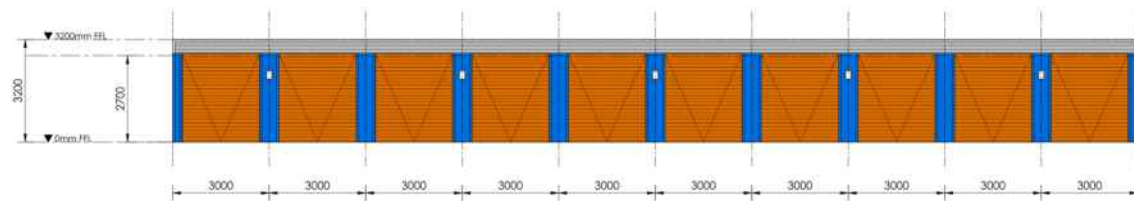
E12 ELEVATION
NORTHERN ELEVATION



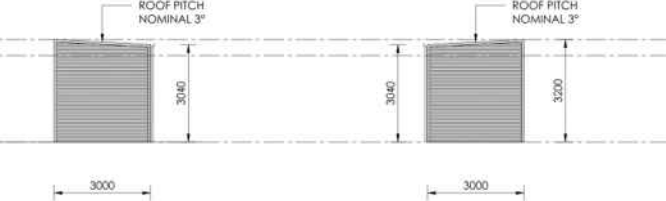
E13 ELEVATION
NORTHERN ELEVATION



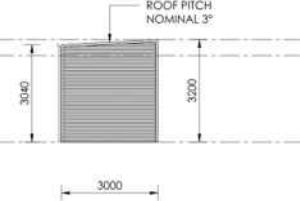
E14 ELEVATION
EASTERN ELEVATION



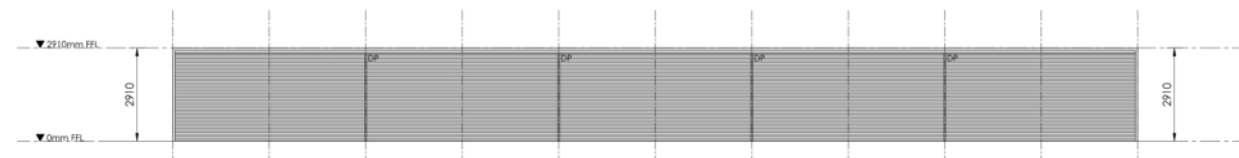
E15 ELEVATION
EASTERN ELEVATION



E16 ELEVATION
NORTHERN ELEVATION



E17 ELEVATION
SOUTHERN ELEVATION



E18 ELEVATION
WESTERN ELEVATION

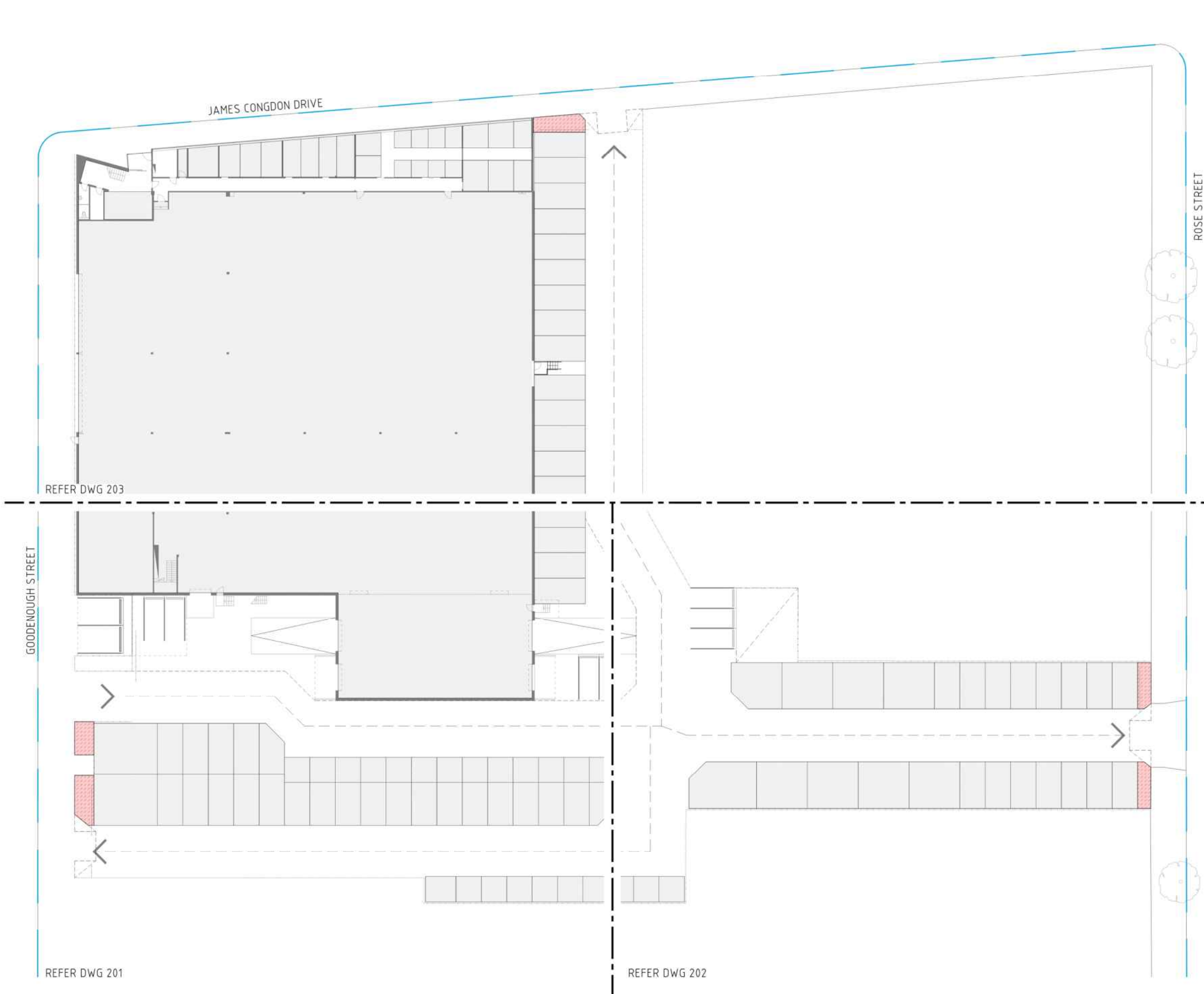
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FIGURED DIMENSIONS SHALL BE USED. DO NOT SCALE THE DRAWING. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS ON SITE PRIOR TO THE COMMENCEMENT OF WORK.

ISSUE	DATE	REASON
P1	13.12.2021	PRELIMINARY ISSUE
P2	03.06.2022	AMENDMENTS TO INCLUDE COLOUR SELECTIONS FOR BUILDINGS AS TO K35 SPECIFICATION DOCUMENT

CLIENT DETAILS	
COHEN GROUP PO BOX 145, GLENIDE SA 5065	
PROJECT DETAILS	
KENNARDS STORAGE 11 JAMES CONGDON DRIVE, MILE END SA	
PROJECT NUMBER	JDV0085
DRAWING NUMBER	JDV0085_SK04
DRAWING NAME	PROPOSED EXTERNAL ELEVATIONS
REVISION	P2
DRAWN BY	JAA
PROJECT:	KENNARDS STORAGE
ADDRESS:	11 JAMES CONGDON DRIVE MILE END, SA

JACOB DESIGN & VISUALISATION
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BRIDGWATER, SA 5155
mobile: 0403 974 946
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Plot File Created: May 24, 2022



TREE SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
EE	Eucalyptus erythronyx	AS SHOWN	45L	7
ET	Eucalyptus taranta	AS SHOWN	45L	2

PLANT SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
AP	Anagallis 'Bush Pioneer'	3.0/m ²	140mm	23
AB	Anagallis 'Bush Revolution'	3.0/m ²	140mm	10
CW	Callistemon 'White Anzac'	3.0/m ²	140mm	25
DL	Dianella caerulea 'Little Jess'	3.0/m ²	140mm	12
EA	Eremophila 'Amber Carpet'	3.0/m ²	140mm	15
GB	Grevillea 'Bronze Rambler'	3.0/m ²	140mm	16
RP	Rosaehuis officinalis prostratus	3.0/m ²	140mm	10
WS	Westringia fruticosa 'Smoky'	3.0/m ²	140mm	26
WH	Westringia fruticosa 'Hud'	3.0/m ²	140mm	12

NOTES

EXISTING SERVICES
THE CONTRACTOR MUST LOCATE AND MARK ALL UNDERGROUND SERVICES BEFORE COMMENCING WORK ON SITE.

TREE PLANTING
PREPARE TREE HOLES TO A MINIMUM SIZE OF THE DEPTH OF THE ROOTBALL x 1m WIDE AND BREAK THE SUBGRADE TO A MINIMUM DEPTH OF 200MM BELOW. TAKE PARTICULAR CARE TO BREAK UP ANY GLAZING TO SIDES OF TREE HOLE. FINISH THE ROOTBALL LEVEL WITH THE FINAL SURROUNDING SOIL LEVEL AND BACKFILL THE PLANTING HOLE WITH SITE TOPSOIL BLENDED WITH 20% ORGANIC MIX. PROVIDE A 1m DIAMETER MULCHED WATERING BOWL TO THE BACK OF THE TREE. STAKE TREES WITH 25x 2500-3000 HARDWOOD STAKES AND TIE WITH 50mm HESSIAN TIES SECURELY STAPLED TO THE STAKES. ENSURE STAKES AND TIES REMAIN CLEAR OF BRANCHES, FOLIAGE AND ROOTBALL.

PLANTING BEDS
CULTIVATE EXISTING GROUND TO A MINIMUM DEPTH OF 300 MM AND PLACE 300MM IMPORTED 'ORGANIC MIX'. PLACE PLANTS IN THE CENTRE OF THE PLANTING HOLE AND FINISH THE TOP OF THE ROOT BALL LEVEL WITH THE FINISHED SURFACE OF THE SURROUNDING SOIL. APPLY TERRACOTTE FERTILISER TO MANUFACTURERS RATES AT THE OF PLANTING AND AFTER PLANTING PLACE A 100MM MINIMUM DEPTH OF COTTAGE MULCH. THOROUGHLY WATER PLANTS BEFORE AND IMMEDIATELY AFTER PLANTING, AND AS REQUIRED TO MAINTAIN HEALTH AND VIGOUR. AVERAGE 3 PLANTS/M².

IRRIGATION
PROVIDE AN AUTOMATIC IN-LINE DRIP IRRIGATION SYSTEM TO ALL PLANTING BEDS AND TREES.
DRIP IRRIGATION SPECIFIED AS NETAFIM TECHLINE 16 POLY TUBE 16LPH @ 0.5M SPACINGS OR SIMILAR APPROVED.
FOR ALL TREE PLANTING INSTALL AT BASE OF TREE 4mm 4LPH PC DRIP EMITTERS ON 10MM POLY LOOP OR IN-LINE EQUIVALENTS.
ALL POLY TUBING TO BE LAID ON SURFACE AND COVERED WITH MULCH.
PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.

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Project
Mile End DA

Landscape Architecture
Oxygen

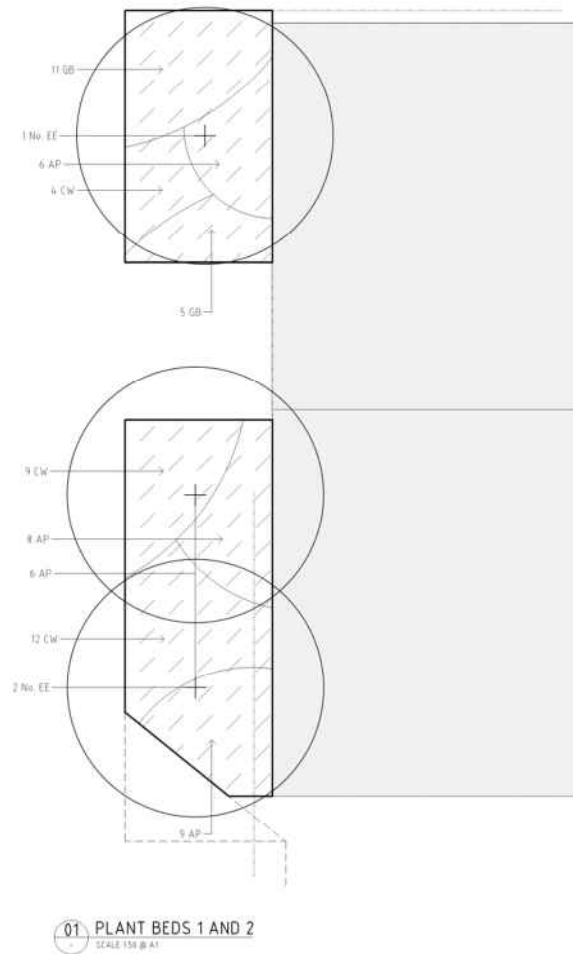
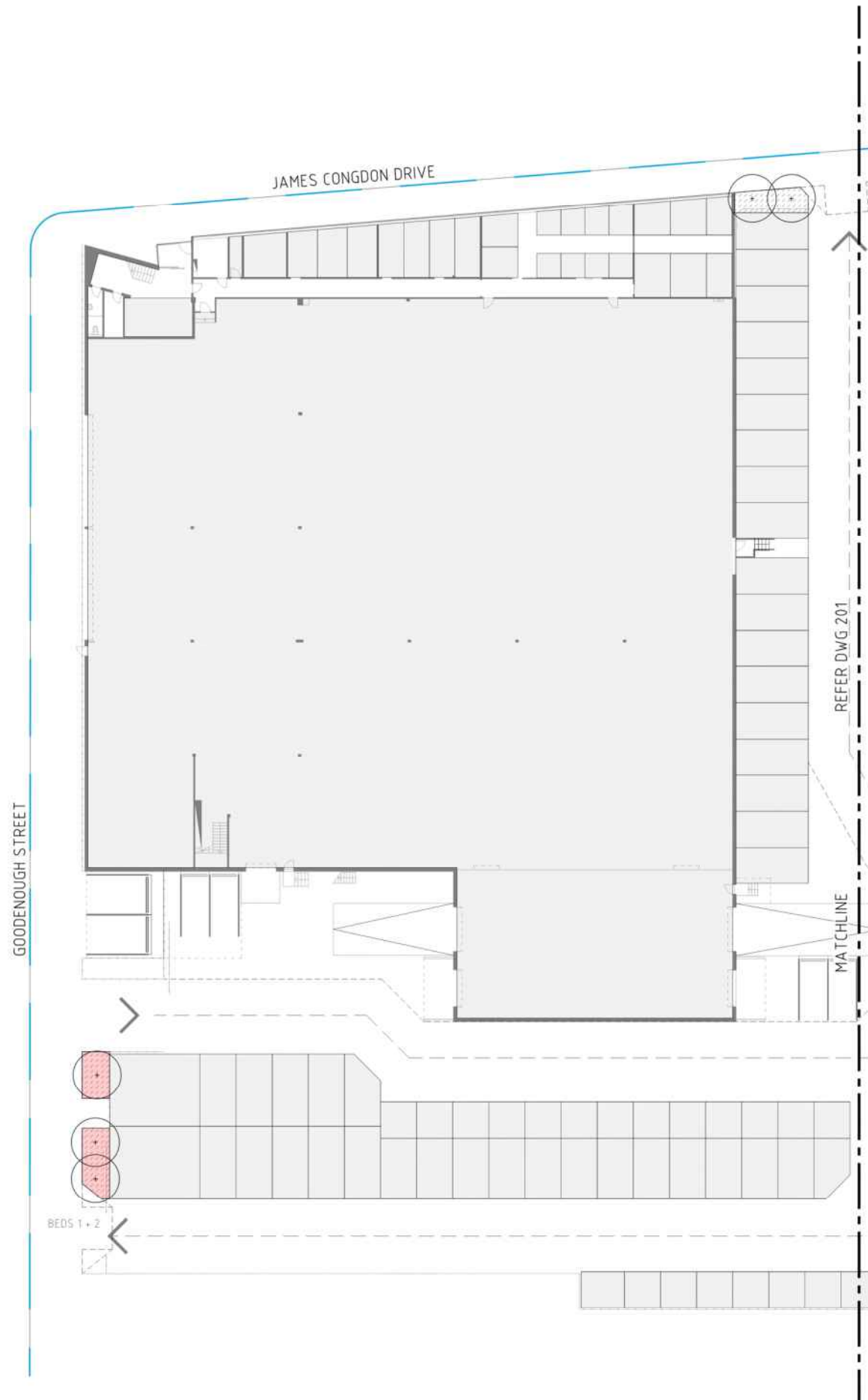
Client
Cohen Group

Drawing Title
PLANTING PLAN

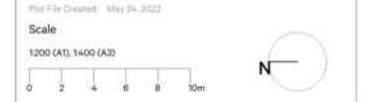
Project number	Drawing number	Revision
22.015	101	B

Rev	Date	Issue	DWN	CHK	APP
A	17.06.22	DRAFT	EH		JH
B	23.06.22	DRAFT	EH		JH

DRAFT



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TREE SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
EE	Eucalyptus erythrocorys	AS SHOWN	45L	1
ET	Eucalyptus taranta	AS SHOWN	45L	2

PLANT SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
AP	Anigozanthos 'Bush Pioneer'	3.0/m ²	140mm	23
AB	Anigozanthos 'Bush Revolution'	3.0/m ²	140mm	10
CW	Callistemon 'White Anzac'	3.0/m ²	140mm	25
DL	Dianella caerulea 'Little Jess'	3.0/m ²	140mm	12
EA	Eremophila 'Amber Carpet'	3.0/m ²	140mm	15
GB	Orevillea 'Bronze Rambler'	3.0/m ²	140mm	16
RP	Rosmarinus officinalis prostratus	3.0/m ²	140mm	10
WJ	Westringia fruticosa 'Smoky'	3.0/m ²	140mm	26
WH	Westringia fruticosa 'Rhub'	3.0/m ²	140mm	12

NOTES

EXISTING SERVICES
THE CONTRACTOR MUST LOCATE AND MARK ALL UNDERGROUND SERVICES BEFORE COMMENCING WORK ON SITE.

TREE PLANTING
PREPARE TREE HOLES TO A MINIMUM SIZE OF THE DEPTH OF THE ROOTBALL x 1m WIDE AND BREAK THE SUBGRADE TO A MINIMUM DEPTH OF 200MM BELOW. TAKE PARTICULAR CARE TO BREAK UP ANY GLAZING TO SIDES OF TREE HOLE. FINISH THE ROOTBALL LEVEL WITH THE FINAL SURROUNDING SOIL LEVEL AND BACKFILL THE PLANTING HOLE WITH SITE TOPSOIL BLENDED WITH 20% ORGANIC MIX. PROVIDE A 1m DIAMETER MULCHED WATERING BOWL TO THE BASE OF THE TREE. STAKE TREES WITH 2No. 250x50x50 HARDWOOD STAKES AND TIE WITH 50mm HESSIAN TIES SECURELY STAPLED TO THE STAKES. ENSURE STAKES AND TIES REMAIN CLEAR OF BRANCHES, FOLIAGE AND ROOTBALL.

PLANTING BEDS
CULTIVATE EXISTING GROUND TO A MINIMUM DEPTH OF 300 MM AND PLACE 300MM IMPORTED 'ORGANIC MIX'. PLACE PLANTS IN THE CENTRE OF THE PLANTING HOLE AND FINISH THE TOP OF THE ROOT BALL LEVEL WITH THE FINISHED SURFACE OF THE SURROUNDING SOIL. APPLY TERRACOTTE FERTILISER TO MANUFACTURERS RATES AT TIME OF PLANTING AND AFTER PLANTING PLACE A 100MM MINIMUM DEPTH OF COITAGE MULCH. THROUGHLY WATER PLANTS BEFORE AND IMMEDIATELY AFTER PLANTING, AND AS REQUIRED TO MAINTAIN HEALTH AND VIGOUR. AVERAGE 3 PLANTS/M².

IRRIGATION
PROVIDE AN AUTOMATIC IN-LINE DRIP IRRIGATION SYSTEM TO ALL PLANTING BEDS AND TREES.
DRIP IRRIGATION SPECIFIED AS NETAFIM TECHLINE 16 POLY TUBE 16LPH @ 0.5M SPACINGS OR SIMILAR APPROVED.
FOR ALL TREE PLANTING INSTALL AT BASE OF TREE 4No. 4LPH PC DRIP EMITTERS ON 10MM POLY LOOP OR INLINE EQUIVALENTS.
ALL POLY TUBING TO BE LAID ON SURFACE AND COVERED WITH MULCH.
PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.

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Project
Mile End DA

Landscape Architecture
Oxygen

Client
Cohen Group

Drawing Title
PLANTING PLAN

Project number
22.015

Drawing number
201

Revision
B

Rev	Date	Issue	DWN	CHK	APP
A	17/06/22	DRAFT	DH	JH	
B	23/06/22	DRAFT	DH	JH	

DRAFT



Eucalyptus erythrocorys
Eucalypt
Height: 3-10m
Width: 3-6m



Dianella caerulea 'Little Jess'
Flax Lily
Height: 0.5-1.3m
Width: 1-1.2m

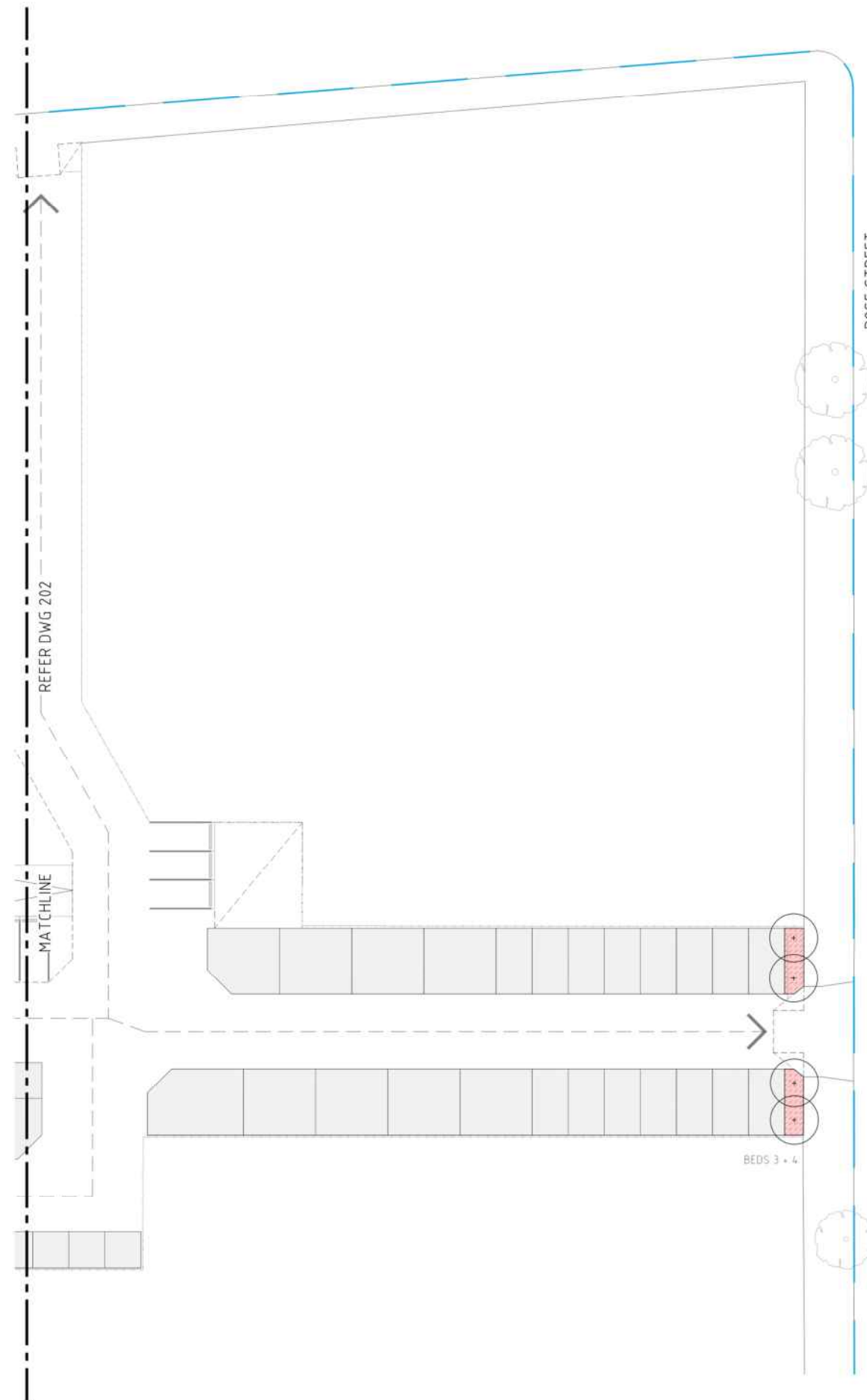
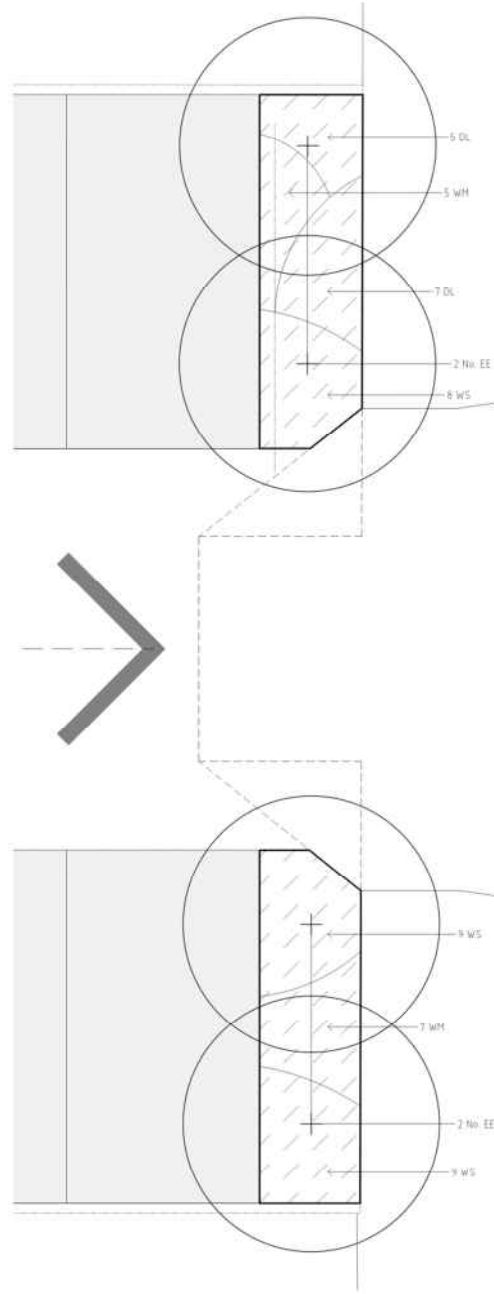


Westringia fruticosa 'Smokey'
Coastal Rosemary
Height: 1-1.2m
Width: 1-1.5m



Westringia fruticosa 'Mundi'
Coastal Rosemary
Height: 0.4-0.5m
Width: 1.5m

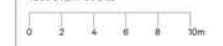
01 PLANT BEDS 3 AND 4
SCALE: 1:50 @ A1



This drawing must be read in conjunction with all other contract documents including the project specifications, schedules and any instructions issued during the course of the contract. The Contractor must verify all dimensions on site and check the location of services before commencement of work. The Contractor is to notify the Superintendent of any discrepancies between the drawings or specifications. Drawings are not to be used for construction unless identified in the title block as 'for construction'. All drawings to be read at A1 unless otherwise stated. Drawings are intended for digital setout and DWG files will be issued upon request. Copyright Oxigen Pty Ltd.

Plot File Created: May 24, 2022

Scale
1:200 (A1), 1:400 (A3)



TREE SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
EE	Eucalyptus erythrocorys	AS SHOWN	45L	7
ET	Eucalyptus taranta	AS SHOWN	45L	2

PLANT SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
AP	Anagallis 'Bush Pioneer'	3.0/m ²	140mm	23
AB	Anagallis 'Bush Revolution'	3.0/m ²	140mm	10
CW	Callistemon 'White Anzac'	3.0/m ²	140mm	25
DL	Dianella caerulea 'Little Jess'	3.0/m ²	140mm	12
EA	Eremophila 'Amber Carpet'	3.0/m ²	140mm	15
GB	Grevillea 'Bronze Rambler'	3.0/m ²	140mm	16
SP	Stemodia officinalis prostratus	3.0/m ²	140mm	10
WS	Westringia fruticosa 'Smokey'	3.0/m ²	140mm	26
WH	Westringia fruticosa 'Mundi'	3.0/m ²	140mm	12

NOTES
EXISTING SERVICES
THE CONTRACTOR MUST LOCATE AND MARK ALL UNDERGROUND SERVICES BEFORE COMMENCING WORK ON SITE.

TREE PLANTING
PREPARE TREE HOLES TO A MINIMUM SIZE OF THE DEPTH OF THE ROOTBALL x 1m WIDE AND BREAK THE SUBGRADE TO A MINIMUM DEPTH OF 200MM BELOW. TAKE PARTICULAR CARE TO BREAK UP ANY GLAZING TO SIDES OF TREE HOLE. FINISH THE ROOTBALL LEVEL WITH THE FINAL SURROUNDING SOIL LEVEL AND BACKFILL THE PLANTING HOLE WITH SITE TOPSOIL BLENDED WITH 20% ORGANIC MIX. PROVIDE A 1m DIAMETER MULCHED WATERING BOWL TO THE BACK OF THE TREE. STAKE TREES WITH 250x25x50 HARDWOOD STAKES AND TIE WITH 50mm HESSIAN TIES SECURELY STAPLED TO THE STAKES. ENSURE STAKES AND TIES REMAIN CLEAR OF BRANCHES, FOLIAGE AND ROOTBALL.

PLANTING BEDS
CULTIVATE EXISTING GROUND TO A MINIMUM DEPTH OF 300 MM AND PLACE 300MM IMPORTED 'ORGANIC MIX'. PLACE PLANTS IN THE CENTRE OF THE PLANTING HOLE AND FINISH THE TOP OF THE ROOT BALL LEVEL WITH THE FINISHED SURFACE OF THE SURROUNDING SOIL. APPLY TERRACOTTE FERTILISER TO MANUFACTURERS RATES AT THE OF PLANTING AND AFTER PLANTING PLACE A 100MM MINIMUM DEPTH OF COTTAGE MULCH. THROUGHLY WATER PLANTS BEFORE AND IMMEDIATELY AFTER PLANTING, AND AS REQUIRED TO MAINTAIN HEALTH AND VIGOUR. AVERAGE 3 PLANTS/M².

IRRIGATION
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DRIP IRRIGATION SPECIFIED AS NETAFIM TECHLINE 16 POLY TUBE 16LPH @ 0.5M SPACINGS OR SIMILAR APPROVED.
FOR ALL TREE PLANTING INSTALL AT BASE OF TREE 4mm 4LPH PC DRIP EMITTERS ON 19MM POLY LOOP OR INLINE EQUIVALENTS.
ALL POLY TUBING TO BE LAID ON SURFACE AND COVERED WITH MULCH.
PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.

oxigen

Oxigen Pty Ltd
98-100 Halifax Street
Adelaide SA 5000

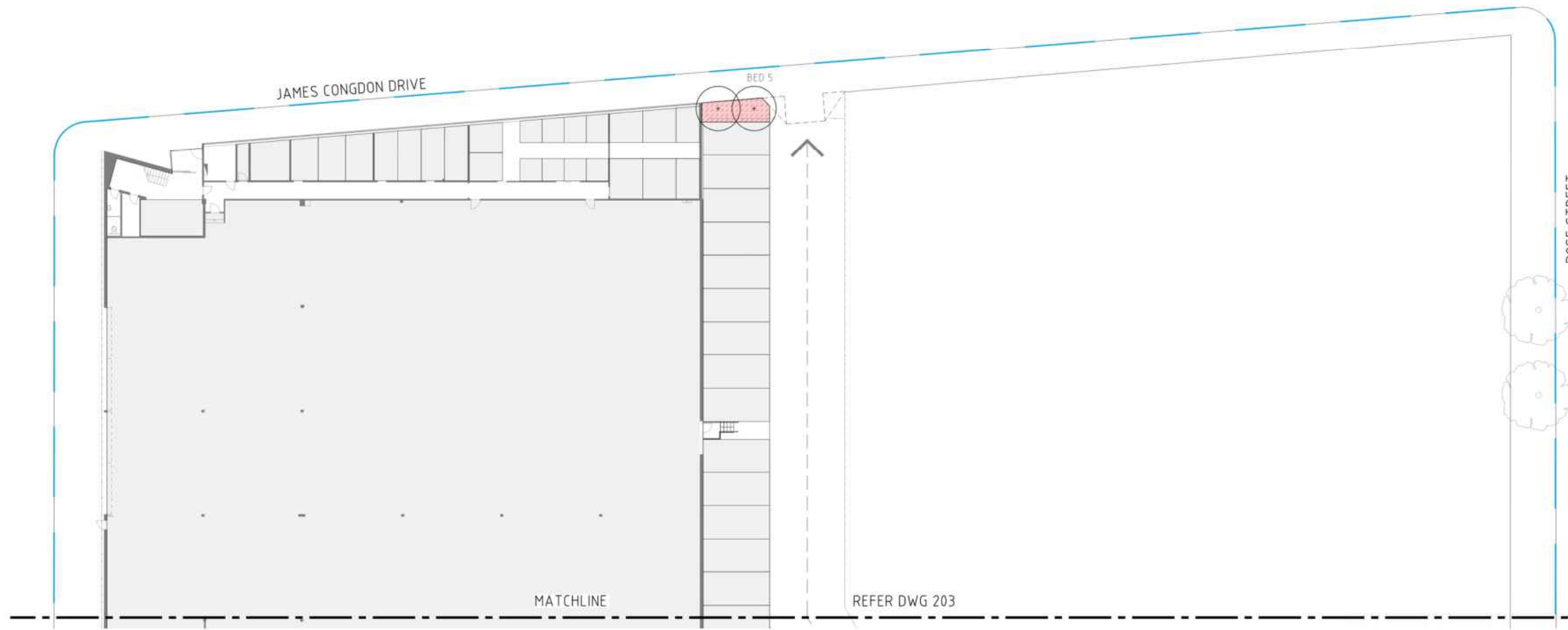
T +61 (08) 7324 9600
design@oxigen.net.au
oxigen.net.au

Project: Mile End DA
Client: Cohen Group
Drawing Title: PLANTING PLAN

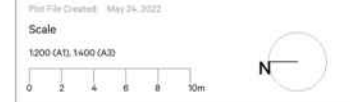
Project number	Drawing number	Revision
22.015	202	B

Rev	Date	Issue	DWN	CHK	APP
A	17.06.22	DRAFT	DH	JH	
B	23.09.22	DRAFT	DH	JH	

DRAFT



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TREE SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
EE	<i>Eucalyptus erythronyx</i>	AS SHOWN	45L	7
ET	<i>Eucalyptus torquata</i>	AS SHOWN	45L	2

PLANT SCHEDULE

CODE	BOTANIC NAME	SPACING	SIZE	QTY
AP	<i>Anigozanthos 'Bush Pioneer'</i>	3.0/m ²	140mm	23
AR	<i>Anigozanthos 'Bush Revolution'</i>	3.0/m ²	140mm	10
CW	<i>Callistemon 'White Anzac'</i>	3.0/m ²	140mm	25
DL	<i>Dianella caerulea 'Little Jess'</i>	3.0/m ²	140mm	12
EA	<i>Eremophila 'Amber Carpet'</i>	3.0/m ²	140mm	15
GB	<i>Grevillea 'Bronze Rambler'</i>	3.0/m ²	140mm	16
RP	<i>Rosmarinus officinalis prostratus</i>	3.0/m ²	140mm	10
WS	<i>Westringia fruticosa 'Smoky'</i>	3.0/m ²	140mm	26
WH	<i>Westringia fruticosa 'Humb'</i>	3.0/m ²	140mm	12

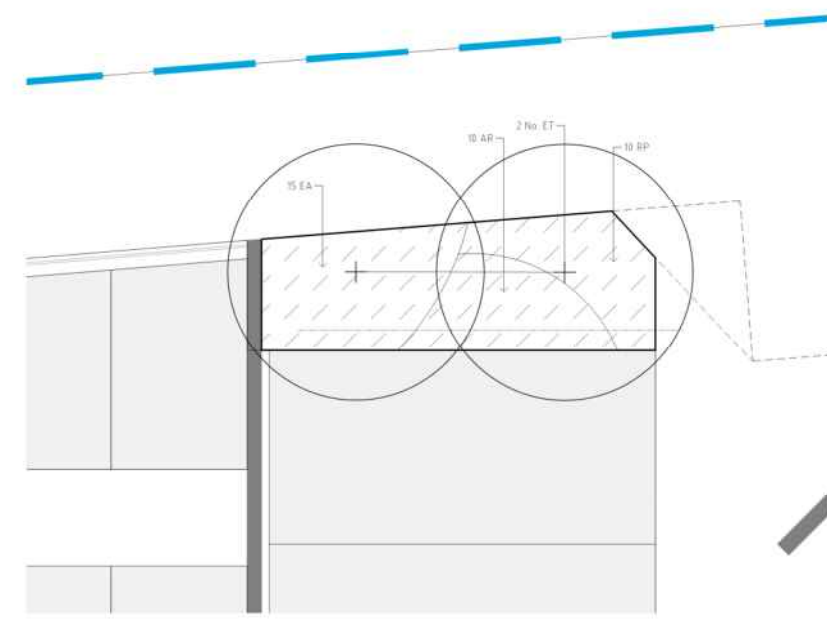
NOTES

EXISTING SERVICES
THE CONTRACTOR MUST LOCATE AND MARK ALL UNDERGROUND SERVICES BEFORE COMMENCING WORK ON SITE.

TREE PLANTING
PREPARE TREE HOLES TO A MINIMUM SIZE OF THE DEPTH OF THE ROOTBALL x 1m WIDE AND BREAK THE SUBGRADE TO A MINIMUM DEPTH OF 200MM BELOW. TAKE PARTICULAR CARE TO BREAK UP ANY GLAZING TO SIDES OF TREE HOLE. FINISH THE ROOTBALL LEVEL WITH THE FINAL SURROUNDING SOIL LEVEL AND BACKFILL THE PLANTING HOLE WITH SITE TOPSOIL BLENDED WITH 20% ORGANIC MIX. PROVIDE A 1m DIAMETER MULCHED WATERING BOWL TO THE BACK OF THE TREE. STAKE TREES WITH 2x 250x25x50 HARDWOOD STAKES AND TIE WITH 50mm HESSIAN TIES SECURELY STAPLED TO THE STAKES. ENSURE STAKES AND TIES REMAIN CLEAR OF BRANCHES, FOLIAGE AND ROOTBALL.

PLANTING BEDS
CULTIVATE EXISTING GROUND TO A MINIMUM DEPTH OF 300 MM AND PLACE 300MM IMPORTED 'ORGANIC MIX'. PLACE PLANTS IN THE CENTRE OF THE PLANTING HOLE AND FINISH THE TOP OF THE ROOT BALL LEVEL WITH THE FINISHED SURFACE OF THE SURROUNDING SOIL. APPLY TERRACOTTE FERTILISER TO MANUFACTURERS RATES AT TIME OF PLANTING AND AFTER PLANTING PLACE A 100MM MINIMUM DEPTH OF COTTAGE MULCH. THOROUGHLY WATER PLANTS BEFORE AND IMMEDIATELY AFTER PLANTING, AND AS REQUIRED TO MAINTAIN HEALTH AND VIGOUR. AVERAGE 3 PLANTS/M².

IRRIGATION
PROVIDE AN AUTOMATIC IN-LINE DRIP IRRIGATION SYSTEM TO ALL PLANTING BEDS AND TREES.
DRIP IRRIGATION SPECIFIED AS NETAFIM TECHLINE 16 POLY TUBE 16LPH @ 0.5M SPACINGS OR SIMILAR APPROVED.
FOR ALL TREE PLANTING INSTALL AT BASE OF TREE 4mm 4LPH PC DRIP EMITTERS ON 100MM POLY LOOP OR INLINE EQUIVALENTS.
ALL POLY TUBING TO BE LAID ON SURFACE AND COVERED WITH MULCH.
PROVIDE BACKFLOW PREVENTION, AUTOMATIC CONTROLLER AND OTHER DEVICES AS REQUIRED.



01 PLANTING BED 5
SCALE 1:50 @ A1



Eucalyptus torquata
Coral Gum
Height: 6-12m
Width: 5-10m



Eremophila 'Amber Carpet'
Emu Bush
Height: 0.3m
Width: 2-3m



Anigozanthos 'Bush Revolution'
Kangaroo Paw
Height: 1.5-2m
Width: 1-1.5m



Rosmarinus officinalis prostratus
Rosemary
Height: 0.3-0.6m
Width: 0.6-0.9m

oxigen

Oxigen Pty Ltd
98-100 Halifax Street
Adelaide SA 5000

T +61 (0)8 7324 9600
design@oxigen.net.au
oxigen.net.au

Project: Mile End DA
Landscape Architecture: Oxigen

Client: Cohen Group

Drawing Title: PLANTING PLAN

Project number: 22.015
Drawing number: 203
Revision: B

Rev	Date	Issue	DWN	CHK	APP
A	17.06.22	DRAFT	EH	JH	
B	23.06.22	DRAFT	EH	JH	

DRAFT

PLAN OF MANAGEMENT

May 2022

1. Preliminary

This Plan of Management (POM) applies to the use of for the purposes of self-storage.

2. Aim

The aim of this POM is:

1. To ensure a consistent management regime applies to all parts of the premises.
2. To facilitate the operation of the use of the premises for self-storage, in accordance with the consent, whilst minimising disturbance to nearby residents.

3. Person responsible for the implementation of this POM

The person currently holding the job title of 'Operations Manager' is responsible for implementing and ensuring compliance with this POM. The Operations Manager may delegate certain tasks under this POM to other employees but may not delegate overall responsibility for implementing and enforcing the POM.

4. Hours of operation and Staff Numbers

The hours during which customers may access their storage units shall be 24 hours a day, 7 days a week. It is proposed to have 2 staff members with at least 1 present in the site office at a time.

5. Security and access control

During the hours over which the premises' office is not staffed, access is to be controlled through an automated access system. Each customer will be issued with an individual and identifiable PIN code.

6. Noise minimization

No work of any manufacturing, repair, processing or any like activity is permitted.

7. Customer advice

All customers to be advised that storage units are to be used for storage and incidental activities only and of the need to minimize noise to neighbours.

8. Handling of community concerns

The Operations Manager is responsible for ensuring that every employee is provided a copy of this POM and is advised they are individually responsible for ensuring that no breach of the POM occurs. The Centre Manager is responsible for maintaining a register of any concerns raised by residents and other community members. Such to include:

1. The time/date of the complaint, the name of the person who took the complaint and the nature of the complaint.
2. The action taken to investigate and resolve the complaint and the time/date on which the complainant was advised of that action and/or the outcome of their complaint.
3. This register to be available to Council upon request.

During the hours over which the administration office is staffed, any complaint will be taken by staff of that office, and appropriate action taken to investigate and resolve that complaint. Outside of the hours during which the administration office is staffed, any complaint will divert to a security company engaged by Kennards Self Storage.

9. Parking & Access

Customers and staff can access the centre and parking freely within the centre during business hours and securely after hours via the pin-controlled gate.

10. Waste

All waste generated is to be stored away from public view. The site is to be kept free of litter. The collection of waste is to be organised so as not to unreasonably interfere with the neighbour's enjoyment.



AECOM Australia Pty Ltd +61 8 7131 0252 tel
Level 28
91 King William Street ABN 20 093 846 925
Adelaide SA 5000
Australia
www.aecom.com

11 January 2022

Assessment Manager
City of West Torrens
165 Sir Donald Bradman Drive
Hilton SA 5033

Dear Assessment Manager

Development Application - Kennards Self Storage Facility, 11 James Congdon Drive, Mile End:

1.0 Introduction

AECOM Australia Pty Ltd (AECOM) has been engaged by the Cohen Group (property owner) to provide planning advice and assist with the lodgement of a Development Application for alterations and additions to an existing self-storage facility at 11 James Congdon Drive, Mile End.

The proposed alterations and additions require a Development Approval pursuant to the *Planning Development and Infrastructure Act 2016* (PDI Act). Please accept this letter and attached documentation as a formal development application.

2.0 Development Approval History

The site is currently used as a self-storage facility which is operated by Kennards. The site has been subject to a number of development applications and approvals associated with its use as a self-storage facility. A summary of the site's approval history is provided below:

- **Development Application 211/984/2015** - Development approval was granted on 18/04/2016 for the *'establishment of a self-storage facility with associated car parking, plus minor building modifications including demolition of shed and construction of a free-standing storage building and fencing/gates.'*

Conditions 2 and 6 of the approval restrict the use of the facility to the following hours:

- 24-hour access to the storage units on the ground floor of the main facility (as identified on the approved plans); with the remainder of the main facility being restricted to 8:30am – 5:00pm Monday to Friday and 8:30am – 2:00pm on Saturday.
- 7:00am and 7:00pm Monday to Friday for the free-standing external storage building.

It is noted that the free-standing external storage building was proposed along the western site boundary within the southern portion of the site, however, this building was not constructed.

- **Development Application 211/409/2016** - Development approval was granted on 14/06/2016 for *'signage to northern and eastern facades'*.
- **Development Application 211/1130/2019** Development approval was granted on 15/11/2019 for *'internal office fit-out within warehouse.'*



3.0 Subject Land and Locality

The existing Kennards self-storage facility is located at 11 James Congdon Drive Mile End and the subject land comprises the following allotments:

- Allotment 1 in Deposited Plan 64577 – Certificate of Title Volume 5922 Folio 940
- Allotment 4 in Deposited Plan 64577 – Certificate of Title Volume 5922 Folio 943
- Allotment 5 in Deposited Plan 64577 – Certificate of Title Volume 5974 Folio 676
- Allotment 6 in Deposited Plan 64577 – Certificate of Title Volume 5974 Folio 677

Copies of the Certificates of Title are provided in Attachment B.

The above allotments are subject to a Land Management Agreement (LMA) established under Section 57(2) of the *Development Act 1993* (copy attached – Attachment B). The overall intent of the LMA is to ensure that the four allotments that form the subject land are used as a single site.

The subject land has a total area of approximately 7,270 square metres and frontages of 67 metres to James Congdon Drive to the east, 85 metres to Goodenough Street to the north and 17.5 metres to Rose Street to the south.

Vehicle access is currently provided from all street frontage as per the following arrangement:

- Goodenough Street - one entry point and one exit point
- James Congdon Drive - one two-way access point
- Rose Street via a single access point (exit only)

The subject land contains a large two storey building positioned in the north-east corner of the site that is occupied by self-storage units and an associated office on the ground floor. The upper level of the building was previously used as an office; however, is currently unoccupied. The western portion of the building includes an internal vehicle loading area which is accessed via an entry and exit ramp.

The balance of the site principally comprises open areas containing car parking and vehicle storage spaces.

The subject land is located within two zones under the Planning and Design Code (the Code). The majority of the site is located within the Urban Corridor (Boulevard) Zone, while the two western allotments are zoned Established Neighbourhood Zone. The land is not located within any subzone. The following overlays also apply to the subject land:

- Aircraft Noise Exposure (ANEF 20)
- Airport Building Heights (Regulated) (All structures over 15 metres)
- Affordable Housing
- Building Near Airfields
- Character Area (WeToC6)
- Design
- Hazards (Flooding - Evidence Required)
- Major Urban Transport Routes
- Noise and Air Emissions
- Prescribed Wells Area
- Regulated and Significant Tree
- Stormwater Management
- Traffic Generating Development
- Urban Tree Canopy



Development and land uses in the locality comprise a mixture of commercial, residential and open space, and includes:

- A two storey building, containing medical consulting rooms and laboratory to the north
- The Adelaide Parklands, on the opposite site of James Congdon Drive, to the east.
- A display home centre adjacent to the south-east
- Residential properties directly adjoining the western boundary
- Residential properties to the south, on the southern site of Rose Street.

The subject land, locality and zoning are further illustrated on Figure 1 and Figure 2.

Figure 1 Subject Land



Source: SA Property & Planning Atlas (<https://sappa.plan.sa.gov.au/>)



Figure 2 Locality and Zoning



Source: SA Property & Planning Atlas (<https://sappa.plan.sa.gov.au/>)

4.0 Proposed development

The proposed development is seeking approval for alterations and additions to the existing self-storage facility. The proposed alterations and additions include:

- Construction of 90 external single storey self-storage units. The units will be located along the southern elevation of the existing building and either side of the internal driveways which connect to Goodenough Street and Rose Street. The proposed arrangement includes storage units being constructed along portion of the western and eastern property boundaries.

The external storage units range in area from 9m² to 45m² and comprise a combined floor area of 1,542.5m². The building heights of the units range from 2.9 metres to 3.5 metres. All development to be sited on the property boundary will comprise a height of 2.9 metres.

Access to each unit will be provided via a roller door and wall lighting is proposed on the front of the units.

- Conversion of the former (disused) ground floor office component facing James Congdon Drive within the existing building to 26 storage units, ranging in area from 3.6m² to 18m². The proposed development will increase the internal storage capacity by 205.4m²
- Alterations to the internal driveway arrangement to create a single one-way entry to the site from Goodenough Street and a one-way internal driveway arrangement with exit only from the access points to Goodenough Street, James Congdon Drive and Rose Street. Sliding gates are proposed for all vehicle access points.

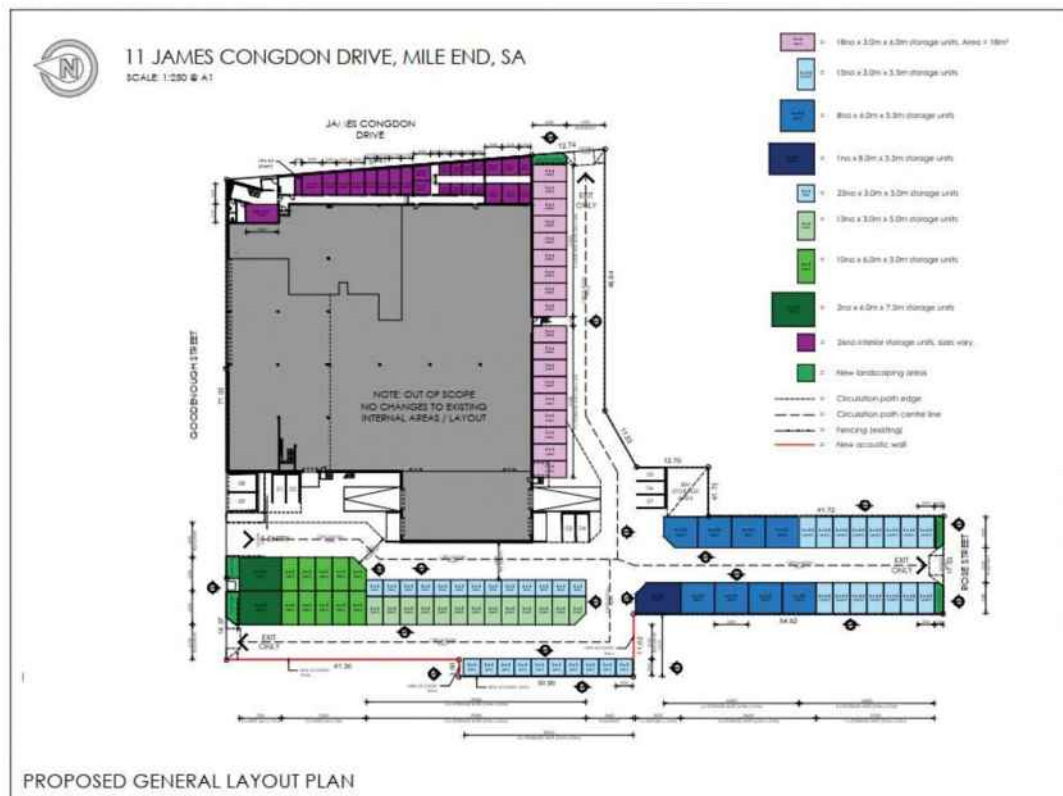
The proposed access arrangement will require the relocation of the Rose Street crossover to a central position along the allotment boundary. This amendment will require the removal of an existing street tree.

- Rearrangement of onsite car parking spaces with 9 formal parking spaces to be retained.
- Construction of a 3 metre high acoustic wall along portion of the western property boundary, where storage units are not proposed to be constructed.
- Addition of landscaping adjacent each of the site's access points.
- Amendments to the approved operating hours of the facility, to allow 24-hour access to the entire site, seven days a week.



Details of the proposed development as illustrated on Figure 3 and further detailed in the attached plans and supporting documents.

Figure 3 Proposed Site Plan



4.1 Technical Assessments

In support of the development application and to guide the design of the proposed development, the following technical assessments have been undertaken:

- Transport and Parking Assessment, prepared by Stantec (Attachment C)
- Environmental Noise Assessment, prepared by Resonate (Attachment D)

A summary of the conclusions for each assessment is provided below:

Transport and parking assessment

- The proposal seeks to maintain 9 dedicated parking spaces within the site, as per the previous approvals for the site.
- The proposed additional storage areas are likely to generate a peak parking demand of 7 to 9 car parking spaces based on Store/Warehouse and empirical parking rates.
- It is expected vehicles will park adjacent to each self-storage area, with the proposal providing more than 10 informal car parking spaces in front of the proposed external units, meeting the parking requirements.

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5 of 14



- The proposed parking layout will be consistent with the relevant Australian/New Zealand Standards.
- The provision of on-site loading facilities is considered appropriate for a domestic use self-storage facility with vehicles up to an 8.8 metre MRV able to access and circulate the site.
- The anticipated traffic generation is considered to be low and not expected to impact on the safety or function of the adjoining road network respectively.

Environmental Noise Assessment

- In order to reduce the potential noise impact from the development to the nearest receivers, the following mitigation is recommended:
 - Installation of a 3m high boundary fence to the western boundary of the site - only required where proposed storage units are not situated along the western boundary.
- With the proposed noise mitigation treatments, the noise emissions from the operation of the proposed development will be able to comply with the relevant environmental noise criteria.
- The proposed additions will be able to operate within the relevant noise provisions in the Code and Environmental Protection (Noise) Policy requirements.

5.0 Procedural Matters

In accordance with the provisions of the Urban Corridor (Boulevard) Zone and the Established Neighbourhood Zone, the proposed alterations and additions to an existing self-storage facility is a Performance Assessed development that is required to be assessed against all relevant provisions of the Code.

The proposed development is not a form of development that is excluded for public notification under Table 5 of the Urban Corridor (Boulevard) Zone or the Established Neighbourhood Zone and therefore public notification is required, unless Council forms the view that the development is of a minor nature, as per clause 1 of Table 5, included in both Zones.

There are six overlays applicable to the subject land which can trigger referrals as outlined within Part 9 of the Code. These overlays include:

- Affordable Housing
- Airport Building Heights (Regulated) (All structures over 15 metres)
- Design
- Major Urban Transport Routes
- Prescribed Wells Area
- Traffic Generating Development

It is noted that the subject site is located within the Major Urban Transport Routes Overlay, due to its frontage to James Congdon Drive. The site is currently serviced by a two-way access point to James Congdon Drive. The proposed development is not creating a new access and there will be no material change to the nature of vehicle movements or an increase to the number or frequency of movements through the existing access. If anything, a lesser impact is anticipated as access to James Congdon Drive will be limited to exit only. On this basis the proposed development is considered to be minor, with regard to any impacts to James Congdon Drive, and therefore a referral to the Commission for Highways is not required.

The proposed development does not trigger any agency referrals having regard to the requirements of the other Overlays.



LMA

The site is subject to an LMA (copy provided in Attachment B). The overall intent of the LMA is to ensure that the four allotments, that form the site, are used together to service the existing building on the site (commercial premises).

Part 3 of the LMA outlines the relevant requirements in terms of the use of the site. The proposed development aligns with the requirements of the LMA.

It is noted that clause 3.5 prevents the *'construction of any boom gate, fence or other device that would prevent vehicle access'* from the adjoining roads to the carpark. The existing gates provided at the site's vehicles entrances were approved as part of the original development application and are required for security purposes associated with the approved use of the site. This feature is to be retained as part of the proposed development.

6.0 Development Assessment

The following development assessment considers the relevant provisions of the Code, and covers the following:

- Land use
- Design and appearance
- Interface between land use
- Parking, site manoeuvring and traffic
- Stormwater management

Land use

The majority of the site is located within the Urban Corridor (Boulevard) Zone, while the two western allotments are within the Established Neighbourhood Zone. It is noted that the Established Neighbourhood zoning of the western portion of the site, does not reflect the longstanding use of the site for commercial purposes.

PO1.1 of the Urban Corridor (Boulevard) Zone envisages a vibrant mix of land use adding to the vitality of the area and an extension of activities outside shop hours. PO 1.1 of the Establishment Neighbourhood Zone seeks predominantly residential development with complementary non-residential activities.

The proposed development comprises alterations and additions to the existing self-storage facility. No changes to the existing land use are proposed.

The proposal therefore continues to contribute to the variety of land uses envisaged in the Urban Corridor (Boulevard) Zone and maintains a complementary non-residential activity on the subject land and within the Established Neighbourhood Zone.

Potential interface issues are discussed in further detail below.

Design and appearance

The key changes to the appearance of the site relate to the construction of the external storage units.

The proposed external storage structures are to be positioned to the rear of the existing substantial warehouse building on the site and the storage units comprise a modest single-storey height and simple form. As such, the proposed structures will not be readily visible from the surrounding roads and locality and will be consistent with the built form character of the site and surrounding properties.

Some of the external storage units and acoustic fencing will be constructed along the boundary shared with the neighbouring residential properties to the west. These structures are of a modest height (up to 3 metres) and akin to a conventional residential outbuilding and therefore, no unreasonable visible intrusion or overshadowing to adjoining properties are expected. It noted that the development on the boundary will minimise potential noise impacts, which is an improvement on the current situation.



Additional landscaping areas along the street frontages have also been introduced to improve the current appearance of the site within the streetscape.

Interface between land uses

Given the land adjoins residential properties to the west and south, the proposed development has the potential to create interface issues, particularly relating to noise and traffic.

To consider these issues an Environmental Noise Assessment and Transport and Parking Assessment have been undertaken.

The Environmental Noise Assessment considered the potential noise impacts on neighbouring residential properties from the proposed additional storage units, alterations to access arrangements and the increase in hours of operation of the facility.

The assessment report considered the potential impacts against the *Environment Protection (Noise) Policy* criteria and recommended specific design mitigation measures to ensure that any impacts are within acceptable limits. These mitigation measures have been incorporated into the proposed development and include 3 metre acoustic fencing along the western property boundary in areas where no storage units are proposed.

General Development Policy *Interface between Land Uses PO 4.1* seeks to ensure that '*development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).*' The corresponding DPF 4.1 requires '*noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.*'

As demonstrated by the Environmental Noise Assessment, the proposed development would meet the relevant *Environment Protection (Noise) Policy* criteria and therefore any noise impacts on surrounding residential properties would not unreasonably impact the amenity of the nearby residents.

The Environmental Noise Assessment acknowledges that there is potential for other noise sources, such as voices, car radios, noise from goods being loaded and unloaded to and from storage units. It is not generally practicable to quantify noise emissions from these sources as they are highly variable.

To minimise potential impacts for the above sources, it is recommended that Kennards inform prospective storage unit tenants of the noise sensitive nature of the surrounding area and include appropriate contract clauses which prohibit unnecessary noise. These management practices are proposed to be adopted.

In addition, the operation of the facility is carefully managed and includes controlled access gates that log customers entering the site, CCTV and short-term contracts that can be terminated should any inappropriate behaviour be observed. These measures will help ensure that any noise and disturbance associated with the facility will be well managed and minimised.

The Transport and Parking Assessment concludes that the proposed parking provisions are sufficient to accommodate demand and the traffic generated by the proposed development is considered to be low and not expected to impact on the safety or function of the adjoining road network. On this basis, minimal interface issues associated traffic and parking are expected.

General Development Policy *Interface between Land Uses PO 6.1* seeks to ensure that '*external lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or lawfully approved sensitive receivers).*'

Lighting is proposed to the front walls of the external storage units. The proposed lights will be orientated internally and screened from adjoining properties by the proposed building and fencing elements which will minimise any light spill impacts.

The lighting details are not provided at this stage and will be confirmed as part of the detailed design. However, it is anticipated that the proposed lighting will be designed in accordance with the relevant Australian Standard to ensure light spill is acceptable. It is noted that this issue can be addressed via a suitable condition if required.



Parking, site manoeuvring and traffic

The Transport and Parking Assessment considered:

- The adequacy of the car parking supply
- The design car parking and driveway layout
- Traffic generation

Based on Stantec's assessment, which considered the Code requirements and car parking demand survey data from a range of self-storage developments, the proposed development requires an additional 7 to 9 car parking spaces to be provided.

The proposed development seeks to maintain nine formal carparking spaces consistent with the previous approval. Given the functionality of storage units, it would be expected that drivers would park alongside their storage unit when using the space. The proposed development provides more than 10 informal car parking spaces which can be used without impacting the safety or movement throughout the site.

Based on the Stantec assessment, the proposed parking arrangement and supply is deemed adequate.

Subject to some minor adjustment to parking spaces 5, 6 and 7 (to be addressed as part of the detailed design) the proposed car parking layout accords with the relevant Australian Standard (AS/NZS2890.1:2004).

A swept path analysis has been undertaken to confirm the design is appropriate with regard to site manoeuvring and circulation requirements. The analysis demonstrates that the proposed layout can accommodate both a Small Rigid Vehicle and Medium Rigid Vehicle allowing both vehicles to access the site, circulate around the site and exit the site in a forward direction.

The proposed development is expected to generate approximately 6 to 9 vehicle trips during the peak hour. The additional 9 vehicle trips to/from the site is considered to be low and is not expected to have an adverse impact on the safety or operation of the adjoining road network.

With regard to the traffic generation, it is noted that the additional external storage units will replace the open storage areas predominantly used by tradespeople for the storage of vehicles, trailers and equipment. Tradespeople using the site tend to access the site more frequently (in some instance daily) to collect and return the stored items. People using the storage are likely to access the site less frequently and therefore, the proposed development may result in a reduction in the traffic generated by the facility.

With regard to the above and the conclusion provided in the Transport and Parking Assessment, the proposed development largely complies with the relevant Transport, Access and Parking provisions of the Code, relating to:

- safe and convenient access;
- minimising interruption to the adjoining road network;
- provision of appropriate onsite vehicle manoeuvring; and
- sufficient car parking.

Stormwater management

With respect to stormwater management, the proposed external storage units will be located on existing sealed areas. As a result, no increase in the extent of impervious surfacing on the land is proposed, and thus, no increase in stormwater flow from the site will occur.

The proposed buildings will be appropriately designed and connected to the existing stormwater system to ensure stormwater is appropriately managed on site and no impacts occurs to neighbouring properties.



7.0 Summary

The proposed development consists of alterations and additions to an existing self-storage facility.

The proposal does not alter the existing use of the land and is consistent with the intent of the Urban Corridor (Boulevard) Zone and Established Neighbourhood Zone.

Potential land use interface issues have been carefully addressed through the appropriate design and siting of buildings, acoustic boundary treatment and operational management practices. In addition, the proposed development is provided with sufficient parking and will not impact on the safety or operation of the adjoining road network.

The proposed development responds positively to the relevant provisions of the Planning and Design Code and accordingly Development Plan Consent is warranted.

Yours faithfully

A handwritten signature in blue ink, appearing to read 'Tom Hateley'.

Tom Hateley
Principal Planner
tom.hateley@aecom.com

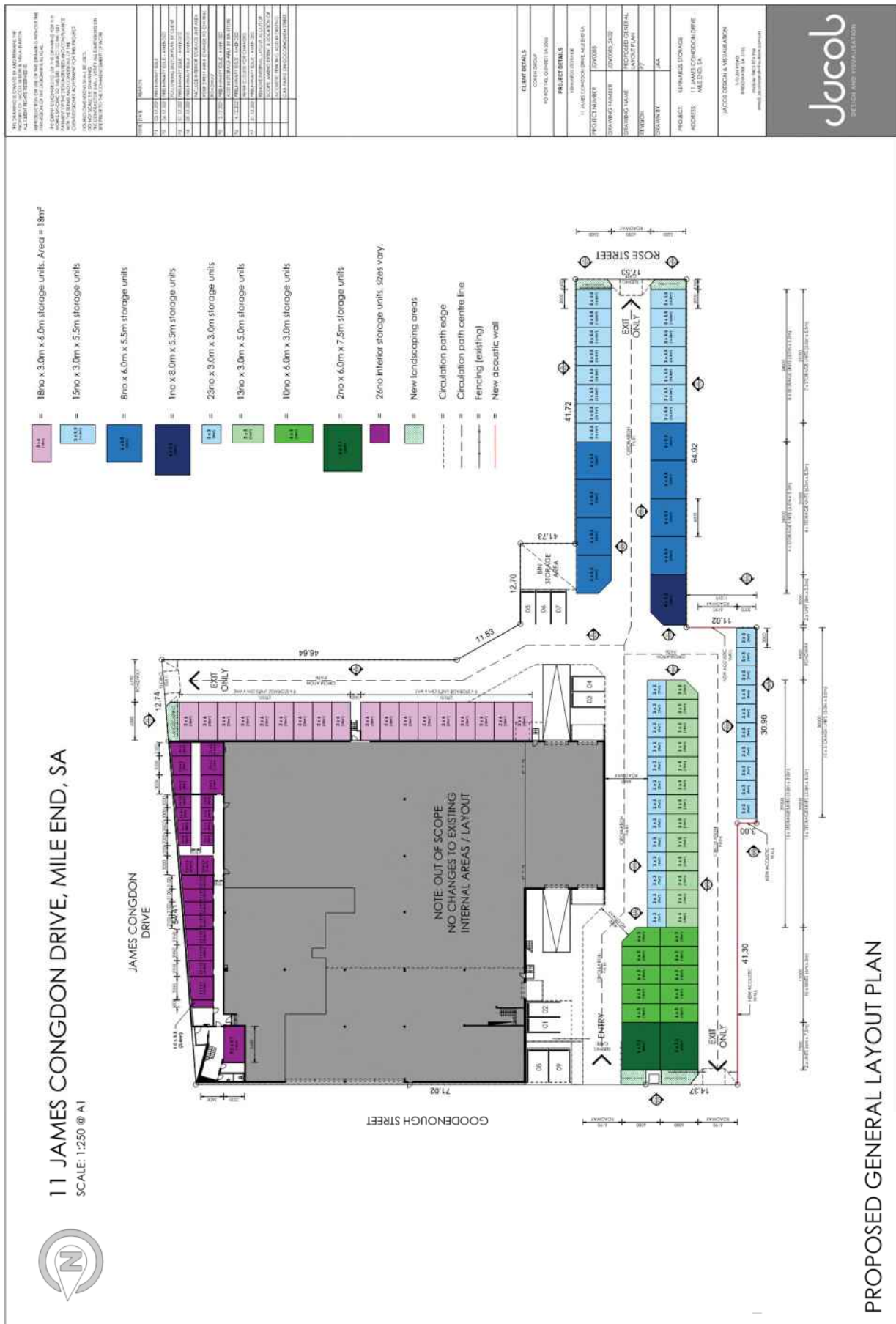
Mobile: +61 418 838 290

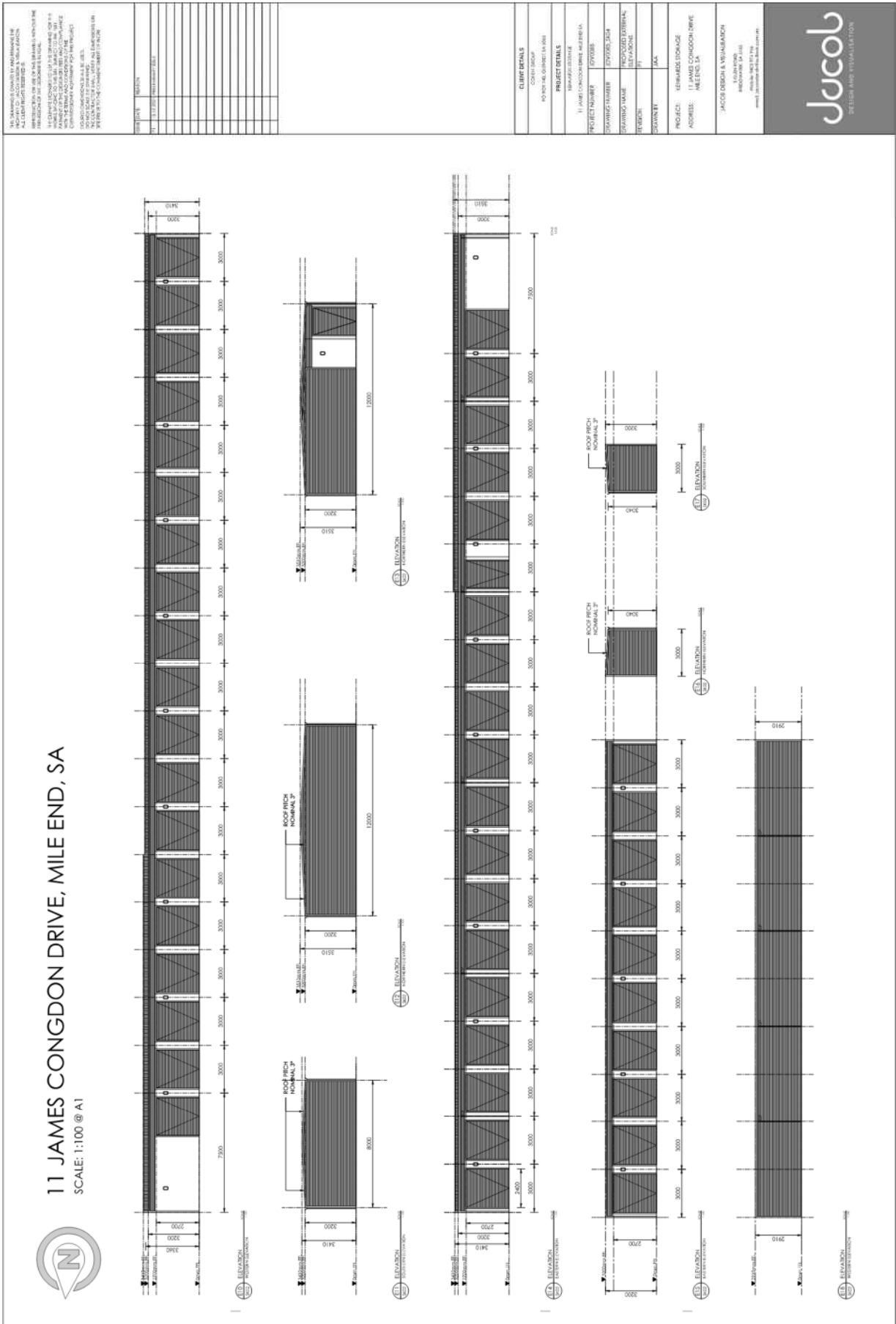
enc: Attachment A - Proposal Plans
Attachment B - Certificates of Title
Attachment C - Transport and Parking Assessment
Attachment D - Environmental Noise Assessment

cc: Richard Stabile, Cohen Group



Attachment A Proposal Plans







Attachment B Certificates of Title and LMA

CERTIFICATE OF TITLE
REAL PROPERTY ACT, 1886



VOLUME 5974 FOLIO 676

Edition 1
Date Of Issue 09/11/2006
Authority TG 10485311

South Australia

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.

David Mulesky



REGISTRAR-GENERAL

WARNING: BEFORE DEALING WITH THIS LAND, SEARCH THE CURRENT CERTIFICATE

REGISTERED PROPRIETOR IN FEE SIMPLE

XAX PROPERTIES PTY. LTD. OF PO BOX 145 GLENSIDE SA 5065

DESCRIPTION OF LAND

ALLOTMENT 5 DEPOSITED PLAN 64577
IN THE AREA NAMED MILE END
HUNDRED OF ADELAIDE

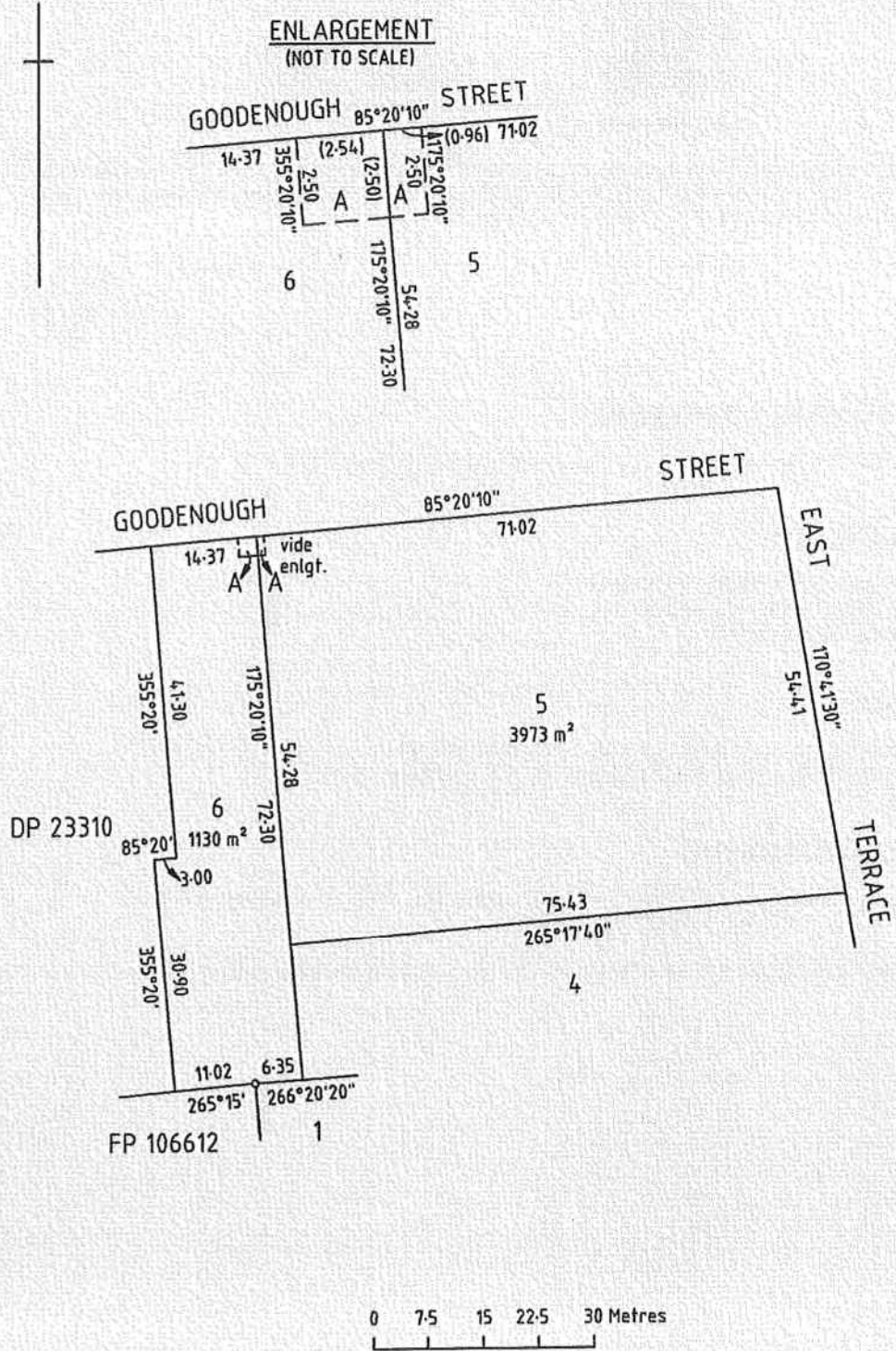
EASEMENTS

SUBJECT TO THE EASEMENT OVER THE LAND MARKED A TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (TG 10485311)

SCHEDULE OF ENDORSEMENTS

- 8710435 LEASE TO ENVELOPES UNLIMITED SUPPLY CO. PTY. LTD. COMMENCING ON 15.7.1999 AND EXPIRING ON 30.6.2009
- 9841004 AGREEMENT UNDER DEVELOPMENT ACT, 1993 PURSUANT TO SECTION 57(2)

VOLUME 5974 FOLIO 676



CERTIFICATE OF TITLE

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IN THE AREA NAMED MILE END
HUNDRED OF ADELAIDE

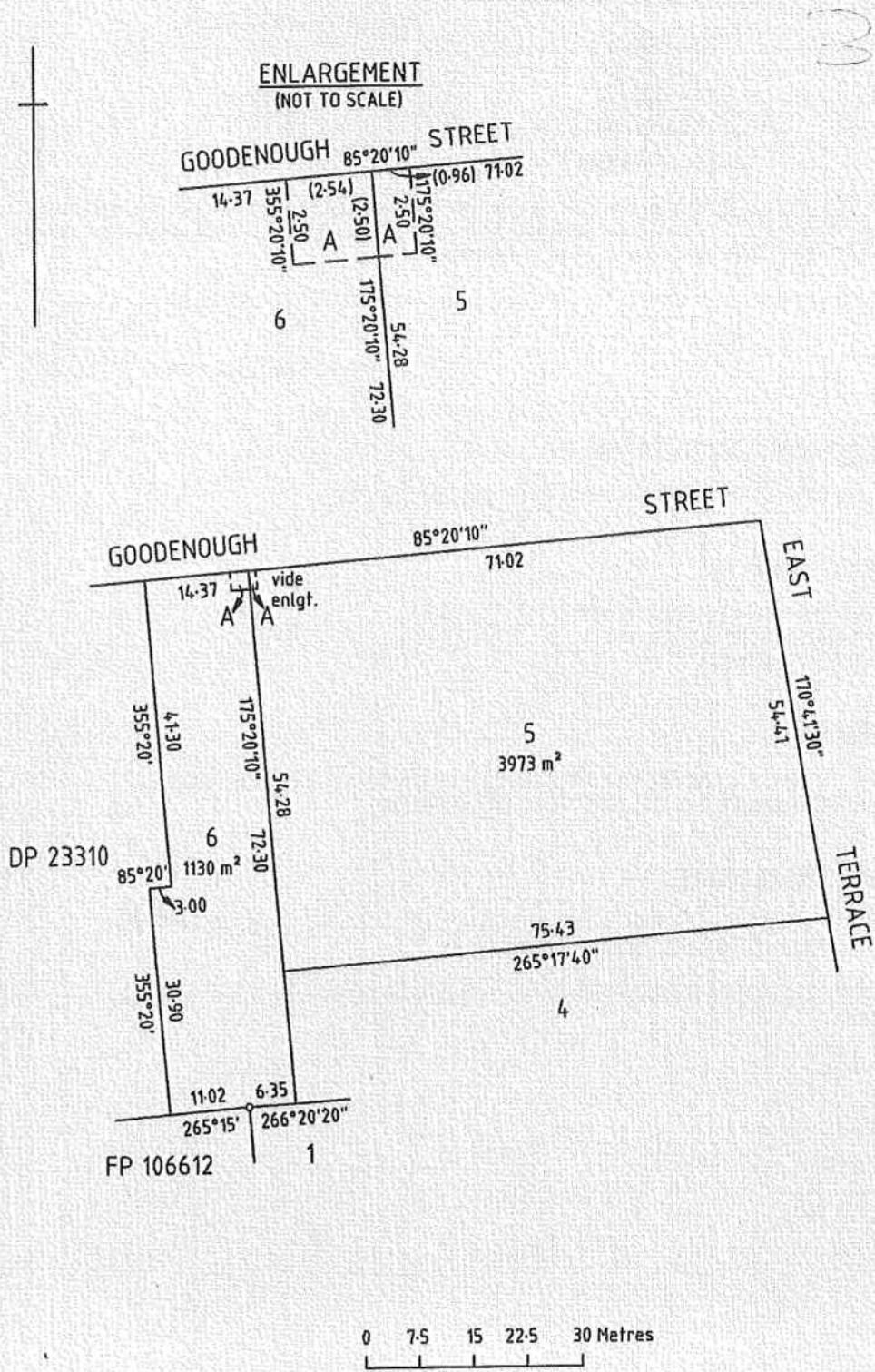
EASEMENTS

SUBJECT TO THE EASEMENT OVER THE LAND MARKED A TO DISTRIBUTION LESSOR CORPORATION (SUBJECT TO LEASE 8890000) (TG 10485311)

SCHEDULE OF ENDORSEMENTS

- 8710435 LEASE TO ENVELOPES UNLIMITED SUPPLY CO. PTY. LTD. COMMENCING ON 15.7.1999 AND EXPIRING ON 30.6.2009
- 9841004 AGREEMENT UNDER DEVELOPMENT ACT, 1993 PURSUANT TO SECTION 57(2)

VOLUME 5974 FOLIO 677



PAGE 2 OF 2

CERTIFICATE OF TITLE

REAL PROPERTY ACT, 1886

VOLUME 5922 FOLIO 943



Edition 2
Date Of Issue 23/08/2004
Authority RTC 9841003

South Australia

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.



REGISTRAR-GENERAL

WARNING: BEFORE DEALING WITH THIS LAND, SEARCH THE CURRENT CERTIFICATE

REGISTERED PROPRIETOR IN FEE SIMPLE

XAX PROPERTIES PTY. LTD. OF PO BOX 145 GLENSIDE SA 5065

DESCRIPTION OF LAND

ALLOTMENT 4 DEPOSITED PLAN 64577
IN THE AREA NAMED MILE END
HUNDRED OF ADELAIDE

EASEMENTS

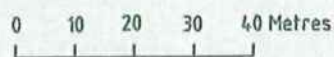
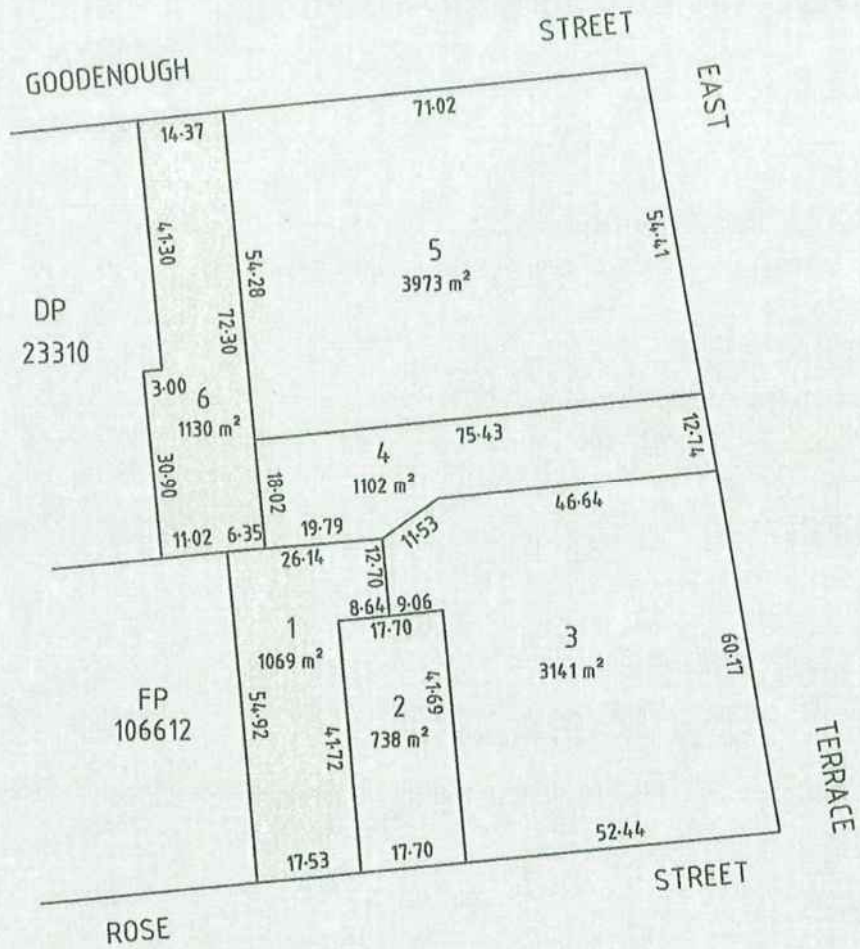
NIL

SCHEDULE OF ENDORSEMENTS

- 8710435 LEASE TO ENVELOPES UNLIMITED SUPPLY CO. PTY. LTD. COMMENCING ON 15.7.1999 AND EXPIRING ON 30.6.2009
- 9841004 AGREEMENT UNDER DEVELOPMENT ACT, 1993 PURSUANT TO SECTION 57(2)

VOLUME 5922 FOLIO 943

443



PAGE 2 OF 2

CERTIFICATE OF TITLE

REAL PROPERTY ACT, 1886

VOLUME 5922 FOLIO 940



Edition 2
Date Of Issue 23/08/2004
Authority RTC 9841003

South Australia

I certify that the registered proprietor is the proprietor of an estate in fee simple (or such other estate or interest as is set forth) in the land within described subject to such encumbrances, liens or other interests set forth in the schedule of endorsements.



REGISTRAR-GENERAL

WARNING: BEFORE DEALING WITH THIS LAND, SEARCH THE CURRENT CERTIFICATE

REGISTERED PROPRIETOR IN FEE SIMPLE

XAX PROPERTIES PTY. LTD. OF PO BOX 145 GLENSIDE SA 5065

DESCRIPTION OF LAND

ALLOTMENT 1 DEPOSITED PLAN 64577
IN THE AREA NAMED MILE END
HUNDRED OF ADELAIDE

EASEMENTS

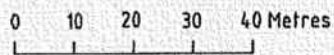
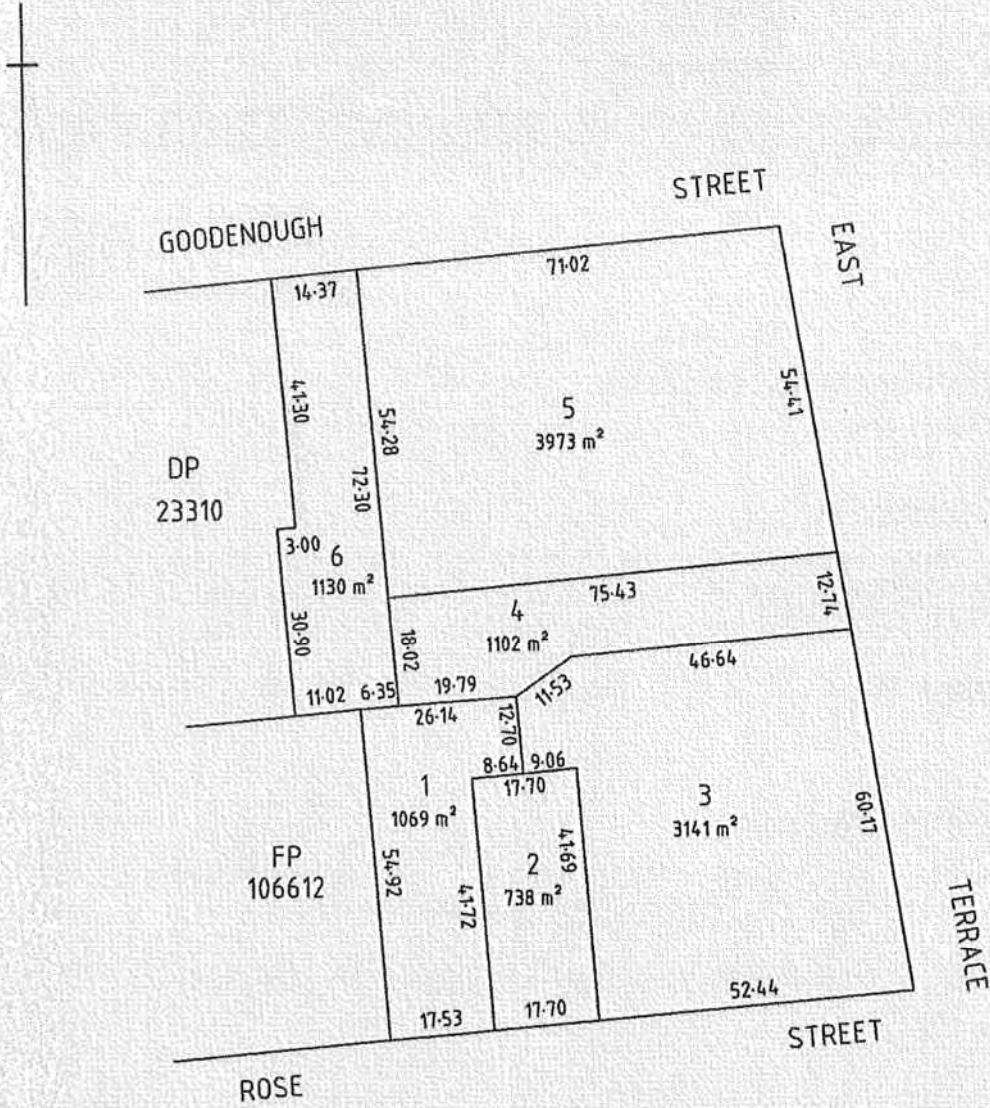
NIL

SCHEDULE OF ENDORSEMENTS

- 8710435 LEASE TO ENVELOPES UNLIMITED SUPPLY CO. PTY. LTD. COMMENCING ON 15.7.1999 AND EXPIRING ON 30.6.2009 OF PORTION (B IN GP 185/1999) TOGETHER WITH CERTAIN RIGHTS
- 9841004 AGREEMENT UNDER DEVELOPMENT ACT, 1993 PURSUANT TO SECTION 57(2)

VOLUME 5922 FOLIO 940

443



PAGE 2 OF 2

186

Orig. AG 9841004



12:14 7-May-2004

2 of 2

Fees: \$94.00

LANDS TITLES REGISTRATION OFFICE SOUTH AUSTRALIA

FORM APPROVED BY THE REGISTRAR GENERAL

BELOW THIS LINE FOR AGENT USE ONLY

CERTIFIED CORRECT FOR THE PURPOSES OF THE REAL PROPERTY ACT 1886

[Signature]

Solicitor/Registered Conveyancer/Applicant

KYLIE CARUSO

AGENT CODE

Lodged by:

Correction to: CAMATTA LEMPENS PTY LTD "CLAC"

TITLES, CROWN LEASES, DECLARATIONS ETC. LODGED WITH INSTRUMENT (TO BE FILLED IN BY PERSON LODGING)

- 1.
2.
3.
4.
5.

Assessor

PLEASE ISSUE NEW CERTIFICATES OF TITLE AS FOLLOWS

- 1.
2.
3.

Table with 2 columns: Series No., Prefix. Values: 2, AG

D 64577

BELOW THIS LINE FOR OFFICE USE ONLY

Table with columns: Date, Time, FEES, R.G.O., POSTAGE, NEW C.T. Value: 94

* TP. This agreement is to be registered on the new CST to issue for Allots 1, 9, 5 & 6.

Table with 2 columns: CORRECTION, PASSED. Values: 0V96, 7/6/04, [Signature]

Table with 2 columns: REGISTERED, REGISTRAR-GENERAL. Values: 2/6/04, [Signature], REGISTRAR-GENERAL

© CORPORATE FORMS PTY LTD (May 2000)

Lic No.1

DELIVERY INSTRUCTIONS (Agent to complete) PLEASE DELIVER THE FOLLOWING ITEM(S) TO THE UNDERMENTIONED AGENT(S)

Table with 2 columns: ITEM(S), AGENT CODE

Form B2

Insert type of

document here **APPLICATION TO NOTE LAND MANAGEMENT AGREEMENT**

(Pursuant to s 57(5) of the *Development Act 1993*)

To the Registrar- General:

1. **CITY OF WEST TORRENS** ("the council") of 165 Burbidge Road, Hilton in the State of South Australia has entered into the attached Land Management Agreement dated the 24 day of March 2004 ("the Agreement") with **REMLAW PTY LTD (ACN 007 961 266)** of 194 Magill Road Norwood SA 5067 as to one undivided second part and **SHEAVE PTY LTD (ACN 008 144 233)** of 345 King William Street Adelaide SA 5000 as to the remaining one undivided second part pursuant to s 57(2) of the *Development Act 1993* ("the Act")

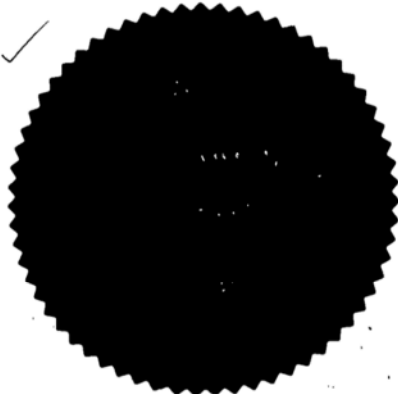
2. The Agreement relates to the management of the whole of Allotments 1, 4, 5 and 6 in Deposited Plan No 64577 being a portion of the land comprised in Certificates of Title Volume 5274 Folios 363 to 366 (inclusive) and Certificates of Title Volume 5274 Folios 368 and 369 ("the Land").

NOW THEREFORE the Council applies pursuant to s 57(5) of the Act to note the Agreement against the Land.

Dated the 24th day of March 2004

THE COMMON SEAL of
CITY OF WEST TORRENS
was hereunto affixed
in the presence of:-

[Signature]
Mayor
[Signature]
Chief Executive Officer



THE COMMON SEAL of
REMLAW PTY LTD (ACN 007 961 266)
was affixed with the authority of the Board
of Directors and in the presence of:-

[Signature]
Director
[Signature]
Director/Secretary



THE COMMON SEAL of
SHEAVE PTY LTD (ACN 008 144 233)
was affixed with the authority of the Board
of Directors and in the presence of:-

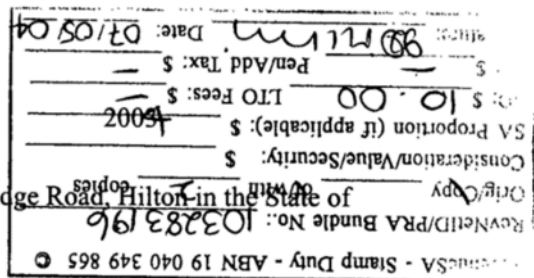
[Signature]
Director
[Signature]
Director/Secretary



NB: This form may be used only when no panel form is suitable. A penalty of up to \$2000 or 6 months imprisonment applies for improper witnessing.

THIS DEED made the 24th day of March

BETWEEN: **CITY OF WEST TORRENS** of 165 Burbridge Road, Hilton in the State of South Australia ("the council")



AND The person named in Item 1 of the Schedule of the address specified in Item 1 of the Schedule ("the owner")

RECITALS

- A. The owner has applied for amalgamation of the land specified in Item 2 of the Schedule ("land").
- B. The land is currently leased to Salmat Document Management Solutions Pty Ltd with the land being utilised as both commercial premises and an accompanying carpark.
- C. The council wish to ensure that the occupier of the commercial premises has continued access to the carparking facilities that form a portion of the land.
- D. It is voluntarily agreed by the parties that the amalgamation of the land by the owner shall be proceeded by the owner entering into this Deed with the council.
- E. This Deed is entered into by the parties pursuant to *Section 57 of the Development Act 1993* as the Land Management Agreement relating to the continued management, preservation and conservation of the land following the amalgamation of the land by the owner.
- F. The owner and the council have agreed to enter into this Deed pursuant to *Section 57 of the Development Act 1993*.

NOW THIS DEED WITNESSETH and it is agreed by and between the parties:-

1. *Recitals*

The parties acknowledge and declare both that the matters referred to in the Recitals to this Deed are true and correct in every particular and that the Recitals shall form part of this Deed.

2. *Definition and Interpretation*

2.1 Definitions

In this Deed unless the context otherwise requires the following words and expressions shall have the following meanings:

- 2.1.1 *The land* means the land specified in Item 2 of the Schedule and includes any improvements situated thereon;
- 2.1.2 *The owner* means the person named in Item 1 of the Schedule as the owner during the term of this deed and where the owner is a company includes its successors, assigns and transferees and where the owner is a person, includes his heirs, executors, administrators and transferees and where the owner consists of more than one person or company the term includes each and every one or more of such persons or companies jointly and each of them severally and their respective successors, assigns, heirs, executors, administrators and transferees of the companies or persons being registered or entitled to be registered as the proprietor of an estate in fee simple to the land or to each of them of all separate allotments into which the land may be divided after the date of this deed subject however to such encumbrances, liens and interests as are registered and notified by memoranda endorsed on the Certificate of Title thereof;
- 2.1.3 *The Schedule* means the Schedule attached to this Deed;
- 2.1.4 *The term* means the duration of the existing Lease held by Salmat Document Management Solutions Pty Ltd over the land or until this Deed is mutually surrendered by an Agreement between the owner and the council whichever is the earlier;
- 2.1.5 *Commercial Premises* means the portion of the land marked as “Commercial Premises” on the plan at Annexure B hereto.;
- 2.1.6 *Carpark* means the portion of the land marked as “Carpark” on the plan at Annexure B hereto.

2.2 Interpretation

Subject to any inconsistency of subject or context the following rules of construction shall be used in interpretation of this Deed;

- 2.2.1 Any word importing the plural includes a singular and vice versa;
- 2.2.2 Any word importing a gender shall include all other genders;
- 2.2.3 Reference to a body corporate shall include the natural person and vice versa;
- 2.2.4 Reference to a recital party clause schedule or annexure is reference to a recital party clause schedule or annexure to this Deed;
- 2.2.5 The captions heading section numbers and clause numbers appearing in this Deed are inserted only as a matter of convenience and in no way effect the construction of this Deed;
- 2.2.6 A reference to a statute shall include all statutes amending, consolidating or replacing the statutes referred to and shall also include any statutory instruments issued under the statute (including regulations, proclamations, rules, by-laws, orders, notices and codes or standards approved or adopted under the statute;
- 2.2.7 When two or more persons are parties to this Deed as owner the covenants obligations and agreement on their part contained in this Deed refer to and shall bind them or any two or greater number of them jointly and each of them severally;

3. *Covenants by the Owner*

- 3.1 The owner shall not cause, suffer or permit the use of the commercial premises unless the carpark is available for use by the commercial premises as contemplated by clauses 3.2 to 3.5 (inclusive);
- 3.2 The owner shall not cause, suffer or permit the use of the accompanying carpark for any purposes other than as a carpark or other uses associated with the commercial premises;

- 3.3 The owner shall ensure that the accompanying carpark is maintained at all times as a carpark to the reasonable satisfaction of the council;
- 3.4 The owner shall ensure that the carpark is open and available without charge to the patrons of the commercial premise at all times;
- 3.5 The owner shall not cause, suffer or permit the construction of any boom gate, fence or other device that would prevent vehicle access from Goodenough Street, East Terrace or Rose Street to the carpark.

4. *Release*

The owner uses, occupies and keeps the land at the risk in all things as the owner and the owner hereby releases to the full extent permitted by law the council from all past, present and future claims, demands, actions, losses, costs and expenses which the owner may have or may have had or may suffer in respect of the land caused because of the owner's compliance with Clause 3 of this Deed.

5. *Execution of Deed Poll*

The owner shall prior to the sale of the land to any third party at any time during the term procure the execution by the said third party of three (3) original copies of a Deed Poll in the form of the relevant Deed Poll annexed to this Deed and shall prior to any such sale to the said third party provide the council with a signed original of the Deed Poll duly executed by the said third party.

6. *Compliance*

The owner shall not grant any lease, licence, easement or other right of any nature whatsoever which may give any person the right to possession or control of or entry on to the land which right would enable such persons to breach any of the obligations imposed on the owner by this deed unless such grant:

- 6.1 is expressed in writing;
- 6.2 is made with previous written consent of the council; and
- 6.3 contained as an essential term a covenant by the guarantee not to do or admit to do (or suffer or permit any other person to do or admit to do)

any act, matter or thing which would constitute a breach by the owner of the owner's obligations under this deed.

7. *Council's Powers of Entry, etc*

7.1 The council and any employer or agent of the council authorised by the council may at any reasonable time enter the land for the purpose of:

7.1.1 inspecting the land and any building or structure thereupon;

7.1.2 exercising any other powers of the council under this deed or pursuant to law;

7.2 If the owner is in breach of any provision of this deed, the council may, by notice in writing served on the owner, specify the nature of the breach and require the owner to remedy the breach within such time as may be nominated by the council in the notice (being not less than twenty eight (28) days from the date of service of the notice) and if the owner fails to remedy the breach, the council or its servants or agents may carry out the requirements of the notice and in doing so may enter and perform any necessary works upon the land and recovery any costs thereby incurred from the owner.

7.3 If in a notice referred to in clause 7.2 the council requires the removal of the building or structure from the land the council and its servants or agents are hereby authorised and empowered by the owner to enter and remove the building or structure from the land and to dispose of it in any manner determined by the council provided that if the building or structure shall have any monetary value then the council shall use its best endeavours to realise that monetary value and shall after the disposal account to the owner and pay to him the realised value less all expenses incurred.

7.4 The council may delegate any of its powers under this Deed to any person.

8. *Severance*

If any term or condition of this Deed shall be for any reason unlawful, void, invalid or unenforceable then the offended term or condition shall be severed

here from without affecting the validity or enforceability the remainder of this Deed.

9. *Entire Agreement*

This Deed constitutes the entire Agreement of the parties in respect to the subject matter hereof and the parties agree that the Deed supersedes all prior arrangements, understandings and negotiations in respect of the matters dealt with in this Deed. Further, no agreement in respect to the continued management, preservation and conservation of the land, whether collateral or otherwise, shall be taken to have been formed between the parties by reason of any promise, representation, inducement or undertaking (if any) given or made by one party to the other prior to the date of execution of this Deed.

10. *Modification*

No variation, addition to or modification of any provision of this Agreement shall be binding upon the parties unless such variation, addition or modification is made in writing, signed by each party and expressed to be either supplemental to or in substitution for the whole or part of this Deed.

11. *Waiver*

11.1 A waiver of any provision of this Deed must be in writing and be signed by the parties or by persons duly authorised to execute such a document on a parties part;

11.2 No waiver by a party of a breach of a term or condition contained in this Deed shall operate as a waiver of any breach of the same or any other term or condition contained in this Deed;

11.3 No forbearance, delay or indulgence by either party in enforcing the provisions of this Deed shall prejudice or restrict the rights of that party;

12. *Notices*

Notices may be served by delivering the same personally, by mail or facsimile transmission and shall be:-

12.1 In writing addressed to the parties at their respective addresses as set out in this Deed;

- 12.2 Signed by the sender or on behalf of the sender by a person duly authorised by the sender in that regard;
- 12.3 Deemed to be duly served or made in the following circumstances;
- 12.3.1 if delivered personally;
- 12.3.2 if sending by mail 2 days after posting;
- 12.3.3 if sent by facsimile upon the sender's facsimile producing a transmission report that the facsimile was satisfactorily sent in its entirety to the addressee's facsimile number;
- 12.3.4 but if delivery is not made before 5.00 pm on any day it shall be deemed to be received at 9.00 am on the next day and at that place;
- 12.4 A party may modify either its address or facsimile number from time to time by written notice served on the other party.

13. *Costs and Stamp Duty*

The owner hereby indemnifies the council and agrees to keep it forever indemnified in respect to the whole of its costs and expenses (including without limitation legal costs and expenses) of and incidental to the negotiation, preparation, stamping and registration of this Deed.

14. *Registration*

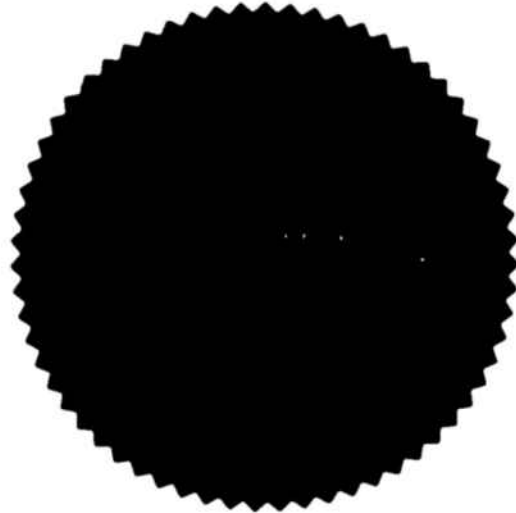
- 14.1 The owner hereby consents to and requests a registration of this Deed pursuant to *Section 57(5) of the Development Act 1993*;
- 14.2 The parties shall do and execute all acts, documents and things as are necessary to ensure that this Deed is registered in the Lands Titles Registration Office and a memorial of this Deed entered on the Certificates of Title for the land pursuant to *Section 57(5) of the Development Act 1993* in priority to any other registrable interest save and except for the estate and interest of the owners therein.
- 14.3 Certification by owners except for those persons who have a legal interest in the land and who have consented to the owner entering into this Deed by signing the execution clause at the end of this Deed,

the owner hereby certifies that pursuant to *Section 57(4) of the Development Act 1993* that no other person has a legal interest in the land.

Executed unconditionally as a Deed.

THE COMMON SEAL of
CITY OF WEST TORRENS
was affixed in the presence of:-

)
)
)



Mayor
Name

Chief Executive Officer

Executed for and on behalf of
REMLAW PTY LTD
by authority of the directors
in the presence of:

)
)
)
)
)
)

DIRECTOR

SECRETARY

Executed for and on behalf of
SHEAVE PTY LTD
by authority of the directors
in the presence of:

A.S. Barber

)
)
)
)
)

DIRECTOR - Sole

SECRETARY

SCHEDULE**ITEM 1 OWNER****REMLAW PTY LTD (ACN 007 961 266)**

Of 194 Magill Road Norwood SA 5067 of one undivided second part

and SHEAVE PTY LTD (ACN 008 144 223)

Of First Floor, 345 King William Street, Adelaide SA 5000 of one undivided second part

ITEM 2 LAND DESCRIPTION

That piece of land in the Hundred of Adelaide being Allotment numbered 1, 4, 5 and 6 in the Application for Deposited Plan attached as Annexure "A" to this Deed.

ITEM 3 FULL NAME OF PERSON WITH LEGAL INTEREST IN THE LAND (IF ANY)

1. **ADELAIDE BANK LTD (ACN)**
of 169 Pirie Street Adelaide SA 5000

2. **SALMAT DOCUMENT MANAGEMENT SOLUTIONS PTY LTD (ACN 000 553 284)**
of 152 Miller Road Chester Hill NSW 2162


ITEM 4 FULL DETAILS OF LEGAL INTEREST HELD BY PERSON REFERRED TO IN ITEM 3

1. Mortgagee
2. Lessee of the premises

SCHEDULE

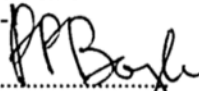
The person named in Item 3 of the Schedule who has a legal interest in the land by virtue of the interest specified in Item 4 of the Schedule hereby consents to the owner entering into this Deed.

EXECUTED by **ADELAIDE BANK LIMITED**
who consents to the within Land Management Agreement:

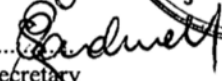
ADELAIDE BANK LIMITED ABN 54 061 461 550 BY ITS APPOINTED ATTORNEY John Michael Smedley Senior Business Banking Manager Property OF 169 PIRIE STREET ADELAIDE 5000 UNDER POWER OF ATTORNEY No. 9163527 (ABL)	IN THE PRESENCE OF: SIGNATURE OF WITNESS  ----- FULL NAME OF WITNESS MATT JOWY ----- ADDRESS OF WITNESS 169 PIRIE ST ADELAIDE
--	--

THE COMMON SEAL of
SALMAT DOCUMENT MANAGEMENT SOLUTIONS PTY LTD (ACN 000 553 284)
was affixed with the authority of the Board of Directors and in the presence of:-

PETER RICHARD BOYLE


Director

STEPHEN GEORGE BARDWELL


Director/Secretary



ANNEXURE

DEED POLL

Date:

TO:

AND TO: **CITY OF WEST TORRENS** of 165 Burbridge Road, Hilton in the State of South Australia

I >> of >>

and I, > of >

jointly and severally, acknowledge and agree as follows:-

1. I/We am/are aware that (delete whichever is inapplicable) the whole of the land comprised and described in Certificate of Title Register Book Volume > Folio> /that portion of the land comprised and described in Certificate of Title Register Book Volume > Folio > (“the Land”) is subject to a Land Management Agreement, being registered as Land Management Agreement No. > (“the Land Management Agreement”).
2. I/We have read the Land Management Agreement and am/are conversant with its terms and conditions.
3. I/We acknowledge that I/we will comply with and am/are bound by the terms of the Land Management Agreement at all times while I/we am/are either registered or entitled to be registered as proprietor of the Land.

SIGNED by the said)
 >)
 in the presence of:) _____

 Witness
 Name:

SIGNED by the said)
 >)
 in the presence of:) _____

 Witness
 Name:

THE ALTERNATIVE DEED POLL BELOW ONLY TO BE USED WHERE A COMPANY IS THE PURCHASER

ANNEXURE

DEED POLL

Date:

TO:

AND TO: **CITY OF WEST TORRENS** of 165 Burbridge Road, Hilton in the State of South Australia

I >> of >>

Director of >>>

and I, > of >

Director of >>>

jointly and severally, acknowledge and agree as follows:-

1. We are aware that (delete whichever is inapplicable) the whole of the land comprised and described in Certificate of Title Register Book Volume > Folio > /that portion of the land comprised and described in Certificate of Title Register Book Volume > Folio > (“the Land”) is subject to a Land Management Agreement, being registered as Land Management Agreement No. > (the “Land Management Agreement”).
2. We have read the Land Management Agreement and are conversant with its terms and conditions.
3. We, [the Directors of the Company] acknowledge that we will use our best endeavours to ensure that [name of company] complies with the terms of the Land Management Agreement at all times while [name of company] is registered or entitled to be registered as proprietor of the Land.
4. [name of company] acknowledges that it will comply with and is bound by the terms of the Land Management Agreement at all times while [name of company] is registered or entitled to be registered as proprietor of the Land.

SIGNED by the said)
(Director))

in the presence of:)

Witness
Name:

SIGNED by the said)
(Director))
in the presence of:)

Witness
Name:

THE COMMON SEAL of)
(Company))
was hereunto affixed in the)
presence of:)

Director
Name:

Secretary
Name:



Attachment C Transport and Parking Assessment

I:\legacy\aecom_projects\60667845 cohen group planning\400_technical\430_technical_working_documents\1.0 kennards mile end\da submission\60667845 kennards mile end - development application letter - draft.docx
13 of 14

Stantec Australia Pty Ltd.
Level 5, 75 Hindmarsh Square, Adelaide SA 5000



December 22, 2021
File: 301401631

Attention: Andrew Raeburn
AECOM
Level 28, 91 King William Street
Adelaide, Australia

Dear Andrew,

Reference: Storage Facility Expansion – James Congdon Drive, Mile End

BACKGROUND

Stantec has completed a transport and parking assessment of the proposed development at 11 James Congdon Drive, Mile End. The proposed development incorporates an expansion of the existing storage facility to incorporate additional internal and external storage units.

EXISTING CONDITIONS

SUBJECT SITE

The subject site is located at 11 James Congdon Drive, Mile End and is under the *Established Neighbourhood* zone and *Urban Corridor (Boulevard)* zone. The site of approximately 7,380 sq.m has frontages of 68 metres to James Congdon Drive, 85 metres to Goodenough Street and 18 metres to Rose Street. Its current use is warehouse storage facility by *Kennards Self Storage*. Based on the information provided, the previous approvals for the subject site provided a total 7 car parking spaces, accessed from James Congdon Drive.

An overview of the subject site and its surrounds is shown in Figure 1.

Design with community in mind

December 22, 2021
 Andrew Raeburn
 Page 2 of 10

Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Figure 1: Subject Site and Surrounding Environs



ROAD NETWORK/ACCESS

The adjoining roads in relation to the subject site are Goodenough Street (local road with a 11 metre carriageway) that runs along the northern side of the site, James Congdon Drive (sub-arterial road with a 19 metre carriageway) that runs along the eastern side of the site and Rose Street (local road with a 12 metre carriageway) that runs along the southern side of the site. There is currently one entry and one exit access point to the subject site from Goodenough Street that is used for the public and an access point from Rose Street. There is also an entry/exit access point accessing James Congdon Drive.

PROPOSED DEVELOPMENT

The proposed development includes the expansion of the existing storage facility with an additional Gross Leasable Floor Area (GLFA) of approximately 1,750 sqm through the provision of external storage units and some additional internal storage units. The existing storage units are to be maintained.

The proposed development retains the provision of 9 car parking spaces across the site, with informal car parking provided in front of the external storage units.

Access to/from the site will utilise the existing access points, providing entry and exit to Goodenough Street and exit only to Rose Street and James Congdon Drive.

An overview of the proposed development layout and access arrangement is presented in Figure 2.

December 22, 2021
 Andrew Raeburn
 Page 3 of 10

Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Figure 2: Proposed Site Plan Layout and Access Arrangements



CAR PARKING ASSESSMENT

SOUTH AUSTRALIAN PLANNING AND DESIGN CODE

The South Australian Planning and Design Code sets out the minimum and maximum car parking rates for a non-residential development in Table 2 - Off-Street Car Parking Requirements in Designated Areas. The subject site is located both within an Urban Corridor (Boulevard) Zone and a Established Neighbourhood Zone. Based on the Planning and Design Code, a non-residential development in an Urban Corridor (Boulevard) Zone would require a minimum 3 spaces per 100 sq.m and a maximum of 5 spaces per 100 sq.m. Stantec believes these rates are high and not reflective of a storage facility.

Therefore, the rate that is considered to be applicable from Table 1 - General Off-Street Car Parking Requirements is a Store/Warehouse, requiring a parking rate of 0.5 spaces per 100sq.m.

December 22, 2021
 Andrew Raeburn
 Page 4 of 10

Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Based on an additional storage area of 1,750sq.m, the proposal generates an additional parking requirement of 9 parking spaces.

EMPIRICAL ASSESSMENT – SELF STORAGE FACILITY

The proposed land use is a self-storage facility which employs less people and attracts less visitors than a typical warehouse use. Stantec has previously compiled a car parking demand survey data for a range of self-storage developments. The results of these surveys indicate that self-storage developments generate an average car parking demand of 0.37 spaces per 100 square metres of floor area on the weekend which was the parking demand peak. Using the above rate to the proposed floor area, the proposed development could be expected to generate a peak parking demand of 6 spaces.

An overview of the Parking assessment is presented in Table 1.

Table 1: Empirical Assessment

Type	(GLFA) Sq.m	Parking rate per 100 sq.m	Spaces required
Storage Facility	1,750	0.37	7

ADEQUACY OF CAR PARKING SUPPLY

The previous approvals for the existing storage facility provided a total of 7 car parking spaces within the site. The proposal seeks to maintain 9 formal car parking spaces across the site, and maintain the existing storage units as per the previous approvals.

In regards to the additional storage units, based on either the Planning and Design Code Store/Warehouse rates or the empirical assessment, the development requires an additional 7 to 9 car parking spaces to be provided across the site.

Given the functionality of storage units, it would be expected that drivers would park alongside their storage unit when using the space. The proposal provides more than 10 informal car parking spaces and would not be expected to negative impact on the safety or movement throughout the site.

Therefore, based on the above assessment, the parking supply is deemed adequate for the proposed development.

CAR PARKING LAYOUT

The proposed car parking layout is generally consistent with the guidelines set out within the Australian / New Zealand Standards for off-street car parking (AS/NZS2890.1:2004).

Some of the key design features are described below:

- 90 degree angled parking spaces shall be 2.5 metres wide and 5.4 metres long, set within a minimum 5.8 metre wide aisle, meeting the requirements for User Class 2 parking facility.

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Andrew Raeburn
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Reference: Storage Facility Expansion – James Congdon Drive, Mile End

- Informal parallel parking spaces can provide a minimum 2.7 metres wide parking space, maintaining a minimum 3.5m wide circulation roadway.
- Minimum 300mm clearance shall be provided between parking spaces and vertical obstructions (walls/columns). It is recommended that parking spaces 5, 6 and 7 be adjusted to meet these requirements. This can be reviewed in detailed design.
- The ability for a B99 to circulate around the site is outlined in Figure 3

Figure 3: B99 Site Circulation



LOADING AND WASTE COLLECTION

No formal loading areas are provided on-site. It is envisaged that any commercial vehicles loading at the storage facility will use the parallel parking spaces or informal parking adjacent to the storage units.

A swept path assessment of the access/egress and circulatory road within the site by an 8.8 metre Medium Rigid Vehicle (MRV) is shown in Figure 4 and Figure 5. Both vehicles can access the site, travel around the site and exit the site in a forward motion.

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Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Figure 4: 8.8m MRV Site Circulation 1



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Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Figure 5: 8.8m MRV Site Circulation 2



The existing access into the building is provided via existing ramps on the northern and southern sides of the building. It is considered that access into the building is to be via vehicles up to a 6.4m Small Rigid Vehicle (SRV). Swept path assessment for the 6.4m SRV is outlined in Figure 6.

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Andrew Raeburn
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Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Figure 6: 6.4m SRV Building Access



TRAFFIC GENERATION

DESIGN RATES

RMS Guide

Traffic generation rates have been sourced from the Roads and Maritime Services of New South Wales (formerly RTA) in the “*Guide to Traffic Generating Developments*” published in 2002 (henceforth referred to as the RTA Guide). The rate applicable to the prosed development is:

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Reference: Storage Facility Expansion – James Congdon Drive, Mile End

Warehouse **Peak Hour Trips** **0.5 movements per 100 sq.m**

Stantec database

Stantec has conducted traffic surveys at multiple self-storage facilities across Australia, with the applicable traffic generation rate as follows:

Self-Storage **Peak Hour Trips** **0.3 movements per 100 sq.m**

TRAFFIC GENERATION ASSESSMENT

Estimates of peak hour volumes resulting from the proposal are set out in Table 2.

Table 2: Traffic Generation Estimates

Size (sq.m)	Rate Source	Traffic generation Rate (Movements per hour)	Vehicle Movements / Hour
1,750	GTA (now Stantec database)	0.3 / 100 sq.m	6
	RTA Guide	0.5 / 100 sq.m	9

Based on the above, it is expected the storage facility to generated approximately 6 7to 9 vehicle trips during the peak hour.

TRAFFIC IMPACT

On the above basis, the additional 9 vehicle trips to/from the site is considered to be low and is not expected to have an adverse impact on the safety or operation of the adjoining road network.

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Reference: Storage Facility Expansion – James Congdon Drive, Mile End

CONCLUSION

Based on the analysis and discussions presented within this report, the following conclusions are made:

1. The proposed development generates a statutory car parking requirement of approximately 1,750 sq.m (GLFA).
2. The proposal seeks to maintain 9 dedicated parking spaces within the site, as per the previous approvals for the site.
3. The proposed additional storage areas are likely to generate a peak parking demand of 7 to 9 car parking spaces based on Store/Warehouse and empirical parking rates.
4. It is expected to park adjacent to each self-storage area, with the proposal providing more than 10 informal car parking spaces in front of the proposed external units, meeting the parking requirements.
5. The proposed parking layout will be consistent with the dimensional requirements as set out in the Australian/New Zealand Standards for Off-Street Car Parking (AS/NZS2890.1:2004 and AS/NZS2890.6:2009).
6. The provision of on-site loading facilities is considered appropriate for a domestic use self-storage facility with vehicles up to an 8.8 metre MRV around the site and a 6.4m SRV through the building.
7. The site is expected to generate between 6 and 9 vehicle movements during the peak hour.
8. The anticipated traffic generation is considered to be low and not expected to impact on the safety or function of the adjoining road network respectively.

Regards,

Stantec Australia Pty Ltd.



Paul Morris M.Trans&Traff, MAITPM
Senior Principal Transport Engineer,
Group Leader Transportation SA
Phone: +61 8 8334 3604

paul.morris@stantec.com

December 17, 2021

10



Attachment D Environment Noise Assessment

I:\legacy\aecom_projects\6066\60667845 cohen group planning\400_technical\430_technical_working_documents\1.0 kennards mile end\da submission\60667845 kennards mile end - development application letter - draft.docx
14 of 14

Resonate

11 James Congdon Drive, Mile End

Environmental Noise Assessment

A210937RP1 Revision B
Tuesday, 11 January 2022

www.resonate-consultants.com

**Document Information**

Project	11 James Congdon Drive, Mile End
Client	Cohen Group
Report title	Environmental Noise Assessment
Project Number	A210937

Revision Table

Report revision	Date	Description	Author	Reviewer
0	16/12/2021	First Issue	Lachlan Newitt	Nick Henrys
A	22/12/2021	Response to comments	Lachlan Newitt	Nick Henrys
B	11/01/2021	Response to comments	Lachlan Newitt	Nick Henrys

Resonate

Glossary

A-weighting	A spectrum adaption that is applied to measured noise levels to represent human hearing. A-weighted levels are used as human hearing does not respond equally at all frequencies.
Characteristic	Associated with a noise source, means a tonal, impulsive, low frequency or modulating characteristic of the noise that is determined in accordance with the Guidelines for the use of the Environment Protection (Noise) Policy (Noise EPP) to be fundamental to the nature and impact of the noise.
Continuous noise level	A-weighted noise level of a continuous steady sound that, for the period over which the measurement is taken using fast time weighting, has the same mean square sound pressure as the noise level which varies over time when measured in relation to a noise source and noise-affected premises in accordance with the Noise EPP
Day	Between 7 am and 10 pm as defined in the Noise EPP
dB	Decibel—a unit of measurement used to express sound level. It is based on a logarithmic scale which means a sound that is 3 dB higher has twice as much energy. We typically perceive a 10 dB increase in sound as a doubling of loudness.
dB(A)	Units of the A-weighted sound level.
Frequency (Hz)	The number of times a vibrating object oscillates (moves back and forth) in one second. Fast movements produce high frequency sound (high pitch/tone), but slow movements mean the frequency (pitch/tone) is low. 1 Hz is equal to 1 cycle per second.
Indicative noise level	Indicative noise level determined under clause 5 of the Noise EPP.
L ₉₀	Noise level exceeded for 90 % of the measurement time. The L ₉₀ level is commonly referred to as the background noise level.
L _{eq}	Equivalent Noise Level—Energy averaged noise level over the measurement time.
L _{max}	The maximum instantaneous noise level.
Night	Between 10.00 p.m. on one day and 7.00 a.m. on the following day as defined in the Noise EPP
Noise source	Premises or a place at which an activity is undertaken, or a machine or device is operated, resulting in the emission of noise
Quiet locality	A locality is a quiet locality if the Planning & Design Code provisions that make land use rules for the locality principally promote land uses that all fall within either or both of the following land use categories: (a) Residential; (b) Rural Living;



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1 Introduction

This report outlines the environmental noise assessment for the proposed extension of the Kennards Self Storage located at 11 James Congdon Drive, Mile End. The proposed development includes formalising 24 hour operation of the site and the construction of 90 external and 26 internal storage lockers. The external storage lockers will be constructed in place of existing outdoor vehicle storage.

The development will result in changes to vehicle movements through the site, including the usage of the crossover at Rose Street along the southern boundary. The major noise sources associated with the development are likely to be light and heavy vehicle movements through the site, and occasional noise from storage locker loading activity. The closest noise affected receptors are located along the western boundary of the site, fronting both Goodenough and Rose Streets.

The potential noise emissions from the development have been assessed against the requirements of the Planning & Design Code and the South Australian Environmental Noise Policy, 2007 (the *Noise EPP*).

Resonate

2 Proposed development

The proposed development is located at 11 James Congdon Drive, Mile End. Figure 1 shows the extent of the proposed development with respect to the nearest noise sensitive receivers. The proposed storage units will be accessible to customers 24 hours a day, 7 days a week.



Figure 1: Locality of proposed development with respect to noise sensitive receptors

Resonate

3 Planning & Design Code

3.1 Zoning

3.1.1 Subject site

The subject site is located within the City of West Torrens Council and spans both an Established Neighbourhood and Urban Corridor (Boulevard) zone. The relevant Desired Outcomes for the two zones are outlined in Table 1 and Table 2.

Table 1 Relevant Desired Outcomes—Established Neighbourhood

Desired Outcome	
DO 1	A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.
DO 2	Maintain the predominant streetscape character, having regard to key features such as roadside plantings, footpaths, front yards, and space between crossovers.

Table 2 Relevant Desired Outcome—Urban Corridor (Boulevard)

Desired Outcome	
DO 1	Buildings that achieve a consistent, tall, uniform facade to frame the primary road corridor that are consistently well set back with areas of significant open space in front, other than in specified areas where a lesser or no setback is desired. Buildings accommodate a mix of compatible residential and non-residential uses including shops and other business activities at ground and lower floor levels with residential land uses above.

3.1.2 Adjacent land

The closest noise affected premises are located directly west of the site along Goodenough Street and Rose Street. Noise affected premises are also located to the south of the site across Rose Street.

The receptors are located in the Established Neighbourhood zone. The relevant Desired Outcomes for the Established Neighbourhood zone is outlined in Table 1 above.



3.2 Interface between land uses

Interface between Land Uses is a General Development Policy that is relevant to the subject site. The relevant Assessment Provisions relating to noise are outlined in Table 3.

Table 3 Relevant Assessment Provisions—Activities generating noise or vibration

Relevant Assessment Provisions	
Desired Outcome	
DO1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.
Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 4.1 Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).	DTS/DPF 4.1 Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.
PO 4.2 Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including: <ul style="list-style-type: none"> a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers c) housing plant and equipment within an enclosed structure or acoustic enclosure d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone. 	DTS/DPF 4.2 None are applicable.



4 Noise criteria

4.1 Environmental noise policy

As noted in DTS/DPF 4.1, environmental noise emissions from the subject site should comply with the *Environment Protection (Noise) Policy 2007* (Noise EPP).

The noise goals in the Noise EPP are based on the zoning of the development and the closest noise affected premises in the Planning and Design Code. The land uses primarily promoted by the zones are used to determine the environmental noise criteria with the indicative noise factors shown in Table 4.

Table 4 Excerpt from Noise EPP—Table 2(subclause(1)(b))

Land use category	Indicative noise factor dB(A)	
	Day (7 am to 10 pm)	Night (10 pm to 7 am)
Rural living	47	40
Residential	52	45
Rural industry	57	50
Light industry	57	50
Commercial	62	55
General industry	65	55
Special industry	70	60

Based on the zoning and the relevant Desired Outcomes for the zones of the subject site and the adjacent receptors, the primarily promoted land uses and the relevant criteria for the receptors in each zone are outlined in Table 5. In accordance with Part 5 of the Noise EPP, the relevant criteria is the average of the relevant indicative noise factors less 5 dB(A).

Table 5 Summary of zones, land uses, and Noise EPP criteria

Site	Zone	Land use(s)	Criteria	
			Day (7 am to 10 pm)	Night (10 pm to 7 am)
Subject site	Urban Corridor (Boulevard) / Established Neighbourhood	Residential & Commercial	N/A	N/A
Nearby noise sensitive premises	Established Neighbourhood	Residential	50	43

Additionally, as the noise affected premises is situated in a 'quiet locality', being a Residential zone, a maximum noise criterion of L_{max} 60 dB(A) at night, 10 pm to 7 am, is applicable.

Penalties can also be applied to a noise source for a variety of characteristics, such as impulsive, low frequency, modulating or tonal characters. For a characteristic penalty to be applied to a noise source it must be fundamental to the impact of the noise and dominate the overall noise impact. Application of the characteristic penalty is discussed in the noise emission assessment.



We note that under Part 5, Clause 20(6) of the Noise EPP, exceedance of the recommended criterion does not necessarily mean action is required under the Noise EPP. Some of the following matters should be considered when considering action:

- the amount by which the criterion is exceeded (in dB(A))
- the frequency and duration for which the criterion is exceeded
- the ambient noise that has a noise level similar to the predicted noise level
- the times of occurrence of the noise source
- the number of persons likely to be adversely affected by the noise source and whether there is any special need for quiet.



5 Assessment

5.1 Noise modelling

5.1.1 Modelling parameters

Noise emissions from site have been modelled in SoundPLAN Environmental Software v8.2 program, using ISO-9613-2:1996 standard for outdoor noise propagation. The model takes into consideration:

- geometrical divergence
- screening by obstacles
- air absorption
- reflection from surfaces
- ground effect
- downwind conditions, or, equivalently, propagation under a well-developed moderate ground-based temperature inversion, such as commonly occurs at night.

5.1.2 Noise scenarios

Noise emissions from the proposed development have been assessed for an indicative worst case 15 minute period. The vehicle movements for the assessed period are based on existing gate access data, with respect to the following:

- The development generates existing peak trips per hour of approximately 10 trips for both AM and PM peak periods.
- Doubling of peak trips generated as a result of the approximate doubling in rentable storage space with the proposed development.

On this basis, 5 vehicle movements have been assumed for the assessed 15 minute period. It has been assumed that these 5 vehicles will consist of

- 4 cars
- 1 medium rigid vehicle (MRV).

We note that a significant portion of existing morning peak hour activity is likely due to use of the external site area for vehicle storage (including heavy vehicles), predominantly used by tradespeople. This use will be replaced with additional storage lockers with the proposed development, therefore the assumption that the existing AM peak vehicle movements will double as a result of the development is likely to be conservative.

It should be noted that vehicle movements through the site are generally infrequent, and the scenario above likely over predicts the noise levels generated from typical operation of the proposed development.

5.2 Characteristic noise penalties

Penalties to the source level should be applied in accordance with the Noise EPP to recognise annoyance associated with noise that is dominated by tonal, modulating, low frequency, or impulsive characteristics. A 5 dB(A) penalty is applied for one characteristic, an 8 dB(A) penalty is applied for two characteristics, and a 10 dB(A) penalty is applied for three or more characteristics.

For a characteristic penalty to be applied to a noise source it must be fundamental to the impact of the noise and dominate the overall noise impact.

A characteristic penalty could be considered for the modulating character of noise from vehicle movements through the site. The modulating character may be masked at certain times, particularly during day time hours. However,



noise from vehicle movements through the site may have a fundamental impact on the noise character when ambient noise levels from nearby road traffic are lower, as is typical during early morning hours.

5.3 Predicted noise levels

A summary of the predicted noise levels at the receptors is presented in Table 6.

Table 6 Predicted noise levels

Prediction location	Predicted noise level L_{eq} dB(A)	Noise EPP day / night time criteria, dB(A)
5 Rose Street	$34 + 5 = 39^{(1)}$	50 / 43
7 Rose Street	$38 + 5 = 43^{(1)}$	
9 Rose Street	$33 + 5 = 38^{(1)}$	
12-12A Rose Street	$34 + 5 = 39^{(1)}$	
1/5 Goodenough Street	$40 + 5 = 45^{(1)}$	
Unit 5/5 Goodenough Street	$35 + 5 = 40^{(1)}$	

(1) A 5 dB(A) penalty may be applicable to account for modulating noise associated with vehicle movements. The presented level includes a 5 dB(A) penalty.

Based on the predicted noise levels above, there is a minor exceedance of the night time Noise EPP criteria at the receptor at 1/5 Goodenough Street when accounting for a modulating noise characteristic penalty.

5.4 Mitigation

In order to reduce the potential noise impact from the development to the nearest receivers, we recommend the following mitigation:

- Install a 3m high boundary fence to the western boundary of the site, as shown in Appendix A. The fence should be minimum 0.48 BMT steel or alternative solid material without gaps, with a minimum surface density of 10kg/m^2 .

We note that the proposed storage units located along the boundary are approximately 3m high and are expected to provide sufficient mitigation for vehicle noise without the need for additional treatment. The recommended boundary fencing is therefore only required where proposed storage units are not situated along the western boundary.

A summary of the predicted noise levels at the receptors with the proposed mitigation is presented in Table 6.

Table 7 Predicted noise levels – with proposed mitigation

Prediction location	Predicted noise level L_{eq} dB(A)	Noise EPP day / night time criteria, dB(A)
5 Rose Street	$34 + 5 = 39^{(1)}$	50 / 43
7 Rose Street	$38 + 5 = 43^{(1)}$	
9 Rose Street	$33 + 5 = 38^{(1)}$	
12-12A Rose Street	$34 + 5 = 39^{(1)}$	
1/5 Goodenough Street	$37 + 5 = 42^{(1)}$	



Prediction location	Predicted noise level L _{eq} dB(A)	Noise EPP day / night time criteria, dB(A)
Unit 5/5 Goodenough Street	35 + 5 = 40 ⁽¹⁾	

(1) A 5 dB(A) penalty may be applicable to account for modulating noise associated with vehicle movements. The presented level includes a 5 dB(A) penalty.

With the recommended mitigation above, the noise levels from the peak operation of the proposed development are predicted to comply with both the day and night time Noise EPP criteria.

5.5 Rubbish removal

According to the Noise EPP, if noise from garbage removal activities exceeds a maximum noise level of 60 dB(A) at a noise sensitive receptor it must only occur between 9 am and 7 pm on a Sunday or public holiday and 7 am and 7 pm on any other day. Note that this is unless it can be shown that a high noise environment exists.

Note that if garbage removal is restricted to 9 am and 7 pm on a Sunday or public holiday and 7 am and 7 pm on any other day, there will be no noise restrictions under the Noise EPP.

5.6 Other noise sources

There is potential for noise from other sources, including voices, car radios, noise from goods being loaded and unloaded to and from storage units, and the like. It is not generally practicable to quantify noise emissions from these sources as they are highly variable.

It is recommended that Kennards inform prospective storage unit tenants of the noise sensitive nature of the surrounding area and include appropriate contract clauses which prohibit unnecessary noise from car radios, vehicle idling, conversation at the like, during night time hours.

We understand that Kennards maintain a detailed log of secure gate access times linked to each customer, and also operate on the basis of month-to-month customer contracts. Therefore in the event of noise complaints from neighbouring residents, Kennards can therefore identify the customer(s) potentially responsible and have the ability to not renew contracts if required.



6 Conclusion

An environmental noise impact assessment has been undertaken for proposed extension of the Kennard's Self Hire located at 11 James Congdon Drive, Mile End.

This assessment has demonstrated that, with the noise mitigation treatments detailed in this report, the noise emissions from the operation of the proposed development will be able to comply with the relevant environmental noise criteria.

On this basis the proposed additions to Kennards Self Hire will be able to operate within the relevant noise provisions in the Planning & Design Code and Environmental Protection (Noise) Policy requirements.



Appendix A – Extent of acoustic fencing

Details of Representations

Application Summary

Application ID	22000566
Proposal	Alterations and additions to existing self storage facility comprising 90 additional single storey self storage units, conversion of existing ground level office space to 26 storage units, variation to conditions 2, 3, and 6 in DA 211/984/2015 to allow for 24/7 access to the site , relocation of access, reconfiguration of on-site parking, associated landscaping and construction of a 3m high acoustic wall along portion of the western boundary
Location	11-17 JAMES CONGDON DR MILE END SA 5031, 11-17 JAMES CONGDON DR MILE END SA 5031, 11-17 JAMES CONG..

Representations

Representor 1 - Andrew Mimidas

Name	Andrew Mimidas
Address	11 Rose Street MILE END SA, 5031 Australia
Phone Number	
Email Address	
Submission Date	11/04/2022 12:34 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons	This sight which is to be enlarged has trucks and heavy equipment such as Tipper trucks , plant equipment diggers , lawn mowers industrial , food vans operating out of this storage facility at times from 0500 AM on weekdays which has noise impacts of air brakes trailers shunting work conversions reversing warning tones etc . This is a working truck machinery Depo in a residential street . This awakening noise is not fair on the impacted residential homes an do not believe that it is fair and reasonable for which workings should be conducted in this area we are not a industrial estate or park there are other areas approved for such works.

Attached Documents

Representations

Representor 2 - Emily Hatfield

Name	Emily Hatfield
Address	Riverside Centre, North Terrace ADELAIDE SA, 5000 Australia
Phone Number	
Email Address	
Submission Date	12/04/2022 04:25 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
Reasons	See supporting documents

Attached Documents

22000566_11-17_James_Cogdon_Drive_Submission.pdf

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Government of South Australia

SA Housing Authority

SA Housing Authority

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12 April 2022

City of West Torrens Development Assessment Panel
C/O PlanSA
165 Sir Donald Bradman Drive,
Hilton SA 5033

Dear Panel Members,

**SUBMISSION IN RESPONSE TO THE NOTIFICATION OF DA22000566 -
11-17 JAMES CONGDON DRIVE, MILE END SA 5031**

This submission is made in response to the notification of Development Application (DA) 22000566, relating to 11-17 James Congdon Drive, Mile End (the site). The DA seeks approval for alterations and additions to the existing self-storage facility (the proposed development), comprising:

- 90 additional single storey self-storage units
- conversion of existing ground level office space to 26 storage units
- relocation of access
- reconfiguration of on-site parking
- associated landscaping and construction of a 3m high acoustic wall; and
- operational hours of 24 hours, 7 days

This submission has been prepared on behalf of the SA Housing Authority (the Authority). The Authority was notified of this development as the owner of properties within the vicinity of the site.

The Authority supports the intent of the Urban Corridor (Boulevard) Zone and generally does not object to proposed developments, except where the proposed development will result in significant impact on its tenants.

This submission is made in **objection of the proposed development in its current form**. Key matters supporting this objection include:

- The proposed development does not adequately address the appropriateness of storage units within the Established Neighbourhood Zone

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- The proposed development does not adequately address potential noise impacts on adjoining land uses, namely residential dwellings
- The proposed development does not adequately address operational matters and their impact on adjoining land uses
- The proposed development does not adequately detail mitigation measures relating to these impacts

These matters greatly impact the amenity of tenants and are discussed below.

Land Use Zoning

The proposed development is partially located on lands zoned Established Neighbourhood. A review of aerial images indicates that no storage units are currently located within these lands. The land is currently used for car parking and vehicle access associated with the Kennards Self-Storage facility.

The main building of the storage facility is located within Urban Corridor (Boulevard) Zone. In discussing land use, the Planning Report prepared by AECOM (dated 11 January 2022) states:

PO 1.1 of the Urban Corridor (Boulevard) Zone envisages a vibrant mix of land use adding to the vitality of the area and an extension of activities outside shop hours. PO 1.1 of the Establishment Neighbourhood Zone seeks predominantly residential development with complementary non-residential activities.

*The proposed development comprises alterations and additions to the existing self-storage facility. **No changes to the existing land use are proposed.***

*The proposal therefore continues to contribute to the variety of land uses envisaged in the Urban Corridor (Boulevard) Zone and **maintains a complementary non-residential activity on the subject land and within the Established Neighbourhood Zone.*** (Page 7).

***Our emphasis**

The report states that no change to the existing land use is proposed. However, the proposed addition of storage units (within the Established Neighbourhood Zone) represents a substantial change to the operation of the site through an intensification of activity. The Authority does not share the view that the proposed development maintains a complementary non-residential activity within the Established Neighbourhood Zone.

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PO 1.3 of the Established Neighbourhood states:

Non-residential development sited and designed to complement the residential character and amenity of the neighbourhood.

The addition of storage units within these lands generates significant impacts on neighbourhood amenity, particularly noise to adjacent dwellings. This is discussed throughout this submission. Further, the intensification of activity effectively removes the transitional buffer between the two land use zones, created by the existing car parking. Ideally, a buffer between the two land uses should be maintained.

Noise Impacts

It is acknowledged that the proposed development incorporates a 3-metre acoustics wall along sections of the western boundary, adjacent existing residential development (considered sensitive receivers). The use of an acoustic wall is supported and is essential in supporting noise mitigation.

However, the acoustic wall is not provided in areas where storage units abut the western boundary. Rather, the proposal relies on the structure of the storage units to mitigate noise generated within or near the storage units, and the site.

An Environmental Noise Assessment has been prepared by Resonate (dated 11 January 2022) in support of the proposed development. This assessment states:

*We note that the proposed storage units located along the boundary are approximately 3m high and **are expected** to provide sufficient mitigation for vehicle noise without the need for additional treatment. (page 9)*

***our emphasis**

The proposal includes ten (10) 3mx3m storage units along the western boundary that abut the adjacent dwelling at 4/5 Goodenough Street, Mile End (**Figure 1-2**).

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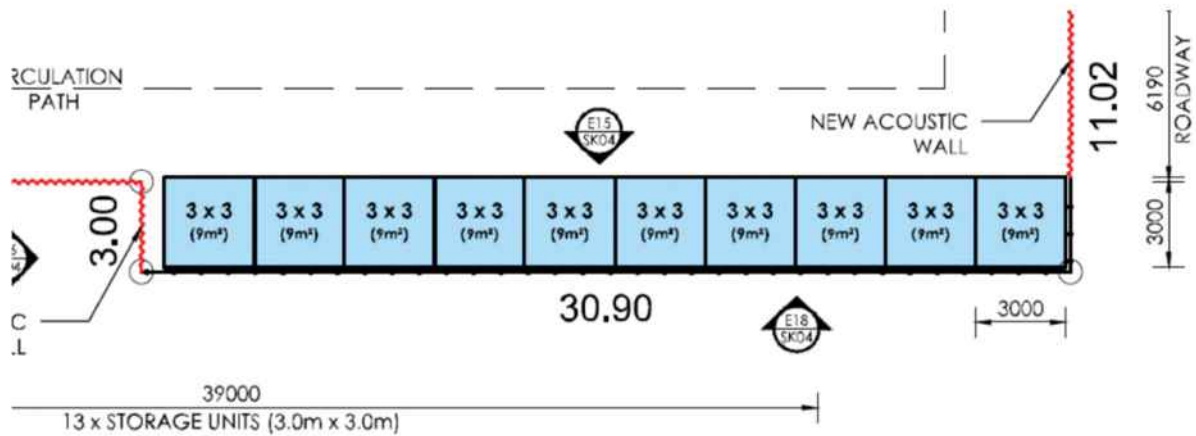


Figure 1 – Location of 3mx3m storage units along western boundary
Source: Jacob



Figure 2 – Aerial showing the location of the adjacent dwelling at 4/5 Goodenough Street, Mile End
Source: Nearmap, 2022

Given the proximity of the dwelling and the storage units, assessment of the storage unit's acoustic properties should be undertaken to determine if an adequate acoustic barrier is created by the selected material. Further, the fire rating of these structures should be considered. A review of the proposal suggests that it is a 'Class 7B' building under the Building Code of Australia, requiring a specific fire rating for structures and material. It is unclear if the proposed materiality of the storage units would achieve the required fire rating for this building class.

OFFICIAL: Sensitive

The assessment also states:

In order to reduce the potential noise impact from the development to the nearest receivers, we recommend the following mitigation:

- Install a 3m high boundary fence to the western boundary of the site, as shown in Appendix A. The fence should be minimum 0.48 BMT steel or alternative solid material **without gaps**, with a minimum surface density of 10kg/m² (page 9)

***our emphasis**

The Layout Plan prepared by Jacob (dated 21 December 2021) shows a clear wall gap between the subject storage units and the western boundary fence (Figure 3).

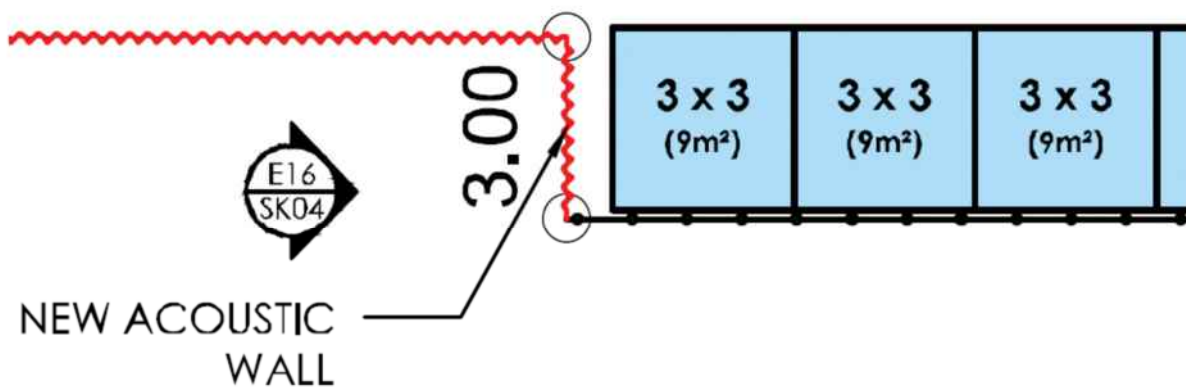


Figure 3 – Layout Plan showing acoustic wall extent (red line)
Source: Jacob

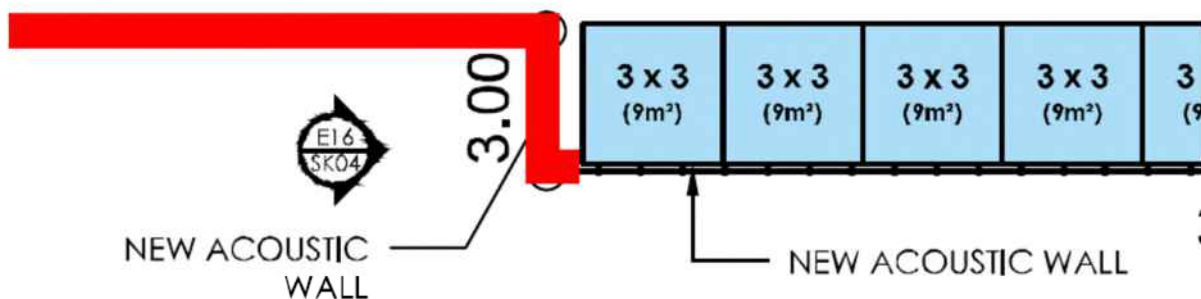


Figure 4 –Plan showing recommended acoustic wall extent (red line)
Source: Resonate

OFFICIAL: Sensitive

An amended plan provided by Resonate shows the extension of the acoustic wall to the edge of the storage unit (**Figure 4**). It is therefore recommended that the proposed Layout Plan be amended to accurately reflect Resonate's recommendations.

Operational Matters

As discussed above, noise impacts generated by the proposed development are a key concern. While a physical noise barrier is proposed, noise mitigation is not adequately address by the proposed development. Specifically, the proposed development does not adequately detail operational management. This is particularly concerning given the proposed 24-hour, 7-day operation of the site and the rigour of Resonate's assessment.

The Environmental Noise Assessment has been prepared by Resonate (dated 11 January 2022) states:

*There is potential for noise from other sources, including voices, car radios, noise from goods being loaded and unloaded to and from storage units, and the like. It is **not generally practicable to quantify noise emissions from these sources as they are highly variable.***

*It is recommended that Kennards inform prospective storage unit tenants of the noise sensitive nature of the surrounding area and **include appropriate contract clauses which prohibit unnecessary noise from car radios, vehicle idling, conversation at the like, during night time hours.***

*We understand that Kennards maintain a detailed log of secure gate access times linked to each customer, and also operate on the basis of month-to-month customer contracts. Therefore **in the event of noise complaints from neighbouring residents, Kennards can therefore identify the customer(s) potentially responsible and have the ability to not renew contracts if required.***

*our emphasis

Accordingly, noise generated by the proposed development which is likely to impact residential amenity has been omitted from Resonate's assessment. Noting variability, noise generated by the identified sources is proposed to be managed through contract terms and site surveillance. However, the proposed development is not supported by an Operational Plan of Management (OPoM) detailing how unnecessary noise is defined and how complaints are managed. This is essential in supporting the site's transition to 24-hour, 7-day operation. It is recommended that an OPoM be prepared and submitted to the relevant authority for consideration.

OFFICIAL: Sensitive

Given the interface with residential uses, the proposed operational hours should be reviewed. The variability of noise generated by the existing development is considered tolerable within the existing operational hours but would cause significant nuisance during the proposed hours of operation. As shown in the below table, the proposed operational hours are vastly different from the current operational hours.

Day	Current Hours of Operation	Proposed Hours of Operation
Monday-Friday	8.30am-6.00pm	24 hours, 7 days
Saturday	9.00am-5.00pm	
Sunday	10.00am-4.00pm	

As these potential noise sources have been omitted from the Environmental Noise Assessment, it is also recommended that the relevant authority condition Development Approval on compliance with the *Environment Protection (Noise) Policy 2007*.

Summary

The Authority requests that the Panel does not support the proposal in its current form and that the following modifications to the proposed development are considered:

- Extend the proposed 3-meter acoustic wall to span the entire western boundary
- Review the required fire rating of the storage units abutting the western boundary
- Prepare an Operational Plan of Management detailing how unnecessary noise is defined and how complaints are managed
- Maintain current hours of operation
- Condition Development Approval on compliance with the Environment Protection (Noise) Policy 2007 and other applicable EPA policy.

We trust this information is sufficient in supporting your assessment of the proposed development. Should you have any questions relating to this submission, please contact me via the details provided through PlanSA.

Sincerely,

Emily Hatfield
Urban Planner
Investment & Portfolio Planning – SA Housing Authority



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17 May 2022

Sonia Gallarello / Kieron Barnes
City of West Torrens
165 Sir Donald Bradman Drive
Hilton SA 5033

Dear Sonia

Development Application: 22000566 Kennards Self Storage Facility, 11 James Congdon Drive, Mile End

I write in response to the representations that were received during the public notification of the above development application and to your subsequent email dated 14 April 2022, in which you summarised responses from the Council's internal referral departments and requested additional information.

Below I have summarised the representations received and provide a response to each of the concerns raised. Following this, I have addressed your request for additional information.

Representation 1 – Andrew Mimidas at 11 Rose Street, Mile End.

Mr Mimidas opposes the application and wishes to address the Panel.

The reasons stated by Mr Mimidas for opposing the application relate solely to the existing use of the property and do not relate to the development proposed by the application.

The concerns raised particularly relate to the current use of the open hardstand area located to the rear of the site (west of the main building), which is used to store trade vehicles, trailers, small trucks, and associated trade equipment; and the noise and disturbance that is associated with the movement of these vehicles entering and exiting the site.

Whilst the concerns raised by Mr Mimidas relate to the existing operation of the site and are therefore a development compliance matter, we argue that the proposed application will notably improve noise and disturbance impacts on the surrounding area for the following reasons:

- The storage of trade vehicles, small trucks, and trade equipment on the site (typically overnight) and the noise and disturbance associated with their movement into and out of the site will cease as a result of the proposed development.
- The storage units that will replace the open hardstand area will be accessed less frequently and are typically not in the early hours of the morning.
- The proposed acoustic fencing and storage units positioned on the boundary, will help reduce noise transmission from the site.

We acknowledge the concerns raised by Mr. Mimidas, however, it is our opinion that they do not relate to the development proposed by the application and that the development will materially improve the impact of the existing use on surrounding residents.

Representation 2 – South Australia Housing Authority

The response supports the application but raises some concerns about the development. They have not requested to address the Panel.

The concerns raised in the response are summarised as follows:

1. Land use - the appropriateness of the proposed storage units within the Established Neighbourhood Zone.



2. Noise impacts and fire rating - potential noise impacts on adjoining residential dwellings and the fire rating of storage units positioned on the boundary.
3. Operational matters - the application does not adequately address operational matters, their impact on adjoining land uses and mitigation measures.

We provide the following response to these concerns:

1. Land use:

A portion of the rear of the site (the two western allotments) falls within the Established Neighbourhood Zone. This area of the site has been used for non-residential purposes and has been associated with the larger allotments to the east since the late 1970s. There is also a Land Management Agreement on the land that requires the four allotments (that comprise the application site) to be used together to service the existing commercial building on the site.

The proposed development, therefore, continues the longstanding commercial activity on the land and does not materially change the land use of this area of the site.

2. Noise impacts and fire rating:

An Environmental Noise Impact Assessment was prepared by Resonate and submitted with the development application. The report recommends that a 3.0 metre high acoustic fence be constructed along the western boundary (in any areas where storage units are not located) to mitigate noise impacts from the proposed development.

The report concludes that subject to the construction of the acoustic fence, the proposed development would comply with the relevant noise provisions in the Planning and Design Code and Environmental Protection (Noise) Policy requirements.

The Representor requests that the acoustic properties of the proposed storage units positioned on the boundary be assessed to ensure that they provide an adequate acoustic barrier. We respectfully contend that the storage units, which will be clad in steel (pre-coated) sheeting, will provide an equal or greater acoustic barrier than the proposed fence and therefore no further technical noise impact assessment is required.

The Representor correctly identified that a portion of the western boundary (to the north of the proposed storage units) did not show the proposed acoustic fencing, as recommended in the Resonate report. Accordingly, the site plan has been amended to show the storage unit extended slightly to the north so that there is no gap between the storage unit and the proposed acoustic fence.

With respect to the concerns relating to the fire rating of the proposed boundary structures, this matter will be considered during the Building Rules Consent application assessment and the proposed structures will meet the relevant standards of the Building Code of Australia. To reinforce this, the proposed site plan has been amended to include a notation that the proposed boundary development will be designed and constructed in accordance with the Building Code of Australia.

3. Operational matters

The Representor has raised concerns with the proposed hours of operation and that an Operational Management Plan has not been provided to mitigate potential noise impacts on neighbouring residents.

With regards to the proposed hours of operation, whilst the development would allow 24-hour access, these types of storage facilities generally attract low numbers of customers during late-night hours and the vast majority of visits occur between 6:30 am to 10:00 pm each day.

Additionally, the operation of the facility is carefully managed and includes controlled access gates that log customers entering the site, CCTV and short-term contracts that can be terminated should any inappropriate behaviour be observed. These measures will help ensure that any noise and disturbance associated with the facility will be well managed and minimised.



With respect to the requested Operational Management Plan, all Kennards Self Storage facilities are required to have a nominated 'Operations Manager' and a Plan of Management in place designed to minimise noise and disturbance impacts on neighbouring residents; and to effectively manage complaints and make available to Council a register of any complaints.

Enclosed with this response is an example Plan of Management that will be utilised for the proposed development and in our view will adequately mitigate noise and disturbance issues and complaints. Our client will accept a condition of approval requiring a specific Plan of Management to be provided to the Council.

Internal referral comments and requests for information:

Council's City Assets and Traffic teams have requested additional information to be provided. A response to these requests is detailed below.

1. Finished Floor Levels (FFL) of the proposed storage units to be a minimum of 350mm above the 'highest adjacent street water table'.

This level of detail, for this type of commercial development, is typically provided at the Building Rules Consent stage and furthermore, the new storage units will be located on an existing flat, hard-surfaced area of the site. As such, we request that the matter be dealt with by a condition of approval or a reserved matter requiring a civil and drainage management plan, that demonstrates appropriate FFL above the top of the street kerb, be provided to the satisfaction of the Council.

2. Details of all existing verge features.

An amended existing site plan is enclosed with this response that details the existing verge features adjacent to the Rose Street access.

The verge details adjacent to the remaining site accesses have not been provided as there are no changes proposed to these access points.

3. Provide two additional car parking spaces

Council's Consultant Traffic Engineer does not raise any concerns with the proposed development, however, does suggest that two additional car parking spaces can be located adjacent to the current row of spaces 5-7.

It is our preference to keep this area clear to allow a generous area for vehicle manoeuvring and for short term informal car parking for customers accessing storage units. However, our client would



accept a condition of approval being imposed requiring an amended site plan to be provided showing the additional two car parking spaces, as suggested by the Council's Consultant Traffic Engineer.

4. Details of waste management

The facility currently provides a 3m³ general waste bin and a 3m³ recycling bin on-site for customers to dispose of waste. These bins are collected once a month by a private contractor. No change to on-site waste management is proposed under this application.

5. Stormwater discharge

Council has requested a notation on the site plan stating that stormwater associated with the proposed storage units will be discharged offsite via the existing stormwater system.

The requested notation has been provided on the amended proposed site plan enclosed herewith.

6. Landscaping plan

A landscaping plan is enclosed with this response that details the proposed planting within the landscaping strip.

The planting will soften the appearance of the site when viewed from the street and partially screen the proposed storage units.

I trust this response and enclosed amended plans adequately address the Council's comments and the concerns raised by the representations.

Please feel free to contact me should you have any further questions.

Yours faithfully

A handwritten signature in black ink, appearing to read 'A. Raeburn', written over a light blue rectangular background.

Andrew Raeburn
Principal Planner
andrew.raeburn@aecom.com

Mobile: 0478 701 398

encl: Updated site plan
Landscaping plan
Example Plan of Management

Memo

From Richard Tan
Date 29/3/2022
Subject 22000566, 11-17 JAMES CONGDON DR MILE END SA 5031

Sonia,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

- 1.1 Council seeks to ensure that the FFL of all new development is protected from inundation when considering a 350mm stormwater flow depth in the adjacent street water table.

This is typically achieved through establishing the FFL of new development a minimum of 350mm above the highest adjacent street water table.

In association with the above proposed development, no site or road verge level information has been provided and as such it is impossible to determine if the proposal will satisfy the above consideration. It should also be noted that the storage FFL should be suitably elevated from the internal road level.

Simply conditioning that a development satisfy this consideration can have its complications with regards to the ultimately required level of the development in relation to neighbouring properties and the related planning considerations this brings about. It may also bring about the necessity for alterations to the design of the development which are outside of the expectations of the applicant (for example; requiring step(s) up from existing buildings to additions).

It is recommended that appropriate site and adjacent road verge survey information be provided to correctly assess the required minimum FFL for this proposal.

2.0 Verge Interaction

- 2.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically desired to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram

ramps, etc. (as measured at the kerb line, except for driveway separation which will be measured from property boundary). An absolute minimum offset of 0.5m from new crossovers and stormwater connections to other existing road verge elements is acceptable in cases where space is limited.

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council's Technical Officer (Arboriculture).

2.1.1 A detailed site plan should be provided with all existing and proposed verge features indicated.

It is recommended that a detailed site plan indicating satisfaction to the above requirements should be provided to Council.

2.1.2 The proposed crossover to Rose Street is in direct conflict with an existing street tree

It is recommended that further assessment from Council's Arboriculture team is required.

3.0 Traffic Requirements

The following comments have been provided by Council's Traffic Consultant, Mr Frank Siow:

I refer to the above development, which involves alterations and additions to an existing self-storage facility on the subject site.

From the proposal plans and documentation provided, I note the following:

- There are 26 new interior storage units proposed on the eastern side of the existing storage building.
- There are 90 new storage units of various sizes proposed over the current 'open storage' areas within the existing roadways around the existing building. I understand that these 90 units are individually and directly accessible from the roadways, ie they do not require the customer to park and walk into the existing building to access the storage units.
- From the documentation provided and from my review of aerial imagery, I note that these 90 'external' units would essentially occupy areas that are currently used for 'open storage'. I understand that the 'open storage' areas are used mainly to park commercial vehicles (trucks and buses) and caravans, boats etc. The proposed new storage units could be seen as a replacement of the current 'open storage' units, with new individualised and closed storage units proposed.

- The proposed access arrangements would be as follows: Goodenough Street entry and exit (no change); James Congdon Road exit only (existing entry and exit); Rose Street exit only (no change).
- The existing authorised use has 7 parking spaces. The proposal would increase the overall parking provision to 9 spaces with informal parking being available within the wide aiseways adjacent to the 90 new storage units.
- Aisleway widths would range for 6.2m to 6.7m approximately and aiseways would operate as one-way roadways.

3.1 Parking Assessment

From my previous experience with self-storage facilities, I understand that they are typically low parking and low traffic generating land uses. Peak usage would likely occur on weekends or after hours. Previous study data that I have sighted from the Self-Storage Association of Australia showed that facilities that are up to 3,000m² in floor area would only generate a parking requirement of up to 6 spaces and PM peak hour trips of 5-20 vehicles.

The new land uses of the development are 26 internal storage units (which require the customer to park and walk) and 90 external storage units (where customer could informally park within the one-way aisleway adjacent to their unit).

I accept that it would not be unreasonable for informal parking to occur outside their units for customers of the 90 external storage units. The one-way aisleway is sufficiently wide (6.2m to 6.7m) to enable another vehicle to pass. That is, such an informal parking arrangement would not result in traffic obstruction.

The new internal storage units would require the customer to park and walk into the building where their units are located. I note that there appears to be sufficient area for 2 additional parking spaces to be provided adjacent to current Space 5 to 7 row. This would increase the parking provision by 4 spaces. I recommend that the proposed plans be amended accordingly to include these 2 additional spaces.

The swept path diagrams provided show that an MRV service vehicle would be able to satisfactorily enter, exit and circulate around the site. If not already done so in the previous approvals of the site, I recommend that a condition be included to restrict service vehicle access up to MRV size (as per AS 2890.2-2018).

3.2 Parking Layout and Traffic Impact

I am satisfied that the proposed roadways would accommodate the vehicles that are likely to use the facility.

I note that the pedestrian sight line requirement for the exit points would be satisfied, as shown by the sight triangles plotted on the plans.

The proposed exit only to James Congdon Drive would remove the current entry movement from the main road. I support this revised arrangement as it would remove one potential traffic conflict at this entrance.

I am not concerned about the additional traffic generated by the proposal, given that the number of additional trips generated would likely be relatively low. I am also not concerned about the extension to opening times, given the low traffic generating nature of the development.

4.0 Waste Management

- 4.1** Due to the nature of this application, it is recommended that further assessment from Council's Waste Management Team is required.

It is recommended that further assessment from Council's Waste Management Team is required.

5.0 Stormwater Requirements

- 5.1** Roof runoff from the storage unit should be directed to the surface and discharged offsite via existing stormwater system.
- 5.2** Give that the storage units are constructed on existing sealed parking area, hence the impact on existing stormwater system is considered minor. Under this context, addition stormwater detention and quality is not required.

Regards
Richard Tan
Civil Engineer

Archived: Friday, 20 May 2022 1:37:41 PM
From: [Enio Trombetta](#)
Sent: Friday, 20 May 2022 12:32:10 PM
To: [Sonia Gallarello](#)
Cc: [Rick Holmes](#)
Subject: RE: 22000566 - 11-17 JAMES CONGDON DR MILE END SA 5031
Sensitivity: Normal

Dear Sonia,

I have examined the plans as requested and provide the following observations/comments.

The Planting Plan, Project number 22.015; Drawing number 101; Revision A; Date - 17.05.22; Issue - Draft, has made reference to a Plant schedule and while I acknowledge there has been no request for me to comment I cannot ignore the fact that over the whole site there been no attempt to create space to accommodate a tree and not one tree will be planted.

The international National Park City Foundation awarded Adelaide, the National Park City status in 2021, beginning the journey to improve city life through everyday actions to enjoy connecting with nature and each other. Adelaide National Park City covers all of Greater Adelaide. This includes Salisbury to the north, Aldinga to the south, Burnside to the east, and our fair community to the west, plus the city centre. The City of West Torrens is a signatory to the National Park City and are pleased to acknowledge how important it is in working towards a city that is cooler and greener, that supports the health and wellbeing of our community.

With this in mind, every effort should have been made to take the opportunity to increase canopy cover across the site.

The design provided DRAWING NUMBER JDV0085_SK01 has indicated that on the Rose Street frontage there is conflict with the 1 street trees, Tree #164015 Platanus orientalis (Oriental Plane Tree).

Any proposed development that does not consider "AS4970 Protection of Trees on Development Sites", is likely to require revision until all plans accurately correspond with the specific tree information detailed in this standard.

Verge interaction must consider all services that cross council land including stormwater outlets (and other) which will need to be maintained a minimum of 2.0m from any existing street tree (unless existing or otherwise negotiated). All services must be indicated /documented on appropriate plans for Council assessment and approval.

With reference to the City of West Torrens, Fees and Charges Document 2021-2022 "Tree removal for driveway construction", once Council has assessed all circumstances and considered it acceptable that a street tree can be removed, a fee is calculated based on Council's standard schedule of fees and charges.

The fee is used to offsets the loss of the asset (street tree) to the community, with funds received invested in Council's annual Greening Program.

As a result of the proposed crossovers on Rose Street, City Operations has considered the health, structure, form, useful life expectancy, and age of the street tree and will support its removal.

A fee of \$ \$1500.00 will be required prior to the commencement of any work.

Please note, under no circumstances is any individuals other than council staff permitted to interfere with a street tree. If pruning etc. is required, council must be notified via the appropriate customer request, and council staff will perform all works associated with the community asset.

Final crossover locations will be confirmed via the appropriate development assessment process.

Kind regards,

Enio Trombetta
Senior Technical Officer Arboriculture
City of West Torrens
165 Sir Donald Bradman Drive
Hilton SA 5033

Telephone: 8416 6332
Email: etrombetta@wtcc.sa.gov.au



From: Sonia Gallarello <sgallarello@wtcc.sa.gov.au>
Sent: Thursday, 19 May 2022 8:39 AM
To: Richard Tan <rtan@wtcc.sa.gov.au>
Cc: Enio Trombetta <etrombetta@wtcc.sa.gov.au>; Kieron Barnes <kieron@planningstudio.com.au>
Subject: FW: 22000566 - 11-17 JAMES CONGDON DR MILE END SA 5031

Hi Richard and Enio,

Could you please provide comment on the proposal Enio re street tree removal and City Assets if a reserved matter would be acceptable in this instance.

Kind regards,

Sonia Gallarello
Senior Development Officer - Planning
City of West Torrens
165 Sir Donald Bradman Drive
Hilton SA 5033

Phone: 08 84166354
Email: sgallarello@wtcc.sa.gov.au

From: Kieron Barnes <kieron@planningstudio.com.au>
Sent: Wednesday, 18 May 2022 7:16 AM
To: Sonia Gallarello <sgallarello@wtcc.sa.gov.au>
Subject: FW: 22000566 - 11-17 JAMES CONGDON DR MILE END SA 5031

Hi Sonia,

Would you mind forwarding these amended plans to City Assets for review against their initial comments (at the bottom of this email trail). The key elements for City Assets relate to the proposed removal of the street tree on Rose Street and the

applicant's request that the final FFLs of the storage units be dealt with via a Reserved Matter.

Thanks for this.

Kind regards

Kieron



Kieron Barnes | Director

kieron@planningstudio.com.au | 0438 741 747 | www.planningstudio.com.au | 347 Unley Road, Malvern | PO Box 32 Bridgewater SA 5155

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From: Raeburn, Andrew <Andrew.Raeburn@aecom.com>
Sent: Tuesday, 17 May 2022 3:37 PM
To: Kieron Barnes <kieron@planningstudio.com.au>
Cc: Hateley, Tom <Tom.Hateley@aecom.com>; Sonia Gallarelo <sgallarelo@wtcc.sa.gov.au>
Subject: RE: 22000566 - 11-17 JAMES CONGDON DR MILE END SA 5031

Hi Kieron and Sonia,

Please find attached our response to the representations, amended site plans and new landscaping plans.

Feel free to give me a call if you have any questions.

Regards,

Andrew Raeburn
Principal Planner, SA Civil Infrastructure
M +61 478 701 398
andrew.raeburn@aecom.com

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From: Kieron Barnes <kieron@planningstudio.com.au>
Sent: Monday, 16 May 2022 12:59 PM
To: Raeburn, Andrew <Andrew.Raeburn@aecom.com>

7 REVIEW OF ASSESSMENT MANAGER DECISION

Nil

8 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER

8.1 5-9 Palmyra Avenue, TORRENSVILLE

Application No. DA 211/279/2021

Reason for Confidentiality

It is recommended that this Report be considered in CONFIDENCE in accordance with regulation 13(2)(a) (vii) of the *Planning, Development and Infrastructure (General) Regulations 2017*, which permits the meeting to be closed to the public for business relating to the following:

- (vii) matters that must be considered in confidence in order to ensure that the assessment panel, or any other entity, does not breach any law, or any order or direction of a court or tribunal constituted by law, any duty of confidence, or other legal obligation or duty;

As this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.

RECOMMENDATION

It is recommended to the Council Assessment Panel that:

1. On the basis that this matter is before the Environment Resources and Development Court so any disclosure would prejudice the position of Council, the Council Assessment Panel orders pursuant to regulation 13(2) of the *Planning, Development and Infrastructure (General) Regulations 2017*, that the public, with the exception of the Chief Executive Officer, members of the Executive and Management Teams, Assessment Manager, City Development staff in attendance at the meeting, and meeting secretariat staff, and other staff so determined, be excluded from attendance at so much of the meeting as is necessary to receive, discuss and consider in confidence, information contained within the confidential reports submitted by the Assessment Manager on the basis that this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.
2. At the completion of the confidential session the meeting be re-opened to the public.

9 RELEVANT AUTHORITY ACTIVITIES REPORT

9.1 Activities Summary - May 2022

Brief

This report presents information in relation to:

1. Any development appeals before the Environment, Resources and Development (ERD) Court where the Council Assessment Panel (CAP) is the relevant authority;
2. Other appeal matters before the ERD Court of which SCAP are the relevant authority;
3. Any deferred items previously considered by the CAP
4. Summary of applications that have been determined under delegated authority where CAP is the relevant authority; and
5. Any matters being determined by the State Commission Assessment Panel (SCAP).

RECOMMENDATION

The Council Assessment Panel receive and note the information.

Development Application appeals before the ERD Court

CAP is the relevant authority

DA number	Address	Description of development	Status
211/279/2021	5 Palmyra Avenue, TORRENSVILLE	Demolition of existing buildings and construction of 19 two-storey dwellings with common driveway access and associated landscaping.	Conciliation conference adjourned to resume Monday 4 July, 2022. Compromise proposal being considered at June 2022 CAP meeting
21014960	437 Henley Beach Road, BROOKLYN PARK	Variation to Condition 3 in DA 211/262/2016 - extension to hours of operation to include Mondays 11.00am to 11.00pm and Sundays 11.00am to 11.00pm.	Compromise reached. Matter resolved by way of Court Order issued 13 May 2022 Refer to consent order in Attachment 1 .
21014495	233-235 Richmond Road, RICHMOND	Demolition of existing dwelling and shed and the construction of warehouse and ancillary offices and showroom with associated carparking and landscaping	This application was refused due to non-supply of information. Appeal lodged on 2 May 2022 to ERDC. Preliminary conference to scheduled Friday 3 June 2022.

DA number	Address	Description of development	Status

SCAP is the relevant authority

DA number	Address	Description of development	Status
211/M022/17	79 Port Road, THEBARTON	Multi-storey mixed use development, incorporating commercial tenancy, 2 storey car park, 9-storey residential flat building, four x 3-storey residential flat buildings and car parking.	<p>Compromise plans have been received and Council comments provided to SCAP 9 November 2020.</p> <p>The compromise proposal was scheduled for conciliation conference 28 January 2021.</p> <p>No further update available.</p>

Deferred CAP Items

Nil

Development Applications determined under delegation (CAP is the relevant authority)

Awaiting Plan SA Portal functionality to report on relevant applications.

Development Applications pending determination by SCAP

DA Number	Reason for referral	Address	Description of development
211/M135/21 Lodged 16 March 2021	Schedule 10	1 Selby Street, KURRALTA PARK	<p>Construction of a 10-storey residential flat building with associated car parking and site works.</p> <p>Under Assessment.</p>
211/M134/21 Lodged 16 March 2021	Schedule 10	4-10 Railway Terrace, MILE END	<p>Construction of a mixed use residential/commercial development comprising 51m² commercial tenancy, two (2) residential flat buildings comprising 6 dwellings and 28 dwellings associated landscaping, car parking, communal spaces and public realm improvements (Stage 2).</p> <p>Under Assessment.</p> <p>Council comments sent to SCAP 2 December 2021.</p> <p>Tabled for 8 June 2022 SCAP meeting.</p>

Conclusion

This report is current as at 2 June 2022.

Attachments

- 1. ERD Consent Order**

Case Number: ERD-22-000022

Date Filed: 17 May 2022

FDN: 8



ENVIRONMENT, RESOURCES AND DEVELOPMENT COURT OF SOUTH AUSTRALIA

No. 22 of 2022

BETWEEN

DOMENICO MAURICI
Appellant

and

CITY OF WEST TORRENS
Respondent

ORDER

Judicial Officer: Commissioner Dyer
Date of Order: 13 May 2022

BY CONSENT THE COURT ORDERS that:

- A. The appeal is allowed.
- B. Planning Consent is hereby granted to Development Application No 21014960 for a variation to DA 211/262/2016 to extend hours of operation of the restaurant at 437 Henley Beach Road, Brooklyn Park to include Mondays 11.00am to 11.00pm and Sundays 11.00am to 9.00pm subject to the following conditions:
 1. That all original planning conditions of Development Approval 211/262/2016 dated 14 September 2016 shall remain applicable, except where varied by the following conditions.
 - 1.1 That the hours of operation shall not exceed the following:
 - (a) 11:00am to 11:00pm Monday to Saturday inclusive; and
 - (b) 11:00am to 9:00pm Sunday.
 - 1.2 Waste collection shall only be carried out prior to 11:00am Monday to Friday inclusive.

DEPUTY REGISTRAR

10 OTHER BUSINESS

10.1 Planning Policy Considerations

11 MEETING CLOSE