

CITY OF WEST TORRENS



Notice of Panel Meeting

Notice is Hereby Given that a Meeting of the
COUNCIL ASSESSMENT PANEL

will be held in the George Robertson Room, Civic Centre
165 Sir Donald Bradman Drive, Hilton
Public access will be via livestream only

on

TUESDAY, 14 DECEMBER 2021
at 5.00pm

Representors and applicants eligible to be heard will be provided
with log-in details prior to the meeting.

Public access to the meeting will be livestream only. Access via the following internet
address: <https://www.westtorrens.sa.gov.au/livestream>

Hannah Bateman
Assessment Manager

City of West Torrens Disclaimer

Council Assessment Panel

Please note that the contents of this Council Assessment Panel Agenda have yet to be considered and deliberated by the Council Assessment Panel therefore the recommendations may be adjusted or changed by the Council Assessment Panel in the process of making the formal Council Assessment Panel decision.

Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.

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1 MEETING OPENED**1.1 Acknowledgement of Country****1.2 Evacuation Procedures****2 PRESENT****3 APOLOGIES****4 CONFIRMATION OF MINUTES****RECOMMENDATION**

That the Minutes of the meeting of the Council Assessment Panel held on 9 November 2021 be confirmed as a true and correct record.

5 DISCLOSURE STATEMENTS

In accordance with section 7 of the *Assessment Panel Members – Code of Conduct* the following information should be considered by Council Assessment Panel members prior to a meeting:

A member of a Council Assessment Panel who has a direct or indirect personal or pecuniary interest in a matter before the Council Assessment Panel (other than an indirect interest that exists in common with a substantial class of persons) –

- a. must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the panel; and
- b. must not take part in any hearings conducted by the panel, or in any deliberations or decision of the panel, on the matter and must be absent from the meeting when any deliberations are taking place or decision is being made.

If an interest has been declared by any member of the panel, the Assessment Manager will record the nature of the interest in the minutes of meeting.

6 REPORTS OF THE ASSESSMENT MANAGER**6.1 TRANSITIONAL APPLICATIONS**

Nil

6.2 PDI ACT APPLICATIONS

6.2.1 14 Rankine Road, TORRENSVILLE

Application No 21013814

Appearing before the Panel via electronic platform will be:

Representor: **Ms Linh Woodley** of 7A Rawlings Avenue, Torrensville wishes to appear in support of the representation.

Applicant: **Peter Meline** of Adelaide Hills Development Services wishes to appear in response to the representation.

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT NUMBER	21013814
APPLICANT	MWM Drafting
ADDRESS	14 Rankine Road, Torrensville
NATURE OF DEVELOPMENT	Internal alterations and additions to existing dwelling, including an upper level, and construction of a verandah attached to the rear of the dwelling
ZONING INFORMATION	<p>Zones</p> <ul style="list-style-type: none"> • Established Neighbourhood <p>Overlays</p> <ul style="list-style-type: none"> • Aircraft Noise Exposure (ANEF 25) • Airport Building Heights (Regulated) • Affordable Housing • Building Near Airfields • Character Area <ul style="list-style-type: none"> ◦ Cowanadilla / Mile End West Character Area Statement (WeTo-C2) • Hazards (Flooding - Evidence Required) • Prescribed Wells Area • Regulated and Significant Tree • Stormwater Management • Traffic Generating Development • Urban Tree Canopy <p>Technical Numeric Variations (TNVs)</p> <ul style="list-style-type: none"> • Minimum Site Area (Minimum site area for a detached dwelling is 340 sqm; semi-detached dwelling is 340 sqm) • Maximum Building Height (Levels) (Maximum building height is 1 level) • Minimum Side Boundary Setback (Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher)
LODGEMENT DATE	19 June 2021
RELEVANT AUTHORITY	Council Assessment Panel
PLANNING & DESIGN CODE VERSION	2021.7
CATEGORY OF DEVELOPMENT	Code Assessed - Performance Assessed

NOTIFICATION	Yes
REFERRALS STATUTORY	Nil
REFERRALS NON-STATUTORY	Nil
DELEGATION	CAP <ul style="list-style-type: none"> • A representor has lodged a vaild representation and wishes to be heard
RECOMMENDING OFFICER	Amelia De Ruvo
RECOMMENDATION	Support with conditions

SUBJECT LAND AND LOCALITY

The subject land is formally described as Allotment 57 Filed Plan 144285 in the area named Torrensville, Hundred of Adelaide, Volume 5685 Folio 896, more commonly known as 14 Rankine Road, Torrensville. The subject site is rectangular in shape with a 12 metre (m) wide frontage to Rankine Road, a depth of 45m and a site area of 537 square metres (m²).

There are three listed easements on the Certificate of Title. The subject land is subject to Party Wall Rights over the land marked A, together with Party Wall Rights over the land marked B and Easement over the land Marked C however there is no information on who this easement is in favour of.

It is noted that there are no encumbrances or Land Management Agreements on the Certificate of Title.

The site currently contains one half of a semi-detached dwelling along with an outbuilding. The land is relatively flat with only minor variances across the site. There are no Regulated trees on the site or on adjoining land that would be affected by the development.

The locality is predominantly residential in nature and is largely comprised of single storey character style dwellings. There is however an example of a two storey detached dwelling fronting Rawlings Avenue and a four storey residential flat building fronting Rankine Road both of which are anomalies within the locality. The wider locality sees a transition from residential to more mixed use / commercial land uses. The Urban Corridor Zone is approximately 50m to the north, with Henley Beach Road being approximately 140m north of the subject land. Henley Beach Road comprises of commercial land uses and is a secondary arterial road which experiences a high volume / frequency of traffic.

The amenity within the immediate locality is considered to be moderate to high due to the consistency of the allotment pattern and built form as well as the tree lined street. The wider locality however has a lower level of amenity due to the change in both the development pattern coupled with the zoning.

The subject land and locality are shown on the aerial imagery and maps below.



RELEVANT APPLICATIONS

Nil

PROPOSAL

For the purposes of an assessment the proposal is broken down into elements. Each element will have an assessment pathway as set out in the Planning and Design Code

Elements	Application Category
Dwelling alterations and additions <ul style="list-style-type: none"> Alterations and additions to be located to the rear of the dwelling; To include an upper floor The lower level addition is to be constructed of colour treated Zinc corrugated cladding in the colour 'Nite Sky' The upper level is to be constructed of colour treated Zinc corrugated cladding in the colour 'Wind Spray' 	Performance Assessed
Partial demolition/Internal building work	Exempt/Accepted (does not require planning consent)
Verandah <ul style="list-style-type: none"> Attached to the the dwelling addition and located behind the building line / facade 3.43m post height Overall height of 3.6m 	Deemed to Satisfy

The relevant plans and documents are contained in **Attachment 2**.

PUBLIC NOTIFICATION

The application required public notification because it was performance assessed and not exempt from notification by *Table 5 - Procedural Matters* of the zone in the Planning and Design Code (The Code).

Properties notified	44 properties were notified during the public notification process.
Representations	Two (2) representations were received.
Persons wishing to be heard	One (1) representor who wishes to be heard. <ul style="list-style-type: none"> Ms Linh Woodley
Summary of representations	Concerns were raised regarding the following matters: <ul style="list-style-type: none"> Building height; Overlooking.
Applicant's response to representations	Summary of applicant's response: <ul style="list-style-type: none"> Amended plans provided show fixed obscured glass to a height of 1.5m above upper floor FFL; and Proposed addition does not increase the height of the dwelling.

A copy of the representations and the applicant's response is contained in **Attachment 3**.

INTERNAL REFERRALS

Nil

EXTERNAL REFERRALS

Nil

RELEVANT PLANNING AND DESIGN CODE PROVISIONS

The subject land is located within the Established Neighbourhood Zone as described in the Code. The subject land is also affected a series of Overlays and Technical Numeric Variations (TNVs). The relevant Code extracts are contained in **Attachment 1**.

QUANTITATIVE STANDARDS

All elements of the proposal are assessed for consistency with the quantitative requirements of the Code as outlined in the table below:

Dwelling Alterations and Additions

CODE PROVISIONS	STANDARD	ASSESSMENT
ESTABLISHED NEIGHBOURHOOD ZONE		
Site Coverage Established Neighbourhood Zone PO / DPF 3.1	Max Site Coverage: 50%	37% Satisfies
Building Height Established Neighbourhood Zone PO / DPF 4.1	Max Building Height (Levels): 1 Level	2 building levels Does not Satisfy
Boundary Walls / Side Setback Established Neighbourhood Zone PO / DPF 7.1 & 8.1	Min. Side Setback: 1m for first building level, 2m for any second building level or higher	2.63m - ground floor 2.87m - upper level Satisfies
Rear Setback Established Neighbourhood Zone PO / DPF 9.1	4m for first building level; 6m for any second building level	22m - ground floor 28m - upper level Satisfies

GENERAL PROVISIONS		
Overlooking Design in Urban Areas PO / DPF 10.1	<p>Either:</p> <p>Windows are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm</p> <p>have sill heights greater than OR equal to 1.5m above finished floor level</p> <p>incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.</p>	<p>Fixed Obscured glass to a height of 1.5m above upper level FFL</p> <p>Satisfies</p>
Private Open Space Design in Urban Areas PO / DPF 21.1	<p>Site Area >301m²: Min. 60m² located behind the building line</p> <p>Min: 16m² directly access from a living room with min. dimension of 3m</p>	<p>212m² (total) 6.8m (dimension) 212m² (accessed from habitable room)</p> <p>Satisfies</p>
Landscaping Design in Urban Areas PO / DPF 10.1	<p>Site Area >450m²: Min. 25% of site</p>	<p>30.6%</p> <p>Satisfies</p>

Ancillary structure (verandah)

DEEM TO SATISFY PROVISIONS	ASSESSMENT										
<p>a) are ancillary to a dwelling erected on the same site;</p> <p>b) have a floor area not exceeding 60m²</p> <p>c) are constructed, added to or altered so that they are situated at least</p> <p style="margin-left: 20px;">i. 500mm behind the building line of the dwelling to which they are ancillary; or</p> <p style="margin-left: 20px;">ii. 900mm from a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads)</p> <p>d) In the case of a garage or carport, the garage or carport;</p> <p style="margin-left: 20px;">i. is setback at least 5.5m from the boundary of the primary street</p> <p style="margin-left: 20px;">ii. when facing a primary street or secondary street has a total door/ opening not exceeding 7m or 30% of the site frontage (whichever is the lesser) when facing a primary street or secondary street)</p> <p>e) If situated on a boundary (not being a boundary with a primary street or secondary street), a length not exceeding 8m unless:</p> <p style="margin-left: 20px;">i. A longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and</p> <p style="margin-left: 20px;">ii. The proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent.</p> <p>f) If situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary not exceeding 45% of the length of that boundary;</p> <p>g) Will not be located within 3m of any other wall along the same boundary unless on an adjacent site on the boundary there is an existing wall of a building that would be adjacent to or abut the proposed wall or structure;</p> <p>h) have a wall height or post height not exceeding 3m above natural ground level, and where located to the side of the associated dwelling, have a wall height or post height no higher than the wall height of the associated dwelling</p> <p>i) have a roof height where no part of the roof is more than 5m above the natural ground level</p> <p>j) if clad in sheet metal, are pre-colour treated or painted in a non-reflective colour.</p> <p>k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:</p> <p style="margin-left: 20px;">i. a total area as determined by the following table:</p> <table border="1" style="margin-left: 40px; margin-top: 10px;"> <thead> <tr> <th style="text-align: center;">Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="text-align: center;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;"><150</td> <td style="text-align: center;">10%</td> </tr> <tr> <td style="text-align: center;">150-200</td> <td style="text-align: center;">15%</td> </tr> <tr> <td style="text-align: center;">201-450</td> <td style="text-align: center;">20%</td> </tr> <tr> <td style="text-align: center;">>450</td> <td style="text-align: center;">25%</td> </tr> </tbody> </table> <p style="margin-left: 20px;">ii. the amount of existing soft landscaping prior to the development occurring.</p>	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	201-450	20%	>450	25%	<p>Satisfies</p> <p>Satisfies</p> <p>Satisfies</p> <p>N/A</p> <p>Satisfies</p> <p>Satisfies</p> <p>Satisfies</p> <p>Satisfies</p> <p>Satisfies (abutting a boundary wall on the adjacent site)</p> <p>Satisfies</p> <p>Satisfies</p> <p>Satisfies</p>
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
201-450	20%										
>450	25%										

ASSESSMENT

In assessing the merits or otherwise of the application, only those quantitative requirements that do not meet the Code requirements will be discussed along with the respective qualitative provisions. The proposed development is therefore discussed under the following sub headings:

Land Use

The subject land is an existing residential property. The proposal seeks to undertake alterations and additions and the construction of a verandah to the existing half of a semi-detached dwelling on the site. The proposed dwelling addition and verandah are envisaged forms of development within the Established Neighbourhood Zone and are therefore considered appropriate.

Built Form

Within the Established Neighbourhood Zone any new development should be designed in a manner that will maintain the predominant streetscape character within the locality. Although the proposal exceeds the maximum building height of one level, as per Designated Performance Feature (DPF) 4.1 and the Technical and Numerical Variation (TNV) layer of the Established Neighbourhood Zone, the proposal has been designed and is of a scale that is sympathetic and compatible with the predominant single storey built form seen along Rankine Road. As such the Desired Outcome (DO) 1 of the Established Neighbourhood Zone is appropriately satisfied. The alterations and additions will predominantly be located behind the main façade of the building, with the exception of the roof which will be altered from a hip to a gable end. Being a semi-detached dwelling, it will alter the roof form however it is not considered fatal to the application. The gable end roof form will be consistent with the streetscape character and existing dwellings within the immediately and wider locality, specifically 6 & 9 Rankine Road, 3 Elizabeth Street, 8, 10 and 14 Rawlings Avenue and 1-9 Elm Avenue. The roof form will not detract from the character attributes of the dwelling as it will maintain the roof line, height and pitch and the material of the existing dwelling, satisfying the intent of Performance Outcome (PO) 1.1, 2.3 and 2.5 of the Character Area Overlay.

The alteration and additions proposed result in a dwelling with two building levels, contrary to the TNV requirements. A TNV recognises unique character attributes of an area and allows for local variations to the policies that apply to a site within a Zone, Sub-Zone and / or Overlay. It is noted that the Maximum Building Height, detailed as part of DPF 4.1 of the Established Neighbourhood Zone, and the TNV layer seeks for buildings with a maximum building height of one level. It is considered that the variation to the Building Height requirement and TNV is appropriate. A streetscape elevation plan, prepared by *MWM Drafting*, was provided to Council. The upper storey addition is designed so that it does not exceed the overall building height of the existing built form a concern that was raised by the representor. The upper storey addition will maintain the side setback of the existing building and will only be readily viewed when travelling north along Rankine Road. The positioning of the addition satisfies DO 1 and PO 2.4 of Character Area Overlay.

Performance Outcome 4.2 of the Established Neighbourhood Zones discourages additions from adversely impacting on the street scape character. The associated DPF advises that one way to achieve the PO is for the additions to be fully contained within the roof space of the dwelling. This has not been achieved however, during the assessment, the applicant was requested to consider amending the colour of the upper storey addition from '*Nite Sky*' to '*Surfmist*', which was agreed upon. It was considered that the amended colour to the upper storey addition would be more compatible and cohesive with the existing dwelling on site and assist in reducing the visual impact and dominance of the second building level when viewed from the public realm.

For the reasons detailed above, the upper storey addition was considered acceptable. It has been designed in a manner that sufficiently maintains the character of the existing dwelling, as well as the locality, whilst making the dwelling more conducive to contemporary living, therefore satisfying PO 3.2 of Character Area Overlay.

Aircraft Noise Exposure Overlay

For the purposes of the application a habitable room is considered to be a, '*room used for domestic activities but does not include a bathroom, laundry, hallway, lobby or other service or access area or space that is not occupied for extended periods*'. In accordance with the above the proposed addition seeks to increase the habitable room floor area by an additional 40m². As the existing dwelling has a total floor area of 115m² the addition does not result in a habitable room floor area increase of more than 50% of the existing dwelling, satisfying PO and DPF 2.1 of Aircraft Noise Exposure Overlay. No further attenuation measures were considered to be necessary.

Overlooking

The representor has expressed concerns with the proposed development and the potential for overlooking into their site. The applicant was requested for the upper floor windows to habitable rooms to incorporate fixed obscured windows to a minimum height of 1.5m above the upper level Finish Floor Level in accordance with DPF 10.1 of the General Development Provisions - Design in Urban Areas. With the amendment to the upper level windows, the potential for direct overlooking to adjoining residential properties has been sufficiently mitigated, satisfying PO 10.1 of General Development Provisions - Design in Urban Areas.

Furthermore, to reinforce this requirement a condition has been imposed within the recommendation to ensure to the windows are maintained for the life of the development.

It should be acknowledged that the intent of the Code policy is not to completely prevent overlooking to adjoining residential properties. Instead development should minimise the potential for direct overlooking to adjoining properties useable primary private open space area and other habitable rooms through a range of design techniques such as raised sill heights and /or obscured glazing to upper level windows. The proposal has suitably addressed this requirement.

Overshadowing

Due to the east- west orientation of the subject land, overshadowing to the adjoining allotment to the south is inevitable. Notwithstanding the above, the shadows cast by the proposed addition are not considered fatal to the proposal.

The applicant has provided overshadowing diagrams from both an aerial view and in a cross-section form. The cross-section shadow diagram accurately determines the extent of shadows cast by the development to the adjoining site. At times aerial shadow diagrams can exaggerate the shadows cast by a development as they do not consider the vertical height of the adjoining dwellings. The applicant has shown that the north-facing window to 16 Rankine Road, Torrensville will be provided with the minimum of three hours of direct winter sunlight on 21 June, therefore satisfying PO and DPF 3.1 of General Development Provisions - Interface between Land Uses.

The proposal also satisfies PO and DPF 3.2 of General Development Provisions - Interface between Land Uses as it provides the adjoining dwellings Private Open Space areas with an excess of two hours of direct winter sunlight.

Verandah

The proposed verandah has met the Deemed to Satisfy (DTS) criteria within provision 11.1 of the Established Neighbourhood Zone and therefore is considered appropriate form of development.

SUMMARY

Having considered all the relevant provisions of the Planning and Design Code, the proposal is considered to be not seriously at variance with the Planning and Design Code Version 2021.7.

The dwelling alterations and additions have been sympathetically designed so that it enables the residents to revitalise the dwelling for contemporary living without detracting from its character. The additions will be primarily located behind the main façade of the dwelling and maintains appropriate offsets from side boundaries so to preserve the prevailing pattern of development within the locality.

On balance, the proposal reasonably satisfies the relevant provisions of the Planning and Design Code Version 2021.7 and therefore the application warrants the granting of Planning Consent.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107 (2)(c) of the *Planning Development and Infrastructure Act 2016*, and having undertaken an assessment of the applicant against the Planning and Design Code, the applicant is NOT seriously at variance with the provisions of the Planning and Design Code Versions 2021.7.
2. Application No. 21013814 by MWM Drafting to undertake internal alterations and additions to existing dwelling, including an upper level, and construction of a verandah attached to the rear of the dwelling at 14 Rankine Road, Torrensville (CT5865/896) is GRANTED Planning Consent subject to the following conditions of consent:

Development Plan Consent Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application except where varied by any conditions listed below:
 - Planning Report, prepared by MWM Drafting, dated Aug 2020
 - Site Plan, prepared by MWM Drafting, Drawing No. 1, Version 4, dated 15 August 2021
 - Demolition Plan, prepared by MWM Drafting, Drawing No. 2, Version 4, dated 3 August 2021
 - Lower Floor, prepared by MWM Drafting, Drawing No. 3, Version 4, dated 3 August 2021
 - Upper Floor, prepared by MWM Drafting, Drawing No. 4, Version 4, dated 4 August 2021
 - Roof Plan, prepared by MWM Drafting, Drawing No. 5, Version 4, dated 3 August 2021
 - Streetscape Elevation, prepared by MWM Drafting, Drawing No. 8, Version 1, dated 31 August 2021
 - Elevations, prepared by MWM Drafting, Drawing No. 7, Version 5, dated 31 August 2021
 - Request for Information Response, prepared by MWM Drafting, dated 2 August 2021
2. All stormwater design and construction shall be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
 - a) Result in the entry of water into a building;
 - b) Affect the stability of a building;
 - c) Create unhealthy or dangerous conditions on the site or within the building;
 - d) Flow or discharge onto the land of an adjoining owner;
 - e) Flow across footpaths or public ways; or
 - f) Discharge to the adjacent creek.

3. The upper level south and west facing windows to habitable rooms of the dwelling shall be provided with fixed obscure glass to a minimum height of 1.5 metres above the upper floor level to minimise the potential for direct overlooking of adjoining properties, and shall be installed prior to the occupation of the building. The glazing in these windows will be maintained in a reasonable condition at all times to the satisfaction of the relevant authority.
4. The external materials and finishes shall match/be complementary to those of the associated dwelling.

Attachments

1. **Relevant Planning and Design Code Provisions**
2. **Proposal Plans & Supporting Documents**
3. **Representation & Applicants Response**

Policy24 - Enquiry

14 RANKINE RD TORRENSVILLE SA 5031

Address:

Click to view a detailed interactive [SAILS](#) in SAILIS

To view a detailed interactive property map in SAPP click on the map below



Property Zoning Details

Local Variation (TNV)

Minimum Site Area (*o_*Minimum site area for a detached dwelling is 340 sqm; semi-detached dwelling is 340 sqm)

Maximum Building Height (Levels) (*Maximum building height is 1 level*)

Minimum Side Boundary Setback (*Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher*)

Overlay

Aircraft Noise Exposure (ANEF 25)

Airport Building Heights (Regulated) (*All structures over 15 metres*)

Affordable Housing

Building Near Airfields

Character Area (*WeToC2*)

Hazards (Flooding - Evidence Required)

Prescribed Wells Area

Regulated and Significant Tree

Stormwater Management

Traffic Generating Development

Urban Tree Canopy

Zone

Established Neighbourhood

Selected Development(s)

Dwelling addition

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards.

If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

Policy24 - Enquiry

Dwelling addition - Code Assessed - Performance Assessed

Part 2 - Zones and Sub Zones

Established Neighbourhood Zone

Assessment Provisions (AP)

Desired Outcome	
DO 1	A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.
DO 2	Maintain the predominant streetscape character, having regard to key features such as roadside plantings, footpaths, front yards, and space between crossovers.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature		
Site coverage			
<p>PO 3.1</p> <p>Building footprints are consistent with the character and pattern of the neighbourhood and provide sufficient space around buildings to limit visual impact, provide an attractive outlook and access to light and ventilation.</p>	<p>DTS/DPF 3.1</p> <p>Development does not result in site coverage exceeding:</p> <p>In instances where:</p> <ul style="list-style-type: none"> (a) no value is returned (i.e. there is a blank field), then a maximum 50% site coverage applies (b) more than one value is returned in the same field, refer to the Site Coverage Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development. 		
Building Height			
<p>PO 4.1</p> <p>Buildings contribute to the prevailing character of the neighbourhood and complements the height of nearby buildings.</p>	<p>DTS/DPF 4.1</p> <p>Building height (excluding garages, carports and outbuildings) is no greater than:</p> <ul style="list-style-type: none"> (a) the following: <table border="1" style="margin-left: 20px;"> <thead> <tr> <th>Maximum Building Height (Levels)</th> </tr> </thead> <tbody> <tr> <td>Maximum building height is 1 level</td> </tr> </tbody> </table> (b) in all other cases (i.e. there are blank fields for both maximum building height (metres) and maximum building height (levels)) - 2 building levels up to a height of 9m. <p>In relation to DTS/DPF 4.1, in instances where:</p> <ul style="list-style-type: none"> (c) more than one value is returned in the same field, refer to the <i>Maximum Building Height (Levels) Technical and</i> 	Maximum Building Height (Levels)	Maximum building height is 1 level
Maximum Building Height (Levels)			
Maximum building height is 1 level			

Policy24 - Enquiry

	<p><i>Numeric Variation layer or Maximum Building Height (Meters) Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development.</i></p> <p>(d) only one value is returned for DTS/DPF 4.1(a) (i.e. there is one blank field), then the relevant height in metres or building levels applies with no criteria for the other.</p>		
<p>PO 4.2</p> <p>Additions and alterations do not adversely impact on the streetscape character.</p>	<p>DTS/DPF 4.2</p> <p>Additions and alterations:</p> <p>(a) are fully contained within the roof space of a building with no external alterations made to the building elevation facing the primary street</p> <p>or</p> <p>(b) meet all of the following:</p> <p>(i) do not include any development forward of the front façade building line</p> <p>(ii) where including a second or subsequent building level addition, does not project beyond a 45 degree angle measured from ground level at the building line of the existing building.</p>		
<p>Secondary Street Setback</p>			
<p>PO 6.1</p> <p>Buildings are set back from secondary street boundaries (not being a rear laneway) to maintain the established pattern of separation between buildings and public streets and reinforce streetscape character.</p>	<p>DTS/DPF 6.1</p> <p>Building walls are set back from the secondary street boundary (other than a rear laneway):</p> <p>(a) no less than:</p> <table border="1" data-bbox="799 1104 1385 1196"> <tr> <td style="text-align: center;">Minimum Side Boundary Setback</td> </tr> <tr> <td>Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher</td> </tr> </table> <p>or</p> <p>(b) 900mm, whichever is greater</p> <p>or</p> <p>(c) if a dwelling on any adjoining allotment is closer to the secondary street, the distance of that dwelling from the boundary with the secondary street.</p> <p>In instances where no value is returned in DTS/DPF 6.1(a) (i.e. there is a blank field), then it is taken that the value for DTS/DPF 6.1(a) is zero.</p>	Minimum Side Boundary Setback	Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher
Minimum Side Boundary Setback			
Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher			
<p>Boundary Walls</p>			
<p>PO 7.1</p> <p>Dwelling boundary walls are limited in height and length to manage visual and overshadowing impacts on adjoining properties.</p>	<p>DTS/DPF 7.1</p> <p>Dwellings do not incorporate side boundary walls where a side boundary setback value is returned in (a) below:</p> <p>(a)</p> <table border="1" data-bbox="799 1872 1385 1901"> <tr> <td style="text-align: center;">Minimum Side Boundary Setback</td> </tr> </table>	Minimum Side Boundary Setback	
Minimum Side Boundary Setback			

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	<p>Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher</p> <p>or</p> <p>(b) where no side boundary setback value is returned in (a) above, and except where the dwelling is located on a central site within a row dwelling or terrace arrangement, side boundary walls occur only on one side boundary and satisfy (i) or (ii) below:</p> <p>(i) side boundary walls adjoin or abut a boundary wall of a building on adjoining land for the same or lesser length and height</p> <p>(ii) side boundary walls do not:</p> <p>A. exceed 3.2m in height from the lower of the natural or finished ground level</p> <p>B. exceed 8m in length</p> <p>C. when combined with other walls on the boundary of the subject development site, exceed a maximum 45% of the length of the boundary</p> <p>D. encroach within 3m of any other existing or proposed boundary walls on the subject land.</p>		
<p>PO 7.2</p> <p>Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a low density suburban streetscape character.</p>	<p>DTS/DPF 7.2</p> <p>Dwellings in a semi-detached, row or terrace arrangement are setback from side boundaries shared with allotments outside the development site at least the minimum distance identified in Established Neighbourhood Zone DTS/DPF 8.1.</p>		
<p>Side Boundary Setback</p>			
<p>PO 8.1</p> <p>Buildings are set back from side boundaries to provide:</p> <p>(a) separation between buildings in a way that complements the established character of the locality</p> <p>(b) access to natural light and ventilation for neighbours.</p>	<p>DTS/DPF 8.1</p> <p>Other than walls located on a side boundary in accordance with Established Neighbourhood Zone DTS/DPF 7.1, building walls are set back from the side boundary:</p> <p>(a) no less than:</p> <table border="1" data-bbox="799 1402 1385 1496"> <tr> <th style="text-align: center;">Minimum Side Boundary Setback</th> </tr> <tr> <td>Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher</td> </tr> </table> <p>(b) in all other cases (i.e. there is a blank field), then:</p> <p>(i) at least 900mm where the wall is up to 3m</p> <p>(ii) other than for a south facing wall, at least 900mm plus 1/3 of the wall height above 3m</p> <p>(iii) at least 1.9m plus 1/3 of the wall height above 3m for south facing walls.</p>	Minimum Side Boundary Setback	Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher
Minimum Side Boundary Setback			
Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher			
<p>Rear Boundary Setback</p>			
<p>PO 9.1</p> <p>Buildings are set back from rear boundaries to provide:</p> <p>(a) separation between dwellings in a way that</p>	<p>DTS/DPF 9.1</p> <p>Other than in relation to an access lane way, buildings are set back from the rear boundary at least:</p>		

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complements the established character of the locality (b) access to natural light and ventilation for neighbours (c) private open space (d) space for landscaping and vegetation.	(a) 4m for the first building level (b) 6m for any second building level.
Appearance	
PO 10.1 Garages and carports are designed and sited to be discrete and not dominate the appearance of the associated dwelling when viewed from the street.	DTS/DPF 10.1 Garages and carports facing a street (other than an access lane way): (a) are set back at least 0.5m behind the building line of the associated dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a total garage door / opening width not exceeding 30% of the allotment or site frontage, to a maximum width of 7m.
PO 10.2 The appearance of development as viewed from public roads is sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality.	DTS/DPF 10.2 None are applicable.

Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

A class of development listed in Column A is excluded from notification provided that it does not fall within a corresponding exclusion prescribed in Column B. In instances where development falls within multiple classes within Column A, each clause is to be read independently such that if a development is excluded from notification by any clause, it is, for the purposes of notification excluded irrespective of any other clause.

Class of Development (Column A)	Exceptions (Column B)
1. A kind of development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. All development undertaken by: (a) the South Australian Housing Trust either individually or jointly with other persons or bodies or (b) a provider registered under the Community Housing National Law participating in a program relating to the renewal of housing endorsed by the South Australian Housing Trust.	Except development involving any of the following: 1. residential flat building(s) of 3 or more building levels 2. the demolition of a State or Local Heritage Place 3. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
3. Any development involving any of the following (or of any combination of any of the following):	Except development that:

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<ul style="list-style-type: none"> (a) air handling unit, air conditioning system or exhaust fan (b) ancillary accommodation (c) building work on railway land (d) carport (e) deck (f) dwelling (g) dwelling addition (h) fence (i) outbuilding (j) pergola (k) private bushfire shelter (l) residential flat building (m) retaining wall (n) shade sail (o) solar photovoltaic panels (roof mounted) (p) swimming pool or spa pool (q) verandah (r) water tank. 	<ul style="list-style-type: none"> 1. exceeds the maximum building height specified in Established Neighbourhood Zone DTS/DPF 4.1 or 2. involves a building wall (or structure) that is proposed to be situated on a side boundary (not being a boundary with a primary street or secondary street) and: <ul style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 8m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
<p>4. Any development involving any of the following (or of any combination of any of the following):</p> <ul style="list-style-type: none"> (a) consulting room (b) office (c) shop. 	<p>Except development that:</p> <ul style="list-style-type: none"> 1. does not satisfy Established Neighbourhood Zone DTS/DPF 1.2 or 2. exceeds the maximum building height specified in Established Neighbourhood Zone DTS/DPF 4.1 or 3. involves a building wall (or structure) that is proposed to be situated on a side boundary (not being a boundary with a primary street or secondary street) and: <ul style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 8m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
<p>5. Any of the following (or of any combination of any of the following):</p> <ul style="list-style-type: none"> (a) internal building works (b) land division (c) recreation area (d) replacement building (e) temporary accommodation in an area affected by bushfire (f) tree damaging activity. 	<p>None specified.</p>
<p>6. Demolition.</p>	<p>Except any of the following:</p>

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	<ol style="list-style-type: none"> 1. the demolition of a State or Local Heritage Place 2. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
Placement of Notices - Exemptions for Performance Assessed Development	
None specified.	
Placement of Notices - Exemptions for Restricted Development	
None specified.	

Part 3 - Overlays

Aircraft Noise Exposure Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development sensitive to aircraft noise is designed and located to manage noise intrusion to reduce land use conflict and protect human health.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 2.1 Additions to buildings involving the addition or extension of habitable rooms are designed and located to minimise aircraft noise intrusion and provide appropriate interior acoustic amenity.	DTS/DPF 2.1 Dwelling additions involving the addition or extension of habitable rooms: <ol style="list-style-type: none"> (a) do not result in an increase in the total floor area of the existing dwelling by greater than 50 percent (b) do not occur in areas having an ANEF value of 30 or more.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Airport Building Heights (Regulated) Overlay

Assessment Provisions (AP)

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Desired Outcome	
DO 1	Management of potential impacts of buildings and generated emissions to maintain operational and safety requirements of registered and certified commercial and military airfields, airports, airstrips and helicopter landing sites.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Building height does not pose a hazard to the operation of a certified or registered aerodrome.	DTS/DPF 1.1 Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas. In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Any of the following classes of development: (a) building located in an area identified as 'All structures' (no height limit is prescribed) or will exceed the height specified in the <i>Airport Building Heights (Regulated) Overlay</i> (b) building comprising exhaust stacks that generates plumes, or may cause plumes to be generated, above a height specified in the <i>Airport Building Heights (Regulated) Overlay</i> .	The airport-operator company for the relevant airport within the meaning of the <i>Airports Act 1996</i> of the Commonwealth or, if there is no airport-operator company, the Secretary of the Minister responsible for the administration of the <i>Airports Act 1996</i> of the Commonwealth.	To provide expert assessment and direction to the relevant authority on potential impacts on the safety and operation of aviation activities.	Development of a class to which Schedule 9 clause 3 item 1 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Building Near Airfields Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Maintain the operational and safety requirements of certified commercial and military airfields, airports, airstrips and helicopter landing sites through management of non-residential lighting, turbulence and activities that may attract or result in the congregation of wildlife.

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Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.3 Buildings are adequately separated from runways and other take-off and landing facilities within certified or registered aerodromes to minimise the potential for building-generated turbulence and windshear that may pose a safety hazard to aircraft flight movement.	DTS/DPF 1.3 The distance from any part of a runway centreline to the closest point of the building is not less than 35 times the building height.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Character Area Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Valued streetscape characteristics and development patterns are reinforced through contextually responsive development, design and adaptive reuse that responds to the attributes expressed in the Character Area Statement.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
PO 1.1 All development is undertaken having consideration to the valued attributes expressed in the Character Area Statement.	DTS/DPF 1.1 None are applicable.
Built Form	
PO 2.1 The form of new buildings and structures that are visible from the public realm are consistent with the valued streetscape characteristics of the character area.	DTS/DPF 2.1 None are applicable.
PO 2.2 Development is consistent with the prevailing building and wall	DTS/DPF 2.2 None are applicable.

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heights in the character area.	
PO 2.3 Design and architectural detailing of street-facing buildings (including but not limited to roof pitch and form, openings, chimneys and verandahs) are consistent with the prevailing characteristics in the character area.	DTS/DPF 2.3 None are applicable.
PO 2.4 Development is consistent with the prevailing front and side boundary setback pattern in the character area.	DTS/DPF 2.4 None are applicable.
PO 2.5 Materials are either consistent with or complement those within the character area.	DTS/DPF 2.5 None are applicable.
Alterations and Additions	
PO 3.1 Additions and alterations do not adversely impact on the streetscape character.	DTS/DPF 3.1 Additions and alterations: (a) are fully contained within the roof space of a building with no external alterations made to the building elevation facing the primary street or (b) meet all of the following: (i) do not include any development forward of the front façade building line (ii) any side or rear extensions are no closer to the side boundary than the existing building (iii) do not involve the construction or alteration of a second or subsequent building level.
PO 3.2 Adaptive reuse and revitalisation of buildings to retain local character consistent with the Character Area Statement.	DTS/DPF 3.2 None are applicable.
Context and Streetscape Amenity	
PO 6.1 The width of driveways and other vehicle access ways are consistent with the prevalent width of existing driveways in the character area.	DTS/DPF 6.1 None are applicable.
PO 6.2 Development maintains the valued landscape pattern and characteristics that contribute to the character area, except where they compromise safety, create nuisance, or impact adversely on existing buildings or infrastructure.	DTS/DPF 6.2 None are applicable.

Character Area Statements

Statement#	Statement
Character Areas affecting City of West Torrens	

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WeToC2	Cowandilla / Mile End West Character Area Statement (WeTo-C2)	
	The Character Area Overlay identifies localities that comprise valued character attributes. They can be characterised by a consistent rhythm of allotment patterns, building setting and spacing, landscape or natural features and the scale, proportion and form of buildings and their key elements.	
	These attributes have been identified in the below table. In some cases State and / or Local Heritage Places within the locality contribute to the attributes of a Character Area.	
	The preparation of a Contextual Analysis can assist in determining potential additional attributes of a Character Area where these are not identified in the below table.	
	Eras, themes and context	1920s - 1940.'s
	Allotments, subdivision and built form patterns	Strong uniformity of allotment pattern. Low to very low density site areas. Narrow and deep allotments. Detached and semi-detached dwellings on substantial allotments.
	Architectural styles, detailing and built form features	Predominantly Californian and State Bank Bungalows with some examples of Spanish Mission; Dutch Colonial dwellings; Detached and semi-detached federation cottages/villas; Victorian era cottages/villas; and Early inter-war dwellings. Gable and Dutch gable roofs with hips. Verandahs / porticos. Additions are generally located to the rear of the dwelling.
	Building height	Generally single storey. Second storey generally within the roof space or otherwise designed to complement single storey character.
	Materials	Brick. Painted brick. Stone. Rendered masonry. Corrugated pre-coloured or galvanised iron sheeting. Timber windows. Brick/red brick chimneys.
Fencing	Low fencing facilitating views of the dwelling. Post and rail with woven wire. Low level stone or masonry. Timber picket.	
Setting, landscaping, streetscape and public realm features	Strong uniformity of layout and buildings. Consistent side and rear setbacks, large private yards. Carports and garages are behind the main face of dwellings. Generous traditional well-maintained gardens. Tree lined streets.	
Representative Buildings	<i>[Not identified]</i>	

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Hazards (Flooding - Evidence Required) Overlay

Assessment Provisions (AP)

Desired Outcome

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DO 1	Development adopts a precautionary approach to mitigate potential impacts on people, property, infrastructure and the environment from potential flood risk through the appropriate siting and design of development.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Flood Resilience	
PO 1.1 Development is sited, designed and constructed to minimise the risk of entry of potential floodwaters where the entry of flood waters is likely to result in undue damage to or compromise ongoing activities within buildings.	DTS/DPF 1.1 Habitable buildings, commercial and industrial buildings, and buildings used for animal keeping incorporate a finished floor level at least 300mm above: <ul style="list-style-type: none"> (a) the highest point of top of kerb of the primary street or (b) the highest point of natural ground level at the primary street boundary where there is no kerb

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Part 4 - General Development Policies

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.	DTS/DPF 1.1 One of the following is satisfied: <ul style="list-style-type: none"> (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to

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	<p>the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i></p> <p>(b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.</p>
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Design in Urban Areas

Assessment Provisions (AP)

Desired Outcome	
DO 1	<p>Development is:</p> <ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
Earthworks and sloping land	
<p>PO 8.1</p> <p>Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.</p>	<p>DTS/DPF 8.1</p> <p>Development does not involve any of the following:</p> <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
<p>PO 8.2</p> <p>Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.</p>	<p>DTS/DPF 8.2</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):</p> <ul style="list-style-type: none"> (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
<p>PO 8.3</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):</p> <ul style="list-style-type: none"> (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of 	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>

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<p>(c) people and goods to and from the development are designed to integrate with the natural topography of the land.</p>	
<p>PO 8.4 Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.</p>	<p>DTS/DPF 8.4 None are applicable.</p>
<p>PO 8.5 Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.</p>	<p>DTS/DPF 8.5 None are applicable.</p>
<p>Overlooking / Visual Privacy (low rise buildings)</p>	
<p>PO 10.1 Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.</p>	<p>DTS/DPF 10.1 Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone: (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.</p>
<p>PO 10.2 Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.</p>	<p>DTS/DPF 10.2 One of the following is satisfied: (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases</p>
<p>All residential development</p>	
<p>Outlook and Amenity</p>	
<p>PO 18.1 Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 18.1 A living room of a dwelling incorporates a window with an external outlook of the street frontage, private open space, public open space, or waterfront areas.</p>
<p>Residential Development - Low Rise</p>	
<p>External appearance</p>	

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<p>PO 20.3</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 20.3</p> <p>None are applicable</p>										
<p>Private Open Space</p>											
<p>PO 21.1</p> <p>Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.</p>	<p>DTS/DPF 21.1</p> <p>Private open space is provided in accordance with Design in Urban Areas Table 1 - Private Open Space.</p>										
<p>PO 21.2</p> <p>Private open space is positioned to provide convenient access from internal living areas.</p>	<p>DTS/DPF 21.2</p> <p>Private open space is directly accessible from a habitable room.</p>										
<p>Landscaping</p>											
<p>PO 22.1</p> <p>Soft landscaping is incorporated into development to:</p> <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) contribute shade and shelter (c) provide for stormwater infiltration and biodiversity (d) enhance the appearance of land and streetscapes. 	<p>DTS/DPF 22.1</p> <p>Residential development incorporates soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b):</p> <ul style="list-style-type: none"> (a) a total area as determined by the following table: <table border="1" data-bbox="869 896 1380 1288" style="margin-left: 20px;"> <thead> <tr> <th style="background-color: #0056b3; color: white;">Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="background-color: #0056b3; color: white;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td><150</td> <td>10%</td> </tr> <tr> <td>150-200</td> <td>15%</td> </tr> <tr> <td>>200-450</td> <td>20%</td> </tr> <tr> <td>>450</td> <td>25%</td> </tr> </tbody> </table> (b) at least 30% of any land between the primary street boundary and the primary building line. 	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
>200-450	20%										
>450	25%										
<p>Car parking, access and manoeuvrability</p>											
<p>PO 23.1</p> <p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single width car parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m per space (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double width car parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.4m (iii) minimum garage door width of 2.4m per space. 										

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<p>PO 23.2</p> <p>Uncovered car parking space are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.
<p>PO 23.3</p> <p>Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, domestic waste collection, landscaped street frontages and on-street parking.</p>	<p>DTS/DPF 23.3</p> <p>Driveways and access points satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) sites with a frontage to a public road of 10m or less, have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site (b) sites with a frontage to a public road greater than 10m: <ul style="list-style-type: none"> (i) have a maximum width of 5m measured at the property boundary and are the only access point provided on the site; (ii) have a width between 3.0 metres and 3.2 metres measured at the property boundary and no more than two access points are provided on site, separated by no less than 1m.
<p>PO 23.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 23.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: <ul style="list-style-type: none"> (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
<p>PO 23.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 23.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport is not steeper than 1-in-4 on average (b) they are aligned relative to the street so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the road boundary. (c) if located so as to provide access from an alley, lane or

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	right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site
PO 23.6 Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.	DTS/DPF 23.6 Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements: (a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
Waste storage	
PO 24.1 Provision is made for the convenient storage of waste bins in a location screened from public view.	DTS/DPF 24.1 Where dwellings abut both side boundaries a waste bin storage area is provided behind the building line of each dwelling that: (a) has a minimum area of 2m ² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space); and (b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.

Table 1 - Private Open Space

Dwelling Type	Dwelling / Site Configuration	Minimum Rate
Dwelling (at ground level, other than a residential flat building that includes above ground dwellings)		Total private open space area: (a) Site area <301m ² : 24m ² located behind the building line. (b) Site area ≥ 301m ² : 60m ² located behind the building line. Minimum directly accessible from a living room: 16m ² / with a minimum dimension 3m.
Cabin or caravan (permanently fixed to the ground) in a residential park or caravan and tourist park		Total area: 16m ² , which may be used as second car parking space, provided on each site intended for residential occupation.
Dwelling in a residential flat building or mixed use building which incorporate above ground level dwellings	Dwellings at ground level:	15m ² / minimum dimension 3m
	Dwellings above ground level:	
	Studio (no separate bedroom)	4m ² / minimum dimension 1.8m

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	One bedroom dwelling	8m ² / minimum dimension 2.1m
	Two bedroom dwelling	11m ² / minimum dimension 2.4m
	Three + bedroom dwelling	15 m ² / minimum dimension 2.6m

Infrastructure and Renewable Energy Facilities

Assessment Provisions (AP)

Desired Outcome	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Wastewater Services	
PO 12.2 Effluent drainage fields and other wastewater disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.	DTS/DPF 12.2 Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.

Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Overshadowing	
PO 3.1 Overshadowing of habitable room windows of adjacent residential land uses in: a. a neighbourhood-type zone is minimised to maintain access	DTS/DPF 3.1 North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.

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<p>to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight.</p>	
<p>PO 3.2 Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in: a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight.</p>	<p>DTS/DPF 3.2 Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following: a. for ground level private open space, the smaller of the following: i. half the existing ground level open space or ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m) b. for ground level communal open space, at least half of the existing ground level open space.</p>
<p>PO 3.3 Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account: (a) the form of development contemplated in the zone (b) the orientation of the solar energy facilities (c) the extent to which the solar energy facilities are already overshadowed.</p>	<p>DTS/DPF 3.3 None are applicable.</p>

Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Vehicle Parking Rates	
<p>PO 5.1 Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as: (a) availability of on-street car parking (b) shared use of other parking areas</p>	<p>DTS/DPF 5.1 Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant: (a) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements (b) Transport, Access and Parking Table 2 - Off-Street</p>

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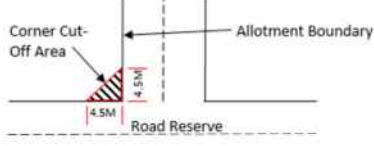
<p>(c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared</p> <p>(d) the adaptive reuse of a State or Local Heritage Place.</p>	<p>Vehicle Parking Requirements in Designated Areas if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.</p>
<p>Corner Cut-Offs</p>	
<p>PO 10.1 Development is located and designed to ensure drivers can safely turn into and out of public road junctions.</p>	<p>DTS/DPF 10.1 Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:</p>  <p>The diagram illustrates a corner cut-off area at a road junction. A road reserve of 4.5M is shown on the left side of the road. A diagonal hatched area represents the corner cut-off area, extending from the road reserve towards the junction. An allotment boundary is shown as a vertical dashed line to the right of the road reserve.</p>

Table 1 - General Off-Street Car Parking Requirements

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate (unless varied by Table 2 onwards) Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.
Residential Development	
Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Group Dwelling	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
Residential Flat Building	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p> <p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom)</p>

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Row Dwelling where vehicle access is from the primary street	- 1 space per dwelling. Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Semi-Detached Dwelling	Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Aged / Supported Accommodation	
Retirement village	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling. 0.2 spaces per dwelling for visitor parking.
Supported accommodation	0.3 spaces per bed.
Residential Development (Other)	
Ancillary accommodation	No additional requirements beyond those associated with the main dwelling.
Residential park	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling. 0.2 spaces per dwelling for visitor parking.
Student accommodation	0.3 spaces per bed.
Workers' accommodation	0.5 spaces per bed plus 0.2 spaces per bed for visitor parking.
Tourist	
Caravan park / tourist park	Parks with 100 sites or less - a minimum of 1 space per 10 sites to be used for accommodation. Parks with more than 100 sites - a minimum of 1 space per 15 sites used for accommodation. A minimum of 1 space for every caravan (permanently fixed to the ground) or cabin.
Tourist accommodation	1 car parking space per accommodation unit / guest room.
Commercial Uses	

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Auction room/ depot	1 space per 100m ² of building floor area plus an additional 2 spaces.
Automotive collision repair	3 spaces per service bay.
Call centre	8 spaces per 100m ² of gross leasable floor area.
Motor repair station	3 spaces per service bay.
Office	4 spaces per 100m ² of gross leasable floor area.
Retail fuel outlet	3 spaces per 100m ² gross leasable floor area.
Service trade premises	2.5 spaces per 100m ² of gross leasable floor area 1 space per 100m ² of outdoor area used for display purposes.
Shop (no commercial kitchen)	5.5 spaces per 100m ² of gross leasable floor area where not located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared. 5 spaces per 100m ² of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.
Shop (in the form of a bulky goods outlet)	2.5 spaces per 100m ² of gross leasable floor area.
Shop (in the form of a restaurant or involving a commercial kitchen)	Premises with a dine-in service only (which may include a take-away component with no drive-through) - 0.4 spaces per seat. Premises with take-away service but with no seats - 12 spaces per 100m ² of total floor area plus a drive-through queue capacity of ten vehicles measured from the pick-up point. Premises with a dine-in and drive-through take-away service - 0.3 spaces per seat plus a drive through queue capacity of 10 vehicles measured from the pick-up point.
Community and Civic Uses	
Childcare centre	0.25 spaces per child
Library	4 spaces per 100m ² of total floor area.
Community facility	10 spaces per 100m ² of total floor area.
Hall / meeting hall	0.2 spaces per seat.

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Place of worship	1 space for every 3 visitor seats.
Pre-school	1 per employee plus 0.25 per child (drop off/pick up bays)
Educational establishment	<p>For a primary school - 1.1 space per full time equivalent employee plus 0.25 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a secondary school - 1.1 per full time equivalent employee plus 0.1 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a tertiary institution - 0.4 per student based on the maximum number of students on the site at any time.</p>
Health Related Uses	
Hospital	<p>4.5 spaces per bed for a public hospital.</p> <p>1.5 spaces per bed for a private hospital.</p>
Consulting room	4 spaces per consulting room excluding ancillary facilities.
Recreational and Entertainment Uses	
Cinema complex	0.2 spaces per seat.
Concert hall / theatre	0.2 spaces per seat.
Hotel	1 space for every 2m ² of total floor area in a public bar plus 1 space for every 6m ² of total floor area available to the public in a lounge, beer garden plus 1 space per 2 gaming machines, plus 1 space per 3 seats in a restaurant.
Indoor recreation facility	<p>6.5 spaces per 100m² of total floor area for a Fitness Centre</p> <p>4.5 spaces per 100m² of total floor area for all other Indoor recreation facilities.</p>
Industry/Employment Uses	
Fuel depot	<p>1.5 spaces per 100m² total floor area</p> <p>1 spaces per 100m² of outdoor area used for fuel depot activity purposes.</p>
Industry	1.5 spaces per 100m ² of total floor area.
Store	0.5 spaces per 100m ² of total floor area.
Timber yard	1.5 spaces per 100m ² of total floor area

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	1 space per 100m ² of outdoor area used for display purposes.
Warehouse	0.5 spaces per 100m ² total floor area.
Other Uses	
Funeral Parlour	1 space per 5 seats in the chapel plus 1 space for each vehicle operated by the parlour.
Radio or Television Station	5 spaces per 100m ² of total building floor area.

Table 2 - Off-Street Car Parking Requirements in Designated Areas

The following parking rates apply in any zone, subzone or other area described in the 'Designated Areas' column subject to the following:

- (a) the location of the development is unable to satisfy the requirements of Table 2 – Criteria (other than where a location is exempted from the application of those criteria)
or
- (b) the development satisfies Table 2 – Criteria (or is exempt from those criteria) and is located in an area where a lawfully established carparking fund operates, in which case the number of spaces are reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate		Designated Areas
	Minimum number of spaces	Maximum number of spaces	
<p>Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.</p>			
Development generally			
All classes of development	No minimum.	<p>No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is:</p> <p>1 space for each dwelling with a total floor area less than 75 square metres</p> <p>2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres</p> <p>3 spaces for each dwelling with a total floor area greater than 150 square metres.</p> <p>Residential flat building or Residential component of a</p>	<p>Capital City Zone</p> <p>City Main Street Zone</p> <p>City Riverbank Zone</p> <p>Adelaide Park Lands Zone</p> <p>Business Neighbourhood Zone (within the City of Adelaide)</p> <p>The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone</p>

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		multi-storey building: 1 visitor space for each 6 dwellings.	
Non-residential development			
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	5 spaces per 100m ² of gross leasable floor area.	City Living Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	6 spaces per 100m ² of gross leasable floor area.	Strategic Innovation Zone Suburban Activity Centre Zone Suburban Business Zone Business Neighbourhood Zone Suburban Main Street Zone Urban Activity Centre Zone
Tourist accommodation	1 space for every 4 bedrooms up to 100 bedrooms plus 1 space for every 5 bedrooms over 100 bedrooms	1 space per 2 bedrooms up to 100 bedrooms and 1 space per 4 bedrooms over 100 bedrooms	City Living Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Residential development			
Residential component of a multi-storey building	Dwelling with no separate bedroom -0.25 spaces per dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.	None specified.	City Living Zone Strategic Innovation Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone

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Residential flat building	Dwelling with no separate bedroom -0.25 spaces per dwelling	None specified.	City Living Zone
	1 bedroom dwelling - 0.75 spaces per dwelling		Urban Activity Centre Zone
	2 bedroom dwelling - 1 space per dwelling		Urban Corridor (Boulevard) Zone
	3 or more bedroom dwelling - 1.25 spaces per dwelling		Urban Corridor (Business) Zone
	0.25 spaces per dwelling for visitor parking.		Urban Corridor (Living) Zone
			Urban Corridor (Main Street) Zone
			Urban Neighbourhood Zone

Table 2 - Criteria:

The following criteria are used in conjunction with Table 2. The 'Exception' column identifies locations where the criteria do not apply and the car parking rates in Table 2 are applicable.

Criteria	Exceptions
<p>The designated area is wholly located within Metropolitan Adelaide and any part of the development site satisfies one or more of the following:</p> <p>(a) is within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service⁽²⁾</p> <p>(b) is within 400 metres of a bus interchange⁽¹⁾</p> <p>(c) is within 400 metres of an O-Bahn interchange⁽¹⁾</p> <p>(d) is within 400 metres of a passenger rail station⁽¹⁾</p> <p>(e) is within 400 metres of a passenger tram station⁽¹⁾</p> <p>(f) is within 400 metres of the Adelaide Parklands.</p>	<p>(a) All zones in the City of Adelaide</p> <p>(b) Strategic Innovation Zone in the following locations:</p> <p>(i) City of Burnside</p> <p>(ii) City of Marion</p> <p>(iii) City of Mitcham</p> <p>(c) Urban Corridor (Boulevard) Zone</p> <p>(d) Urban Corridor (Business) Zone</p> <p>(e) Urban Corridor (Living) Zone</p> <p>(f) Urban Corridor (Main Street) Zone</p> <p>(g) Urban Neighbourhood Zone</p>

[NOTE(S): (1) Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles. (2) A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.]

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14 RANKINE RD TORRENSVILLE SA 5031

Address:

Click to view a detailed interactive [SABIS](#) in SAILIS

To view a detailed interactive property map in SAPPA click on the map below



Property Zoning Details

Local Variation (TNV)

Minimum Site Area (o_Minimum site area for a detached dwelling is 340 sqm; semi-detached dwelling is 340 sqm)

Maximum Building Height (Levels) (Maximum building height is 1 level)

Minimum Side Boundary Setback (Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher)

Overlay

Aircraft Noise Exposure (ANEF 25)

Airport Building Heights (Regulated) (All structures over 15 metres)

Affordable Housing

Building Near Airfields

Character Area (WeToC2)

Hazards (Flooding - Evidence Required)

Prescribed Wells Area

Regulated and Significant Tree

Stormwater Management

Traffic Generating Development

Urban Tree Canopy

Zone

Established Neighbourhood

Selected Development(s)

Verandah

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards. If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

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Verandah - Code Assessed - Deemed to Satisfy

Part 2 - Zones and Sub Zones

Established Neighbourhood Zone

Assessment Provisions (AP)

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Deemed to Satisfy	
Site coverage	
DTS/DPF 3.1	
Development does not result in site coverage exceeding:	
In instances where:	
<ul style="list-style-type: none"> (a) no value is returned (i.e. there is a blank field), then a maximum 50% site coverage applies (b) more than one value is returned in the same field, refer to the Site Coverage Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development. 	
Ancillary buildings and structures	
DTS/DPF 11.1	
Ancillary buildings and structures:	
<ul style="list-style-type: none"> (a) are ancillary to a dwelling erected on the same site (b) have a floor area not exceeding 60m² (c) are constructed, added to or altered so that they are situated at least <ul style="list-style-type: none"> (i) 500mm behind the building line of the dwelling to which they are ancillary or (ii) 900mm from a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (d) in the case of a garage or carport, the garage or carport: <ul style="list-style-type: none"> (i) is set back at least 5.5m from the boundary of the primary street (ii) when facing a primary street or secondary street has a total door/opening not exceeding 7m or 30% of the site frontage (whichever is the lesser) when facing a primary street or secondary street (e) if situated on a boundary (not being a boundary with a primary street or secondary street), a length not exceeding 8m unless: <ul style="list-style-type: none"> (i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and (ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent (f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary not exceeding 45% of the length of that boundary (g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or abut the proposed wall or structure (h) have a wall height or post height not exceeding 3m above natural ground level, and where located to the side of the associated dwelling, have a wall height or post height no higher than the wall height of the associated dwelling (i) have a roof height where no part of the roof is more than 5m above the natural ground level (j) if clad in sheet metal, are pre-colour treated or painted in a non-reflective colour. (k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less: (i) a total area as determined by the following table: 	
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site

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<150	10%
150-200	15%
201-450	20%
>450	25%

(ii) the amount of existing soft landscaping prior to the development occurring.

DTS/DPF 11.2

Ancillary buildings and structures do not result in:

- (a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space
- (b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.

Part 3 - Overlays

Airport Building Heights (Regulated) Overlay

Assessment Provisions (AP)

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Deemed to Satisfy	
Built Form	
DTS/DPF 1.1	
Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas.	
In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.	

Building Near Airfields Overlay

Assessment Provisions (AP)

Deemed to Satisfy	
DTS/DPF 1.3	
The distance from any part of a runway centreline to the closest point of the building is not less than 35 times the building height.	

Part 4 - General Development Policies

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Deemed to Satisfy	
--------------------------	--

Policy24 - Enquiry

DTS/DPF 1.1

One of the following is satisfied:

- (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the *Electricity Act 1996*
- (b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.

Design in Urban Areas**Assessment Provisions (AP)****Deemed to Satisfy**

All Development

Earthworks and sloping land

DTS/DPF 8.1

Development does not involve any of the following:

- (a) excavation exceeding a vertical height of 1m
- (b) filling exceeding a vertical height of 1m
- (c) a total combined excavation and filling vertical height of 2m or more.

Infrastructure and Renewable Energy Facilities**Assessment Provisions (AP)**

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Deemed to Satisfy

Wastewater Services

DTS/DPF 12.2

Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.

Policy24 - Enquiry

14 RANKINE RD TORRENSVILLE SA 5031

Address:

Click to view a detailed interactive [SARIS](#) in SAILIS

To view a detailed interactive property map in SAPPa click on the map below



Property Zoning Details

Local Variation (TNV)

Minimum Site Area (o_Minimum site area for a detached dwelling is 340 sqm; semi-detached dwelling is 340 sqm)

Maximum Building Height (Levels) (Maximum building height is 1 level)

Minimum Side Boundary Setback (Minimum side boundary setback is 1m for the first building level; 2m for any second building level or higher)

Overlay

Aircraft Noise Exposure (ANEF 25)

Airport Building Heights (Regulated) (All structures over 15 metres)

Affordable Housing

Building Near Airfields

Character Area (WeToC2)

Hazards (Flooding - Evidence Required)

Prescribed Wells Area

Regulated and Significant Tree

Stormwater Management

Traffic Generating Development

Urban Tree Canopy

Zone

Established Neighbourhood

Selected Development(s)

Internal building work

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards.

If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

Policy24 - Enquiry

Internal building work - Accepted Development

Part 2 - Zones and Sub Zones

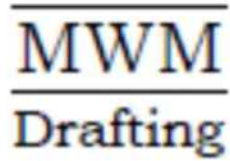
Established Neighbourhood Zone

Table 1 - Accepted Development Classification

The following table identifies Classes of Development that are classified as Accepted Development subject to meeting the Accepted Development Classification Criteria

Class of Development	Accepted Development Classification Criteria
Internal building work Except where any of the following apply: <ul style="list-style-type: none"> • Local Heritage Place Overlay • State Heritage Area Overlay • State Heritage Place Overlay 	<ol style="list-style-type: none"> 1. There will be no increase in the total floor area of the building. 2. Other than where located within the Historic Area Overlay there will be no alteration to the external appearance of the building to any significant degree. 3. There will be no alteration to the external appearance of the building where located within the Historic Area Overlay.

14 Rankine Rd Torrensville



MWM Drafting
Drafting and Design Services
53 Wellington Rd, Mt. BARKER, SA, 5251 Ph 0404 052327

City Of West Torrens

August 2020

PLANNING REPORT
Dwelling Additions and Alteration
14 Rankine Rd Torrensville



REPORT PREPARED BY:

MYLES MELINE
Draftsperson

14 Rankine Rd Torrensville

1.0 INTRODUCTION

This Planning Report supports a development application for the Renovation and addition to an existing dwelling. The relevant approving authority is The City of West Torrens

1.1 The Zone

The land is wholly within the Established Neighbourhood Zone. The land does not adjoin any other Zone, and there are no Sub Zones. See the included map below.



2.0 NATURE OF THE DEVELOPMENT AND NATURE OF THE LOCALITY

2.1 The Proposal

The proposed development is to modify the downstairs interior and extend the upper floor toward the rear western boundary by the inclusion of new upper floor rooms, as per the plans prepared by MWM Drafting dated 15/08/21 (attached).

14 Rankine Rd Torrensville

2.2 The Locality

The locality is in the suburb of Torrensville, close to the commercial district along Henley Beach Rd.

2.3 Background

The subject land is located within the established neighbourhood zone of Torrensville. The proponents currently reside within the dwelling and will continue to use it as their primary residence after the completion of works.

2.4 The Land

The land is described as AL57 in F144285 Rankine Rd, in the suburb of Torrensville and is held in CT 5865/896.

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.

Certificate of Title - Volume 5865 Folio 896

Parent Title(s) CT 2681/122

Creating Dealing(s) CONVERTED TITLE

Title Issued 04/02/2002 Edition 2 Edition Issued 24/07/2017

Estate Type

FEE SIMPLE

Registered Proprietor

OLIVIA ELLEN MAROS

OF UNIT 2 25 WAINE STREET FRESHWATER NSW 2096

Description of Land

ALLOTMENT 57 FILED PLAN 144285

IN THE AREA NAMED TORRENSVILLE

HUNDRED OF ADELAIDE

Easements

SUBJECT TO EASEMENT(S) OVER THE LAND MARKED C (T 2132787)

SUBJECT TO PARTY WALL RIGHT(S) OVER THE LAND MARKED A (T 2132787)

TOGETHER WITH PARTY WALL RIGHT(S) OVER THE LAND MARKED B (T 2132787)

Schedule of Dealings

Dealing Number Description

12759201 MORTGAGE TO BENDIGO & ADELAIDE BANK LTD. (ACN: 068 049 178)

Notations

Dealings Affecting Title NIL

Priority Notices NIL

Notations on Plan NIL

Registrar-General's Notes NIL

Administrative Interests NIL

Product Register Search Plus
(CT 5865/896)

Date/Time 16/08/2021 06:04PM

Customer Reference MWM

Order ID 20210816010571

Land Services SA Page 1 of 2

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Product Register Search Plus
(CT 5865/896)

Date/Time 16/08/2021 06:04PM

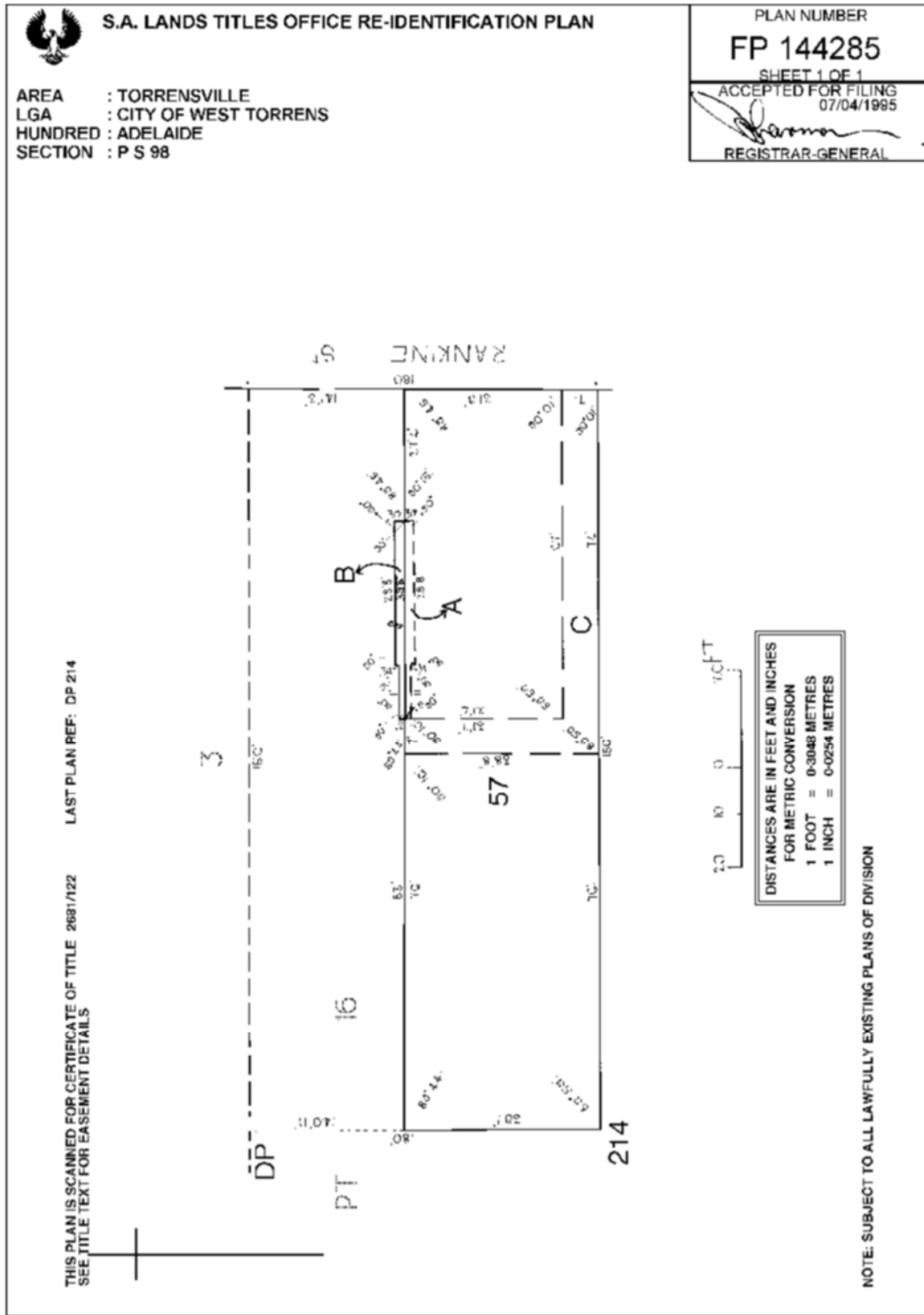
Customer Reference MWM

Order ID 20210816010571

Land Services SA Page 2 of 2

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14 Rankine Rd Torrensvile



14 Rankine Rd Torrensville

2.5 Relevant Provisions of the Development Plan

The following provisions of the PDI Code 2021 ESTABLISHED NEIGHBORHOOD ZONE are considered to be relevant to the proposal.

<p>DO 1 A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>DO 2 Maintain the predominant streetscape character, having regard to key features such as roadside plantings, footpaths, front yards, and space between crossovers.</p>	<p>The streetscape character of the existing dwelling pays due regard to roadside plantings with a generous front yard, and as such clearly adheres to DO2.</p>
<p>PO 3.1 Building footprints are consistent with the character and pattern of the neighbourhood and provide sufficient space around buildings to limit visual impact, provide an attractive outlook and access to light and ventilation.</p>	<p>The existing building footprint of the building is entirely consistent with the character and pattern of the built form in the locality and provides ample space around the curtilages of the dwelling. This footprint will remain unchanged with this proposal and clearly adheres to PO3.1</p>
<p>PO 4.1 Buildings contribute to the prevailing character of the neighbourhood and complements the height of nearby buildings.</p>	<p>The proposed addition will not result in a building whose height and character is at variance with other buildings in close vicinity and the neighbourhood generally.</p>
<p>PO 4.2 Additions and alterations do not adversely impact on the streetscape character.</p>	<p>The proposed addition will not result in a building whose height and character is at variance with other buildings in close vicinity and the neighbourhood generally.</p>
<p>PO 6.1 Buildings are set back from secondary street boundaries (not being a rear laneway) to maintain the established pattern of separation between buildings and public streets and reinforce streetscape character.</p>	<p>The existing setbacks are entirely consistent with PO6.1 and will remain unchanged in this proposal.</p>
<p>PO 7.1 Dwelling boundary walls are limited in height and length to manage visual and overshadowing impacts on adjoining properties.</p>	<p>The only boundary wall in this proposal is an existing wall on the Northern Boundary which currently stands on the boundary. The height of this wall will increase to provide a party wall for the addition to a height which is not greater than the existing party wall to the east. The wall is on the south side of the boundary, and as such will not overshadow the neighbour to the north and clearly supports PO7.1</p>

14 Rankine Rd Torrensville

<p>PO 7.2 Dwellings in a semidetached, row or terrace arrangement maintain space between buildings consistent with a low density suburban streetscape character.</p>	<p>The current dwelling is one half of a semidetached dwelling which currently has ample space around it on three sides, this proposal will not reduce those existing setbacks and as such complies with PO 7.2</p>
<p>PO 8.1 Buildings are set back from side boundaries to provide: separation between buildings in a way that complements the established character of the locality access to natural light and ventilation for neighbours.</p>	<p>The current dwelling is one half of a semidetached dwelling which currently has ample space around it on three sides, this proposal will not reduce those existing setbacks and as such complies with PO 8.1</p>
<p>PO 9.1 Buildings are set back from rear boundaries to provide: separation between dwellings in a way that complements the established character of the locality access to natural light and ventilation for neighbours private open space space for landscaping and vegetation.</p>	<p>The current dwelling is one half of a semidetached dwelling which currently has ample space around it on three sides, this proposal will not reduce those existing setbacks and as such complies with PO 9.1</p>
<p>PO 10.1 Garages and carports are designed and sited to be discrete and not dominate the appearance of the associated dwelling when viewed from the street.</p>	<p>The existing garage on site is set back to a distance that places behind the dwelling, and this proposal includes no new garage or carport or change to the existing one.</p>
<p>PO 10.2 The appearance of development as viewed from public roads is sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality.</p>	<p>The proposed addition will not result in a building whose height and character is at variance with other buildings in close vicinity and the neighbourhood generally.</p>
<p>DO 1 Valued streetscape characteristics and development patterns are reinforced through contextually responsive development, design and adaptive reuse that responds to the attributes expressed in the Character Area Statement.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>PO 1.1 All development is undertaken having consideration to the valued attributes expressed in the Character Area Statement.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>

14 Rankine Rd Torrensville

<p>PO 2.1 The form of new buildings and structures that are visible from the public realm are consistent with the valued streetscape characteristics of the character area.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>PO 2.2 Development is consistent with the prevailing building and wall heights in the character area.</p>	<p>The proposed addition will not result in a building whose height and character is at variance with other buildings in close vicinity and the neighbourhood generally.</p>
<p>PO 2.3 Design and architectural detailing of street facing Buildings (including but not limited to roof pitch and form, openings, chimneys and verandahs) are consistent with the prevailing characteristics in the character area.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>PO 2.4 Development is consistent with the prevailing front and side boundary setback pattern in the character area.</p>	<p>The current dwelling is one half of a semidetached dwelling which currently has ample space around it on three sides, this proposal will not reduce those existing setbacks and as such complies with PO 2.4</p>
<p>PO 2.5 Materials are either consistent with or complement those within the character area.</p>	<p>The amended house as proposed will retain the original brick and stone wall materials, utilize zincalume corrugated steel roof cladding to match the existing, and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>PO 3.1 Additions and alterations do not adversely impact on the streetscape character.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>

14 Rankine Rd Torrensville

<p>PO 3.2 Adaptive reuse and revitalisation of buildings to retain local character consistent with the Character Area Statement.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>PO 6.1 The width of driveways and other vehicle access ways are consistent with the prevalent width of existing driveways in the character area.</p>	<p>The existing dwelling includes a driveway already in keeping with the prevailing driveways of the area. This proposal will not affect the current driveway.</p>
<p>PO 6.2 Development maintains the valued landscape pattern and characteristics that contribute to the character area, except where they compromise safety, create nuisance, or impact adversely on existing buildings or infrastructure.</p>	<p>The amended house as proposed will contain a gable ended roof clad in zincalume corrugated steel to match existing and double hung timber windows all of which are consistent and sympathetic with the predominant built form and character of the neighbourhood.</p>
<p>DTS/DPF 3.1 North facing windows of habitable rooms of adjacent residential land uses in a neighbourhood type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.</p>	<p>As shown on the solar study included on Drawing 1 of plans by MWM Drafting, dated 15/08/2021, the north facing window of the southern neighbour is only shaded until 12:00PM on June 21, allowing for 3 hours of direct sunlight before 3:00pm, in compliance with DTS/DPF 3.1</p>
<p>DTS/DPF 3.2 Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood type zone in accordance with the following: a. for ground level private open space, the smaller of the following: i. half the existing ground level open space or ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m) b. for ground level communal open space, at least half of the existing ground level open space.</p>	<p>As shown on the solar study included on Drawing 1 of plans by MWM Drafting, dated 15/08/2021, back yard of the southern neighbour is no longer shaded by 12:00PM on June 21, allowing for ample hours of direct sunlight before 3:00pm, in compliance with DTS/DPF 3.2</p>

3.0 PUBLIC UTILITIES

The proposal will cause negligible extra load on public utilities than currently exists. No excavation work will affect existing services (see services maps attached).

4.0 OTHER MATTERS

Nil.

14 Rankine Rd Torrensville

5.0 CONCLUSION

The proposed Renovation and Addition is supported by many provisions of the PDI Code 2021 that encourage orderly and economic development.

The development of the site for this purpose will facilitate the achievement of many of the core objectives of the PDI Code.

It is therefore submitted that this development is ideally suited to this site as the proposal is minimal in scale and effect and is not considered at variance with the PDI Code.

It is therefore submitted that the proposal has considerable merit and it should be granted Planning Consent.

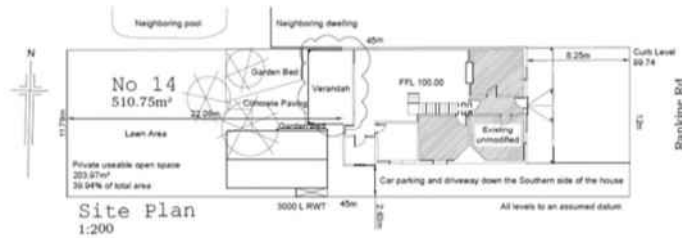
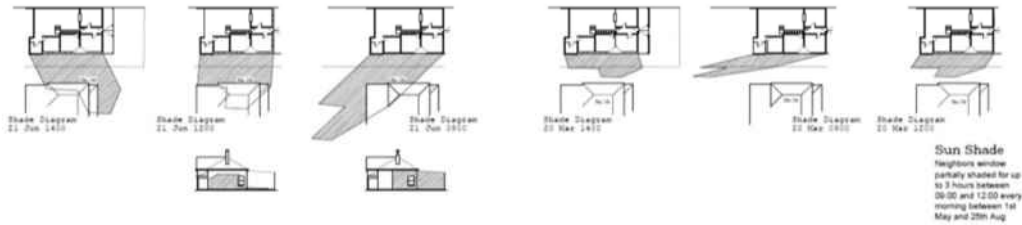
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**Myles Meline
Drafter**

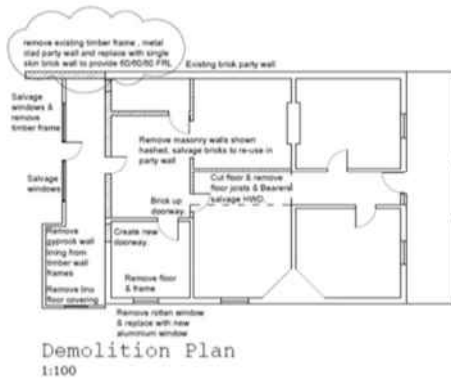
14 Rankine Rd Torrensville

Attachments-

Plans by MWM Drafting dated 15/08/21.



Proposed Home Renovation For Greg McNair At 14 Rankine Rd Torrensville	Dated 14/08/2021 Version 4 Drawing No. 1	
	Do not scale off this drawing	



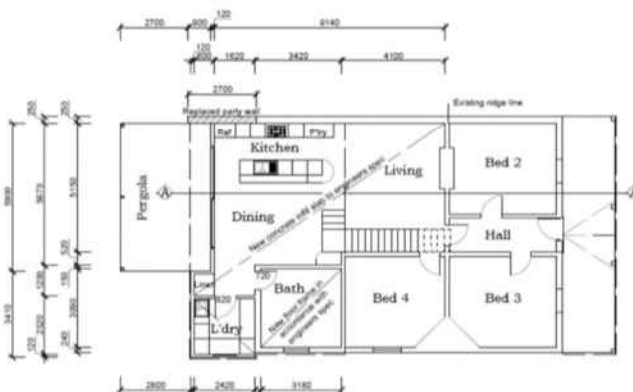
Proposed Home Renovation For Greg McNair At 14 Rankine Rd Torrensville	Dated 09/08/2021 Version 4 Drawing No. 2	
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14 Rankine Rd Torrensvile

CONSTRUCTION NOTES

Drawings must be read in conjunction with engineer's report
 Written statements shall comply
 All dimensions shall be checked on site by builder prior to commencement of any work
 All works to comply to the current National Construction Code (NCC)
 All works to comply to the current Victorian Construction Code (VCC)
 Building protection in accordance with AS1918-2018 and Masonry Spec. SA T8 (optional)
 Brick Facade must not be within line of building work
 Chimney and hearth design to comply with VCC Part 3.7.3 Heating Appliances
 Concrete in accordance with AS1470-2018 and AS1079-2011
 Construction door must be specified
 Draft Protection devices to be fitted to all doors and windows
 Electrical in accordance with AS1910-2018
 Escape all building waste and time is collected and disposal of appropriately
 Cladding in accordance with AS1396-2018
 Masonry wall construction in accordance with AS1700-2018

Roofing in accordance with AS 1590-2018
 Roofing and Trusswork in accordance with AS1700-2018
 Roofing contractor to responsible for waterproofing of the whole roof
 Self contained lead lined waste chute complying with AS1780-2018
 Connected to external mains with heavy locking
 Suitable chute to be interconnected complying with NCC 2019
 All runoff from site must be prevented, and not used shall not be discharged to road by vehicle
 Traps in accordance with VCC Part 3.7.1
 Trapswork in accordance with AS1410-2018
 Trusswork remains in accordance with Masonry Spec. SA T8AA
 Trusswork to be discharged away from building
 Suitable ventilation in accordance with VCC Part 3.4.1
 Trapswork protection in accordance with AS1410-2018
 Trapswork framing in accordance with AS1410-2018
 Waterproofing in wet areas in accordance with VCC 3.8.1 and AS1740-2018
 Windows and door assemblies in accordance with AS1294?



Lower Floor
1:100



Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensvile

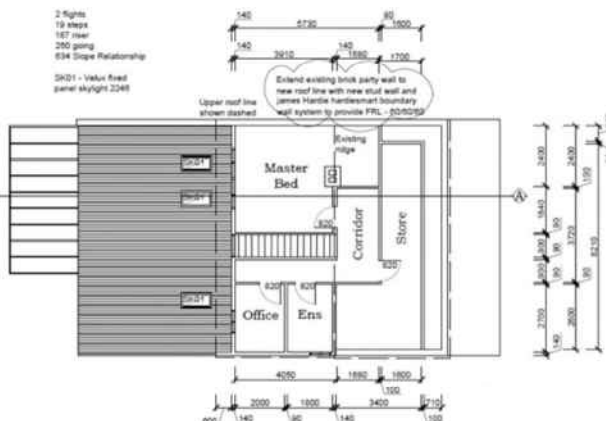
Dated 03/07/2021
 Version 3
 Drawing No. 3
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MWM
 Drafting

CONSTRUCTION NOTES

Drawings must be read in conjunction with engineer's report
 Written statements shall comply
 All dimensions shall be checked on site by builder prior to commencement of any work
 All works to comply to the current National Construction Code (NCC)
 All works to comply to the current Victorian Construction Code (VCC)
 Building protection in accordance with AS1918-2018 and Masonry Spec. SA T8 (optional)
 Brick Facade must not be within line of building work
 Chimney and hearth design to comply with VCC Part 3.7.3 Heating Appliances
 Concrete in accordance with AS1470-2018 and AS1079-2011
 Construction door must be specified
 Draft Protection devices to be fitted to all doors and windows
 Electrical in accordance with AS1910-2018
 Escape all building waste and time is collected and disposal of appropriately
 Cladding in accordance with AS1396-2018
 Masonry wall construction in accordance with AS1700-2018

Roofing in accordance with AS 1590-2018
 Roofing and Trusswork in accordance with AS1700-2018
 Roofing contractor to responsible for waterproofing of the whole roof
 Self contained lead lined waste chute complying with AS1780-2018
 Connected to external mains with heavy locking
 Suitable chute to be interconnected complying with NCC 2019
 All runoff from site must be prevented, and not used shall not be discharged to road by vehicle
 Traps in accordance with VCC Part 3.7.1
 Trapswork in accordance with AS1410-2018
 Trusswork remains in accordance with Masonry Spec. SA T8AA
 Trusswork to be discharged away from building
 Suitable ventilation in accordance with VCC Part 3.4.1
 Trapswork protection in accordance with AS1410-2018
 Trapswork framing in accordance with AS1410-2018
 Waterproofing in wet areas in accordance with VCC 3.8.1 and AS1740-2018
 Windows and door assemblies in accordance with AS1294?



Upper Floor
1:100

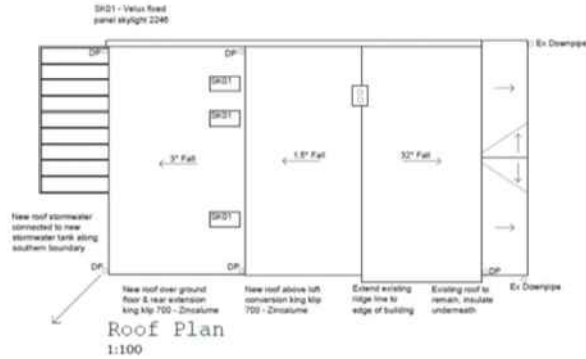


Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensvile

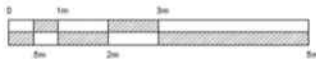
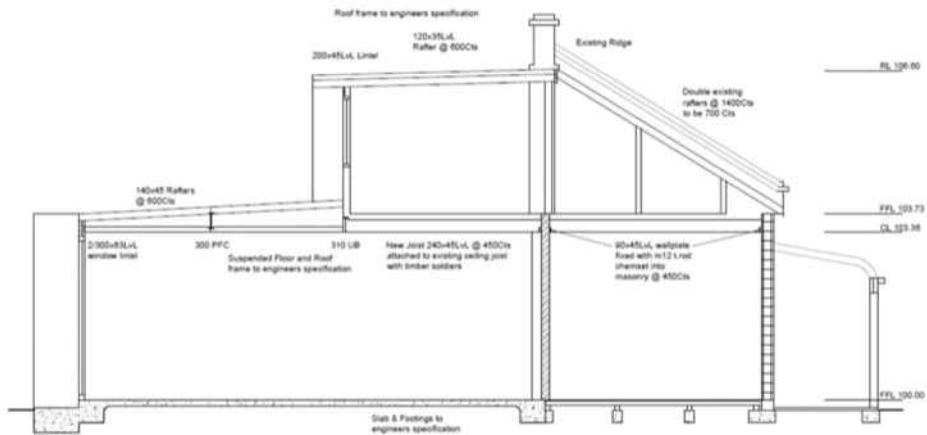
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14 Rankine Rd Torrensville

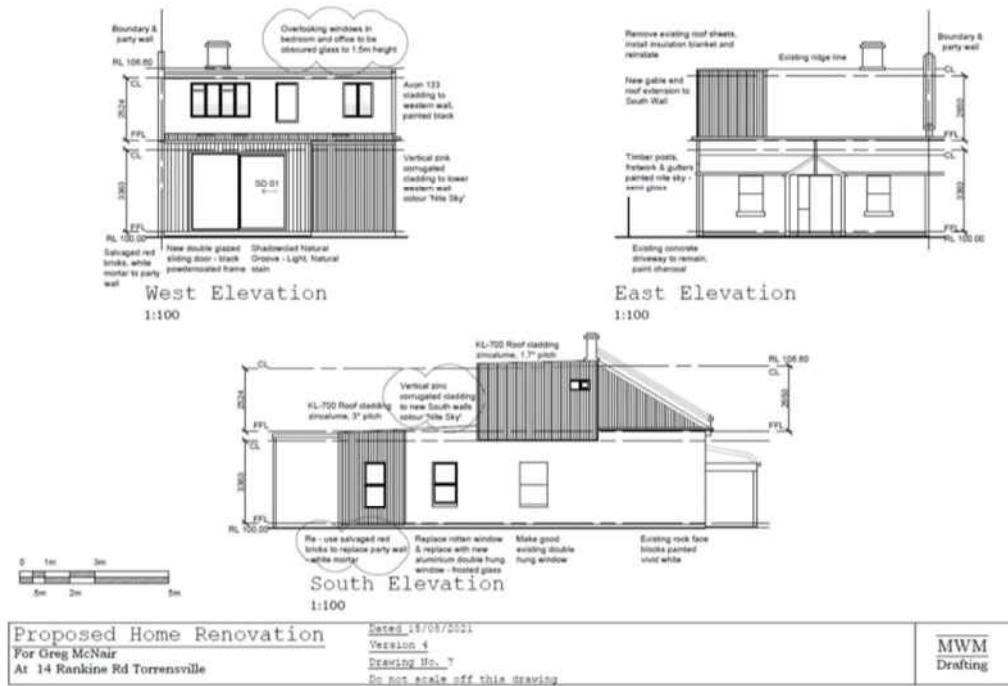


Proposed Home Renovation For Greg McNair At 14 Rankine Rd Torrensville	Dated 03/08/2021	MWM Drafting
	Version 4	
	Drawing No. 6	
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Proposed Home Renovation For Greg McNair At 14 Rankine Rd Torrensville	Dated 03/08/2021	MWM Drafting
	Version 4	
	Drawing No. 6	
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14 Rankine Rd Torrensville



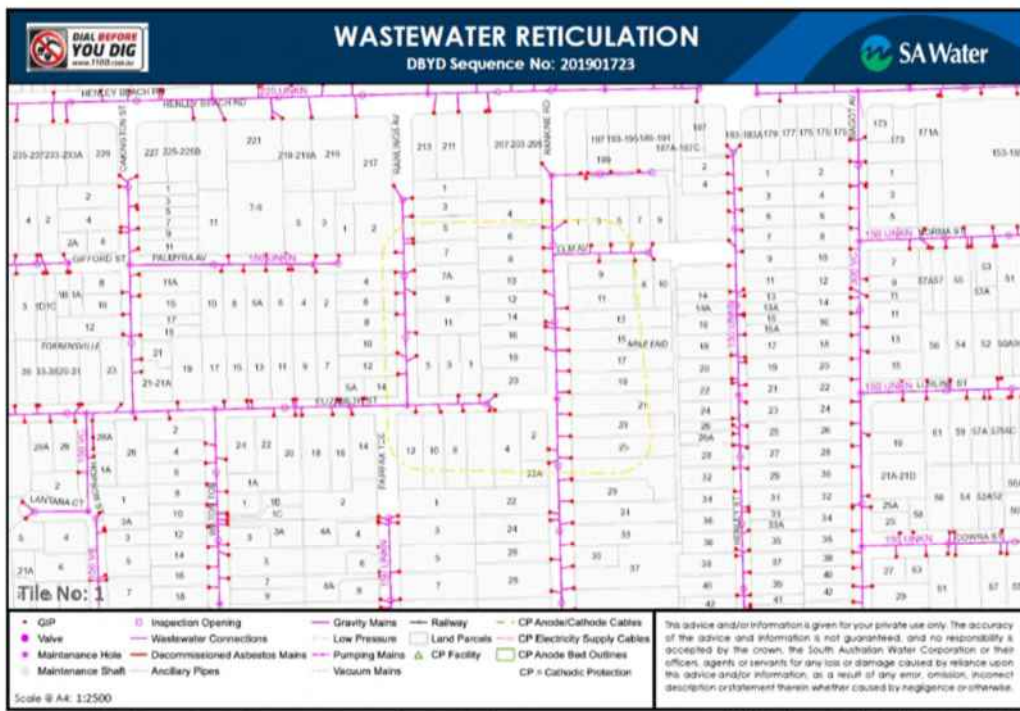
Services Maps



14 Rankine Rd Torrensvile

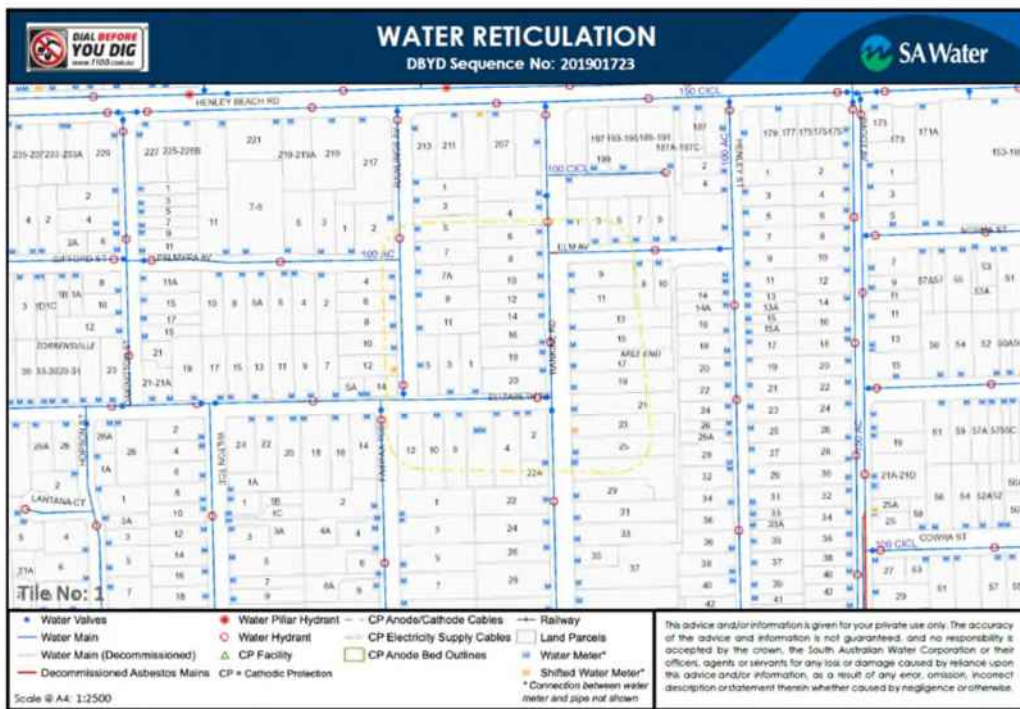


14 Rankine Rd Torrensville



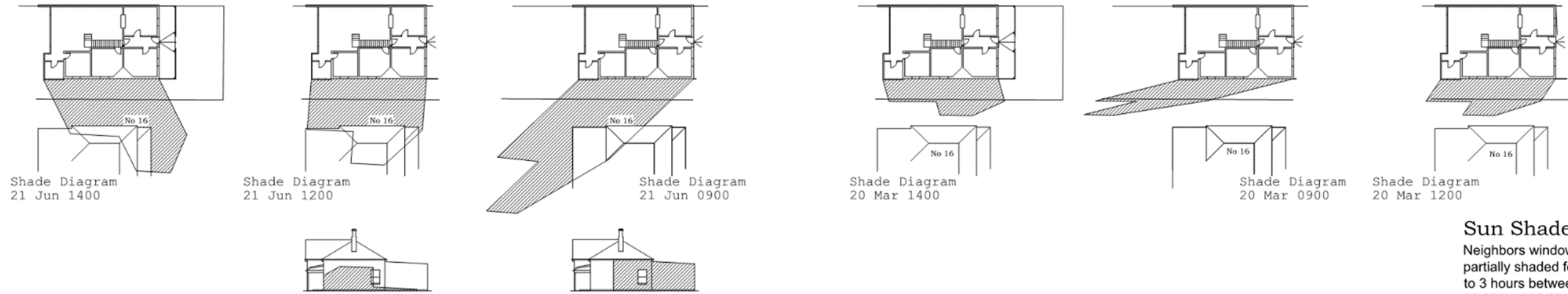
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AU SA Water - Response Plan (Wastewater).docx (rev. 31 Mar 2020)

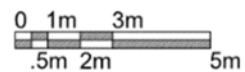
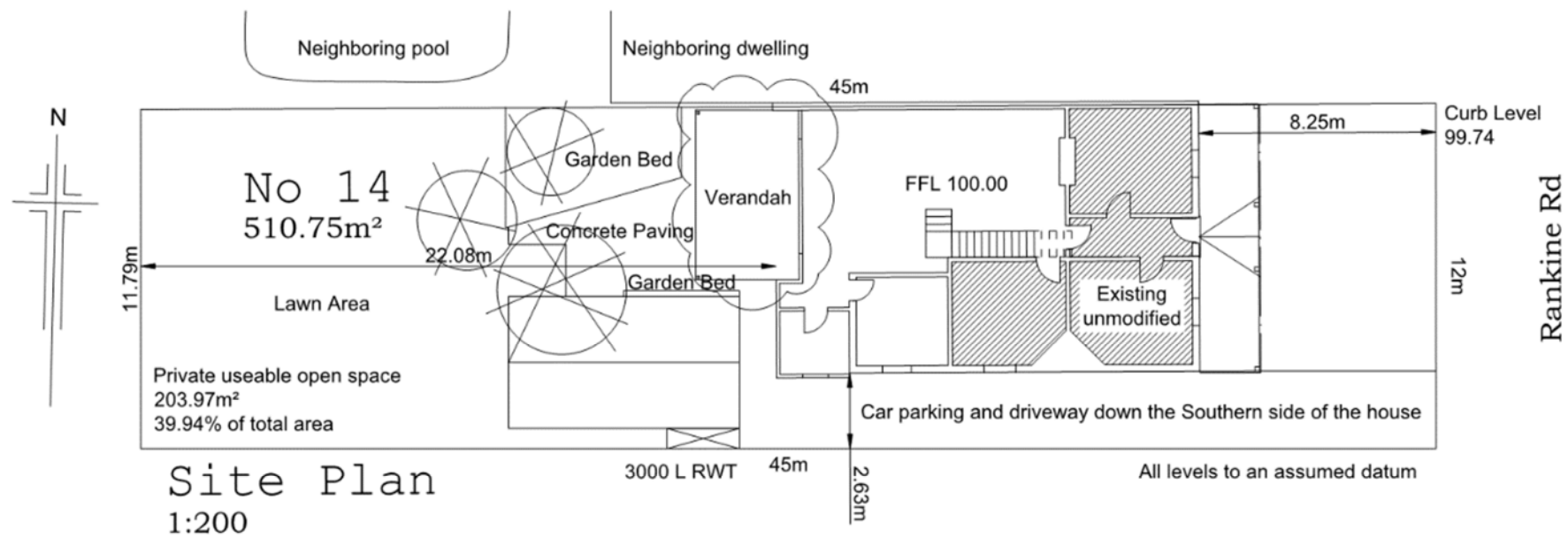


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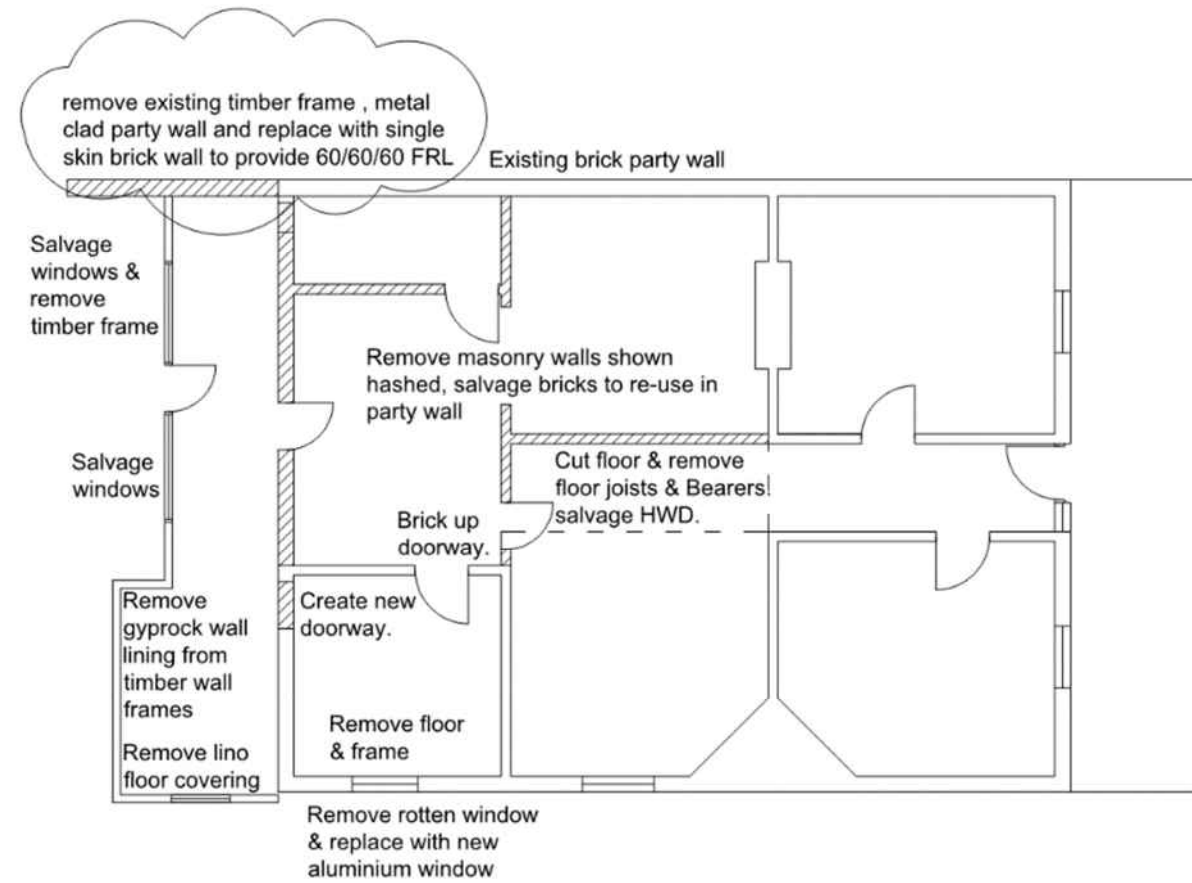
Sun Shade
 Neighbors window partially shaded for up to 3 hours between 09:00 and 12:00 every morning between 1st May and 25th Aug



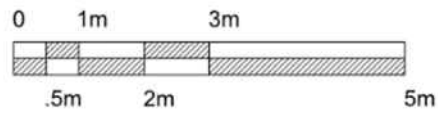
Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensville

Dated 15/08/2021
 Version 4
 Drawing No. 1
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Demolition Plan
1:100



Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensvile

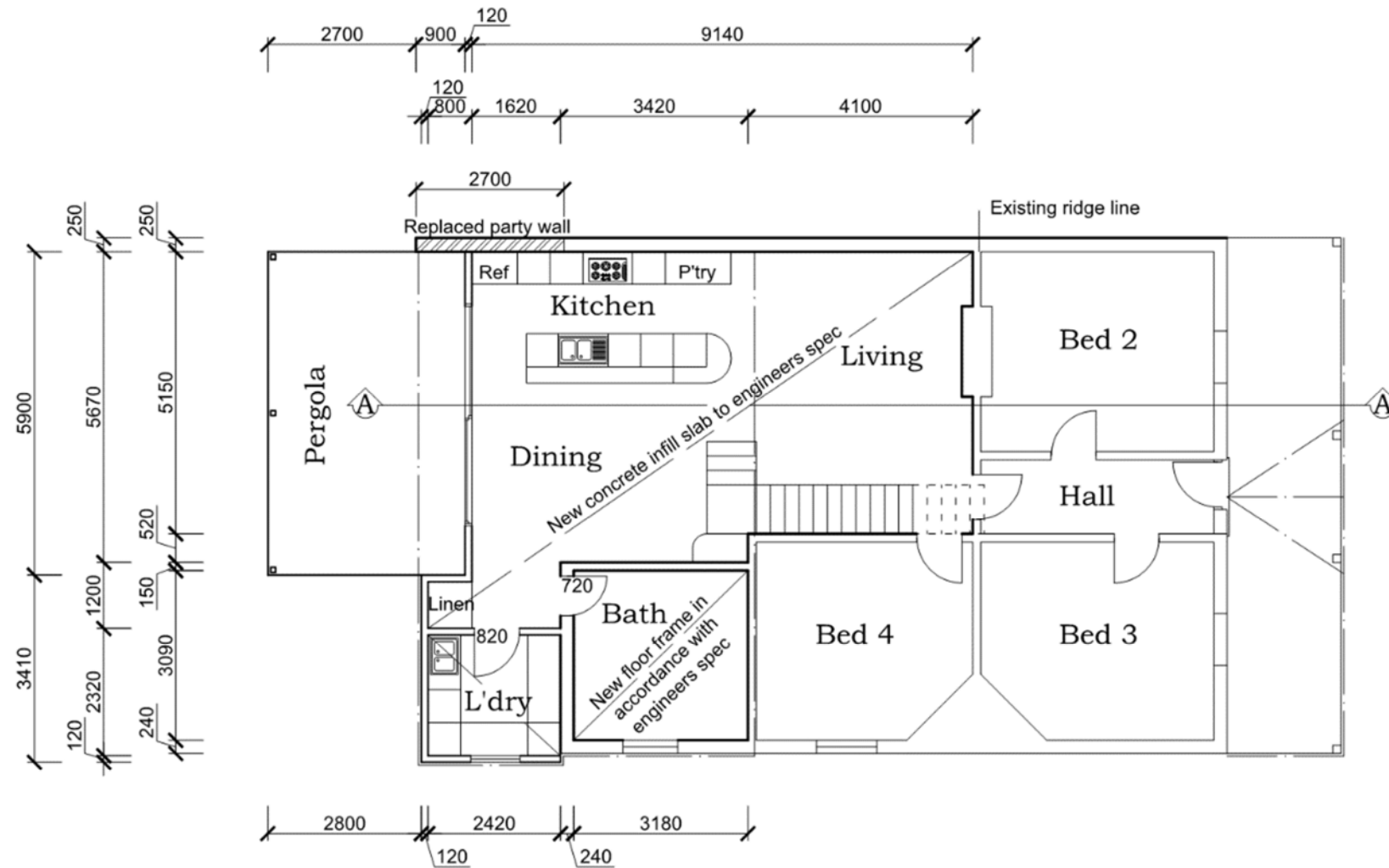
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 Drafting

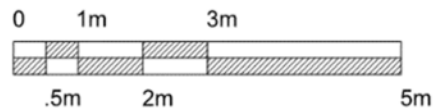
CONSTRUCTION NOTES

- Drawings must be read in conjunction with engineers report
- Written dimensions take priority
- All dimensions shall be checked on site by builder prior to commencement of any work
- All works to comply to the current National Construction Code (NCC)
- All works to be carried out in accordance with SA Work Health and Safety Act 2012
- Bushfire protection in accordance with AS3959-2018 and Ministers Spec. SA 78 (medium)
- Brush Fences must not be within 3m of building works
- Chimney and hearth design to comply with NCC Part 3.7.3 Heating Appliances
- Concrete in accordance with AS3600-2018 and AS2870-2011
- Construction dust must be controlled
- Draft Protection devices to be fitted to all doors and windows.
- Electrical in accordance with AS3000-2018
- Ensure all building waste and litter is collected and disposed of appropriately
- Glazing in accordance with AS1288-2006
- Masonry wall construction in accordance with AS3700-2018

- Plumbing in accordance with AS 3500-2018
- Roofing and Stormwater in accordance with AS3500-2018
- Roofing contractor is responsible for watertightness of the whole roof
- Self contained hard wired smoke alarms complying with AS3786-2014 connected to consumer mains with battery backup
- Smoke alarms to be interconnected complying with NCC 2019
- Silt runoff from site must be prevented, mud and soil shall not be transferred to road by vehicles
- Stairs in accordance with NCC Part 3.9.1
- Steelwork in accordance with AS4100-2020
- Stormwater retention in accordance with ministers spec. SA 78AA
- Stormwater to be discharged away from footings
- Subfloor ventilation in accordance with NCC Part 3.4.1
- Termite protection in accordance with AS3660-2000
- Timber framing in accordance with AS1864-2010
- Waterproofing in wet areas in accordance with NCC 3.8.1 and AS3740-2010
- Window and door assemblies in accordance with AS2047



Lower Floor
1:100



Proposed Home Renovation
For Greg McNair
At 14 Rankine Rd Torrensville

Dated 03/08/2021
Version 3
Drawing No. 3
Do not scale off this drawing

MWM
Drafting

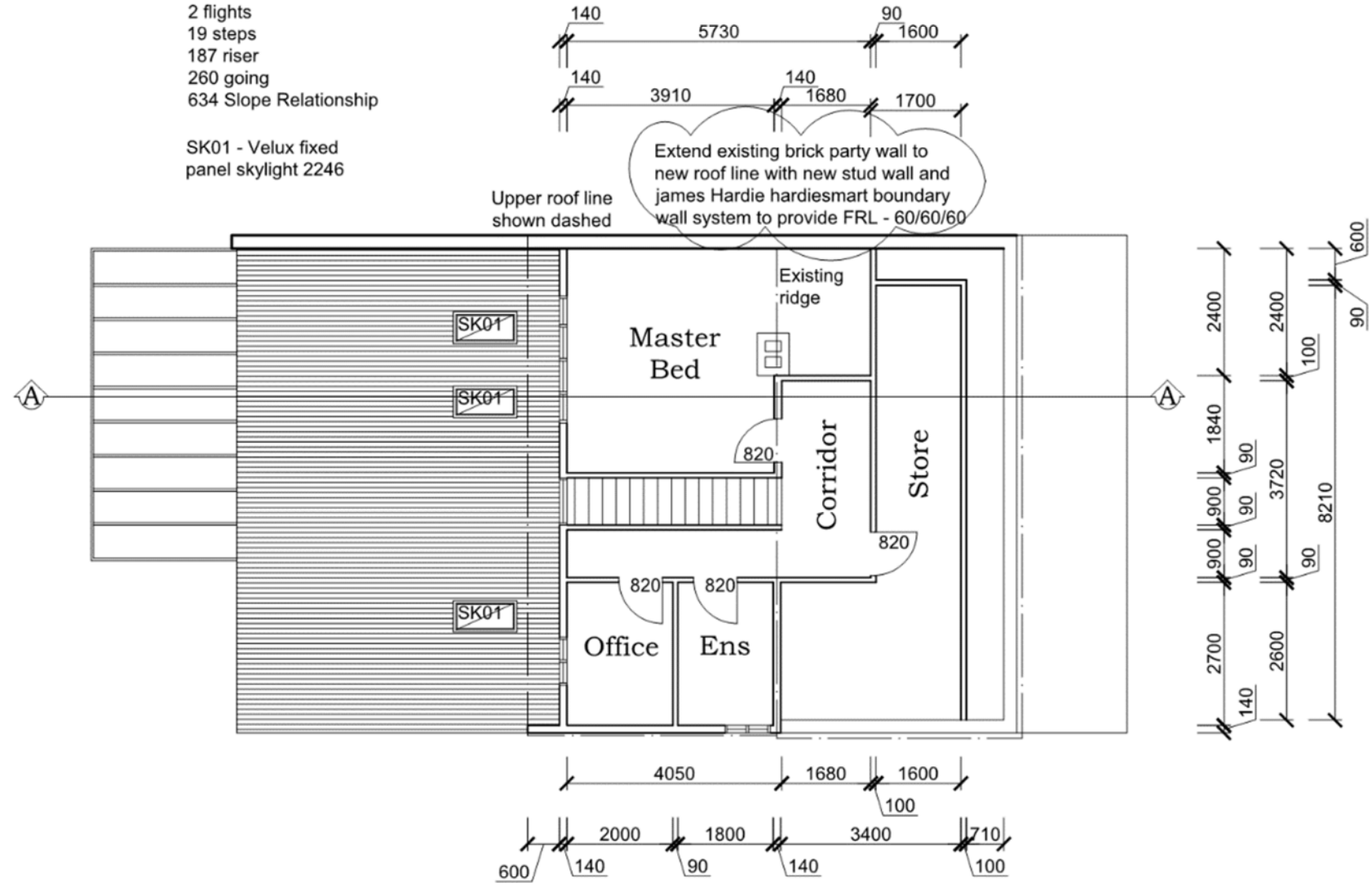
CONSTRUCTION NOTES

- Drawings must be read in conjunction with engineers report
- Written dimensions take priority
- All dimensions shall be checked on site by builder prior to commencement of any work
- All works to comply to the current National Construction Code (NCC)
- All works to be carried out in accordance with SA Work Health and Safety Act 2012
- Bushfire protection in accordance with AS3959-2018 and Ministers Spec. SA 78 (medium)
- Brush Fences must not be within 3m of building works
- Chimney and hearth design to comply with NCC Part 3.7.3 Heating Appliances
- Concrete in accordance with AS3600-2018 and AS2870-2011
- Construction dust must be controlled
- Draft Protection devices to be fitted to all doors and windows.
- Electrical in accordance with AS3000-2018
- Ensure all building waste and litter is collected and disposed of appropriately
- Glazing in accordance with AS1288-2006
- Masonry wall construction in accordance with AS3700-2018

- Plumbing in accordance with AS 3500-2018
- Roofing and Stormwater in accordance with AS3500-2018
- Roofing contractor is responsible for watertightness of the whole roof
- Self contained hard wired smoke alarms complying with AS3786-2014 connected to consumer mains with battery backup
- Smoke alarms to be interconnected complying with NCC 2019
- Silt runoff from site must be prevented, mud and soil shall not be transferred to road by vehicles
- Stairs in accordance with NCC Part 3.9.1
- Steelwork in accordance with AS4100-2020
- Stormwater retention in accordance with ministers spec. SA 78AA
- Stormwater to be discharged away from footings
- Subfloor ventilation in accordance with NCC Part 3.4.1
- Termite protection in accordance with AS3660-2000
- Timber framing in accordance with AS1864-2010
- Waterproofing in wet areas in accordance with NCC 3.8.1 and AS3740-2010
- Window and door assemblies in accordance with AS2047

2 flights
 19 steps
 187 riser
 260 going
 634 Slope Relationship

SK01 - Velux fixed panel skylight 2246

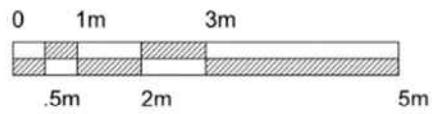
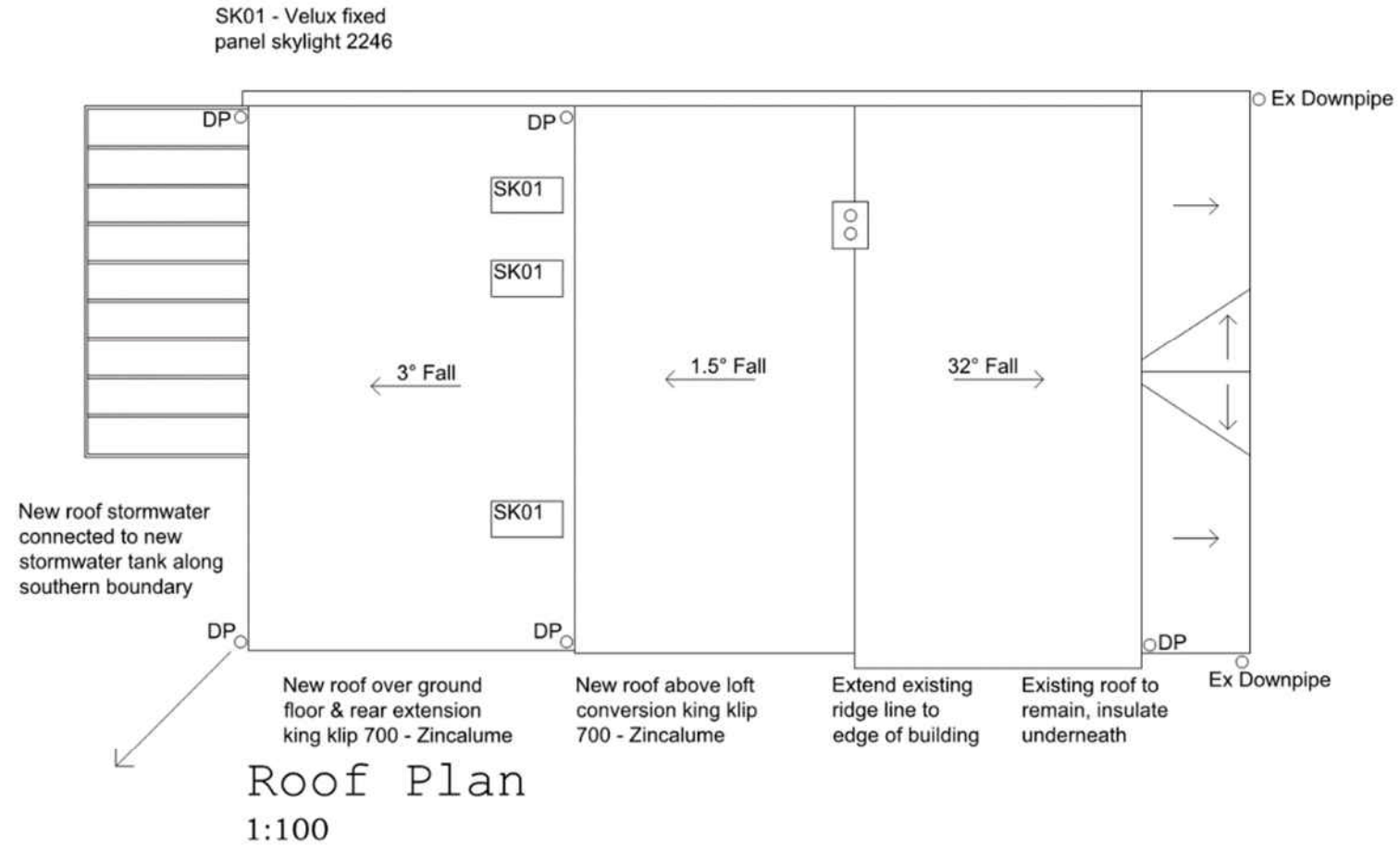


Upper Floor
 1:100

Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensville

Dated 04/08/2021
 Version 4
 Drawing No. 4
 Do not scale off this drawing

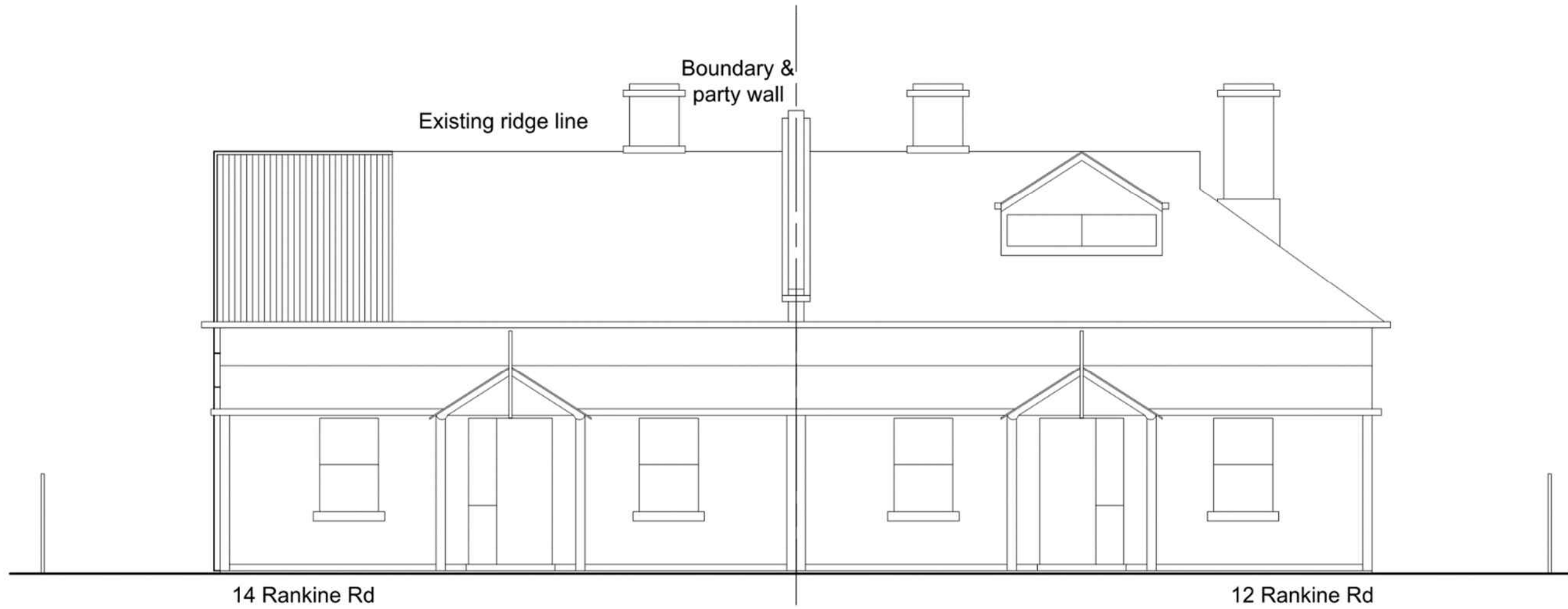
MWM
 Drafting



Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensville

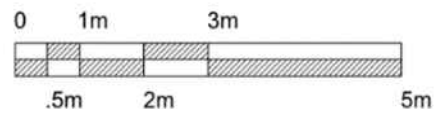
Dated 03/08/2021
 Version 4
 Drawing No. 5
 Do not scale off this drawing

MWM
 Drafting



Streetscape Elevation

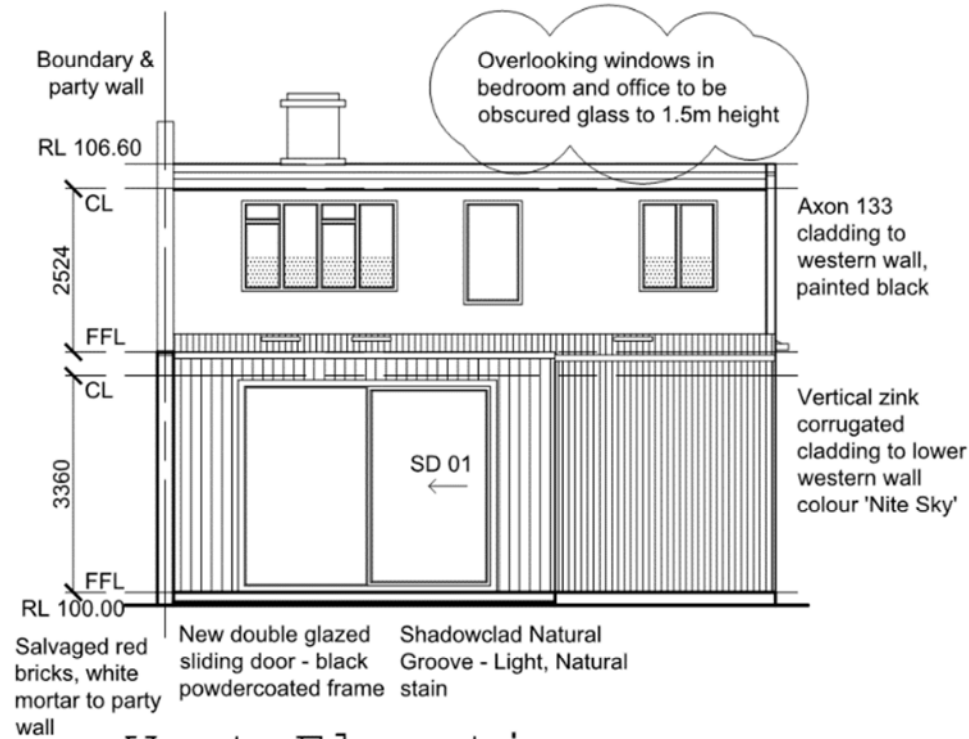
1:100



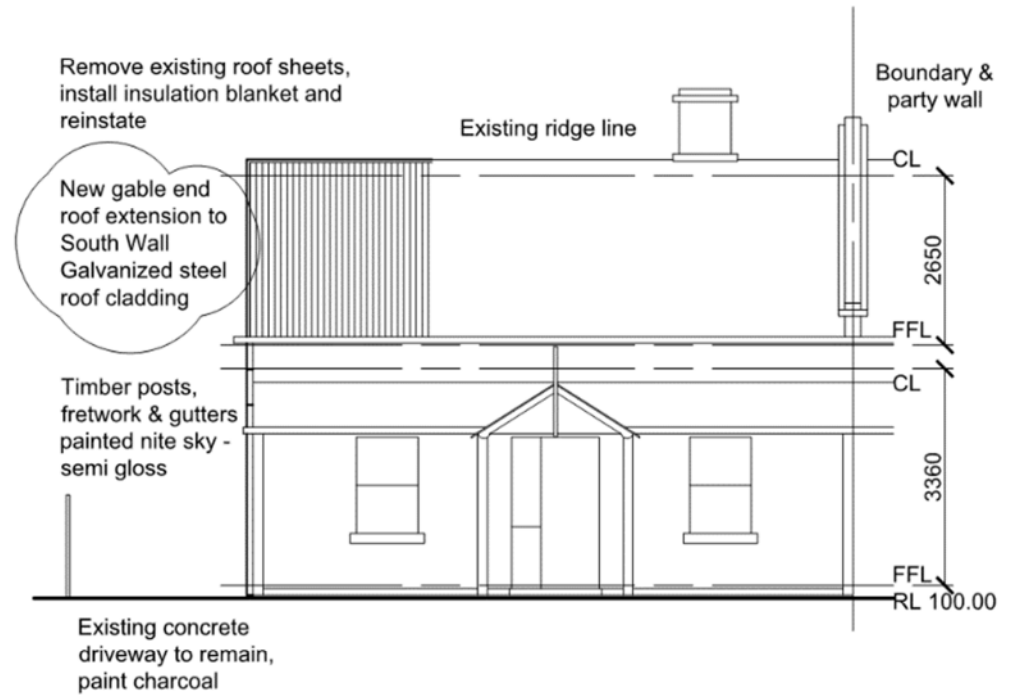
Proposed Home Renovation
For Greg McNair
At 14 Rankine Rd Torrensville

Dated 31/08/2021
Version 1
Drawing No. 8
Do not scale off this drawing

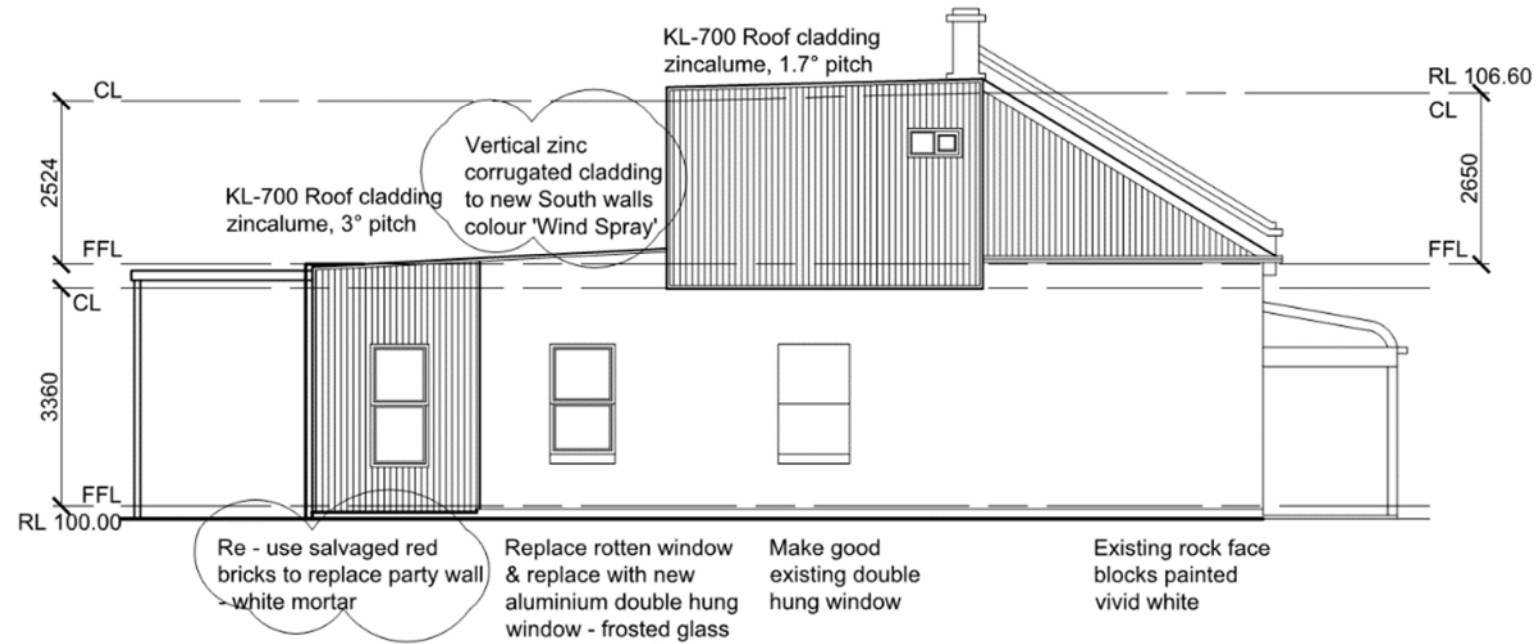
MWM
Drafting



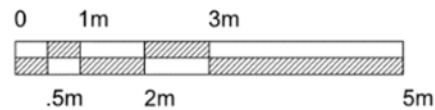
West Elevation
1:100



East Elevation
1:100



South Elevation
1:100



Proposed Home Renovation
 For Greg McNair
 At 14 Rankine Rd Torrensville

Dated 31/08/2021
 Version 5
 Drawing No. 7
 Do not scale off this drawing

MWM
 Drafting

Amelia De Ruvo 02/08/2021

Request for Information Response

Applicant: MWM Drafting Application ID: 21013814 Consent: Planning Consent Proposed
Development: Two storey dwelling additions and alterations and verandah to rear Subject Land:
Location reference: 14 RANKINE RD TORRENSVILLE SA 5031
Title ref.: CT 5865/896 Plan Parcel: F144285 AL57 Council: CITY OF WEST
TORRENS

In response to the Request for Information Letter dated 26/08/21, please see the attached plans with the following amendments.

- 1 Addition of Streetscape Elevation showing Dwellings at 12 and 14 Rankine Rd as requested
- 2 Southern Elevation with colours of colourbond custom orb wall cladding changed to Windspray as proposed in the Request for Information letter dated 26 August 21, in order to provide a more complimentary colour to the existing stonework external wall.
- 3 Eastern Elevation with zincalume roof cladding to new extension roof changed for galvanised steel roof cladding as requested.

Kind Regards,

Myles Meline

Drafter

MWM Drafting.

Details of Representations

Application Summary

Application ID	21013814
Proposal	Alterations and additions to existing dwelling, including an upper level, and construction of a verandah
Location	14 RANKINE RD TORRENSVILLE SA 5031

Representations

Representor 1 - Linh Woodley

Name	Linh Woodley
Address	7A Rawlings Ave TORRENSVILLE SA, 5031 Australia
Phone Number	
Email Address	
Submission Date	02/10/2021 05:53 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I support the development with some concerns
Reasons	The height of the building is too high 6.01m, on the 1st level there are 3 large windows facing its backyard, these windows are big, low and overlook into my back yard. I want these windows to be at least 1.8m above the floor and using obscure glass. Suggest the owner to grow more large trees example oak trees to provide shade and privacy for surrounding neighbors. .

Attached Documents

Representations

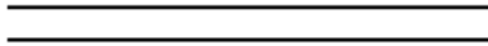
Representor 2 - Zoey Noske

Name	Zoey Noske
Address	26 Ormond Ave DAW PARK SA, 5041 Australia
Phone Number	
Submission Date	12/10/2021 08:10 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development
Reasons	It allows for the character of the home to be maintained whilst making the dwelling more conducive to contemporary living. I don't feel that these sorts of applications need the consultation process that has been followed.

Attached Documents



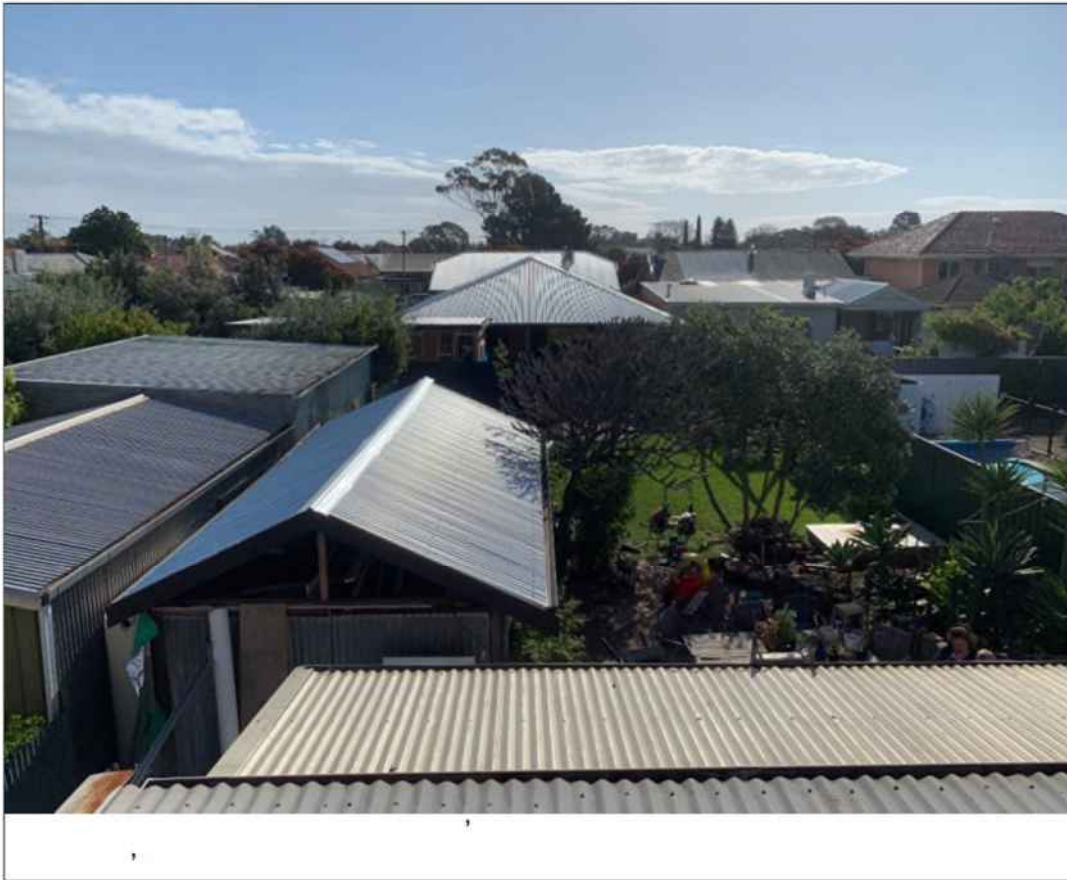
PETER MELINE AND ASSOCIATES,
PO BOX 1508, Mt. BARKER, SA, 5251
8391 3954 mobile 0448 395 299
Email: petermeline@bigpond.com



	<ul style="list-style-type: none">•••••
	<ul style="list-style-type: none">••

•

•

A handwritten signature in blue ink, appearing to read "Arlene". The signature is written in a cursive style with a large loop at the beginning.



6.2.2 437 Henley Beach Road, BROOKLYN PARK

Application No 21014960

Appearing before the Panel via electronic platform will be:

Representor: **Nadia Vernari** of 14 Elston Street, Brooklyn Park wishes to appear in support of the representation.

Applicant: **Domenico Maurici** wishes to appear in response to the representation.

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT NUMBER	21014960
APPLICANT	Mr Domenico Maurici
ADDRESS	437 Henley Beach Road, Brooklyn Park
NATURE OF DEVELOPMENT	Variation to Condition 3 in DA 211/262/2016 - extension to hours of operation to include Mondays 11.00am to 11.00pm and Sundays 11.00am to 11.00pm
ZONING INFORMATION	<p>Zones:</p> <ul style="list-style-type: none"> • Employment <p>Overlays:</p> <ul style="list-style-type: none"> • Airport Building Heights (Regulated) • Advertising Near Signalised Intersections • Building Near Airfields • Heritage Adjacency • Hazards (Flooding - Evidence Required) • Prescribed Wells Area • Regulated and Significant Tree • Traffic Generating Development • Urban Transport Routes
LODGEMENT DATE	9 Jul 2021
RELEVANT AUTHORITY	Council Assessment Panel
PLANNING & DESIGN CODE VERSION	1 July 2021 – Version 2021.8
CATEGORY OF DEVELOPMENT	Code Assessed - Performance Assessed
NOTIFICATION	Yes
REFERRALS STAUTORY	Nil
REFERRALS NON-STAUTORY	City Assets
DELEGATION	<p>CAP</p> <ul style="list-style-type: none"> • A representor has lodged a valid representation and wishes to be heard
RECOMMENDING OFFICER	Brendan Fewster
RECOMMENDATION	Refusal

BACKGROUND

A non-complying Development Application 211/262/2016 for alterations and extension of an existing restaurant and associated car parking was approved by the Development Assessment Panel on 12 July 2016 subject to the concurrence of the Development Assessment Commission.

Condition 3 of that approval states:

That the hours of operation of the premises shall not exceed the following periods:-

- *Closed Monday*
- *11:30am to 11:00pm Tuesday to Saturday inclusive; and*
- *11.30am to 5:00pm Sunday*

This new application is seeking to vary Condition 3 to allow trading on a Monday and additional trading hours on Sunday.

SUBJECT LAND AND LOCALITY

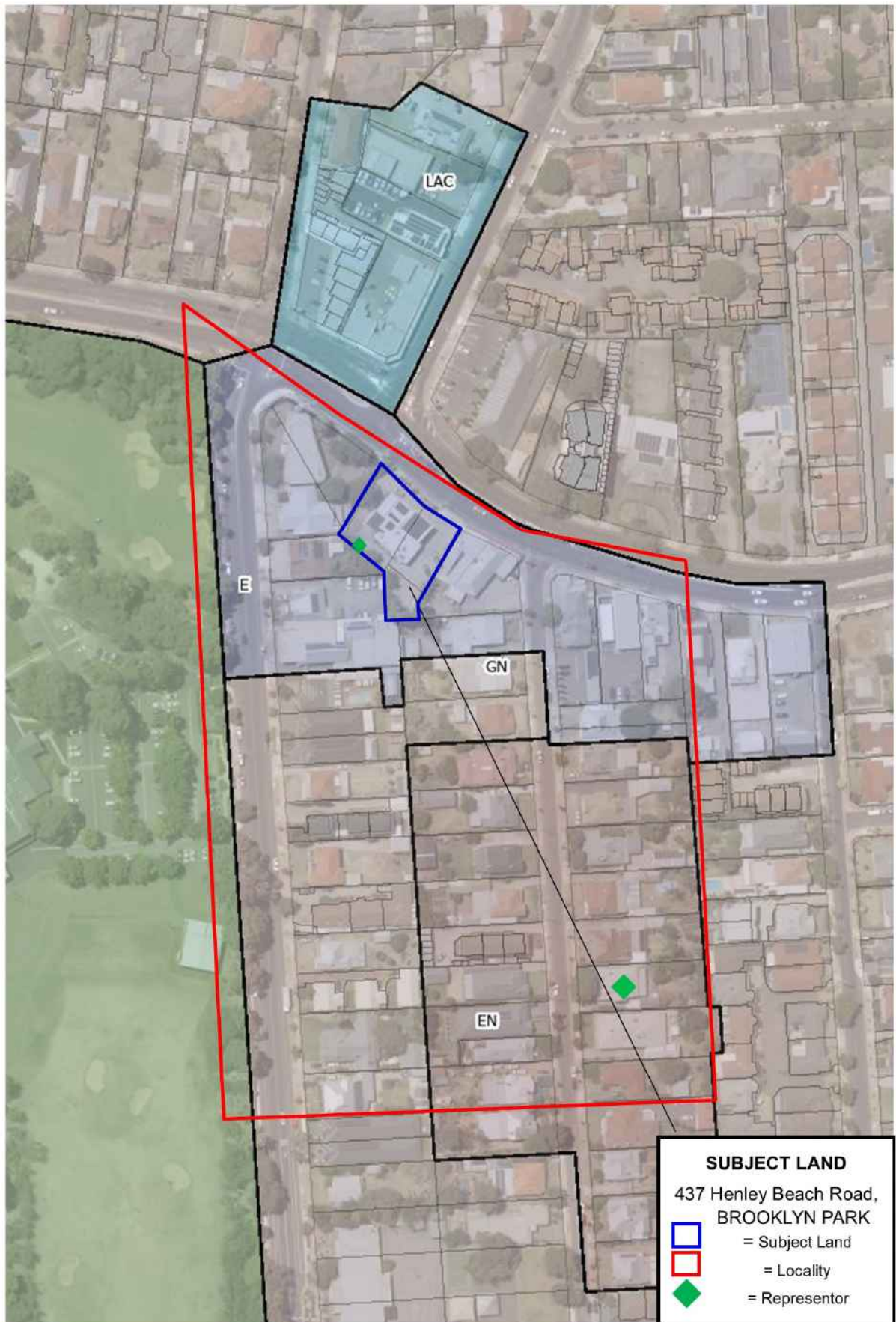
The subject land comprises two contiguous allotments that is commonly known as 437 Henley Beach Road, Brooklyn Park. The land is irregular in shape with a frontage of 40.60 metres (m), a minimum depth of 32.0m and a total site area of 1474 square metres (m²).

The site contains a single storey commercial building that was extended and renovated to accommodate the current restaurant (Chicco Palms). There is a concrete driveway and car parking around the perimeter of the building with parking for around 20 vehicles. The land is flat and there are no regulated trees on the site or on adjoining land.

The locality comprises a mix of land uses that includes small shops, restaurants, a place of worship, childcare, offices and consulting rooms. These uses are surrounded by established residential development on both sides of Henley Beach Road. Immediately east of the subject land is a small group of shops, to the west is the Anglican Church and further west of May Terrace is the Kooyonga Golf Course. On the northern side of Henley Beach Road is a group of commercial tenancies and a childcare centre.

The amenity of the locality in the vicinity of Henley Beach Road is relatively low due to the mixed built form character and the high volume and frequency of traffic. Some of the surrounding residential streets such as Elston Street are affected by overflow parking from the commercial uses, which has resulted in Council introducing on-street parking controls.

The subject land and locality is shown on the aerial imagery below.



RELEVANT APPLICATIONS

DA Number	Description of Development	Decision	Decision Date
211/262/2016	Alterations and extension of existing restaurant and associated car parking (non-complying)	Approved	July 2016

PROPOSAL

The proposal is seeking to vary the development that was approved under Development Application 211/262/2016. The approved development comprised alterations and additions to an existing restaurant.

The proposed variation relates to Condition 3 of the approval that states:

3. *That the hours of operation of the premises shall not exceed the following periods:-*
- *Closed Monday*
 - *11:30am to 11:00pm Tuesday to Saturday inclusive; and*
 - *11.30am to 5:00pm Sunday*

The revised hours of operation proposed are 11:00am to 11:00pm Monday to Sunday inclusive (seven days a week).

The relevant plans and documents are contained in **Attachment 2**.

PUBLIC NOTIFICATION

The application required public notification because it was performance assessed and not exempt from notification by *Table 5 - Procedural Matters* of the zone in the Planning and Design Code (The Code).

Properties notified	72 owners and occupiers were notified during the public notification process.
Representations	Three (3) representations were received.
Persons wishing to be heard	One (1) representors wishes to be heard. <ul style="list-style-type: none"> • Nadia Vernari of 14 Elston Street, Brooklyn Park
Summary of representations	Concerns were raised regarding the following matters: <ul style="list-style-type: none"> • Impact on parking availability; • Traffic congestion; • Parking restrictions implemented by Council; • Impact on amenity; and • Rubbish and property damage
Applicant's response to representations	Summary of applicant's response: <ul style="list-style-type: none"> • Chicco Palms is good for the local area; • Extended hours will provide more workings hours for staff; • Extended hours will provide diversity and vibrancy; • Willingness to work with Council to address traffic; and • Financial impacts from COVID

A copy of the representations and the applicant's response is contained in **Attachment 3**.

INTERNAL REFERRALS

Department	Comments
City Assets	<ul style="list-style-type: none"> • The subject site has limited off-street parking spaces. Since the restaurant has commenced trading at the subject site, City Assets have advised that the parking conditions in the nearby streets have been detrimentally impacted, in particular Elston Street, where overflow parking after hours (during evening periods) have exacerbated on-street parking for residents to the extent that significant parking controls have had to be implemented to manage the parking impact on residents. • Given the known impacts of overflow parking, particularly during evening periods, the proposed extension of trading hours would likely further exacerbate the parking conditions for nearby streets. The Applicant should give consideration to addressing the parking impact issue and consider other parking arrangements that may assist, for example securing off-street parking elsewhere that may assist in minimising the parking impact. • As currently proposed, City Assets has concerns with the proposed extension to the trading hours due to the parking impacts in adjacent residential streets. The extended trading hours would likely further exacerbate the parking impacts on the nearby streets over extended periods of the week. • It should be noted that currently the waste service occurs during the Monday where the restaurant is closed. If this proposal is to be supported, then alternative waste arrangement should be provided.

A copy of the relevant referral response is contained in **Attachment 4**.

RELEVANT PLANNING AND DESIGN CODE PROVISIONS

The subject land is located within the Employment Zone as identified in the Code. The subject land is also affected by a series Overlays, as detailed in the Development Application Details Table at the start of this report.

The relevant Planning and Design Code extracts are contained in **Attachment 1**. Please note this is an abridged version of the extract produced by the PlanSA Portal and sections that were not relevant to this assessment have been removed due to the length of the document.

ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

Existing Development

The subject land has longstanding use rights as a restaurant. The existing building underwent refurbishment in 2017 and has operated as Chicco Palms since this time. The building was formerly occupied by Barnacle Bill.

The proposal to expand the approved trading hours will not change the nature of the approved use of the land.

Traffic Impacts

The representors, who are residents of Elston Street, are concerned that the extension to the operating hours of the restaurant would result in additional traffic and congestion on their street. Elston Street is the nearest local street to the restaurant and extends between Henley Beach Road and Lipsett Terrace.

Over the past few years, City Assets has conducted traffic surveys of Elston Street and surrounding streets in response to concerned residents of Elston Street. These surveys have determined patrons of the Chico Palms restaurant to be the main cause of traffic and on-street parking congestion along Elston Street and have resulted in Council introducing on-street parking controls. The parking controls include a series of yellow lines and a one-hour parking limit between 5:00pm and 9:00pm Friday to Sunday for non-permit holders. It is noted also that on-street parking restrictions are in place near the Lockleys Primary School toward the southern end of Elston Street.

Council's traffic consultant is not supportive of the proposal as "the extended trading hours would likely further exacerbate the parking impacts on the nearby streets over extended periods of the week". While it would not be unreasonable for the restaurant to trade on a Monday as this is a typical business day when most residents are at work, a 5.00pm closure on a Sunday and Monday is considered necessary in this instance given the intensity of the restaurant use and the existing traffic volumes and parking congestion on Elston Street. Therefore, the proposed variation that would allow the restaurant to operate until 11.00pm on a Sunday and Monday would result in further impacts and interruption to nearby local roads to the detriment of traffic safety and the amenity of nearby residents. The existing impacts experienced by residents would be exacerbated and are considered to be unreasonable in the circumstances. On balance, the proposal is at variance to General Policies (Transport, Access and Parking) DO 1, PO 3.1 and 5.1.

In the applicant's response to the representations, the applicant indicated a willingness to provide a traffic impact statement. A traffic impact statement has not been provided and nor has one been requested of the applicant given that the current traffic conditions have been comprehensively reviewed by Council's independent traffic consultant in the context of the existing and proposed operating hours of the restaurant and other businesses in the immediate area.

Interface / Amenity

The applicant has highlighted that some of the adjacent businesses, such as the Viet Thai Restaurant and Joe's Pizza Bar operate until 10.00pm on a Sunday. While this may be the case, it is observed that these businesses are much smaller in size, with lower patronage and a greater focus on takeaway rather than on-site dining. Given the known impacts of traffic and overflow parking associated with the Chicco Palms restaurant during the evening, the extended trading hours would have an unreasonable impact on the amenity of nearby residents, contrary to General Policies (Interface between Land Uses) DO 1, PO 2.1 and DPF 2.1.

Waste Management

The current waste service occurs on a Monday when the restaurant is closed. Should the CAP be minded to approve the application, a condition of consent should be included that requires waste collection to take place prior to 11:00am on a weekday. This would ensure there is no conflict between the service vehicle and patron parking during peak periods.

SUMMARY

Having considered all the relevant provisions of the Planning and Design Code, the proposal is considered to be not seriously at variance with the Planning and Design Code Version 2021.8.

The proposed variation that would allow the restaurant to operate until 11.00pm on a Sunday and Monday would result in further impacts and interruption to nearby local roads to the detriment of traffic safety and the amenity of nearby residents. The proposal is therefore considered to be unreasonable in context of the locality.

On balance, the proposal does not sufficiently accord with the relevant provisions of the Planning and Design Code and therefore does not warrant the granting of Planning Consent.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107(2)(c) of the *Planning, Development and Infrastructure Act 2016*, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code Version 2021.8; and
2. Application No. 21014960 by Mr Domenico Maurici for variation to Condition 3 in DA 211/262/2016 - extension to hours of operation to include Mondays 11.00am to 11.00pm and Sundays 11.00am to 11.00pm is REFUSED Planning Consent as the proposed development is contrary to the following provisions of the Planning and Design Code:
 - General Policies (Transport, Access and Parking) DO 1, PO 3.1 and 5.1.
Reason: The proposal would result in further interruption to nearby local roads to the detriment of traffic safety and the amenity of residents.
 - General Policies (Interface between Land Uses) DO 1, PO 2.1 and DPF 2.1.
Reason: The proposed operating hours would have an unreasonable impact on the amenity of nearby residents due to the generation of traffic and on-street parking during evening periods.

Attachments

1. **Relevant Code Provisions**
2. **Application Documents**
3. **Representations and Applicant's Response**
4. **Internal Referral Response**

Policy24 - Enquiry

437 HENLEY BEACH RD BROOKLYN PARK SA 5032

Address:

Click to view a detailed interactive [SAILIS](#) in SAILIS

To view a detailed interactive property map in SAPPA click on the map below



Property Zoning Details

Overlay

- Airport Building Heights (Regulated) (All structures over 15 metres)
- Advertising Near Signalised Intersections
- Building Near Airfields
- Heritage Adjacency
- Hazards (Flooding - Evidence Required)
- Prescribed Wells Area
- Regulated and Significant Tree
- Traffic Generating Development
- Urban Transport Routes

Zone

Employment

Development Pathways

- Employment

1. Accepted Development

Means that the development type does not require planning consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.

- Brush fence
- Building work on railway land
- Internal building work
- Partial demolition of a building or structure
- Shade sail
- Solar photovoltaic panels (roof mounted)
- Water tank (above ground)
- Water tank (underground)

2. Code Assessed - Deemed to Satisfy

Means that the development type requires consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.

Policy24 - Enquiry

- Replacement building
- Temporary accommodation in an area affected by bushfire

3. Code Assessed - Performance Assessed

Performance Assessed development types listed below are those for which the Code identifies relevant policies. Additional development types that are not listed as Accepted, Deemed to Satisfy or Restricted default to a Performance assessed Pathway. Please contact your local council for more information.

- Advertisement
- Consulting room
- Demolition
- Land division
- Light industry
- Office
- Retaining wall
- Service trade premises
- Shop
- Store
- Telecommunications facility
- Warehouse

4. Impact Assessed - Restricted

Means that the development type requires approval. Classes of development that are classified as Restricted are listed in Table 4 of the relevant Zones.

Property Policy Information for above selection

Part 2 - Zones and Sub Zones

Employment Zone

Assessment Provisions (AP)

Desired Outcome	
DO 1	A diverse range of low-impact light industrial, commercial and business activities that complement the role of other zones accommodating significant industrial, shopping and business activities.
DO 2	Distinctive building, landscape and streetscape design to achieve high visual and environmental amenity particularly along arterial roads, zone boundaries and public open spaces.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 A range of employment-generating light industrial, service trade,	DTS/DPF 1.1 Development comprises one or more of the following:

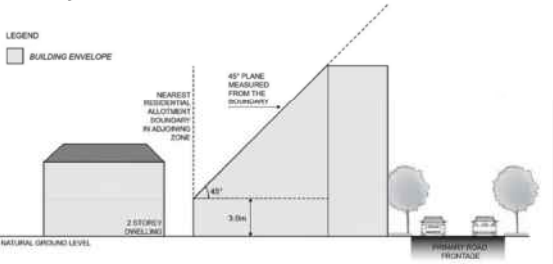
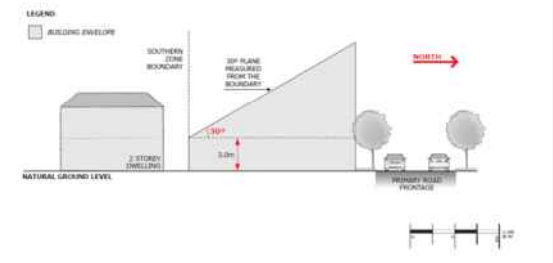
Policy24 - Enquiry

<p>motor repair and other compatible businesses servicing the local community that do not produce emissions that would detrimentally affect local amenity.</p>	<ul style="list-style-type: none"> (a) Advertisement (b) Consulting room (c) Indoor recreation facility (d) Light industry (e) Motor repair station (f) Office (g) Place of worship (h) Research facility (i) Retail fuel outlet (j) Service trade premises (k) Shop (l) Store (m) Telecommunications facility (n) Training facility (o) Warehouse.
<p>PO 1.2 Shops provide convenient day-to-day services and amenities to local businesses and workers, support the sale of products manufactured on-site and otherwise complement the role of Activity Centres.</p>	<p>DTS/DPF 1.2 Shop where one of the following applies:</p> <ul style="list-style-type: none"> (a) with a gross leasable floor area up to 100m² (b) is a bulky goods outlet (c) is a restaurant (d) is ancillary to and located on the same allotment as an industry and primarily involves the sale by retail of goods manufactured by the industry.
<p>PO 1.3 Telecommunication facilities located to mitigate impacts on visual amenity in residential areas.</p>	<p>DTS/DPF 1.3 Telecommunications facility in the form of a monopole:</p> <ul style="list-style-type: none"> (a) up to a height of 30m (b) no closer than 50m to a neighbourhood-type zone.
<p>PO 1.4 Bulky good outlets and standalone shops are located to provide convenient access.</p>	<p>DTS/DPF 1.4 Bulky goods outlets and standalone shops are located on sites with a frontage to a State Maintained Road.</p>
<p>Built Form and Character</p>	
<p>PO 2.1 Development achieves distinctive building, landscape and streetscape design to achieve high visual and environmental amenity particularly along arterial roads, zone boundaries and public open spaces.</p>	<p>DTS/DPF 2.1 None are applicable.</p>
<p>PO 2.2 Building facades facing a boundary of a zone primarily intended to accommodate residential development, public roads, or public open space incorporate design elements to add visual interest by considering the following:</p> <ul style="list-style-type: none"> (a) using a variety of building finishes (b) avoiding elevations that consist solely of metal cladding (c) using materials with a low reflectivity (d) using techniques to add visual interest and reduce large expanses of blank walls including modulation and 	<p>DTS/DPF 2.2 None are applicable.</p>

Policy24 - Enquiry

<p>incorporation of offices and showrooms along elevations visible to a public road.</p>	
<p>Building height and setbacks</p>	
<p>PO 3.1 Buildings are set back from the primary street boundary to contribute to the existing/emerging pattern of street setbacks in the streetscape.</p>	<p>DTS/DPF 3.1 The building line of a building set back from the primary street boundary:</p> <ul style="list-style-type: none"> (a) at least the average setback to the building line of existing buildings on adjoining sites which face the same primary street (including those buildings that would adjoin the site if not separated by a public road or a vacant allotment) (b) where there is only one existing building on adjoining sites which face the same primary street (including those that would adjoin if not separated by a public road or a vacant allotment), not less than the setback to the building line of that building or (c) not less than 3m where no building exists on an adjoining site with the same primary street frontage.
<p>PO 3.2 Buildings are set back from a secondary street boundary to accommodate the provision of landscaping between buildings and the street to enhance the appearance of land and buildings when viewed from the street.</p>	<p>DTS/DPF 3.2 Building walls are no closer than 2m to the secondary street boundary.</p>
<p>PO 3.3 Buildings are set back from rear access ways to provide adequate manoeuvrability for vehicles to enter and exit the site.</p>	<p>DTS/DPF 3.3 Building walls are set back from the rear access way:</p> <ul style="list-style-type: none"> (a) where the access way is 6.5m wide or more, no requirement (b) where the access way is less than 6.5m wide, the distance equal to the additional width required to make the access way at least 6.5m wide.
<p>PO 3.4 Buildings are sited to accommodate vehicle access to the rear of a site for deliveries, maintenance and emergency purposes.</p>	<p>DTS/DPF 3.4 Building walls are set back at least 3m from at least one side boundary, unless an alternative means for vehicular access to the rear of the site is available.</p>
<p>PO 3.5 Building height is consistent with the form expressed in any relevant <i>Maximum Building Height (Levels) Technical and Numeric Variation</i> layer, and is otherwise generally low-rise to complement the established streetscape and local character.</p>	<p>DTS/DPF 3.5 Building height is not greater than:</p> <ul style="list-style-type: none"> (a) the following: (b) in all other cases (i.e. there are blank fields for both maximum building height (metres) and maximum building height (levels)) - 2 building levels up to a height of 9m. <p>In relation to DTS/DPF 3.5, in instances where:</p> <ul style="list-style-type: none"> (c) more than one value is returned in the same field for DTS/DPF 3.5(a) refer to the <i>Maximum Building Height (Levels) Technical and Numeric Variation layer</i> or <i>Maximum Building Height (Metres) Technical and</i>

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	<p><i>Numeric Variation layer</i> in the SA planning database to determine the applicable value relevant to the site of the proposed development</p> <p>(d) only one value is returned for DTS/DPF 3.1(a) (i.e. there is one blank field), then the relevant height in metres or building levels applies with no criteria for the other.</p>
<p>PO 3.6</p> <p>Buildings mitigate visual impacts of building massing on residential development within a neighbourhood-type zone.</p>	<p>DTS/DPF 3.6</p> <p>Buildings are constructed within a building envelope provided by a 45 degree plane, measured from a height of 3m above natural ground level at the boundary of an allotment used for residential purposes in a neighbourhood-type zone as shown in the following diagram, except where the relevant boundary is a southern boundary or where this boundary is the primary street boundary.</p>  <p>LEGEND BUILDING ENVELOPE</p> <p>NATURAL GROUND LEVEL</p> <p>2 STOREY DWELLING</p> <p>NEAREST RESIDENTIAL ALLOTMENT BOUNDARY ADJOINING ZONE</p> <p>45° PLANE MEASURED FROM THE BOUNDARY</p> <p>3.0m</p> <p>PRIMARY ROAD FRONTAGE</p>
<p>PO 3.7</p> <p>Buildings mitigate overshadowing of residential development within a neighbourhood-type zone.</p>	<p>DTS/DPF 3.7</p> <p>Buildings on sites with a southern boundary adjoining an allotment used for residential purposes within a neighbourhood-type zone are constructed within a building envelope provided by a 30 degree plane grading north measured from a height of 3m above natural ground level at the southern boundary, as shown in the following diagram:</p>  <p>LEGEND BUILDING ENVELOPE</p> <p>NATURAL GROUND LEVEL</p> <p>2 STOREY DWELLING</p> <p>SOUTHERN BOUNDARY</p> <p>30° PLANE MEASURED FROM THE BOUNDARY</p> <p>3.0m</p> <p>PRIMARY ROAD FRONTAGE</p> <p>NORTH</p>
<p>PO 3.8</p> <p>Buildings on an allotment fronting a road that is not a State maintained road, and where land on the opposite side of the road is within a neighbourhood-type zone, provides an orderly transition to the built form scale envisaged in the adjacent zone to complement the streetscape character.</p>	<p>DTS/DPF 3.8</p> <p>None are applicable.</p>
<p>Site Dimensions and Land Division</p>	
<p>PO 4.1</p>	<p>DTS/DPF 4.1</p>

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<p>Land division creates allotments that vary in size and are suitable for a variety of commercial and business activities.</p>	<p>Allotments:</p> <ul style="list-style-type: none"> (a) connected to an approved common wastewater disposal service have an area of 1250m² or more and a frontage width of 20m or more (b) that will require the disposal of wastewater on-site have an area of 2000m² or more and a frontage width of 20m or more.
<p>Landscaping</p>	
<p>PO 5.1 Landscaping is provided to enhance the visual appearance of development when viewed from public roads and thoroughfares.</p>	<p>DTS/DPF 5.1 Other than to accommodate a lawfully existing or authorised driveway or access point, or an access point for which consent has been granted as part of an application for the division of land, a landscaped area is provided within the development site:</p> <ul style="list-style-type: none"> (a) where a building is set back less than 3m from the street boundary - 1m wide or the area remaining between the relevant building and the street boundary where the building is less than 1m from the street boundary or (b) in any other case - at least 1.5m wide.
<p>PO 5.2 Development incorporates areas for landscaping to enhance the overall amenity of the site and locality.</p>	<p>DTS/DPF 5.2 Landscape areas comprise:</p> <ul style="list-style-type: none"> (a) not less than 10 percent of the site (b) a dimension of at least 1.5m.
<p>Advertisements</p>	
<p>PO 6.1 Freestanding advertisements are not visually dominant within the locality.</p>	<p>DTS/DPF 6.1 Freestanding advertisements:</p> <ul style="list-style-type: none"> (a) do not exceed 6m in height above natural ground level (b) do not have a face that exceeds 8m².
<p>Concept Plans</p>	
<p>PO 7.1 Development is compatible with the outcomes sought by any relevant Concept Plan contained within Part 12 - Concept Plans of the Planning and Design Code to support the orderly development of land through staging of development and provision of infrastructure.</p>	<p>DTS/DPF 7.1 The site of the development is wholly located outside any relevant Concept Plan boundary. The following Concept Plans are relevant: In relation to DTS/DPF 7.1, in instances where:</p> <ul style="list-style-type: none"> (a) one or more Concept Plan is returned, refer to Part 12 - Concept Plans in the Planning and Design Code to determine if a Concept Plan is relevant to the site of the proposed development. Note: multiple concept plans may be relevant. (b) in instances where 'no value' is returned, there is no relevant concept plan and DTS/DPF 7.1 is met.

Table 5 - Procedural Matters (PM) - Notification

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The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

A class of development listed in Column A is excluded from notification provided that it does not fall within a corresponding exclusion prescribed in Column B. In instances where development falls within multiple classes within Column A, each clause is to be read independently such that if a development is excluded from notification by any clause, it is, for the purposes of notification excluded irrespective of any other clause.

Class of Development (Column A)	Exceptions (Column B)
1. A kind of development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. Any development involving any of the following (or of any combination of any of the following): (a) advertisement (b) air handling unit, air conditioning system or exhaust fan (c) building on railway land (d) carport (e) fence (f) outbuilding (g) retaining wall (h) shade sail (i) solar photovoltaic panels (roof mounted) (j) temporary public service depot (k) verandah (l) water tank.	Except development that exceeds the maximum building height specified in Employment Zone DTS/DPF 3.5 or does not satisfy any of the following: 1. Employment Zone DTS/DPF 3.6 2. Employment Zone DTS/DPF 3.7.
3. Any development involving any of the following (or of any combination of any of the following): (a) consulting room (b) light industry (c) office (d) motor repair station (e) retail fuel outlet (f) store (g) warehouse.	Except where the site of the development is adjacent land to a site (or land) used for residential purposes in a neighbourhood-type zone.
4. Any development involving any of the following (or of any combination of any of the following): (a) internal building works (b) land division (c) replacement building (d) temporary accommodation in an area affected by bushfire (e) tree damaging activity.	None specified.

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<p>5. Demolition.</p>	<p>Except any of the following:</p> <ol style="list-style-type: none"> 1. the demolition of a State or Local Heritage Place 2. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
<p>6. Shop within any of the following:</p> <ol style="list-style-type: none"> (a) Retail Activity Centre Subzone (b) Roadside Service Centre Subzone. 	<p>Except shop that exceeds the maximum building height specified in Employment Zone DTS/DPF 3.5 or does not satisfy any of the following:</p> <ol style="list-style-type: none"> 1. Employment Zone DTS/DPF 3.6 2. Employment Zone DTS/DPF 3.7.
<p>7. Shop.</p>	<p>Except:</p> <ol style="list-style-type: none"> 1. where the site of the shop is adjacent land to a site (or land) used for residential purposes in a neighbourhood-type zone or 2. shop that exceeds the maximum building height specified in Employment Zone DTS/DPF 3.5 or 3. shop that does not satisfy Employment Zone DTS/DPF 1.2.
<p>8. Telecommunications facility.</p>	<p>Except telecommunications facility that does not satisfy Employment Zone DTS/DPF 1.3.</p>

Placement of Notices - Exemptions for Performance Assessed Development

None specified.

Placement of Notices - Exemptions for Restricted Development

None specified.

Part 3 - Overlays

Advertising Near Signalised Intersections Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Provision of a safe road environment by reducing driver distraction at key points of conflict on the road.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Policy24 - Enquiry

None	None	None	None
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Traffic Generating Development Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes and Major Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from urban transport routes and major urban transport routes.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Traffic Generating Development	
<p>PO 1.1</p> <p>Development designed to minimise its potential impact on the safety, efficiency and functional performance of the State Maintained Road network.</p>	<p>DTS/DPF 1.1</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m2 or more (c) retail development with a gross floor area of 2,000m2 or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m2 or more (e) industry with a gross floor area of 20,000m2 or more (f) educational facilities with a capacity of 250 students or more.
<p>PO 1.2</p> <p>Access points sited and designed to accommodate the type and volume of traffic likely to be generated by development.</p>	<p>DTS/DPF 1.2</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m2 or more (c) retail development with a gross floor area of 2,000m2 or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m2 or more (e) industry with a gross floor area of 20,000m2 or more

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	(f) educational facilities with a capacity of 250 students or more.
PO 1.3 Sufficient accessible on-site queuing provided to meet the needs of the development so that queues do not impact on the State Maintained Road network.	DTS/DPF 1.3 Access is obtained directly from a State Maintained Road where it involves any of the following types of development: (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m ² or more (c) retail development with a gross floor area of 2,000m ² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m ² or more (e) industry with a gross floor area of 20,000m ² or more (f) educational facilities with a capacity of 250 students or more.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, any of the following classes of development that are proposed within 250m of a State Maintained Road:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more. 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Urban Transport Routes Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.

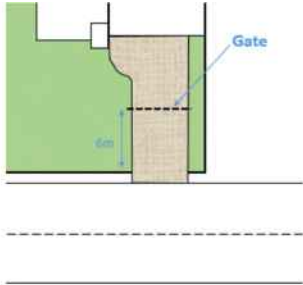
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DO 2	Provision of safe and efficient access to and from Urban Transport Routes.
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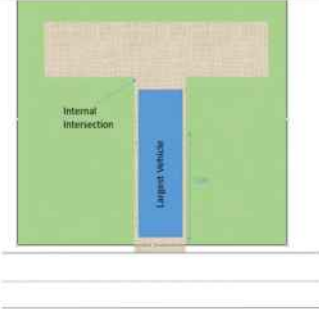
Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Access - Safe Entry and Exit (Traffic Flow)	
<p>PO 1.1</p> <p>Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State maintained roads.</p>	<p>DTS/DPF 1.1</p> <p>An access point satisfies (a), (b) or (c):</p> <p>(a) where servicing a single (1) dwelling / residential allotment:</p> <ul style="list-style-type: none"> (i) it will not result in more than one access point (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 3m and 4m (measured at the site boundary) <p>(b) where the development will result in 2 and up to 6 dwellings:</p> <ul style="list-style-type: none"> (i) (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 5.8m to 6m (measured at the site boundary) and an access depth of 6m (measured from the site boundary into the site) <p>(c) where the development will result in 7 or more dwellings, or is a non-residential land use:</p> <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site using left turn only movements (iii) vehicles can enter and exit the site in a forward direction (iv) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (v) it will have a width of between 6m and 7m (measured at the site boundary), where the development is expected to accommodate vehicles with a length of 6.4m or less (vi) it will have a width of between 6m and 9m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 6.4m to 8.8m (vii) it will have a width of between 9m and 12m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 8.8m to 12.5m (viii) provides for simultaneous two-way vehicle movements at the access: <ul style="list-style-type: none"> A. with entry and exit movements for vehicles with a length up to 5.2m vehicles being fully within the kerbside lane of the

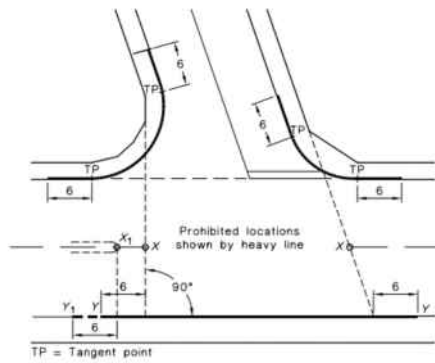
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	<p>road</p> <p>and</p> <p>B. with entry movements of 8.8m vehicles (where relevant) being fully within the kerbside lane of the road and the exit movements of 8.8m vehicles do not cross the centreline of the road.</p>
<p>Access - On-Site Queuing</p>	
<p>PO 2.1</p> <p>Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption on the functional performance of the road and maintain safe vehicle movements.</p>	<p>DTS/DPF 2.1</p> <p>An access point in accordance with one of the following:</p> <p>(a) will not service, or is not intended to service, more than 6 dwellings and there are no internal driveways, intersections, car parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) as shown in the following diagram:</p>  <p>(b) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:</p> <ul style="list-style-type: none"> (i) is expected to be serviced by vehicles with a length no greater than 6.4m (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) <p>(c) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:</p> <ul style="list-style-type: none"> (i) is expected to be serviced by vehicles with a length greater than a 6.4m small rigid vehicle (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) (iii) any termination of or change in priority of movement within the main car park aisle is located far enough into the site so that the largest vehicle expected on-site can store fully within the site before being required to stop (iv) all parking or manoeuvring areas for commercial vehicles are located a minimum of 12m or the length of the longest vehicle expected on site from the access (measured from the site boundary into the site) as shown in the following diagram:

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	 <p>The diagram illustrates a plan view of a road layout. A central blue vertical lane is labeled 'Largest Vehicle'. This lane intersects a horizontal road. The intersection is labeled 'Internal Intersection'. The road continues horizontally on both sides of the intersection. The area above the intersection is shaded in light green, and the area below is also shaded in light green. The road surface is shown in light brown/tan.</p>
<p>Access - (Location Spacing) - Existing Access Point</p>	
<p>PO 3.1</p> <p>Existing access points are designed to accommodate the type and volume of traffic likely to be generated by the development.</p>	<p>DTS/DPF 3.1</p> <p>An existing access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) it will not service, or is not intended to service, more than 6 dwellings (b) it is not located on a Controlled Access Road and will not service development that will result in (b) a larger class of vehicle expected to access the site using the existing access (c) is not located on a Controlled Access Road and development constitutes: <ul style="list-style-type: none"> (i) a change of use between an office <500m² gross leasable floor area and a consulting room <500m² gross leasable floor area or vice versa (ii) a change in use from a shop to an office, consulting room or personal or domestic services establishment (iii) a change of use from a consulting room or office <250m² gross leasable floor area to shop <250m² gross leasable floor area (iv) a change of use from a shop <500m² gross leasable floor area to a warehouse <500m² gross leasable floor area (v) an office or consulting room with a <500m² gross leasable floor area.
<p>Access – Location (Spacing) – New Access Points</p>	
<p>PO 4.1</p> <p>New access points are spaced apart from any existing access point or public road junction to manage impediments to traffic flow and maintain safe and efficient operating conditions on the road.</p>	<p>DTS/DPF 4.1</p> <p>A new access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where a development site is intended to serve between 1 and 6 dwellings and has frontage to a local road (not being a Controlled Access Road) with a speed environment of 60km/h or less, the new access point is provided on the local road and located a minimum of 6.0m from the tangent point as shown in the following diagram:

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NOTE:
The points marked X₁ and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension Y-Y extends to Point X₁.

- (b) where the development site is intended to serve between 1 and 6 dwellings and access from a local road (being a road that is not a State Maintained Road) is not available, the new access:
 - (i) is not located on a Controlled Access Road
 - (ii) is not located on a section of road affected by double barrier lines
 - (iii) will be on a road with a speed environment of 70km/h or less
 - (iv) is located outside of the bold lines on the diagram shown in the diagram following part (a)
 - (v) located minimum of 6m from a median opening or pedestrian crossing

- (c) where DTS/DPF 4.1 part (a) and (b) do not apply and access from an alternative local road at least 25m from the State Maintained Road is not available, and the access is not located on a Controlled Access Road, the new access is separated in accordance with the following:

Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes
50 km/h or less	No spacing requirement	20m
60 km/h	30m	73m
70 km/h	40m	92m
80 km/h	50m	114m
90 km/h	65m	139m
100 km/h	80m	165m
110 km/h	100m	193m

Access - Location (Sight Lines)

PO 5.1

Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.

DTS/DPF 5.1

An access point satisfies (a) or (b):

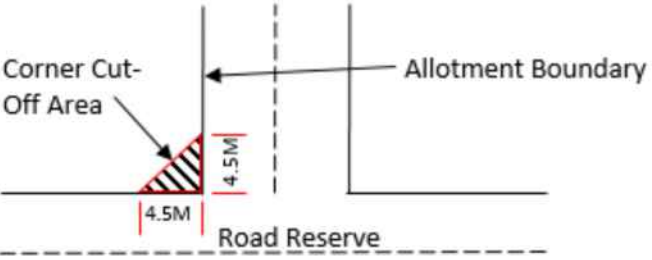
- (a) drivers approaching or exiting an access point have an unobstructed line of sight in accordance with the following (measured at a height of 1.1m above the surface of the road):

Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes
40 km/h or less	40m	73m

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	<table border="1"> <tr> <td>50 km/h</td> <td>55m</td> <td>97m</td> </tr> <tr> <td>60 km/h</td> <td>73m</td> <td>123m</td> </tr> <tr> <td>70 km/h</td> <td>92m</td> <td>151m</td> </tr> <tr> <td>80 km/h</td> <td>114m</td> <td>181m</td> </tr> <tr> <td>90 km/h</td> <td>139m</td> <td>214m</td> </tr> <tr> <td>100 km/h</td> <td>165m</td> <td>248m</td> </tr> <tr> <td>110km/h</td> <td>193m</td> <td>285m</td> </tr> </table> <p>(b) pedestrian sightlines in accordance with the following diagram:</p>	50 km/h	55m	97m	60 km/h	73m	123m	70 km/h	92m	151m	80 km/h	114m	181m	90 km/h	139m	214m	100 km/h	165m	248m	110km/h	193m	285m
50 km/h	55m	97m																				
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80 km/h	114m	181m																				
90 km/h	139m	214m																				
100 km/h	165m	248m																				
110km/h	193m	285m																				
Access – Mud and Debris																						
<p>PO 6.1</p> <p>Access points constructed to minimise mud or other debris being carried or transferred onto the road to ensure safe road operating conditions.</p>	<p>DTS/DPF 6.1</p> <p>Where the road has an unsealed shoulder and the road is not kerbed, the access way is sealed from the edge of seal on the road for a minimum of 10m or to the property boundary (whichever is closer).</p>																					
Access - Stormwater																						
<p>PO 7.1</p> <p>Access points are designed to minimise negative impact on roadside drainage of water.</p>	<p>DTS/DPF 7.1</p> <p>Development does not:</p> <ul style="list-style-type: none"> (a) decrease the capacity of an existing drainage point (b) restrict or prevent the flow of stormwater through an existing drainage point and system. 																					
Building on Road Reserve																						
<p>PO 8.1</p> <p>Buildings or structures that encroach onto, above or below road reserves are designed and sited to minimise impact on safe</p>	<p>DTS/DPF 8.1</p> <p>Buildings or structures are not located on, above or below the road reserve.</p>																					

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movements by all road users.	
Public Road Junctions	
PO 9.1 New junctions with a public road (including the opening of unmade public road junctions) or modifications to existing road junctions are located and designed to ensure safe operating conditions are maintained on the State Maintained Road.	DTS/DPF 9.1 Development does not comprise any of the following: (a) creating a new junction with a public road (b) opening an unmade public road junction (c) modifying an existing public road junction.
Corner Cut-Offs	
PO 10.1 Development is located and designed to maintain sightlines for drivers turning into and out of public road junctions to contribute to driver safety.	DTS/DPF 10.1 Development does not involve building work, or building work is located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram: 

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, development (including the division of land) that involves any of the following to/on a State Maintained Road or within 25 metres of an intersection with any such road:</p> <ul style="list-style-type: none"> (a) creation of a new access or junction (b) alterations to an existing access or public road junction (except where deemed to be minor in the opinion of the relevant authority) (c) development that changes the nature of vehicular movements or increase the number or frequency of movements through an existing access (except where deemed to be minor in the opinion of the relevant authority). 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

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Storage of manure, used litter and other wastes (other than waste water lagoons) is sited, designed, constructed and managed to:	None are applicable.
<ul style="list-style-type: none"> (a) avoid attracting and harbouring vermin (b) avoid polluting water resources (c) be located outside 1% AEP flood event areas. 	
Soil and Water Protection	
<p>PO 3.1</p> <p>To avoid environmental harm and adverse effects on water resources, intensive animal husbandry operations are appropriately set back from:</p> <ul style="list-style-type: none"> (a) public water supply reservoirs (b) major watercourses (third order or higher stream) (c) any other watercourse, bore or well used for domestic or stock water supplies. 	<p>DTS/DPF 3.1</p> <p>Intensive animal husbandry operations are set back:</p> <ul style="list-style-type: none"> (a) 800m or more from a public water supply reservoir (b) 200m or more from a major watercourse (third order or higher stream) (c) 100m or more from any other watercourse, bore or well used for domestic or stock water supplies.
<p>PO 3.2</p> <p>Intensive animal husbandry operations and dairies incorporate appropriately designed effluent and run-off facilities that:</p> <ul style="list-style-type: none"> (a) have sufficient capacity to hold effluent and runoff from the operations on site (b) ensure effluent does not infiltrate and pollute groundwater, soil or other water resources. 	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>

Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General Land Use Compatibility	
<p>PO 1.1</p> <p>Sensitive receivers are designed and sited to protect residents and occupants from adverse impacts generated by lawfully existing land uses (or lawfully approved land uses) and land uses</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>

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desired in the zone.									
PO 1.2 Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.	DTS/DPF 1.2 None are applicable.								
Hours of Operation									
PO 2.1 Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to: (a) the nature of the development (b) measures to mitigate off-site impacts (c) the extent to which the development is desired in the zone (d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.	<p>DTS/DPF 2.1 Development operating within the following hours:</p> <table border="1"> <thead> <tr> <th>Class of Development</th> <th>Hours of operation</th> </tr> </thead> <tbody> <tr> <td>Consulting room</td> <td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td>Office</td> <td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td>Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</td> <td>7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday</td> </tr> </tbody> </table>	Class of Development	Hours of operation	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday
Class of Development	Hours of operation								
Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday								
Overshadowing									
PO 3.1 Overshadowing of habitable room windows of adjacent residential land uses in: a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight.	DTS/DPF 3.1 North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.								
PO 3.2 Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in: a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight.	DTS/DPF 3.2 Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following: a. for ground level private open space, the smaller of the following: i. half the existing ground level open space or								

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	<p>ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m)</p> <p>b. for ground level communal open space, at least half of the existing ground level open space.</p>
<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <p>(a) the form of development contemplated in the zone</p> <p>(b) the orientation of the solar energy facilities</p> <p>(c) the extent to which the solar energy facilities are already overshadowed.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Development that incorporates moving parts, including windmills and wind farms, are located and operated to not cause unreasonable nuisance to nearby dwellings and tourist accommodation caused by shadow flicker.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
<p>Activities Generating Noise or Vibration</p>	
<p>PO 4.1</p> <p>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.1</p> <p>Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.</p>
<p>PO 4.2</p> <p>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</p> <p>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</p> <p>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</p> <p>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</p> <p>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.3</p> <p>The pump and/or filtration system ancillary to a dwelling erected on the same site is:</p> <p>(a) enclosed in a solid acoustic structure located at least 5m from the nearest habitable room located on an</p>

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	<p>adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.</p>				
<p>PO 4.4 External noise into bedrooms is minimised by separating or shielding these rooms from service equipment areas and fixed noise sources located on the same or an adjoining allotment.</p>	<p>DTS/DPF 4.4 Adjacent land is used for residential purposes.</p>				
<p>PO 4.5 Outdoor areas associated with licensed premises (such as beer gardens or dining areas) are designed and/or sited to not cause unreasonable noise impact on existing adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.5 None are applicable.</p>				
<p>PO 4.6 Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 4.6 Development incorporating music includes noise attenuation measures that will achieve the following noise levels:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 50%;">Assessment location</th> <th style="width: 50%;">Music noise level</th> </tr> </thead> <tbody> <tr> <td>Externally at the nearest existing or envisaged noise sensitive location</td> <td>Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)</td> </tr> </tbody> </table>	Assessment location	Music noise level	Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)
Assessment location	Music noise level				
Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)				
Air Quality					
<p>PO 5.1 Development with the potential to emit harmful or nuisance-generating air pollution incorporates air pollution control measures to prevent harm to human health or unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) within the locality and zones primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 5.1 None are applicable.</p>				
<p>PO 5.2 Development that includes chimneys or exhaust flues (including cafes, restaurants and fast food outlets) is designed to minimise nuisance or adverse health impacts to sensitive receivers (or lawfully approved sensitive receivers) by:</p> <ul style="list-style-type: none"> (a) incorporating appropriate treatment technology before exhaust emissions are released (b) locating and designing chimneys or exhaust flues to maximise the dispersion of exhaust emissions, taking into account the location of sensitive receivers. 	<p>DTS/DPF 5.2 None are applicable.</p>				
Light Spill					
<p>PO 6.1 External lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or</p>	<p>DTS/DPF 6.1 None are applicable.</p>				

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lawfully approved sensitive receivers).	
PO 6.2 External lighting is not hazardous to motorists and cyclists.	DTS/DPF 6.2 None are applicable.
Solar Reflectivity / Glare	
PO 7.1 Development is designed and comprised of materials and finishes that do not unreasonably cause a distraction to adjacent road users and pedestrian areas or unreasonably cause heat loading and micro-climatic impacts on adjacent buildings and land uses as a result of reflective solar glare.	DTS/DPF 7.1 None are applicable.
Electrical Interference	
PO 8.1 Development in rural and remote areas does not unreasonably diminish or result in the loss of existing communication services due to electrical interference.	DTS/DPF 8.1 The building or structure: (a) is no greater than 10m in height, measured from existing ground level or (b) is not within a line of sight between a fixed transmitter and fixed receiver (antenna) other than where an alternative service is available via a different fixed transmitter or cable.
Interface with Rural Activities	
PO 9.1 Sensitive receivers are located and designed to mitigate impacts from lawfully existing horticultural and farming activities (or lawfully approved horticultural and farming activities), including spray drift and noise and do not prejudice the continued operation of these activities.	DTS/DPF 9.1 None are applicable.
PO 9.2 Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing intensive animal husbandry activities and do not prejudice the continued operation of these activities.	DTS/DPF 9.2 None are applicable.
PO 9.3 Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing land-based aquaculture activities and do not prejudice the continued operation of these activities.	DTS/DPF 9.3 Sensitive receivers are located at least 200m from the boundary of a site used for land-based aquaculture and associated components in other ownership.
PO 9.4 Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing dairies including associated wastewater lagoons and liquid/solid waste storage and disposal facilities and do not prejudice the continued operation of these activities.	DTS/DPF 9.4 Sensitive receivers are sited at least 500m from the boundary of a site used for a dairy and associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities in other ownership.
PO 9.5	DTS/DPF 9.5

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<p>Sensitive receivers are located and designed to mitigate the potential impacts from lawfully existing facilities used for the handling, transportation and storage of bulk commodities (recognising the potential for extended hours of operation) and do not prejudice the continued operation of these activities.</p>	<p>Sensitive receivers are located away from the boundary of a site used for the handling, transportation and/or storage of bulk commodities in other ownership in accordance with the following:</p> <ul style="list-style-type: none"> (a) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility (b) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals) where the handling of these materials into or from vessels does not exceed 100 tonnes per day (c) 500m or more, where it involves the storage of bulk petroleum in individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1000 cubic metres (d) 500m or more, where it involves the handling of coal with a capacity up to 1 tonne per day or a storage capacity up to 50 tonnes (e) 1000m or more, where it involves the handling of coal with a capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes.
<p>PO 9.6</p> <p>Setbacks and vegetation plantings along allotment boundaries should be incorporated to mitigate the potential impacts of spray drift and other impacts associated with agricultural and horticultural activities.</p>	<p>DTS/DPF 9.6</p> <p>None are applicable.</p>
<p>PO 9.7</p> <p>Urban development does not prejudice existing agricultural and horticultural activities through appropriate separation and design techniques.</p>	<p>DTS/DPF 9.7</p> <p>None are applicable.</p>
<p>Interface with Mines and Quarries (Rural and Remote Areas)</p>	
<p>PO 10.1</p> <p>Sensitive receivers are separated from existing mines to minimise the adverse impacts from noise, dust and vibration.</p>	<p>DTS/DPF 10.1</p> <p>Sensitive receivers are located no closer than 500m from the boundary of a Mining Production Tenement under the <i>Mining Act 1971</i>.</p>

Land Division

Assessment Provisions (AP)

<p>Desired Outcome</p>	
<p>DO 1</p>	<p>Land division:</p> <ul style="list-style-type: none"> (a) creates allotments with the appropriate dimensions and shape for their intended use

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Transport, Access and Parking**Assessment Provisions (AP)**

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Movement Systems	
PO 1.1 Development is integrated with the existing transport system and designed to minimise its potential impact on the functional performance of the transport system.	DTS/DPF 1.1 None are applicable.
PO 1.2 Development is designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.3 Industrial, commercial and service vehicle movements, loading areas and designated parking spaces are separated from passenger vehicle car parking areas to ensure efficient and safe movement and minimise potential conflict.	DTS/DPF 1.3 None are applicable.
PO 1.4 Development is sited and designed so that loading, unloading and turning of all traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.	DTS/DPF 1.4 All vehicle manoeuvring occurs onsite.
Sightlines	
PO 2.1 Sightlines at intersections, pedestrian and cycle crossings, and crossovers to allotments for motorists, cyclists and pedestrians are maintained or enhanced to ensure safety for all road users and pedestrians.	DTS/DPF 2.1 None are applicable.
PO 2.2 Walls, fencing and landscaping adjacent to driveways and corner	DTS/DPF 2.2 None are applicable.

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<p>sites are designed to provide adequate sightlines between vehicles and pedestrians.</p>	
<p>Vehicle Access</p>	
<p>PO 3.1 Safe and convenient access minimises impact or interruption on the operation of public roads.</p>	<p>DTS/DPF 3.1 The access is: (a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land or (b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.</p>
<p>PO 3.2 Development incorporating vehicular access ramps ensures vehicles can enter and exit a site safely and without creating a hazard to pedestrians and other vehicular traffic.</p>	<p>DTS/DPF 3.2 None are applicable.</p>
<p>PO 3.3 Access points are sited and designed to accommodate the type and volume of traffic likely to be generated by the development or land use.</p>	<p>DTS/DPF 3.3 None are applicable.</p>
<p>PO 3.4 Access points are sited and designed to minimise any adverse impacts on neighbouring properties.</p>	<p>DTS/DPF 3.4 None are applicable.</p>
<p>PO 3.5 Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.</p>	<p>DTS/DPF 3.5 Vehicle access to designated car parking spaces satisfy (a) or (b): (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.</p>
<p>PO 3.6 Driveways and access points are separated and minimised in number to optimise the provision of on-street visitor parking (where on-street parking is appropriate).</p>	<p>DTS/DPF 3.6 Driveways and access points: (a) for sites with a frontage to a public road of 20m or less, one access point no greater than 3.5m in width is provided (b) for sites with a frontage to a public road greater than 20m:</p>

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	<ul style="list-style-type: none"> (i) a single access point no greater than 6m in width is provided or (ii) not more than two access points with a width of 3.5m each are provided.
<p>PO 3.7</p> <p>Access points are appropriately separated from level crossings to avoid interference and ensure their safe ongoing operation.</p>	<p>DTS/DPF 3.7</p> <p>Development does not involve a new or modified access or cause an increase in traffic through an existing access that is located within the following distance from a railway crossing:</p> <ul style="list-style-type: none"> (a) 80 km/h road - 110m (b) 70 km/h road - 90m (c) 60 km/h road - 70m (d) 50km/h or less road - 50m.
<p>PO 3.8</p> <p>Driveways, access points, access tracks and parking areas are designed and constructed to allow adequate movement and manoeuvrability having regard to the types of vehicles that are reasonably anticipated.</p>	<p>DTS/DPF 3.8</p> <p>None are applicable.</p>
<p>PO 3.9</p> <p>Development is designed to ensure vehicle circulation between activity areas occurs within the site without the need to use public roads.</p>	<p>DTS/DPF 3.9</p> <p>None are applicable.</p>
Access for People with Disabilities	
<p>PO 4.1</p> <p>Development is sited and designed to provide safe, dignified and convenient access for people with a disability.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
Vehicle Parking Rates	
<p>PO 5.1</p> <p>Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:</p> <ul style="list-style-type: none"> (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place. 	<p>DTS/DPF 5.1</p> <p>Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:</p> <ul style="list-style-type: none"> (a) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements (b) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.
Vehicle Parking Areas	
<p>PO 6.1</p> <p>Vehicle parking areas are sited and designed to minimise impact on the operation of public roads by avoiding the use of public roads when moving from one part of a parking area to another.</p>	<p>DTS/DPF 6.1</p> <p>Movement between vehicle parking areas within the site can occur without the need to use a public road.</p>

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PO 6.2 Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced, and the like.	DTS/DPF 6.2 None are applicable.
PO 6.3 Vehicle parking areas are designed to provide opportunity for integration and shared-use of adjacent car parking areas to reduce the total extent of vehicle parking areas and access points.	DTS/DPF 6.3 None are applicable.
PO 6.4 Pedestrian linkages between parking areas and the development are provided and are safe and convenient.	DTS/DPF 6.4 None are applicable.
PO 6.5 Vehicle parking areas that are likely to be used during non-daylight hours are provided with sufficient lighting to entry and exit points to ensure clear visibility to users.	DTS/DPF 6.5 None are applicable.
PO 6.6 Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.	DTS/DPF 6.6 Loading areas and designated parking spaces are wholly located within the site.
PO 6.7 On-site visitor parking spaces are sited and designed to be accessible to all visitors at all times.	DTS/DPF 6.7 None are applicable.
Undercroft and Below Ground Garaging and Parking of Vehicles	
PO 7.1 Undercroft and below ground garaging of vehicles is designed to enable safe entry and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles.	DTS/DPF 7.1 None are applicable.
Internal Roads and Parking Areas in Residential Parks and Caravan and Tourist Parks	
PO 8.1 Internal road and vehicle parking areas are surfaced to prevent dust becoming a nuisance to park residents and occupants.	DTS/DPF 8.1 None are applicable.
PO 8.2 Traffic circulation and movement within the park is pedestrian friendly and promotes low speed vehicle movement.	DTS/DPF 8.2 None are applicable.
Bicycle Parking in Designated Areas	
PO 9.1 The provision of adequately sized on-site bicycle parking facilities encourages cycling as an active transport mode.	DTS/DPF 9.1 Areas and / or fixtures are provided for the parking and storage of bicycles at a rate not less than the amount calculated using Transport, Access and Parking Table 3 - Off Street Bicycle Parking Requirements.

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<p>PO 9.2</p> <p>Bicycle parking facilities provide for the secure storage and tethering of bicycles in a place where casual surveillance is possible, is well lit and signed for the safety and convenience of cyclists and deters property theft.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>
<p>PO 9.3</p> <p>Non-residential development incorporates end-of-journey facilities for employees such as showers, changing facilities and secure lockers, and signage indicating the location of the facilities to encourage cycling as a mode of journey-to-work transport.</p>	<p>DTS/DPF 9.3</p> <p>None are applicable.</p>
<p>Corner Cut-Offs</p>	
<p>PO 10.1</p> <p>Development is located and designed to ensure drivers can safely turn into and out of public road junctions.</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:</p> 

Table 1 - General Off-Street Car Parking Requirements

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

<p>Class of Development</p>	<p>Car Parking Rate (unless varied by Table 2 onwards)</p> <p>Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.</p>
<p>Residential Development</p>	
<p>Detached Dwelling</p>	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
<p>Group Dwelling</p>	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>

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Residential Flat Building	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p> <p>0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.</p>
Row Dwelling where vehicle access is from the primary street	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Semi-Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Aged / Supported Accommodation	
Retirement village	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
Supported accommodation	0.3 spaces per bed.
Residential Development (Other)	
Ancillary accommodation	No additional requirements beyond those associated with the main dwelling.
Residential park	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling.</p> <p>0.2 spaces per dwelling for visitor parking.</p>
Student accommodation	0.3 spaces per bed.
Workers' accommodation	0.5 spaces per bed plus 0.2 spaces per bed for visitor parking.
Tourist	
Caravan park / tourist park	<p>Parks with 100 sites or less - a minimum of 1 space per 10 sites to be used for accommodation.</p> <p>Parks with more than 100 sites - a minimum of 1 space per 15 sites used for accommodation.</p>

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	A minimum of 1 space for every caravan (permanently fixed to the ground) or cabin.
Tourist accommodation	1 car parking space per accommodation unit / guest room.
Commercial Uses	
Auction room/ depot	1 space per 100m ² of building floor area plus an additional 2 spaces.
Automotive collision repair	3 spaces per service bay.
Call centre	8 spaces per 100m ² of gross leasable floor area.
Motor repair station	3 spaces per service bay.
Office	4 spaces per 100m ² of gross leasable floor area.
Retail fuel outlet	3 spaces per 100m ² gross leasable floor area.
Service trade premises	2.5 spaces per 100m ² of gross leasable floor area 1 space per 100m ² of outdoor area used for display purposes.
Shop (no commercial kitchen)	5.5 spaces per 100m ² of gross leasable floor area where not located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared. 5 spaces per 100m ² of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.
Shop (in the form of a bulky goods outlet)	2.5 spaces per 100m ² of gross leasable floor area.
Shop (in the form of a restaurant or involving a commercial kitchen)	Premises with a dine-in service only (which may include a take-away component with no drive-through) - 0.4 spaces per seat. Premises with take-away service but with no seats - 12 spaces per 100m ² of total floor area plus a drive-through queue capacity of ten vehicles measured from the pick-up point. Premises with a dine-in and drive-through take-away service - 0.3 spaces per seat plus a drive through queue capacity of 10 vehicles measured from the pick-up point.
Community and Civic Uses	
Childcare centre	0.25 spaces per child

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Library	4 spaces per 100m ² of total floor area.
Community facility	10 spaces per 100m ² of total floor area.
Hall / meeting hall	0.2 spaces per seat.
Place of worship	1 space for every 3 visitor seats.
Pre-school	1 per employee plus 0.25 per child (drop off/pick up bays)
Educational establishment	<p>For a primary school - 1.1 space per full time equivalent employee plus 0.25 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a secondary school - 1.1 per full time equivalent employee plus 0.1 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site.</p> <p>For a tertiary institution - 0.4 per student based on the maximum number of students on the site at any time.</p>
Health Related Uses	
Hospital	<p>4.5 spaces per bed for a public hospital.</p> <p>1.5 spaces per bed for a private hospital.</p>
Consulting room	4 spaces per consulting room excluding ancillary facilities.
Recreational and Entertainment Uses	
Cinema complex	0.2 spaces per seat.
Concert hall / theatre	0.2 spaces per seat.
Hotel	1 space for every 2m ² of total floor area in a public bar plus 1 space for every 6m ² of total floor area available to the public in a lounge, beer garden plus 1 space per 2 gaming machines, plus 1 space per 3 seats in a restaurant.
Indoor recreation facility	<p>6.5 spaces per 100m² of total floor area for a Fitness Centre</p> <p>4.5 spaces per 100m² of total floor area for all other Indoor recreation facilities.</p>
Industry/Employment Uses	
Fuel depot	<p>1.5 spaces per 100m² total floor area</p> <p>1 spaces per 100m² of outdoor area used for fuel depot activity purposes.</p>

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Industry	1.5 spaces per 100m ² of total floor area.
Store	0.5 spaces per 100m ² of total floor area.
Timber yard	1.5 spaces per 100m ² of total floor area 1 space per 100m ² of outdoor area used for display purposes.
Warehouse	0.5 spaces per 100m ² total floor area.
Other Uses	
Funeral Parlour	1 space per 5 seats in the chapel plus 1 space for each vehicle operated by the parlour.
Radio or Television Station	5 spaces per 100m ² of total building floor area.

Table 2 - Off-Street Car Parking Requirements in Designated Areas

The following parking rates apply in any zone, subzone or other area described in the 'Designated Areas' column subject to the following:

- (a) the location of the development is unable to satisfy the requirements of Table 2 – Criteria (other than where a location is exempted from the application of those criteria)
- or
- (b) the development satisfies Table 2 – Criteria (or is exempt from those criteria) and is located in an area where a lawfully established carparking fund operates, in which case the number of spaces are reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate		Designated Areas
	Minimum number of spaces	Maximum number of spaces	
Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.			
Development generally			
All classes of development	No minimum.	No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is: 1 space for each dwelling with a total floor area less than 75 square metres	Capital City Zone City Main Street Zone City Riverbank Zone Adelaide Park Lands Zone Business Neighbourhood Zone (within the City of Adelaide)

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		<p>2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres</p> <p>3 spaces for each dwelling with a total floor area greater than 150 square metres.</p> <p>Residential flat building or Residential component of a multi-storey building: 1 visitor space for each 6 dwellings.</p>	The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone
Non-residential development			
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	5 spaces per 100m ² of gross leasable floor area.	<p>City Living Zone</p> <p>Urban Corridor (Boulevard) Zone</p> <p>Urban Corridor (Business) Zone</p> <p>Urban Corridor (Living) Zone</p> <p>Urban Corridor (Main Street) Zone</p> <p>Urban Neighbourhood Zone</p>
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	6 spaces per 100m ² of gross leasable floor area.	<p>Strategic Innovation Zone</p> <p>Suburban Activity Centre Zone</p> <p>Suburban Business Zone</p> <p>Business Neighbourhood Zone</p> <p>Suburban Main Street Zone</p> <p>Urban Activity Centre Zone</p>
Tourist accommodation	1 space for every 4 bedrooms up to 100 bedrooms plus 1 space for every 5 bedrooms over 100 bedrooms	1 space per 2 bedrooms up to 100 bedrooms and 1 space per 4 bedrooms over 100 bedrooms	<p>City Living Zone</p> <p>Urban Activity Centre Zone</p> <p>Urban Corridor (Boulevard) Zone</p> <p>Urban Corridor (Business) Zone</p> <p>Urban Corridor (Living) Zone</p> <p>Urban Corridor (Main Street) Zone</p> <p>Urban Neighbourhood Zone</p>
Residential development			
Residential component of a multi-storey building	<p>Dwelling with no separate bedroom -0.25 spaces per dwelling</p> <p>1 bedroom dwelling - 0.75</p>	None specified.	<p>City Living Zone</p> <p>Strategic Innovation Zone</p> <p>Urban Activity Centre Zone</p>

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	spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.		Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone
Residential flat building	Dwelling with no separate bedroom -0.25 spaces per dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.	None specified.	City Living Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone

Table 2 - Criteria:

The following criteria are used in conjunction with Table 2. The 'Exception' column identifies locations where the criteria do not apply and the car parking rates in Table 2 are applicable.

Criteria	Exceptions
<p>The designated area is wholly located within Metropolitan Adelaide and any part of the development site satisfies one or more of the following:</p> <p>(a) is within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service⁽²⁾</p> <p>(b) is within 400 metres of a bus interchange⁽¹⁾</p> <p>(c) is within 400 metres of an O-Bahn interchange⁽¹⁾</p> <p>(d) is within 400 metres of a passenger rail station⁽¹⁾</p> <p>(e) is within 400 metres of a passenger tram station⁽¹⁾</p> <p>(f) is within 400 metres of the Adelaide Parklands.</p>	<p>(a) All zones in the City of Adelaide</p> <p>(b) Strategic Innovation Zone in the following locations: (i) City of Burnside (ii) City of Marion (iii) City of Mitcham</p> <p>(c) Urban Corridor (Boulevard) Zone (d) Urban Corridor (Business) Zone (e) Urban Corridor (Living) Zone (f) Urban Corridor (Main Street) Zone (g) Urban Neighbourhood Zone</p>

[NOTE(S): (1) Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles. (2) A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.]

Policy24 - Enquiry

Table 3 - Off-Street Bicycle Parking Requirements

The bicycle parking rates apply within designated areas located within parts of the State identified in the Schedule to Table 3.

Class of Development	Bicycle Parking Rate Where a development comprises more than one development type, then the overall bicycle parking rate will be taken to be the sum of the bicycle parking rates for each development type.
Consulting Room	1 space per 20 employees plus 1 space per 20 consulting rooms for customers.
Educational establishment	For a secondary school - 1 space per 20 full-time time employees plus 10 percent of the total number of employee spaces for visitors. For tertiary education - 1 space per 20 employees plus 1 space per 10 full time students.
Hospital	1 space per 15 beds plus 1 space per 30 beds for visitors.
Indoor recreation facility	1 space per 4 employees plus 1 space per 200m ² of gross leasable floor area for visitors.
Licensed Premises	1 per 20 employees, plus 1 per 60 square metres total floor area, plus 1 per 40 square metres of bar floor area, plus 1 per 120 square metres lounge and beer garden floor area, plus 1 per 60 square metres dining floor area, plus 1 per 40 square metres gaming room floor area.
Office	1 space for every 200m ² of gross leasable floor area plus 2 spaces plus 1 space per 1000m ² of gross leasable floor area for visitors.
Pre-school	1 space per 20 full time employees plus 1 space per 40 full time children.
Recreation area	1 per 1500 spectator seats for employees plus 1 per 250 visitor and customers.
Residential flat building	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 for every 10 dwellings for visitors.
Residential component of a multi-storey building	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 space for every 10 dwellings for visitors.
Shop	1 space for every 300m ² of gross leasable floor area plus 1 space for every 600m ² of gross leasable floor area for customers.
Tourist accommodation	1 space for every 20 employees plus 2 for the first 40 rooms and 1 for every additional 40 rooms for visitors.
Schedule to Table 3	

Policy24 - Enquiry

Designated Area	Relevant part of the State
	The bicycle parking rate applies to a designated area located in a relevant part of the State described below.
All zones	City of Adelaide
Business Neighbourhood Zone Strategic Innovation Zone Suburban Activity Centre Zone Suburban Business Zone Suburban Main Street Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone	Metropolitan Adelaide

Waste Treatment and Management Facilities

Assessment Provisions (AP)

Desired Outcome	
DO 1	Mitigation of the potential environmental and amenity impacts of waste treatment and management facilities.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1 Waste treatment and management facilities incorporate separation distances and attenuation measures within the site between waste operations areas (including all closed, operating	DTS/DPF 1.1 None are applicable.

Friday, 18 June 2021

CHICCO PALMS

Request for Documentation

Applicant: Domenico Maurici

Application ID: 21014960

Consent: Planning Consent

Proposed Development: Variation to condition 3 of DA211/262/2016 -
Change in trading hours Subject Land:

Location reference: 437 HENLEY BEACH RD BROOKLYN PARK SA
5032

Title ref.: CT 5163/417 & 418 Plan Parcel: F105648 AL1 & AL2 Council:
CITY OF WEST TORRENS

Dear Seb Anderson,

In response to your request for documentation I have listed a few but key points as to our application for extended trading hours:

. continuing request from our customer base to open especially on Sunday evenings. We have only a limited amount of dates available for group functions extending our trading hours will allow us to accomodate more group request.

. an opportunity to recoup losses made from the onset of Covid in which we still are yet to fully recover from.

. to allow our workers a greater opportunity of full time work as opposed to casual. With these extended hours we a hopeful we can retain more workers by securing full time/permanent part time employment.

Yours sincerely,

Domenico Maurici
Director
Chicco Palms

Vary condition 3

Application # 211/262/2016

Requesting an extension of trading hours

**Extension requested:- Mondays 11am to 11pm
Sundays 4pm to 11pm**

Existing trading hours

**Tuesday 11am to 11pm
Wednesday 11am to 11pm
Thursday 11am to 11pm
Friday 11am to 11pm
Saturday 11am to 11pm
Sunday 11am to 4 pm**

Details of Representations

Application Summary

Application ID	21014960
Proposal	Variation to Condition 3 in DA 211/262/2016 - extension to hours of operation to include Mondays 11.00am to 11.00pm and Sundays 11.00am to 11.00pm
Location	437 HENLEY BEACH RD BROOKLYN PARK SA 5032, 437 HENLEY BEACH RD BROOKLYN PARK SA 5032, 437 HENLEY B..

Representations

Representor 1 - Nadia Vernari

Name	Nadia Vernari
Address	14 Elston St BROOKLYN PARK SA, 5032 Australia
Phone Number	
Email Address	
Submission Date	26/09/2021 10:43 AM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons	<p>The current hours of operation since this restaurant has opened has impacted on the parking available and peaceful amenity for the families living in Elston Street. This street is narrow, and has recently had yellow line parking restrictions imposed, since Chicco Palms began, to alleviate safety/traffic problems from the rise in the number of cars on the street. This resulted in less parking for families, with Chicco Palms employees and customers using available spaces. Two weeks ago, council further restricted resident parking on Friday, Saturday and Sunday nights by imposing a time parking restriction from 5pm to 9pm and because of Chicco Palms, further impacting residents by expecting we pay for annual resident parking permits. The residents in the street held a meeting one week ago. 25% of those invited attended or represented. In addition to parking restriction concerns, residents closer to the site reported an increase in rubbish around their properties and damage to properties. Others reported illegal parking across their driveways. My family and friends now need to park cars further</p>

up the street increasing risk of damage or theft to their cars, if they can find a park. Elston Street was a calm, quiet neighbourhood street. Our peaceful amenity has been eroded. Chicco Palms has already brought stress to many residents, a financial impact, rubbish, damage, and safety and parking issues. For these reasons I do not agree to a further erosion of our peace and quality of life as proposed by this planning development.

Attached Documents

Representations

Representor 2 - Grantley Noble

Name	Grantley Noble
Address	26 Elston Street BROOKLYN PARK SA, 5032 Australia
Phone Number	
Email Address	
Submission Date	01/10/2021 02:12 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development
Reasons	Traffic conditions in Elston Street are already overrun with parked cars on Friday to Sunday nights. The traffic overflow from the Council putting permit parking signs on the Northern End of Elston Street just means cars are parking further South down the street.

Attached Documents

Representations

Representor 3 - Vittorio and Linda De Marchi

Name	Vittorio and Linda De Marchi
Address	3 Elston Street BROOKLYN PARK SA, 5032 Australia
Phone Number	
Email Address	
Submission Date	04/10/2021 02:58 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development
Reasons	We have lived at this address since December 1967. We wish to keep the operating hours of Chicco Palms as they are at present as we look forward to the peace of Sunday evenings and the whole of Monday. The parking is chaotic now even though the West Torrens Council has amended the restrictions thereby further causing parking issues. The change in parking conditions because of the customers of Chicco Palms means that our visitors are severely impeded in their parking.

Attached Documents

Response to representations

In reference to planning application number 21014960

To Whom It May Concern:

The planning application has brought to our attention the current concerns raised by the three residents of Elston Street. We, in the best interest of continuing our future trade in a harmonious and respectful manner, take these concerns seriously and will do as much as possible to address these issues - working in conjunction with our immediate community.

When we began trading on the 2nd of January 2017, the intention was to reinvigorate the western suburbs restaurant scene, offering a complete restaurant experience which, at that time, was only found in a few city establishments and was largely unheard-of in the western suburbs. Our experience is built on providing traditional Italian classic cuisine, representing the seminal dishes we were raised on in a modern, contemporary space (with a mid-century slant). The business concept was a revival of sought, paying homage to our Italian heritage and the mixing of old-world tradition and new world feel and look.

We have all grown up and lived in the western suburbs, with fond memories of growing up with family and friends in this area, which is rich in Italian heritage. As experienced restaurateurs with successful businesses based in the CBD, our wish for Chicco Palms was to bring our knowledge and experience back to the place of our origin - "the west". Investing into this concept was at considerable risk for us, as establishing a business at this scale in an area which, while having a retail precinct, was far from thriving, was not certain to successfully engage with the local community. The success of our business is underpinned by huge investments of time, energy, and financial support from every business partner, away from their family and friends. Yet, we have been overwhelmed by the support of the community in making Chicco Palms what it is today and consider ourselves extremely fortunate to be part of this western suburbs area.

Since day one, we have been well received from the public and continue to be an incredibly popular local venue. Subsequently, we have seen a range of similar businesses invest in the western area, including Rocco's, Blue Velvet, and Melt just to name a few. We are continuously noted in best restaurants in Adelaide to visit, places to go to in the west, and are one of the most popular locations for Instagram venues in Adelaide (with currently over 10,000 followers). Chicco Palms is continuously referred to as a reason to live and or invest in the west, being used/referenced with real-estate advertisements, Travel SA references, and has been showcased as a venue for baptisms, birthdays, and weddings across multiple media platforms.

However, despite all our success since 2017, we, like many other businesses in the sector, were not immune to the effects of COVID. Surviving this period required rapid pivoting and adjustment of what we offer to the community, and we are proud to acknowledge that our efforts have ensured we have not had to retrench one single employee.

Yet, sacrifices were still made to achieve this, which involved the reduction of staff hours and diminished profits due to a decline in turnover. The COVID pandemic also saw increased costs of produce/stock due to unavailability and this continues today. As for other businesses, this will take us many years to fully recover from, if ever. Our continued efforts to adjust to the increased costs and COVID effects and ensure we remain a part of this community is the primary motivation for our application to extend our trading hours. Increasing our hours allows us to provide our employees more hours and improve our ability to retain staff on a more permanent basis.

As I'm sure you are aware, the hospitality industry is still reeling from the loss of a transient workforce due to border closures and restricted movement across the country. Much of the sector is reliant on a workforce comprising University students, back-packers, and recent immigrants into the country. Even if border restrictions are eased, it will be some time (if ever) before the workforce will be at the same levels as pre-COVID. As such, the only way to remain a viable business is by attracting a workforce that sees this work as a viable permanent career. Currently, we employ 41 people; 36 of whom are casual employees. The problem with retention becomes clear when you extrapolate the numbers of people employed, the recruitment and % of casual positions with the available trading hours. As an organisation, we need to secure our workforce by increasing our trading hours, providing more opportunities for our employees to work and thus guaranteeing the continuing success of our business.

We are applying for an extension of Sunday trading to our afternoon operations by extending into the evening, and the possibility of Monday trading. We are happy to further discuss these options. We would also like to highlight the following considerations to show that we are taking the communities best interests quite seriously.

Vibrancy and diversity of business in the area

Extending trading hours will create diversity in available restaurants for dinner service on Sunday evenings, which will in turn bring more patrons to the area and the flow on of customers will benefit other businesses trading in the immediate area

For example the following businesses surrounding Chicco Palms are open on Sunday evenings:

- Viet Thai Restaurant at 427 Henley Beach Road: closing at 9.30pm on a Sunday
- Joe's Pizza Bar at 415 Henley Beach Road: closing at 10pm on a Sunday

This will only add to the vibrancy of the restaurant scene on Henley Beach Road throughout the week.

The extended trading hours will not cause any increase in traffic disturbance with all three restaurants in close vicinity open on a Sunday night, as they all evidently operate late on all other nights.

Traffic Management

Parking in the area has been highlighted to be the significant issue raised by neighbours, we are happy to work with council to obtain a traffic impact statement.

Public transport is close by and accessible, there are a number of bus stops along Henley Beach Road and on Rowells Road (the street across from Chicco Palms).

Liquor Licence

We also suggest the liquor licence for Sunday evening to end at 10pm. An hour earlier rather than the 11pm that we currently have approved on Tuesday to Saturday trading.

As a group, we hope to address these concerns in an inclusive, engaging, and creative manner. This can only be achieved by working alongside (and in collaboration) with our neighbours, local businesses, and council. Currently, to alleviate some of this, we have implemented a new policy around car parking for our workforce. This has restricted staff by classifying strict car parking zones (such as the western side of May Terrace along with the Kooyonga Golf Course). We have also begun communicating with nearby businesses to access unused car park spaces during their non-operational hours.

We also believe we can all assist in creating the best traffic management plan for Elston Street and would welcome the opportunity to open up Chicco Palms as a venue for residents, council and adjacent businesses to come together and openly discuss other options and ideas.

We believe in this area and are confident of a bright and exciting future, where Chicco Palms remains a long-standing member of the western suburbs. However, this will only be achieved by nurturing an engaging community whereby residents, businesses, and the council work together.

Memo

From Richard Tan
 Date 15/10/2021
 Subject 21014960, 437 HENLEY BEACH RD BROOKLYN PARK SA 5032

Brendan,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

1.1 Not applicable

2.0 Verge Interaction

2.1 Not applicable

3.0 Traffic Requirements

The following comments have been provided by Council's Traffic Consultant, Mr Frank Siow:

I understand that the development received DPC in 2016. As part of the approval, Condition (4) deals with the permitted opening times of the restaurant (see below).

4. That the hours of operation of the premises shall not exceed the following periods:-
 Closed Monday
 11:30am to 11:00pm Tuesday to Saturday inclusive; and
 11:30am to 5:00pm Sunday

The Applicant wishes to vary condition (4) to enable the restaurant to trade 7 days a week from 11am to 11pm.

Extension requested:- Mondays 11am to 11pm
 Sundays 4pm to 11pm

Existing trading hours

Tuesday 11am to 11pm
 Wednesday 11am to 11pm
 Thursday 11am to 11pm
 Friday 11am to 11pm
 Saturday 11am to 11pm
 Sunday 11am to 4 pm

The subject site has limited off-street parking spaces. Since the restaurant has commenced trading at the subject site, I am advised by City Assets that the parking conditions in the nearby streets have been detrimentally impacted, in particular Elston Street, where overflow parking after hours (during evening periods) have exacerbated on-street parking for residents to the extent that significant parking controls have had to be implemented to manage the parking impact on residents.

Given the known impacts of overflow parking, particularly during evening periods, the proposed extension of trading hours would likely further exacerbate the parking conditions for nearby streets. The Applicant should give consideration to addressing the parking impact issue and consider other parking arrangements that may assist, for example securing off-street parking elsewhere that may assist in minimising the parking impact.

As currently proposed, I have concerns with the proposed extension to the trading hours due to the parking impacts in adjacent residential streets. The extended trading hours would likely further exacerbate the parking impacts on the nearby streets over extended periods of the week.

4.0 Waste Management

4.1 It should be noted that currently the waste service occurs during the Monday where the restaurant is closed. If this proposal is to be supported, then alternative waste arrangement should be provided.

5.0 Stormwater Requirements

5.1 Not applicable.

Regards
Richard Tan
Civil Engineer

7 REVIEW OF ASSESSMENT MANAGER DECISION

Nil

8 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER

Nil

9 RELEVANT AUTHORITY ACTIVITIES REPORT**9.1 Activities Summary - December 2021****Brief**

This report presents information in relation to:

1. Any development appeals before the Environment, Resources and Development (ERD) Court where the Council Assessment Panel (CAP) is the relevant authority;
2. Other appeal matters before the ERD Court of which SCAP are the relevant authority;
3. Any deferred items previously considered by the CAP
4. Summary of applications that have been determined under delegated authority where CAP is the relevant authority; and
5. Any matters being determined by the State Commission Assessment Panel (SCAP).

RECOMMENDATION

The Council Assessment Panel receive and note the information.

Development Application appeals before the ERD Court**CAP is the relevant authority**

Nil

SCAP is the relevant authority

DA number	Address	Description of development	Status
211/M022/17	79 Port Road, THEBARTON	Multi-storey mixed use development, incorporating commercial tenancy, 2 storey car park, 9-storey residential flat building, four x 3-storey residential flat buildings and car parking	Compromise plans have been received and Council comments provided to SCAP 09 November 2020. The compromise proposal was scheduled for conciliation conference 28 January 2021. No further update available.

Deferred CAP Items

Nil

Development Applications determined under delegation (CAP is the relevant authority)

Awaiting PlanSA Portal functionality to report on relevant applications.

Development Applications pending determination by SCAP

DA Number	Reason for referral	Address	Description of development
21016709	Schedule 6	1A-1B Glenburnie Terrace Plympton	<p>Seven storey residential flat building comprising 32 dwellings with associated carparking and landscaping</p> <p>Council comments sent to SCAP.</p> <p>Application to be considered by SCAP 08/12/2021 at which Council administration is being heard in support of their submission.</p>
211/M101/20 Lodged 12/11/20	Schedule 10	2 Stirling St Thebarton	<p>Construction of a seven (7) storey office building with an associated warehouse and café and an eight (8) storey multi-deck car park building with a ground floor commercial tenancy</p> <p>Refused by SCAP on 24 November 2021 - minutes available at this link: https://www.saplanningcommission.sa.gov.au/_data/assets/pdf_file/0009/958257/Public_Minutes.pdf</p>
211/M135/21 Lodged 16/03/21	Schedule 10	1 Selby Street, Kurralta Park	<p>Construction of a 10-storey residential flat building with associated car parking and site works.</p> <p>Under Assessment.</p>
211/M134/21 Lodged 16/03/21	Schedule 10	4-10 Railway Terrace Mile End	<p>Construction of a mixed use residential/commercial development comprising 51m² commercial tenancy, two (2) residential flat buildings comprising 6 dwellings and 28 dwellings associated landscaping, carparking, communal spaces and public realm improvements (Stage 2)</p> <p>Under Assessment.</p> <p>Public notification closed on 10 November 2021.</p> <p>Council comments sent to SCAP</p>

DA Number	Reason for referral	Address	Description of development
			02/12/2021.
211/M129/21 Lodged 17/02/21	Schedule 10	8 Eton Road, Keswick	Construction of a six (6) storey mixed use building comprising residential and commercial tenancies together with car parking and landscaping. Under Assessment. Council comments sent to SCAP on 18/03/21.

Conclusion

This report is current as at 6 December 2021.

Attachments

Nil

10 OTHER BUSINESS

10.1 Council Assessment Panel Annual Report 2021

Brief

To provide Council with information on the activities of the Council Assessment Panel during 2021.

Introduction

The City of West Torrens Council Assessment Panel (CAP) Terms of Reference stipulates:

8. Reports to Council

The CAP will report to Council at least once per year, detailing issues for consideration by the Council. The Annual Report should include the following information:

- 8.1 *The number of meetings held;*
- 8.2 *The number and nature of applications that were considered (including the number of confidential items considered);*
- 8.3 *Advice in respect of any trends, issues and other matters that have become apparent or arisen through the CAP's assessment of applications, and*
- 8.4 *The number of decisions of the CAP that were appealed to the Environment, Resources and Development Court.*

At the 19 January 2021 Council meeting, the CAP presented the 2020 Annual Report to Council.

Discussion

It is proposed that an annual report for the period 1 January 2021 to 31 December 2021 be submitted to the Council.

The submitted report will contain a summary of the CAP's activities in 2021, including feedback from Panel members with regards to trends, issues and other matters relating to planning or development that have become apparent or arisen through its assessment of applications.

A draft report for the Panel's consideration is included as **Attachment 1**. Any amendments may be considered by Panel during the meeting.

If ratified, the report will be presented to the January 2022 Council meeting.

Conclusion

The 2021 Annual Report is proposed to be submitted to the Council in accordance with the Council Assessment Panel Terms of Reference.

RECOMMENDATION

It is recommended to Council Assessment Panel that:

1. The draft Council Assessment Panel Annual Report 2021 (appended as **Attachment 1** of Agenda report) be ratified for presentation to Council.
2. That the Assessment Manager be authorised to make any changes of a minor or technical nature, including changes to add the December 2021 CAP meeting data.

Attachments

1. **Draft Council Assessment Panel Annual Report 2021**

2021 City of West Torrens Council Assessment Panel Annual Report**1****Introduction**

The following report summarises the activity of the CAP for the 12 month period between 1 January 2021 and 31 December 2021. Development-related issues that have arisen during the course of the CAP's determination of development applications in that period are also discussed.

DiscussionMembership

During 2021 the CAP comprised the following members:

Independent Presiding Member	Shanti Ditter
Independent Member	Jane Strange
Independent Member	Kon Corolis
Independent Member	Michael Arman
Council Member	Graham Nitschke
Deputy Council Member	Jasmine Wood
Deputy Independent Member	Jim Gronthos (1 January - 9 September 2021)
Deputy Independent Member	Heath Edwards (1 - 31 December 2021)

Meetings

The CAP met on eleven (11) occasions during the course of 2021 in line with the normal monthly meeting schedule.

The CAP members also attended the following sessions presented by Gavin Leydon, Norman Waterhouse Lawyers:

- A Council Assessment Panel Induction prior to the 20 January CAP meeting.
- A Planning Reform briefing on 17 February 2021.

Meeting Attendance

Attendance of members at CAP meetings during 2021 is noted in Table 1.

Member	Attended	Apology
Shanti Ditter (Presiding Member)	11	0
Jane Strange	10	1
Kon Corolis	11	0
Michael Arman	11	0
Graham Nitschke	10	1
Jasmine Wood (deputy Council member)	1	N/A
Jim Gronthos (deputy independent member)	0	N/A
Heath Edwards (deputy independent member)	0	N/A

Table 1: Attendance Record

A quorum was achieved for all meetings of the CAP during the year.

Draft version presented to the Council Assessment Panel (A2781413)

14 December 2021

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During a declared State Emergency the CAP Meeting Procedures allow for CAP Meetings to be held in alternative locations or via an electronic platform. During 2021 all meetings were held in person in the George Robertson Room in the City of West Torrens Civic Centre. However for three meetings a small number of attendees appeared via electronic platform (Zoom) as they were unable to attend in person, these included individual CAP members, representors and applicants.

Independent Member Accreditation

From 19 March 2021, independent members were required to be registered as an Accredited Professional - Level 2 Planning with the Accreditation Authority, pursuant to the *Planning, Development and Infrastructure Act 2016*.

All independent members held the required accreditation at meetings attended in 2021.

Governance Matters

From 19 March 2021, with the introduction of the *Planning, Development and Infrastructure Act 2016* the Council Assessment Panel (CAP) became a 'relevant authority' in its own right. In particular the CAP is the relevant authority for the assessment of publically notified performance assessed planning consent applications lodged from 19 March 2021 onwards.

Through 2021, the CAP continued to assess 'transitional' applications lodged under the *Development Act 1993* prior 19 March 2021 under its delegations from Council.

In preparation for the commencement of the *Planning, Development and Infrastructure Act 2016* the CAP considered and implemented the following:

- Delegations of powers as relevant authority to the Assessment Manager and Chief Executive Officer for the assessment and determination of specific types of applications and procedural decision making to facilitate the processing of applications.
- A 'standing referral' for all building consent applications to the Council.
- A CAP *Policy for Assessment Panel Review of Decision of Assessment Manager*.
- Updated the *Procedures at Council Assessment Panel Meetings*.

An information report on the above matters was presented to Council at its meeting on 2 March 2021.

Development Applications

In 2021 CAP considered and determined 39 development applications (DAs) for planning consent (not including confidential items). This number represents a slight decrease from the number of applications determined by CAP in 2020 (46 DAs). This decrease may be attributed to the implementation of the new Planning Reform and changes to the types of developments that are allocated to CAP for assessment.

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The CAP's decisions for 2021 are further disaggregated in the following tables.

Decision	Number	Percent
Approved with recommendation	33	84.6%
Approved against recommendation	0	0%
Refused with recommendation	5	12.8%
Refused against recommendation	1	2.6%
Deferred	0	0%
TOTAL	39	100

Table 2: Summary of Decisions

Table 2 shows that CAP decisions were in line with the staff recommendation.

This result is to be expected given that most development proposals go through a process of negotiation between Council's planning staff and applicants to ensure compliance with the relevant policy provisions before they are presented to the CAP. Council's planning staff have also taken on feedback from CAP members which is conveyed to applicants during negotiations.

Figure 1 below presents this information in chronological form and shows the range in the number of applications considered by CAP throughout the year.

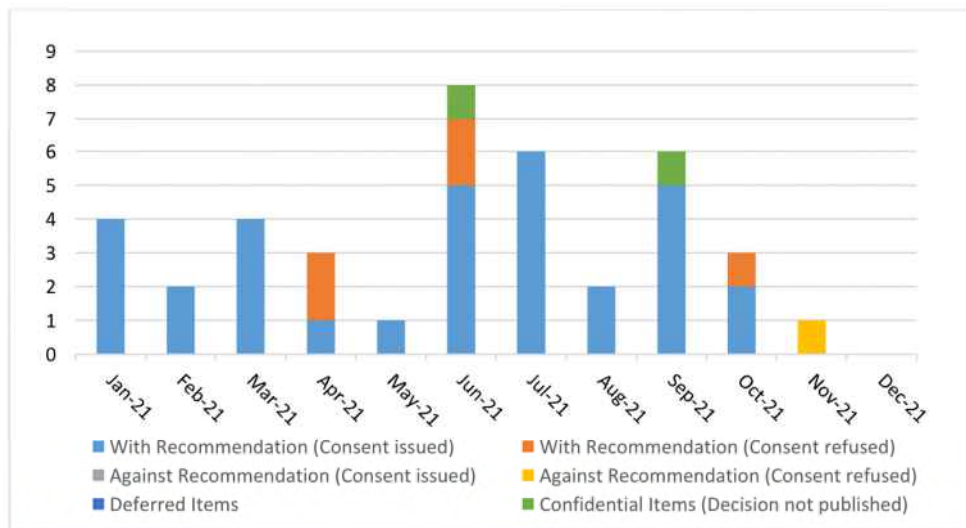


Figure 1: Summary of Decisions

Table 3 (below) shows the type of developments that were determined by the CAP during 2021. Land division, dwellings and combined land division/built form proposals - most of which were for infill development - were especially prominent and constituted nearly 60% of applications determined by the CAP. Commercial/industrial, change of use and signage applications comprised approximately 30% of applications, an increase from 2020. There were three applications for tree damaging activity (including removal of significant/regulated trees) which is comparable to two applications in 2020.

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Type of development	Number	Percent
Land division	4	10.3%
Residential flat building	5	12.8%
Other dwelling (new or addition)	5	12.8%
Combined residential built form and land division	9	23.1%
Commercial/Industrial	6	15.4%
Change of Use	3	7.7%
Signage	3	7.7%
Tree damaging activity	3	7.7%
Community	1	2.6%
TOTAL	39	

Table 3: Types of Development

Of note, CAP assessed an application for the redevelopment of the former Council depot, involving the adaptive reuse of a local heritage place.

Confidential Items

The CAP determined two confidential items during the period, both of which were supported in line with the staff recommendation. Both conditional items related to development applications under appeal to the Environment, Resources and Development Court.

Reviews of Assessment Manager Decisions

From 19 March 2021, with the introduction of the *Planning, Development and Infrastructure Act 2016* a new avenue of review was introduced. Applicants may now apply to the CAP for the review of decisions of the Assessment Manager. The CAP adopted a policy to guide the processing of such reviews.

Figure 2 below shows one application was made for a review of the Assessment Manager's decision and the CAP determined to affirm the Assessment Manager's decision in this instance.

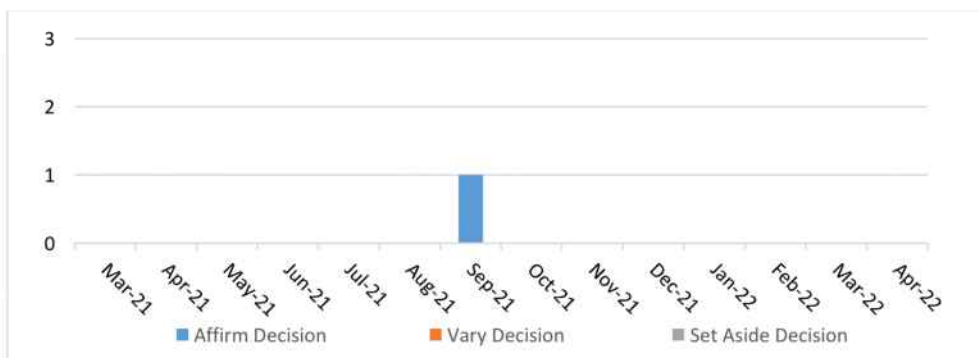


Figure 2: Summary of Reviews of Assessment Manager Decisions

2021 City of West Torrens Council Assessment Panel Annual Report**5**Appeals

In 2020, a development decision of the CAP was appealed to the Environment, Development and Resources Court which was resolved during 2021. The details are as follows:

- *Variation to Development Application 211/356/2016 - Increase Group 'C' building from 3 storeys to 5 storeys containing a total of 98 dwellings (38 additional dwellings) at 48-50 Davenport Terrace, Richmond.*

An appeal hearing was held in the Environment, Resources and Development Court. The Court upheld CAP's decision to refuse the application and the appeal was dismissed.

Two development decisions of the CAP were appealed to the Environment, Development and Resources Court during the year compared to one in 2020 and four in 2019. The applications appealed were:

- *Demolition of existing dwelling and associated structures and construction of two (2) x residential flat buildings, the front building comprising two (2) x three-storey dwellings and one (1) x two-storey dwelling, all including a roof top alfresco area and associated car parking; the rear building comprising three (3) x two-storey dwellings all including a roof top alfresco area and associated car parking; front fencing to a maximum height of 1.4 metres and perimeter retaining walls and fencing to a maximum combined height of 2.3 metres*

The CAP refused the application and this decision was appealed. Following the submission of amended plans, the appeal was resolved through the conciliatory conferencing process and Development Plan Consent was granted.

- *Removal of a significant tree - Eucalyptus camaldulensis (River Red Gum)*

The CAP refused the application and this decision was appealed. Following the submission of additional information, the appeal was resolved through the conciliatory conferencing process and Development Plan Consent was granted.

Feedback to Council from the Council Assessment Panel

The following policy advice has been provided by CAP members in consideration of their assessment of development applications during the past 12 months:

- At a strategic level, development is largely occurring as Council's overall strategic land use framework envisaged, in that CAP is seeing the infill development and more substantive change in locations that are broadly well suited for it, whilst retaining valued character elements in other parts of the Council area. There are of course exceptions to this, but it's worth noting that the quantum of development is largely occurring as envisaged.
- The Council Assessment Panel's key challenge is with the quality of new development, and the insufficient policy levers under the Planning and Design Code (and former Development Plan) to drive quality design. This issue is particularly acute for the smaller allotment medium density developments. The CAP has observed applications with better design outcomes in the former Urban Corridor Zone with, for example, developments up to four storeys, compared with the former Medium Density Policy Area 19 (now mostly Housing Diversity Neighbourhood Zone), within which the CAP are frequently assessing proposals that provide a poor residential amenity for existing residents, as well as future occupants.

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- The trends that the CAP have raised in previous reports to Council regarding overdevelopment of sites, lack of private open space, insufficient landscaping, lack of car parking, insufficient space for vehicle movements and poor design overall remain. One of the issues impacting development is that although a development may meet all the quantitative standards, the design, living and amenity outcomes are not necessarily ideal and these need to be given greater attention by the Planning and Design Code. It is recommended to Council that it approaches the State Planning Commission with a view to significantly improve design standards for medium and high density residential development, including consideration for the following specific policy outcomes:
 - Developments of certain size and site coverage be required to incorporate sustainability initiatives in the design e.g. solar power, cross ventilation, resilient landscape that requires less water usage, inverter air-conditioning units, colour of roofing materials that reduce radiant heat to residences, etc.
 - A more considered and minimum requirement on building height where greater heights are allowed would certainly contribute to increased landscape provision, less covered area reducing the impact of compressed development hard surfaces, easier vehicle movements and better defined entrances to each dwelling.
 - Consideration of secondary frontages / elevations of dwellings similarly as important as primary frontages to incorporate meaningful articulation, materiality contributing to the design outcome of the development.
 - Seek to increase the effectiveness of the affordable housing policies in the Planning and Design Code, including the consideration of the concentration of affordable housing, and to strengthen policies for affordable housing particularly in higher density zones.
 - Bin enclosures in multiple dwelling applications not to be located adjacent to private open space rear yard of any dwelling, nor alongside a dwelling or front boundary of a property near pedestrian foot paths as not to diminish the amenity of the dwelling occupants or community.
 - Address development of existing laneways in established suburbs e.g. Mile End, so that each application contributes financially or through design criteria and surface treatments to driveways / cross overs that enhances the urban design uplift of the lane which creates a safe shared pedestrian and vehicle zones with sound passive security principles and landscaped solutions.
 - Review of required car parking and garaging (width and depth) dimensions to mitigate the potential for garaging not accommodating residents' vehicles and avoid putting additional pressure to on street car parking. This concern is raised by most representors heard by CAP on medium density applications.
 - Require applicants to provide an inspection report prior to construction from a qualified arborist that the tree protection measures have been installed in accordance with the conditioned approval prior to site works commencing, trenching activities have not breached root zones before they are covered up, and a sufficient impervious perimeter to tree drip zone has been provided to limit hard surfaces compromising trees' ability to maintain good health.

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- Set criteria for car parking audits accompanying development applications for the days and times the audit data is collected. The consequence of inadequate traffic audit data is a risk that a development may be approved which may allow for lesser car parking and likely put more strain to the on street car parking and negatively impact surrounding residents.
- Community awareness of the complexity of the South Australian Planning System remains an issue. There is a need for ongoing education around the Planning and Design Code, roles of councils and State Government in planning, and the reasons why development is being approved that is of a different character to the existing character. There is also a need to support residents to understand their rights and responsibilities as the owner of a significant tree.
- The Council Assessment Panel has an interest in undertaking post development reviews of previous decisions, including undertaking site visits (potentially with elected members) of approved developments over the past five years. This would include a broad sample of developments across the council area and would help to ground truth the extent to which issues raised during the planning assessment have/have not materialised post-development.

Conclusion

This year has seen significant change with the introduction of the *Planning, Development and Infrastructure Act 2016*. Two key changes under the new legislative framework included the Council Assessment Panel becoming a 'relevant authority' in its own right and the introduction of the Planning and Design Code. Through this transition the CAP has benefited from new members with different professional expertise, and perspectives.

The CAP has appreciated the sustained quality of reporting from planning staff, and particular efforts to support the CAP to proactively prepare for the transition to the *Planning, Development and Infrastructure Act 2016*.

Members of the CAP would like to express their appreciation to Elected Members, the Chief Executive Officer, Assessment Manager and planning staff of the City of West Torrens for their ongoing support and assistance.

11 MEETING CLOSE