

CITY OF WEST TORRENS



# Notice of Panel Meeting

Notice is Hereby Given that a Meeting of the  
**COUNCIL ASSESSMENT PANEL**

will be held in the George Robertson Room, Civic Centre  
165 Sir Donald Bradman Drive, Hilton

on

**TUESDAY, 8 OCTOBER 2019  
at 5.00pm**

**Donna Ferretti  
Assessment Manager**

## **City of West Torrens Disclaimer**

### **Council Assessment Panel**

Please note that the contents of this Council Assessment Panel Agenda have yet to be considered and deliberated by the Council Assessment Panel therefore the recommendations may be adjusted or changed by the Council Assessment Panel in the process of making the formal Council Assessment Panel decision.

**Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.**

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# **1 MEETING OPENED**

## **1.1 Evacuation Procedures**

# **2 PRESENT**

# **3 APOLOGIES**

## **Officers:**

Dr Donna Ferretti (Assessment Manager)

# **4 CONFIRMATION OF MINUTES**

## **RECOMMENDATION**

That the Minutes of the meeting of the Council Assessment Panel held on 10 September 2019 be confirmed as a true and correct record.

# **5 DISCLOSURE STATEMENTS**

In accordance with section 7 of the *Assessment Panel Members – Code of Conduct* the following information should be considered by Council Assessment Panel members prior to a meeting:

A member of a Council Assessment Panel who has a direct or indirect personal or pecuniary interest in a matter before the Council Assessment Panel (other than an indirect interest that exists in common with a substantial class of persons) –

- a. must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the panel; and
- b. must not take part in any hearings conducted by the panel, or in any deliberations or decision of the panel, on the matter and must be absent from the meeting when any deliberations are taking place or decision is being made.

If an interest has been declared by any member of the panel, the Assessment Manager will record the nature of the interest in the minutes of meeting.

## 6 REPORTS OF THE ASSESSMENT MANAGER

### 6.1 7 Glenburnie Terrace, PLYMPTON

Application No 211/633/2019

Appearing before the Panel will be:

Representor: **Matthew Rechner** of 6 Glenburnie Tce, Plympton wishes to appear in support of the representation.

**Karen Klingberg** of 14 Glenburnie Tce, Plympton wishes to appear in support of the representation.

Applicant: **Phuong Pham** of Advanced Development Group Solutions wishes to appear in response to the representations.

### DEVELOPMENT APPLICATION DETAILS

DESCRIPTION OF DEVELOPMENT	Combined Land Division - Community Title; SCAP No. 211/C067/19; Create six additional allotments and common property; demolition of existing dwellings and construction of a three storey residential flat building containing seven dwellings
APPLICANT	Alexander and Symonds
LODGEMENT DATE	4 July 2019
ZONE	Urban Corridor Zone
POLICY AREA	Boulevard Policy Area 34
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 2
REFERRALS	Internal <ul style="list-style-type: none"> <li>• City Assets</li> <li>• City Operations</li> </ul> External <ul style="list-style-type: none"> <li>• State Commission Assessment Panel (SCAP)</li> <li>• SA Water</li> </ul>
DEVELOPMENT PLAN VERSION	Consolidated 12 July 2019
DELEGATION	<ul style="list-style-type: none"> <li>• The relevant application proposes residential development of three or more storeys above finished ground level.</li> <li>• The relevant application is for a merit, Category 2 or Category 3 form of development, representations have been received and one or more representors wish to be heard on their representation.</li> </ul>
RECOMMENDATION	Support with conditions
AUTHOR	Jordan Leverington



## **SUBJECT LAND AND LOCALITY**

The subject land is formally described as Allotment 178 in Deposited Plan 3690 in the area named Plympton, Hundred of Adelaide, Volume 5143 Folio 616, more commonly known as 7 Glenburnie Terrace, Plympton. The subject site is rectangular in shape with a 17.7 metre (m) wide frontage to Glenburnie Terrace, a depth of 39.6m and a site area of 700.5 square metres (m<sup>2</sup>).

There are no easements, encumbrances or Land Management Agreements on the Certificate of Title and there are no regulated trees on the subject site or on adjoining land that would be affected by the development.

The site is relatively flat and currently contains a single storey detached dwelling, grassed front and rear yards interspersed with semi mature trees and bushes. The street verge directly in front of the subject site contains a street tree and fire post.

The locality is residential in nature comprised of a variety of dwelling types. The most prevalent being residential flat buildings and detached dwellings. The residential flat buildings are concentrated south of the subject site adjacent Anzac Highway.

Glenburnie Terrace forms the boundary between the Urban Corridor Zone, Boulevard Policy Area 34 and the Residential Zone, Medium Density Policy Area 18.

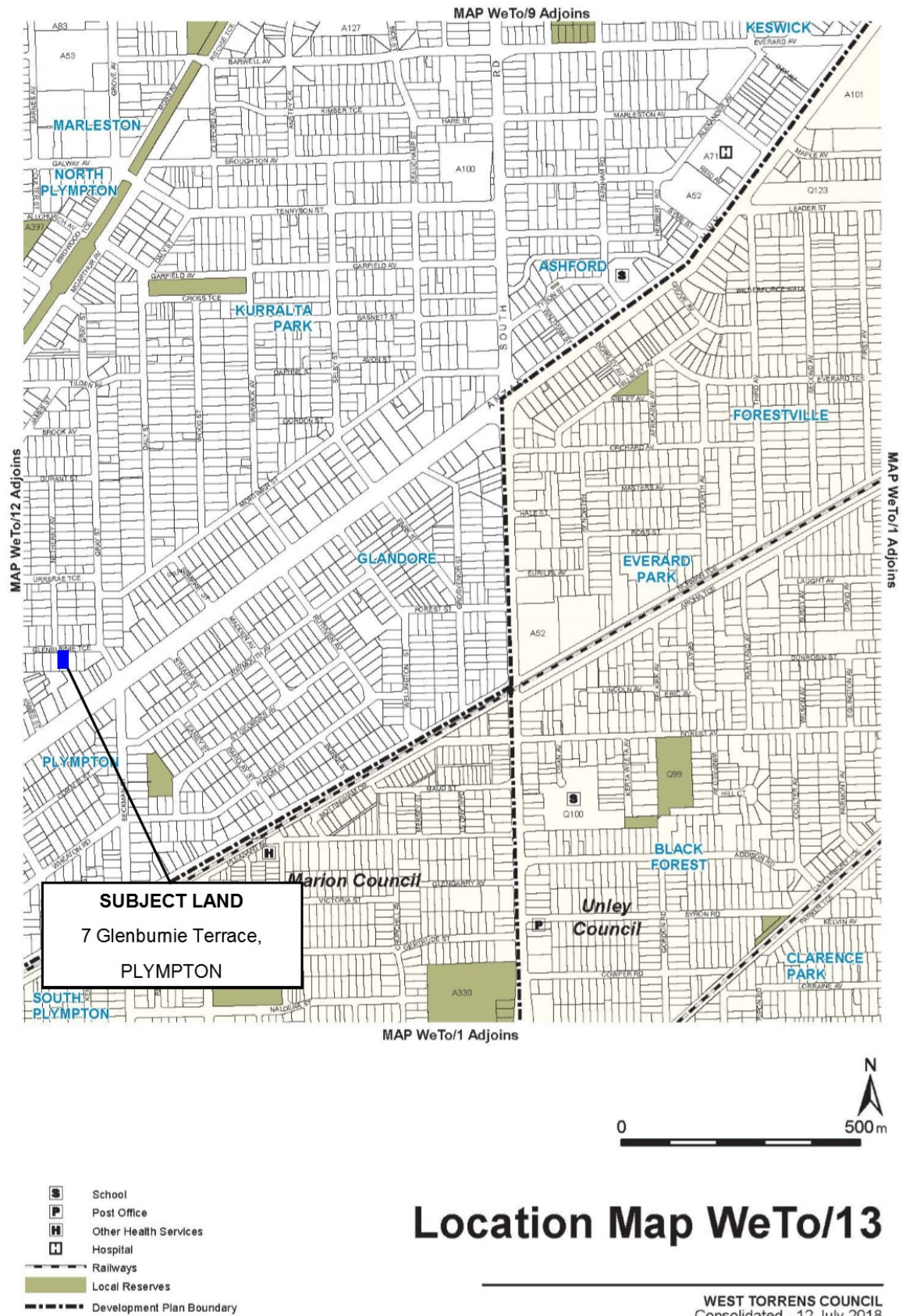
The locality is not located within a flood prone area.

The amenity of the locality is affected by the close proximity of Anzac Highway and associated noise and air quality impacts as well as the five storey residential flat buildings noted above.

The subject land and locality are shown on the aerial imagery and maps below.







## PROPOSAL

Development Plan Consent and Land Division Consent is sought for a land division and construction of a three storey residential flat building containing seven dwellings, and associated landscaping.

Each dwelling will have ground floor vehicular and pedestrian access and contain the following attributes:

- Two bedrooms;
- Single garage;
- Study;
- Two bathrooms;
- Open plan living, kitchen and dining area; and
- Balcony.

The private open space (POS) is to be split between the ground floor area behind the study and the first floor balcony area.

There is one visitor car park and three bike parks located in front of the residential flat building. Water meters and letterboxes will be located within the common area.

Waste will be collected by Council with each of the dwellings sharing general waste, recycling and green waste bins. There will be a maximum of eight bins presented to the street each week comprised of 4 waste and then either three green or four recycling bins.

The relevant plans and documents are contained in **Attachment 2**.

## PUBLIC NOTIFICATION

The application is a Category 2 form of development pursuant to Schedule 9 of the *Development Regulations 2008* or Procedural Matters section of the Urban Corridor Zone.

<b>Properties notified</b>	138 properties were notified during the public notification process.
<b>Representations</b>	Two representations were received.
<b>Persons wishing to be heard</b>	Two representors who wish to be heard. <ul style="list-style-type: none"> <li>• Matthew Rechner; and</li> <li>• Karen Klingberg.</li> </ul>
<b>Summary of representations</b>	Concerns were raised regarding the following matters: <ul style="list-style-type: none"> <li>• Excessive bulk and scale of the proposal;</li> <li>• Overlooking and visual privacy;</li> <li>• Dwelling density;</li> <li>• Building height;</li> <li>• Lack of on-site parking</li> <li>• Disparity with existing character;</li> <li>• Lack of on-street car parking;</li> <li>• Impacts on waste collection; and</li> <li>• Impacts on the amenity of the streetscape.</li> </ul>
<b>Applicant's response to representations</b>	<ul style="list-style-type: none"> <li>• The proposed built form of three storeys is well below the 8 storey limit supported in this zone and policy area;</li> <li>• The proposed building setbacks meet or exceed the minimum set out in the zone and policy area;</li> <li>• Dwelling density is 100 dwellings per hectare which meets the minimum density sought in the policy area;</li> </ul>

	<ul style="list-style-type: none"> <li>• As the proposed building is 3 storeys in height, no privacy screening is necessary in accordance with PDC 27 of the Residential Development section of the Development Plan;</li> <li>• The proposal provides 8 on site car parks and two on street car parks, which is considered sufficient given the proximity to public transport on Anzac Highway and Gray Street; and</li> <li>• The proposal provides adequate areas for the storage of bins on site and will utilise a shared bin system which will be collected by the standard Council service.</li> </ul>
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A copy of the representations and the applicant's response is contained in **Attachment 3**.

### INTERNAL REFERRALS

Department	Comments
<b>City Assets</b>	<ul style="list-style-type: none"> <li>• Verge interaction and FFL have been assessed as satisfying Council requirements;</li> <li>• 0.3m wide flaring should be added to the driveway crossover between the kerb and front property boundary;</li> <li>• Traffic manoeuvrability has been assessed as satisfactory; and</li> <li>• Stormwater collection and reuse method is satisfactory.</li> </ul>
<b>City Operations</b>	<ul style="list-style-type: none"> <li>• The shared bin arrangement is satisfactory.</li> </ul>

### EXTERNAL REFERRALS

Department	Comments
SCAP	<ul style="list-style-type: none"> <li>• No concerns were raised and standard conditions imposed.</li> </ul>
SA Water	<ul style="list-style-type: none"> <li>• No concerns were raised and standard conditions imposed.</li> </ul>

A copy of the relevant referral responses are contained in **Attachment 4**.

### RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the Urban Corridor Zone and, more specifically, Boulevard Policy Area 34 as described in the West Torrens Council Development Plan.

The relevant Desired Character statements are as follows:

**Urban Corridor Zone - Desired Character**

*This zone will contain an innovative mix of medium density (45-70 dwellings per hectare) and high density (70-200 dwellings per hectare) residential development, together with community and employment land uses, along the Port Road, Anzac Highway and Henley Beach Road corridors. The combination of land uses will vary within these corridors. Some locations will contain a genuine land use mix with ground floor shops, restaurants and offices, and upper level residential, while other areas will give primacy to residential development. Other parts of the zone will have a strong employment focus.*

*The function of main roads in the zone, particularly Port Road and Anzac Highway, as major transport corridors will be protected by providing access to allotments from secondary road frontages and rear access ways as much as possible. Parking areas will be consolidated, shared (where possible) and screened from the street or public spaces. Allotments with car parking fronting Port Road, Anzac Highway and Henley Beach Road will be redeveloped with built form closer to the road and reconfigured car parking areas.*

*As one of the key zones in the City of West Torrens where there will be transformation in built form, new buildings will be recognised for their design excellence. These buildings will establish an interesting pedestrian environment and human-scale at ground level through careful building articulation and fenestration, verandas, balconies, canopies and landscaping. In general, the greatest height, mass and intensity of development will be focussed at the main road frontage. Buildings of 3 or more storeys will be the predominant built form. It is for these reasons that dwellings other than detached dwellings will be the predominant form of residential development.*

*Overlooking, overshadowing and noise impacts will be moderated through careful design, Impacts on adjoining zones where development is lower in scale and intensity will be minimised through transition of building heights and setbacks, judicious design and location of windows and balconies, and the use of landscaping. The transition of building heights and setbacks, and judicious design is especially important adjacent Character Policy Areas, including those Character Policy Areas at Glandore and Ashford. The use of blank walls in these transitional areas, especially at the rear and side of allotments, will be avoided. Plant and service equipment will be enclosed and screened from view from the street and neighbouring allotments.*

*Where buildings are set back from main roads, landscaping will contribute to a pleasant pedestrian environment and provide an attractive transition between the public and private realm. Large scale development in the zone will facilitate the establishment of areas of communal and public open space, and create links with existing movement patterns and destinations in the zone. Front fencing in the zone will be kept low and/or visually permeable. Some parts of the zone, including allotments in Thebarton and Keswick, are potentially contaminated because of previous and current industrial activities. In these circumstances, development is expected to occur on a precautionary basis if site contamination investigations identify potential site contamination, particularly where it involves sensitive uses such residential development.*

*The Thebarton brewery has potential to cause nuisance to future users and residents within this zone through noise and odour. To mitigate potential adverse impacts, residential development north of Smith Street that is likely to be sensitive to brewery operations should generally be avoided unless interface mitigation measures have been implemented (or will be implemented within an acceptable period) such that the anticipated impacts are within acceptable limits. Noise and air amenity with the zone is not expected to be equivalent to that expected from living in a purely residential zone.*

Objectives	1, 2, 4, 5, 6, 7, 9
Principles of Development Control	1, 4, 5, 6, 7, 8, 9, 11, 13, 15, 16, 17, 19, 20

**Boulevard Policy Area 34 - Desired Character**

*The policy area will contain a mix of land uses that complement the function of Port Road as a strategic transport route linking central Adelaide with the north western suburbs, and Anzac Highway linking central Adelaide with Glenelg.*

*The redevelopment of existing commercial and industrial allotments into medium-to-high scale, mixed-use development will occur. Where development has a mix of land uses, non-residential activities such as shops, offices and consulting rooms will be located on lower levels with residential land uses above. In order to achieve the desired transformation of the policy area, dwellings other than detached dwellings will be the predominant form of residential development.*

*A mix of complementary land uses will assist in extending the usage of the policy area beyond normal working hours to enhance its vibrancy and safety.*

*Development will take place at medium and high densities, at a scale that is proportionate to the width of Port Road and Anzac Highway respectively. To achieve this, development will take place on large, often amalgamated allotments. Vehicle access points will be located off side streets and new rear laneways where possible, so that vehicle flows, safety and efficient pedestrian movement along Port Road and Anzac Highway are maintained.*

*Pedestrian areas will be enhanced to maximise safety and strong links will be made between development and tram stops along Port Road, and Bonython Park.*

*While the use and address of buildings will be designed to be easily interpreted when driving in a vehicle, the footpath will be sheltered with awnings, verandas and similar structures. Buildings of up to eight storeys will have a strong presence to Port Road and Anzac Highway. At lower levels, buildings will have a human scale through the use of design elements such as balconies, verandas and canopies. Development on corner allotments will enhance the gateway function of such corners by providing strong, built-form edges combined with careful detailing at a pedestrian scale to both street frontages.*

*Podium elements, where higher floors of the building are set back further than lower level floors, may be used to improve air quality (through greater air circulation), as well as enhancing solar access, privacy and outlook for both the residents of the building and neighbours.*

*Buildings along Port Road will have zero setback from the front boundary in order to establish a strong and imposing presence to the road, while short front setbacks along Anzac Highway will allow for some landscaping to contribute to a more open landscaped character.*

*On-site vehicle parking will not be visible from Port Road and Anzac Highway, by locating parking areas behind building facades and shielding undercroft parking areas with landscaping and articulated screens.*

Objectives	1, 2, 4
Principles of Development Control	1, 2, 3, 5, 6

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

## QUANTITATIVE STANDARDS

The proposal is assessed for consistency with the quantitative requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
NET SITE DENSITY <i>Urban Corridor Zone</i> <i>PDC 5</i>	100 dwellings per hectare (min.)	100 dwellings per hectare <b>Satisfies</b>
PRIMARY STREET SETBACK <i>Urban Corridor Zone</i> <i>PDC 17</i>	2m	2.93m <b>Satisfies</b>
SIDE SETBACK <i>Urban Corridor Zone</i> <i>PDC 19</i>	No minimum up to 2 storeys and 3m above this height	Ground floor - 3m First floor - 2m Second Floor - 3m <b>Satisfies</b>
REAR SETBACK <i>Urban Corridor Zone</i> <i>PDC 19</i>	0m	0m <b>Satisfies</b>
STORAGE <i>Residential Development</i> <i>PDC 31</i>	8m <sup>3</sup> (min.)	9m <sup>3</sup> (under stairs and in study) <b>Satisfies</b>
BUILDING HEIGHT <i>Urban Corridor Zone</i> <i>PDC 13</i>	8 storeys or 32.5m	3 storeys <b>Satisfies</b>
ON-SITE CAR PARKING <i>Urban Corridor Zone</i> <i>PDC 20</i>	9 car parks (1.25 per dwelling)	8 car parks <b>Does Not Satisfy</b>
ON STREET CAR PARKING <i>Land Division</i> <i>PDC 12</i>	4 car parks (1 per 2 allotments)	2 car parks <b>Does Not Satisfy</b>
BICYCLE PARKING <i>WeTo/7 Off-street Bicycle Parking Requirements for Designated Areas</i>	3 bicycle parks	4 parks provided <b>Satisfies</b>
LANDSCAPING <i>Medium and High Rise Development (3 or more storeys)</i> <i>PDC 23</i>	49m <sup>2</sup> of deep soil (min.) 3m (min. dimension)	70m <sup>2</sup> <b>Satisfies</b>



## ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### Desired Character and Pattern of Development

The subject site is located within the Urban Corridor Zone which has a primary focus to significantly increase residential densities in mixed use forms of development. Non-residential land uses are envisaged on the ground floor with residential land uses above. The site is also within Boulevard Policy Area 34 which generally seeks the highest density and building height of all the Urban Corridor Zone Policy Areas.

In most instances, the Urban Corridor (UrC) Zone is located along the edges of major transport corridors such as Port Road, Anzac Highway and James Congdon Drive. However in some instances, the UrC Zone extends deeper into adjoining areas accommodating allotments with no direct frontage to a main transport corridor. This has been done in order to encourage the amalgamation of allotments into larger sites that will accommodate more substantial developments.

The subject site is one of the properties that has no direct access to Anzac Highway and is located a street back from it. As previously mentioned, Glenburnie Terrace forms the boundary between the Urban Corridor Zone, Boulevard Policy Area 34 (southern side) and the Residential Zone, Medium Density Policy Area 18 (northern side). Each of the zones and policy areas support different built forms, land uses, densities and setbacks.

The proposed three storey built form is consistent with what could be achieved in Medium Density Policy Area 18. Unsurprisingly, the pattern of development in the local area is as varied as the zoning that relates to it with examples of semi-detached dwellings, detached dwellings and residential flat buildings. Allotments are similarly varied in shape and size and reflect the built form on each site.

In consideration of this diversity in allotments and built form, the proposed development is considered to be satisfactorily consistent with the Desired Character of the Zone and Policy Area, particularly as it constitutes a form of higher density residential development.

### Built Form

The Zone and Policy Area generally encourage structures to be built up to eight storeys or 32.5m in height, except when located adjacent the Residential Character Glandore Policy Area 24 or between Syme Street and South Road where the maximum allowable height is three storeys or 12.5m. In order to maximise the developable area, development can be built up to side and rear boundaries for the first two storeys and then be setback 3m from these boundaries for any additional storey. The proposed development is consistent with the building height and minimum setback provisions as demonstrated in the quantitative table above.

Dwelling 1 has been orientated to the street by having the front door and 10 windows facing Glenburnie Terrace. This is good outcome and is supported by Principle of Development Control (PDC) 8 of the Residential Development section of the Development Plan. This orientation allows casual overlooking of Glenburnie Terrace which is supported by Objective 1, PDCs 2 and 3 of the Crime Prevention section of the Development Plan as it helps create a safe and crime resistant environment.

As this is a solely residential development, there are some Policy Area provisions which are not met, including:

- PDC 5, having a 4.5m floor to ceiling height on the ground floor for adaptive reuse; and
- PDC 6, having 50% of the ground floor frontage of the building being visually permeable.

Neither of these design characteristics would be beneficial for a purely residential development and given the location of the subject site, a ground floor commercial activity is not expected to be viable. Accordingly, the failure of the proposal to meet these provisions is not considered fatal to the application.

### **Amenity**

There are a number of amenity issues raised by the representors in relation to this development such as potential overlooking, impact to on-street car parking and the visual bulk and scale of the building.

As the proposed building is three storeys in height, PDC 27 of the Residential Development section states that visual privacy measures such as obscure glazing and privacy screening are not necessary. The applicant has acknowledged the representors' concerns but has chosen not to make any changes to the proposal.

Overlooking is only considered to have a detrimental impact on the adjoining property to the east (5 Glenburnie Terrace). This is because the property to the south is a five storey residential flat building and the property to the west has significant tree cover. It should be noted that no representation has been received from the owners/occupants of this property.

Those making representations are located at 6 and 14 Glenburnie Terrace. As these properties are located on the opposite side of Glenburnie Terrace to that of the subject site, no unreasonable overlooking to habitable rooms or POS is considered to occur.

The proposed residential flat building is not the tallest building in the locality, however it will be the tallest building on the street, with the majority of built form being single storey in nature. This is a concern of the representors who fear that the three storey height will detrimentally impact on the streetscape.

The proposed residential flat building will incorporate a variety of colours and materials into its design. This helps break up the visual mass and adds interest to the built form. The materials proposed are:

- Recycled face brick (red);
- Texture coated wall (off white and dark grey);
- Axon cladding (stained timber finish);
- Glass; and
- Aluminium (natural anodised).

All of the proposed materials are hard wearing, will not require frequent maintenance and the colours chosen will not cause a reflectivity concern. For these reasons it is consistent with PDC 6 of the Medium and High Rise Development (3 or more storeys) section of the Development Plan.

### **Landscaping**

A landscaping plan and cover letter provided by LCS landscapes demonstrates that landscaping in the courtyard of each dwelling incorporates a 12m<sup>2</sup> landscaped area in addition to the 13m<sup>2</sup> of deep soil area provided in the common areas of the development.

PDC 23 of the Medium and High Rise Development (3 or more storeys) section calls for a minimum of 7% of the site to be used as deep soil areas capable of accommodating deep root vegetation such as trees. The proposal has exceeded this minimum and therefore satisfies this provision.

Each dwelling has an 11m<sup>2</sup> of deep soil area located in the rear yard that will accommodate a Lagerstroemia Indica and some low plantings like Liriope 'Evergreen giant' and Nandina Domesitca 'Moonbay'. There is also a smaller deep soil area located in front of the proposed building which is shown to accommodate a Zelkova serrata 'Musashino' feature tree and smaller shrubs beneath it.

The driveway will incorporate a number of garden beds around the periphery of the common area. These beds are broken up by bin storage areas and garage access. The size of the plants will not provide shade to the driveway, but they will help to improve the amenity of an otherwise purely hard surfaced area.

The proposal is considered to have satisfied the relevant landscaping provisions.

### **Parking and Access**

The Urban Corridor Zone has different car parking requirements to that of other residential zones. Table WeTo/6 outlines that for two bedroom dwellings, 1.25 carparks per dwelling is required. This equates to a total of 9 car parks being required for the proposed development. The proposal has incorporated 8 car parks into the design with each dwelling having an enclosed garage and one visitor car park located at the front of the property.

This deficiency of one on-site car park is not considered fatal to the application due to the proximity of high frequency public transport (buses and tram) on Anzac Highway as well as the availability of on street parking, two spaces of which are located immediately in front of the subject site.

Vehicle movements have been demonstrated as satisfactory in the CIRQA traffic report provided by the applicant. Council's traffic engineers have reviewed and are satisfied with the report's findings.

### **Waste Management**

The proposal will utilise a shared bin arrangement meaning that all dwellings will use 4 general waste, 4 recyclables and 4 green waste bins. The bins will be stored in the common driveway in the south-east corner of the allotment. This is consistent with PDC 5 of the Waste section of the Development Plan which calls for a suitable sized area being made available for the storage of waste receptacles.

Up to eight bins will be presented to the kerb for Council collection each week. This is supported by Council as there is sufficient kerbside space to accommodate the bins.

### **Stormwater Management**

Stormwater runoff from the roof will be directed to 3000L rainwater tanks located in the courtyard of each dwelling. This equates to a total of 21,000L of water being stored for reuse by this proposal. This is well in excess of the mandatory 7000L required by the Building Code of Australia and will reduce the impact of the proposal on Council stormwater infrastructure.

### **SUMMARY**

The proposed three storey residential flat building is an envisaged development within the Zone and Policy Area. While the density of the proposal is marginally below the minimum 100 dwellings per hectare sought in the policy area, it is still consistent with the desired character as it is proposing high density (i.e. >67 dwellings/hectare) development. This proposal is purely residential, but given its location away from arterial roads, it is considered a more appropriate development outcome than a mixed use development.

Having considered all the relevant provisions of the Development Plan, the proposal is not considered to be seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and warrants Development Plan Consent and Land Division Consent.

## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent and Land Division Consent for Application No. 211/633/2019 by Alexander and Symonds to undertake the Combined Land Division - Community Title; SCAP No. 211/C067/19; Create six additional allotments and common property; demolition of existing dwellings and construction of a three storey residential flat building containing seven dwellings at 7 Glenburnie Terrace, Plympton (CT 5143/616) subject to the following conditions of consent;

### Development Plan Consent Conditions:

1. The development must be undertaken, completed and maintained in accordance with the plans and information detailed in this Application except where varied by any conditions listed below.  
*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*
2. Prior to the occupation or use of the development, all driveways, parking and vehicle manoeuvring areas shall be constructed and surfaced with concrete, bitumen or paving, and shall be drained and maintained in a good condition at all times to the satisfaction of Council.  
*Reason: To provide safe and convenient parking and manoeuvring areas for users of the development.*
3. All landscaping shown on the plans forming part of this application shall be established prior to the occupation or operation of the development and shall be maintained in good health at all times to the satisfaction of Council. Any plants that become diseased or die shall be replaced with a suitable species.  
A watering system shall be installed at the time landscaping is established and operated so that all plants receive sufficient water to ensure their survival and growth.  
*Reason: To enhance the amenity of the site and locality and mitigate against heat loading.*
4. All external materials, surface finishes and colours shall be consistent with the information detailed in this application and shall be maintained in a good condition at all times to the satisfaction of Council.  
*Reason: To ensure a high standard of materials and finishes are used in the final presentation of the building.*
5. During construction, stormwater from the site shall be managed to ensure that it does not cause nuisance to any adjoining property until the site is stabilised. Temporary drainage measures shall be installed as soon as the roof is constructed to ensure debris, litter, sediment, fuels and oil products from the construction site do not enter Council's stormwater system, neighbouring properties or the road network.  
*Reason: To provide adequate protection against the possibility of stormwater inundation to neighbouring properties.*

### Land Division Consent Conditions

Nil

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**SCAP Requirements**

- 6 The financial requirements of the S A Water Corporation shall be met for the provision of water supply and sewerage services. (S A Water H0087471).

SA Water advises on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non-standard fees.

The developer must inform potential purchasers of the community lots in regards to the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

*Reason: To satisfy the requirements of the South Australian Water Corporation.*

- 7 Payment of \$43,518.00 into the Planning and Development Fund (6 allotment/s @ \$7,253 / allotment). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Department of Planning, Transport and Infrastructure marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, by cheque or credit card, at Level 5, 50 Flinders Street, Adelaide.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

- 8 A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

---

**Attachments**

1. **Relevant Principles and Objectives of the Development Plan**
2. **Plans and associated information**
3. **Representations and the applicant's response**
4. **Internal and external referrals**

**Relevant Development Plan Provisions**

<u>General Section</u>		
Crime Prevention	Objective	1
	Principles of Development Control	1, 2, 3, 5, 6, 7, 8 & 10
Design and Appearance	Objective	1
	Principles of Development Control	1, 2, 3, 4, 5, 9, 10, 11, 12, 13, 14, 15, 16, 17, 20 & 21
Energy Efficiency	Objectives	1 & 2
	Principles of Development Control	1, 2, 3 & 4
Hazards	Objectives	1, 2, 4, 7 & 10
	Principles of Development Control	1, 2, 3, 4, 5, 6 & 7
Infrastructure	Objective	3
	Principles of Development Control	1, 3, 4, 5, 6 & 8
Interface between Land Uses	Objectives	1, 2 & 3
	Principles of Development Control	1, 2 & 3
Landscaping, Fences and Walls	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4 & 6
Medium and High Rise Development (3 or More Storeys)	Objectives	1, 2, 3, 4, 5 & 7
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 14, 15, 16, 19, 20, 23, 24, 25, 26, 27 & 29
Orderly and Sustainable Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 3, 5, 6 & 7
Residential Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 18, 19, 20, 21, 23, 28, 29, 30 & 31
Transportation and Access	Objective	2
	Principles of Development Control	1, 2, 8, 9, 10, 11, 20, 21, 22, 23, 24, 30, 32, 33, 34, 35, 36, 37, 40, 41, 43, 44 & 45
Waste	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4, 5, 6 & 7



artists impression by realizestudio

PLANNING CONSENT STAGE

# Proposed Residential Development

at 7 Glenburnie Terrace, Plympton, SA  
( 7 x 3 storey dwellings )

ARCHITECTURAL DRAWING SCHEDULE

PD01	SITE PLAN
PD02	OVERALL FLOOR PLANS
PD03	DWELLING 1 -TYPE A - FLOOR PLANS
PD04	DWELLINGS 2, 4 & 6 -TYPE B1 - FLOOR PLANS
PD05	DWELLINGS 3 & 5 -TYPE B2 - FLOOR PLANS
PD06	DWELLING 7 -TYPE B3 - FLOOR PLANS
PD07	ELEVATIONS 1
PD08	ELEVATIONS 2

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Proposed Residential Development at  
7 Glenburnie Tce, Plympton for ADGS

1922
PD
.. A

PLANNING CONSENT STAGE

8 October 2019

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IMPORTANT NOTES:

STORM WATER ASSOCIATED WITH THE PROPOSED CHANGES AND PROPOSED ADDITIONS TO DISCHARGE INTO EXISTING STORM WATER SYSTEM  
- REFER TO ENGINEERS DRAWINGS

RWT 3.0KL RAINWATER TANK IN ACCORDANCE WITH COUNCIL'S REQUIREMENTS & TO MANUF'S DETAILS PLUMBED INTO RESIDENCE COLD WATER SUPPLY TO WC'S & LDY AS PER AS.3500 IN ACCORDANCE WITH THE BCA

S1 SURFACE STORMWATER SUMP AS PER ENGINEERS DRAWINGS

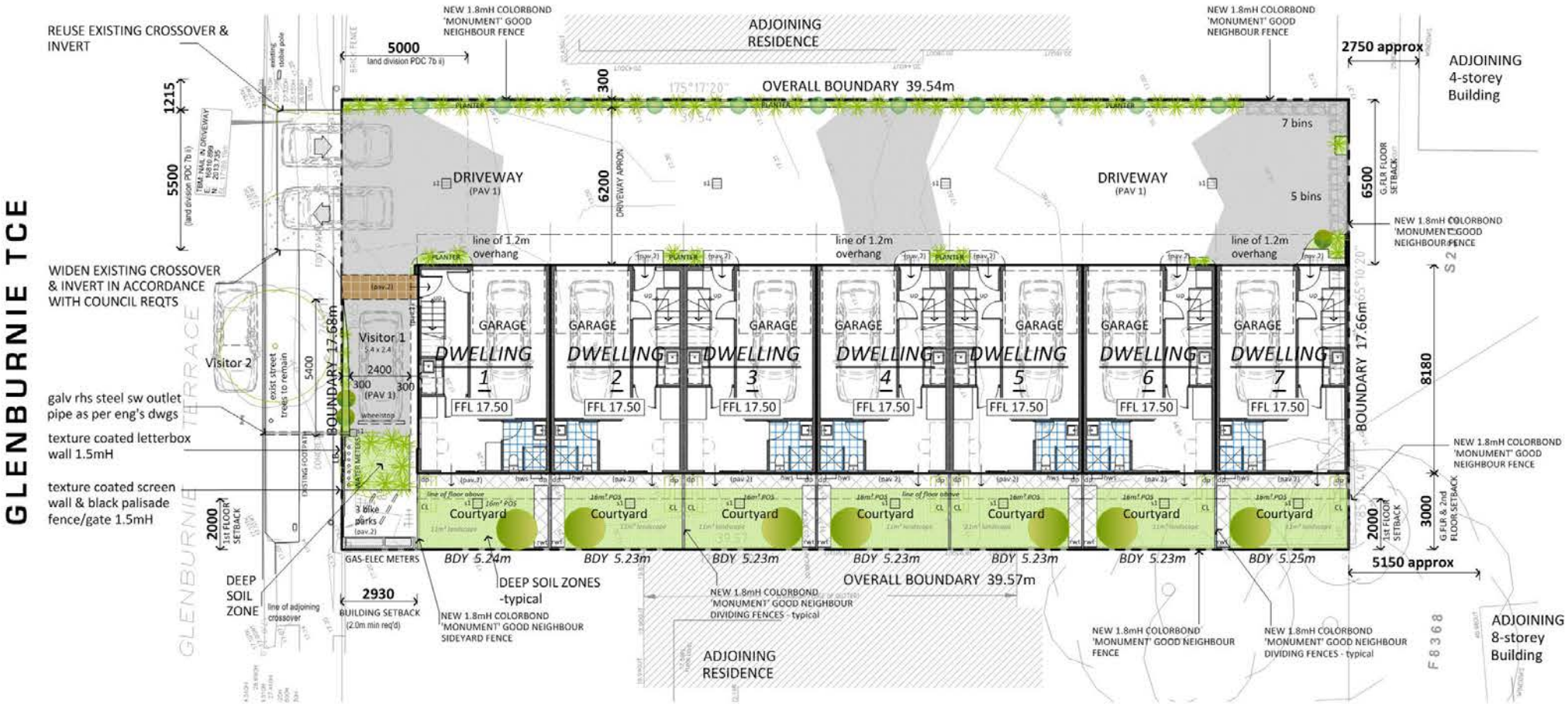
- DP NEW 90Ø DOWNPIPE CONNECTED TO SW DRAINAGE SYSTEM
- CL CLOTHESLINE PROVISION
- AC AIR CON UNIT PROVISION
- LB LETTERBOX PROVISION
- PAV 1 CONC DRIVEWAY PAVERS - CHARCOAL
- PAV 2 CONC TRADITIONAL PAVERS - CHARCOAL

DWELLING AREAS m<sup>2</sup>

DWELLING 1		DWELLINGS 2 - 7	
GF LIVING	24.1	GF LIVING	23.9
GARAGE	18.9	GARAGE	18.9
FF LIVING	43.9	FF LIVING	43.8
BALC	10.5	BALC	10.6
SF LIVING	53.8	SF LIVING	53.1
TOTAL	151.2	TOTAL	150.3

SITE AREAS m<sup>2</sup>

SITE	699
TOTAL BUILDING AREA	1064
TOTAL GF BUILDING AREA	294
SITE COVERAGE	42%
PRIVATE OPEN SPACE	(26/unit) 182
COURTYARDS	(16/unit) 112
COMMON AREAS	269
LANDSCAPE AREAS	(82 deep soil) 97



REFER TO CIRQA DRAWINGS FOR DETAILED VEHICLE MOVEMENT ASSESSMENT

REFER TO LCS DRAWINGS FOR DETAILED LANDSCAPING PLAN

TO BE READ IN CONJUNCTION WITH ENGINEER'S DRAWINGS 190627

PROPOSED SITE PLAN

SCALE 1:200

SITE LEVELS PROVIDED BY OTHERS

Boundary information on this drawing has been provided via a detailed survey only. Prior to any building work commencing, it is the builder's responsibility to arrange a boundary identification survey to confirm all dimensions and set outs.

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1:200	1922
PD	01

PLANNING CONSENT STAGE



DWELLING AREAS m<sup>2</sup>

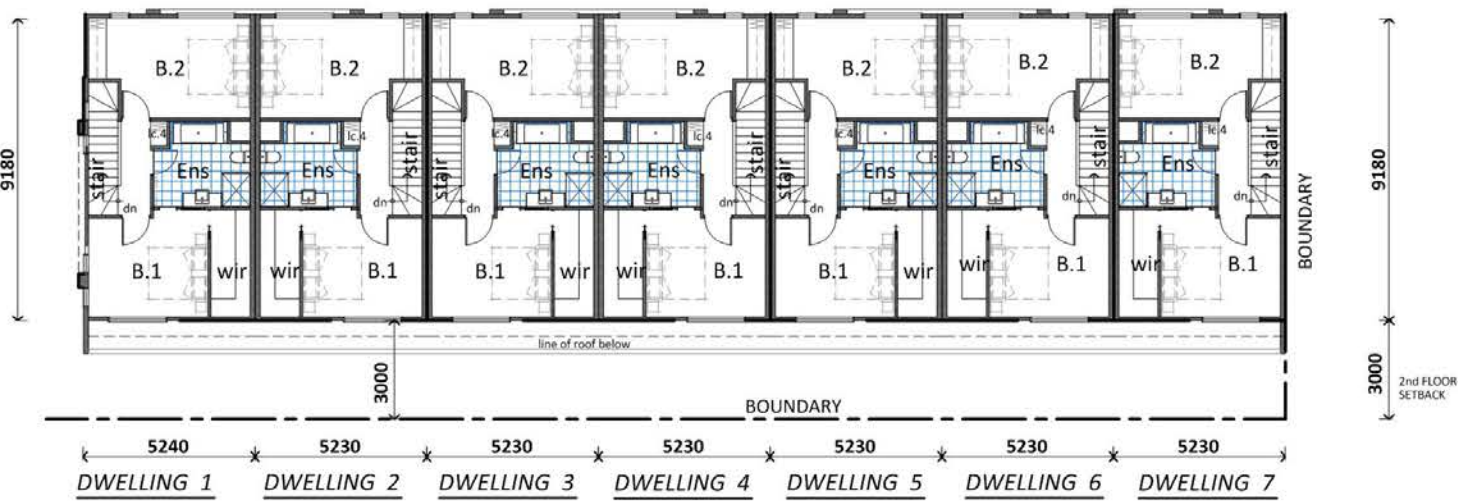
DWELLING 1	
GF LIVING	24.1
GARAGE	18.9
FF LIVING	43.9
BALC	10.5
SF LIVING	53.8
TOTAL	151.2

DWELLINGS 2 - 7	
GF LIVING	23.9
GARAGE	18.9
FF LIVING	43.8
BALC	10.6
SF LIVING	53.1
TOTAL	150.3

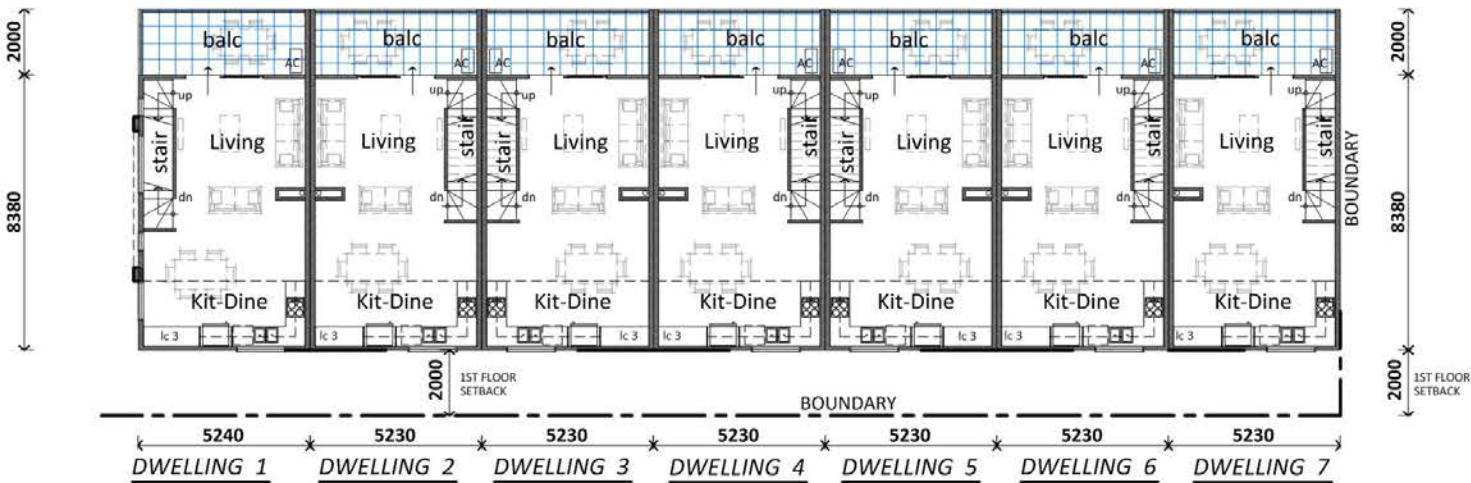
storage m<sup>3</sup>

DWELLING 1	
lc 1	1.8
stair	1.3
lc 2	1.3
lc 3	2.5
lc 4	1.3
total	8.2

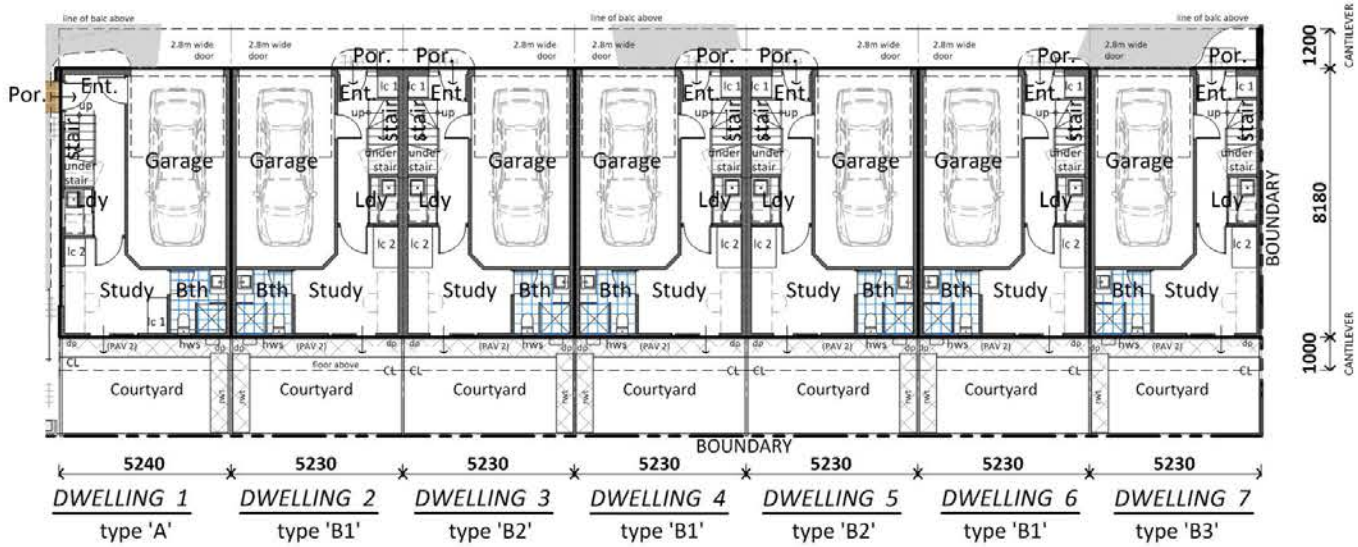
DWELLINGS 2-7	
lc 1	1.1
stair	1.3
lc 2	2.5
lc 3	2.5
lc 4	1.3
total	8.7



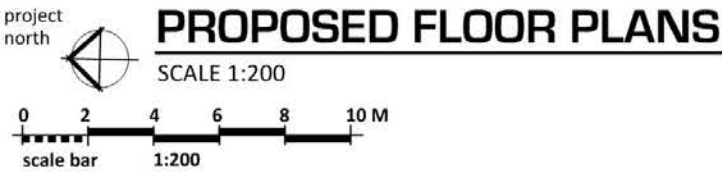
SECOND FLOOR  
SCALE 1:200



FIRST FLOOR  
SCALE 1:200



GROUND FLOOR  
SCALE 1:200



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PD	02 A

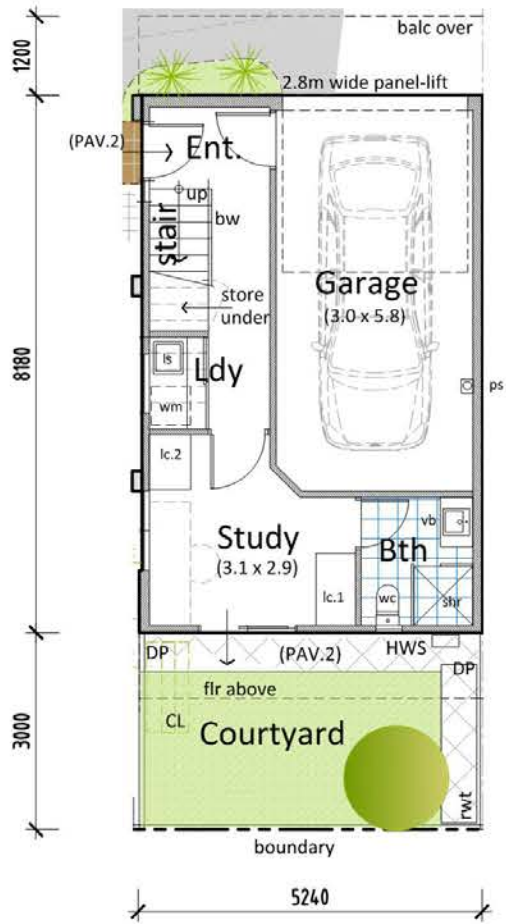
PLANNING CONSENT STAGE

DWELLING AREAS m<sup>2</sup>

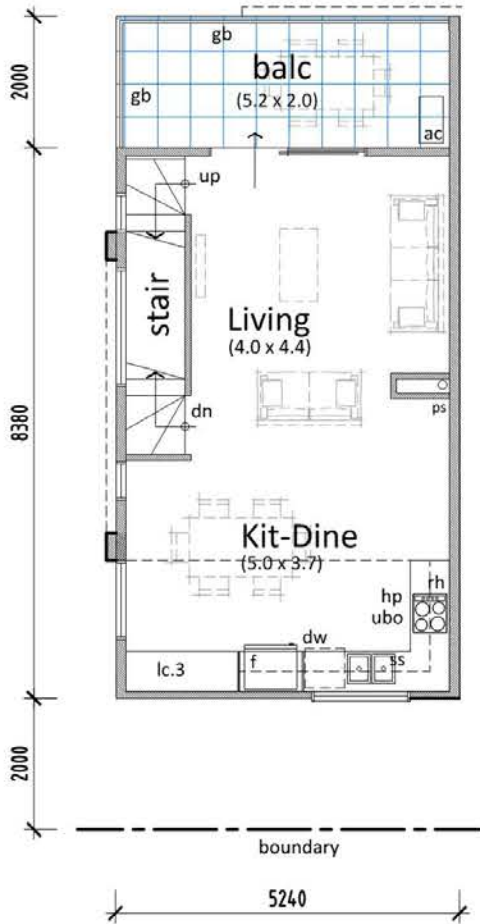
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GARAGE	18.9	GARAGE	18.9
FF LIVING	43.9	FF LIVING	43.8
BALC	10.5	BALC	10.6
SF LIVING	53.8	SF LIVING	53.1
TOTAL	151.2	TOTAL	150.3

LEGEND

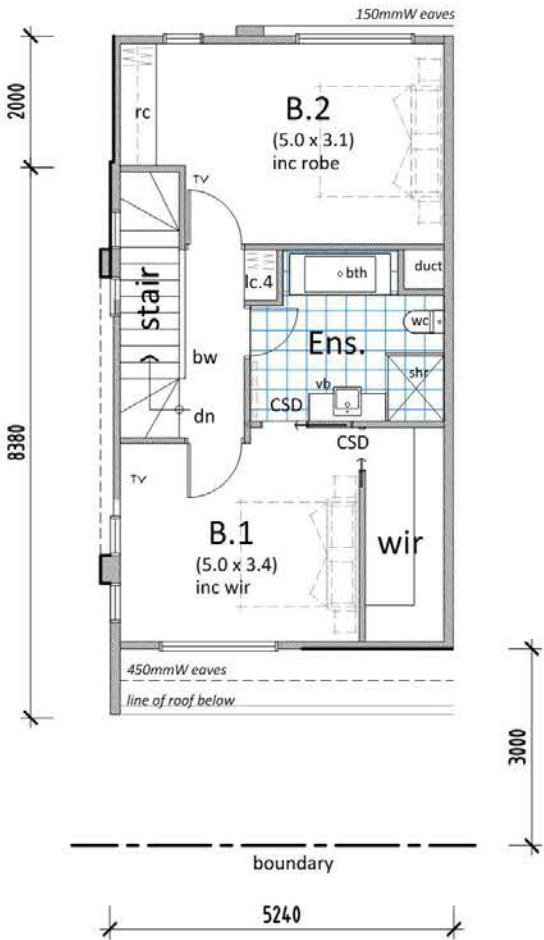
SS	INSET SINK	LC	LINEN CUPBOARD
F	REFRIGERATOR	RC	ROBE CUPBOARD
HP	HOTPLATES	OHC	OVERHEAD CUPBOARD
UBO	UNDERBENCH OVEN	WM	WASHING MACHINE PROVISION
RH	RANGEHOOD	LS	LAUNDRY SINK
DW	DISHWASHER	CSD	CAVITY SLIDING DOOR
P	PANTRY	ps	PLUMBING STACK PROVISION
WC	TOILET PAN	AC	AIR CON UNIT
VB	VANITY BASIN	DP	DOWNPIPE
SHR	SHOWER	CL	CLOTHESLINE PROVISION
HB	HAND BASIN	GB	1.0mH GLASS BALUSTRADE
B	INSET BATH		
BW	1.0mH BALUSTRADE WALL		



GROUND FLOOR



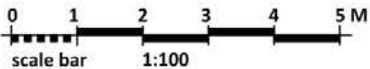
FIRST FLOOR



SECOND FLOOR



PROPOSED FLOOR PLANS  
SCALE 1:100  
DWELLING 1 - type A



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Proposed Residential Development at  
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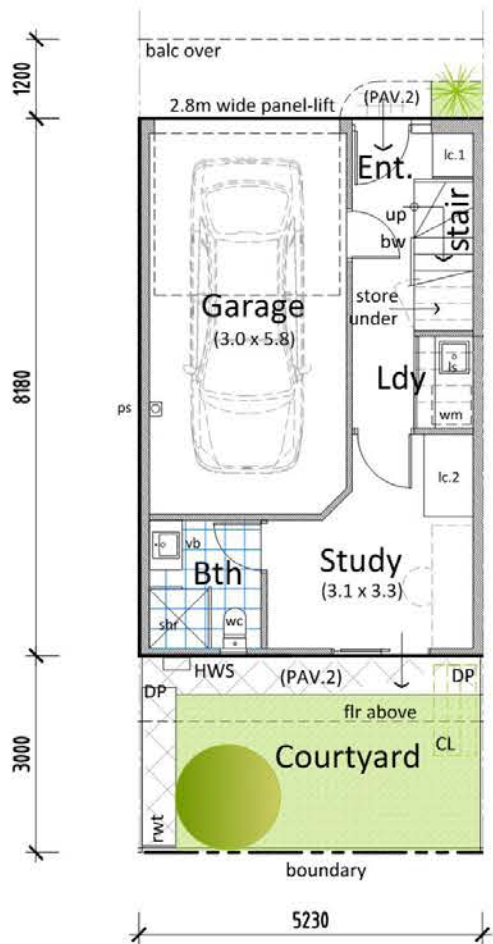


DWELLING AREAS m<sup>2</sup>

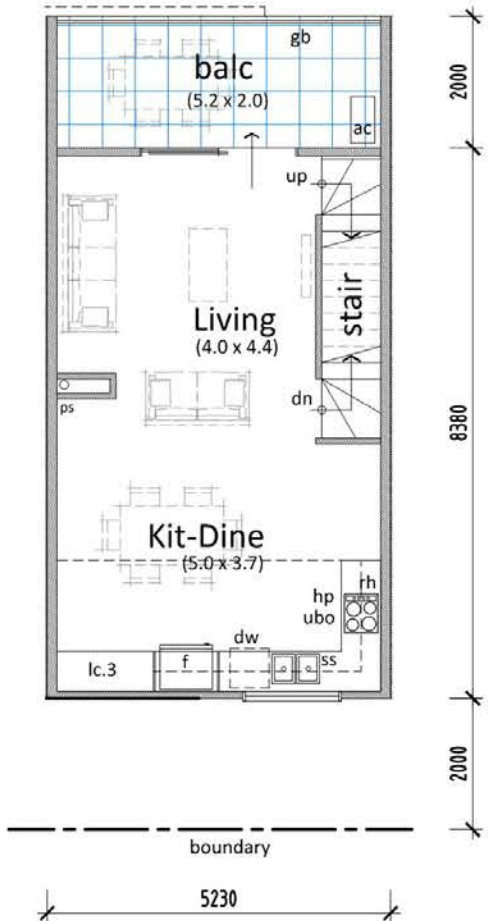
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GARAGE	18.9	GARAGE	18.9
FF LIVING	43.9	FF LIVING	43.8
BALC	10.5	BALC	10.6
SF LIVING	53.8	SF LIVING	53.1
TOTAL	151.2	TOTAL	150.3

LEGEND

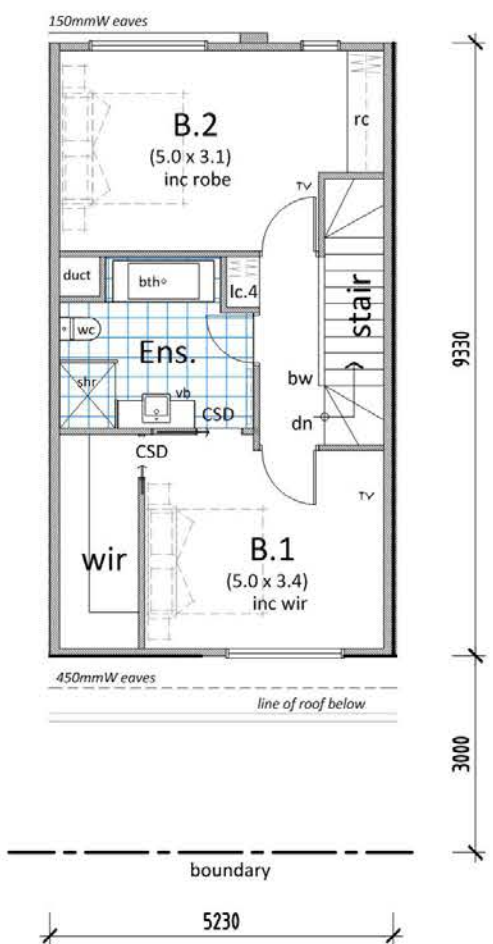
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F	REFRIGERATOR	RC	ROBE CUPBOARD
HP	HOTPLATES	OHC	OVERHEAD CUPBOARD
UBO	UNDERBENCH OVEN	WM	WASHING MACHINE PROVISION
RH	RANGEHOOD	LS	LAUNDRY SINK
DW	DISHWASHER	CSD	CAVITY SLIDING DOOR
P	PANTRY	ps	PLUMBING STACK PROVISION
WC	TOILET PAN	AC	AIR CON UNIT
VB	VANITY BASIN	DP	DOWNPIPE
SHR	SHOWER	CL	CLOTHESLINE PROVISION
HB	HAND BASIN	GB	1.0mH GLASS BALUSTRADE
B	INSET BATH		
BW	1.0mH BALUSTRADE WALL		



GROUND FLOOR



FIRST FLOOR



SECOND FLOOR

project north

PROPOSED FLOOR PLANS

SCALE 1:100

DWELLINGS 2, 4 & 6 -type B1

0 1 2 3 4 5 M

scale bar

1:100

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ARCHITECTURAL DESIGN GROUP

Proposed Residential Development at 7 Glenburnie Tce, Plympton for ADGS

1:100 1922

PD 04 A

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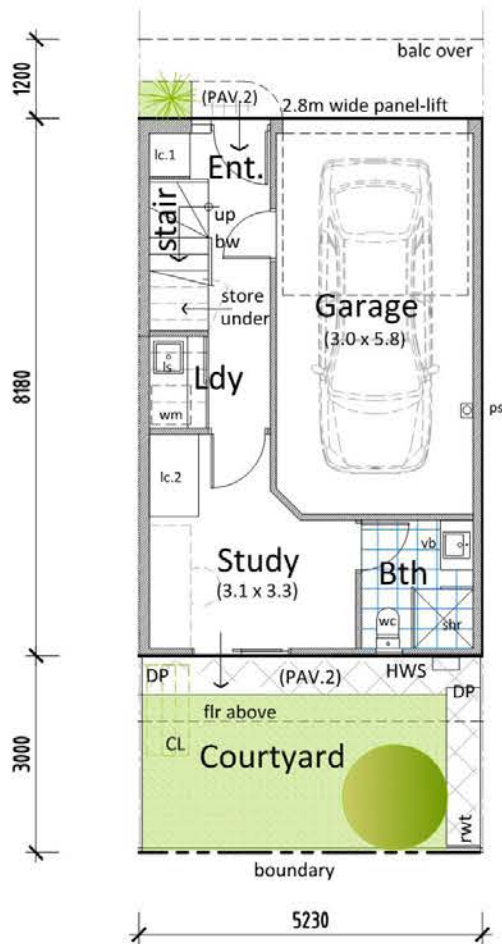
PLANNING CONSENT STAGE

DWELLING AREAS m<sup>2</sup>

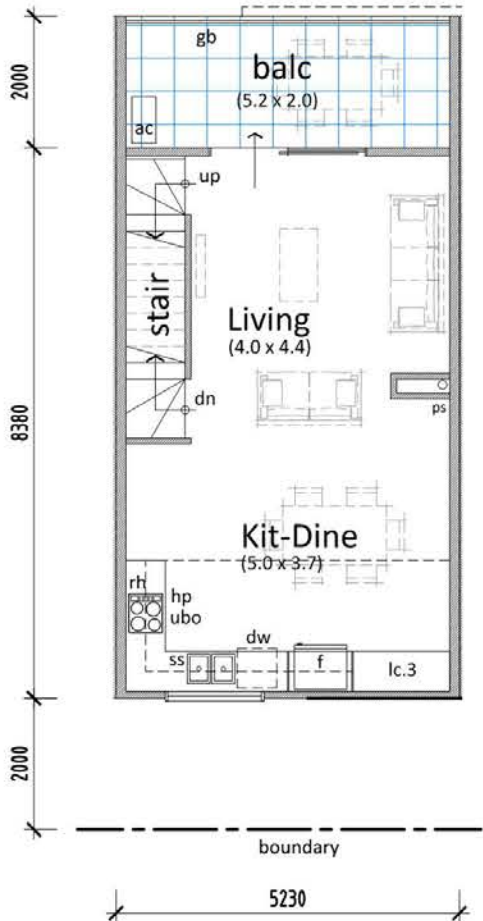
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GARAGE	18.9	GARAGE	18.9
FF LIVING	43.9	FF LIVING	43.8
BALC	10.5	BALC	10.6
SF LIVING	53.8	SF LIVING	53.1
TOTAL	151.2	TOTAL	150.3

LEGEND

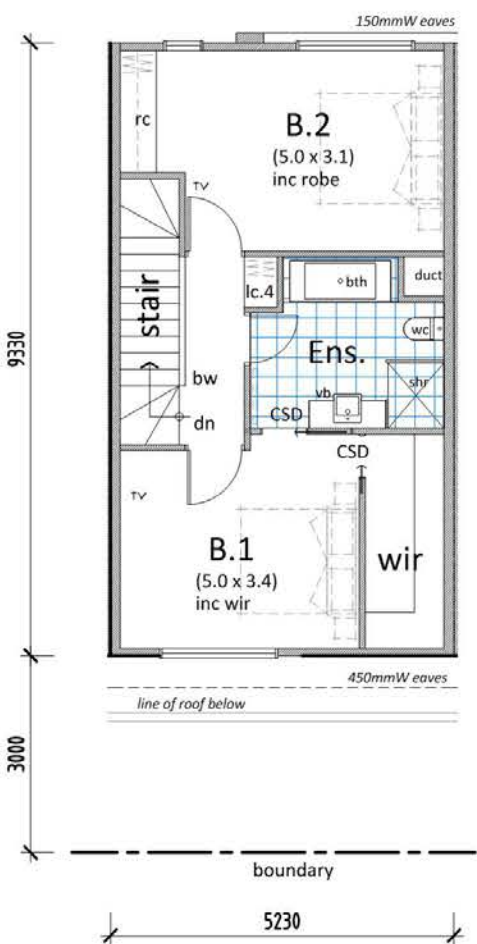
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HP	HOTPLATES	OHC	OVERHEAD CUPBOARD
UBO	UNDERBENCH OVEN	WM	WASHING MACHINE PROVISION
RH	RANGEHOOD	LS	LAUNDRY SINK
DW	DISHWASHER	CSD	CAVITY SLIDING DOOR
P	PANTRY	ps	PLUMBING STACK PROVISION
WC	TOILET PAN	AC	AIR CON UNIT
VB	VANITY BASIN	DP	DOWNPIPE
SHR	SHOWER	CL	CLOTHESLINE PROVISION
HB	HAND BASIN	GB	1.0mH GLASS BALUSTRADE
B	INSET BATH		
BW	1.0mH BALUSTRADE WALL		



GROUND FLOOR



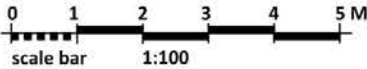
FIRST FLOOR



SECOND FLOOR



PROPOSED FLOOR PLANS  
SCALE 1:100  
DWELLINGS 3 & 5 -type B2



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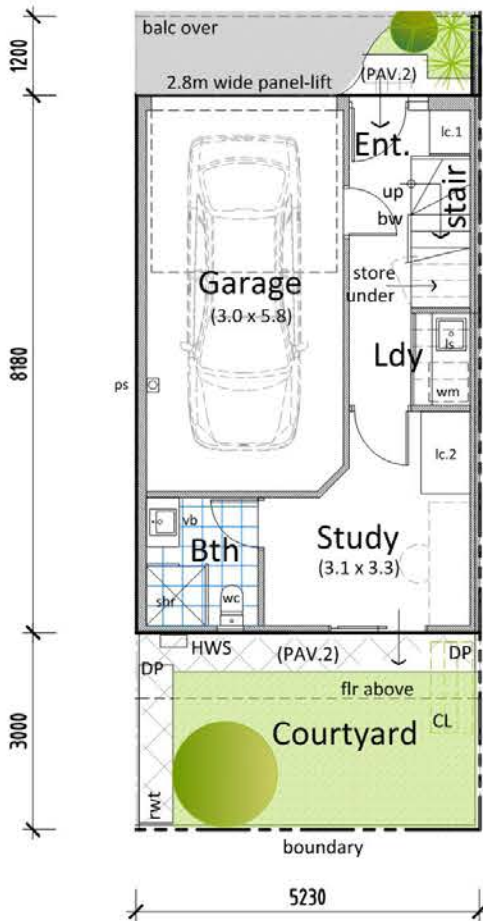


DWELLING AREAS m<sup>2</sup>

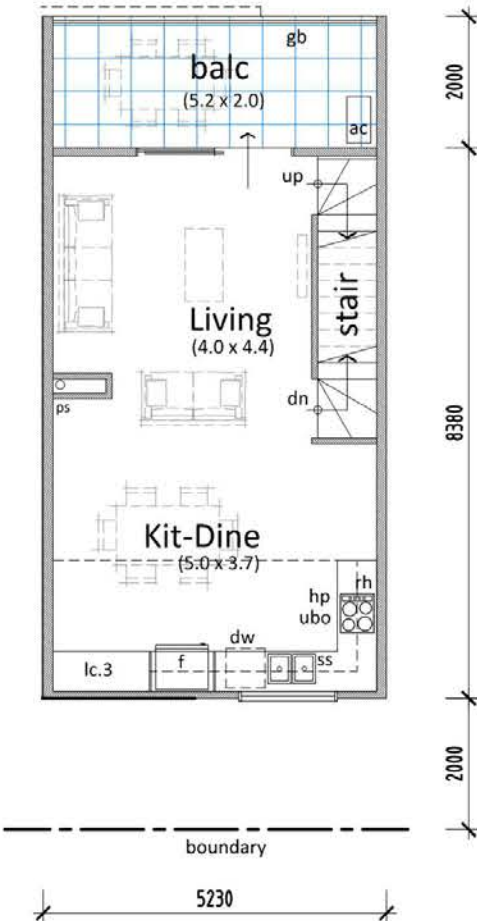
DWELLING 1		DWELLINGS 2 - 7	
GF LIVING	24.1	GF LIVING	23.9
GARAGE	18.9	GARAGE	18.9
FF LIVING	43.9	FF LIVING	43.8
BALC	10.5	BALC	10.6
SF LIVING	53.8	SF LIVING	53.1
TOTAL	151.2	TOTAL	150.3

LEGEND

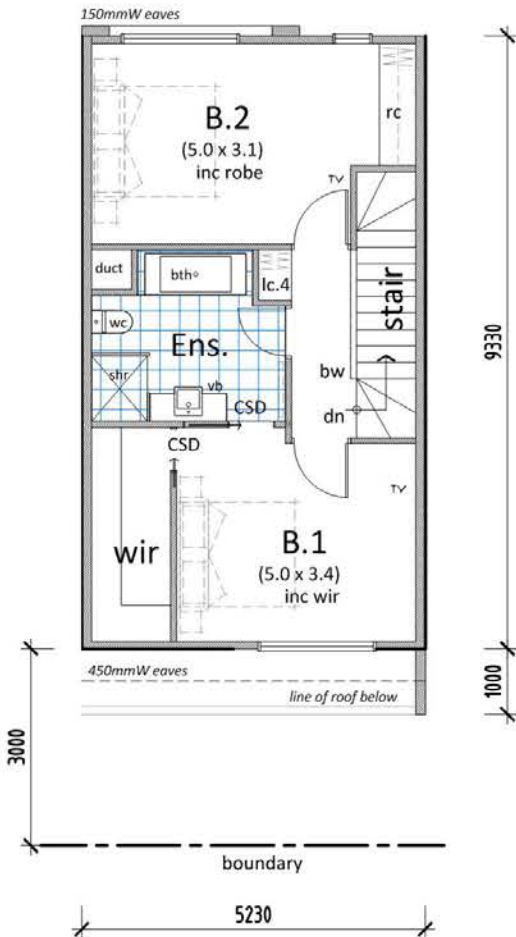
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HP	HOTPLATES	OHC	OVERHEAD CUPBOARD
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P	PANTRY	ps	PLUMBING STACK PROVISION
WC	TOILET PAN	AC	AIR CON UNIT
VB	VANITY BASIN	DP	DOWNPIPE
SHR	SHOWER	CL	CLOTHESLINE PROVISION
HB	HAND BASIN	GB	1.0mH GLASS BALUSTRADE
B	INSET BATH		
BW	1.0mH BALUSTRADE WALL		



GROUND FLOOR



FIRST FLOOR



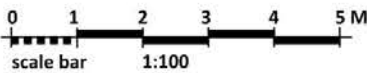
SECOND FLOOR



PROPOSED FLOOR PLANS

SCALE 1:100

DWELLING 7 - type B3



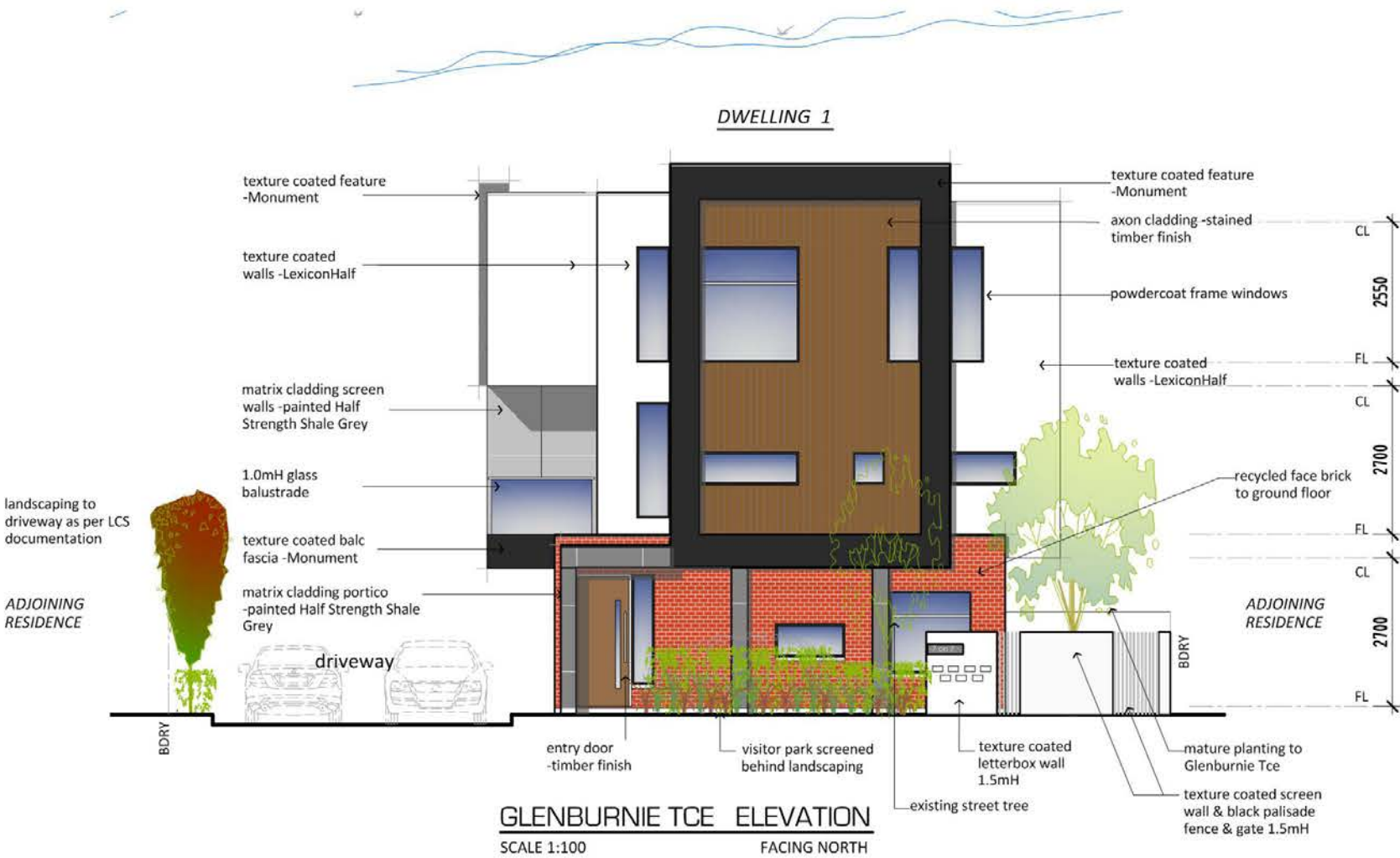
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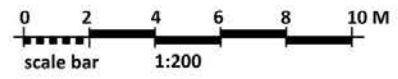


EXTERNAL FINISHES & COLOUR SCHEDULE	
FEATURE BRICK WALLS	RECYCLED BRICK FINISH.
GENERAL EXTERNAL WALLS	APPLIED TEXTURE COAT FINISH. - 'DULUX LEXICON-HALF'
LIGHTWEIGHT WALLS	'AXON SMOOTH 400' CLADDING. - 'DULUX LEXICON-HALF'
FEATURE BLADE WALLS	'MATRIX' CLADDING. - 'DULUX SHALE GREY-HALF STRENGTH'
UPPER LEVEL FEATURE TEXTURE WALLS	APPLIED TEXTURE COAT FINISH. - 'COLORBOND MONUMENT'
UPPER LEVEL LIGHTWEIGHT WALLS	'AXON GRAINED' CLADDING. - 'TIMBER FINISH'
GUTTERS	COLORBOND QUAD GUTTERS - 'COLORBOND MONUMENT'
FASCIAS	METAL FASCIA. - PAINT FINISH MATCH WALL COLOUR
DOWNPIPES	-PAINT FINISH MATCH WALL COLOUR
WINDOWS	ALUMINIUM FRAMED - 'POWDERCOAT BLACK'
ENTRY DOORS	TIMBER VENEER OIL FINISH
GARAGE DOORS	PANEL LIFT DOORS - 'COLORBOND MONUMENT'
ROOFING	COLORBOND ROOFING. - 'COLORBOND WINDSPRAY'

REFER TO LCS DRAWINGS FOR DETAILED LANDSCAPING PLAN



PROPOSED ELEVATIONS  
SCALE AS NOTED



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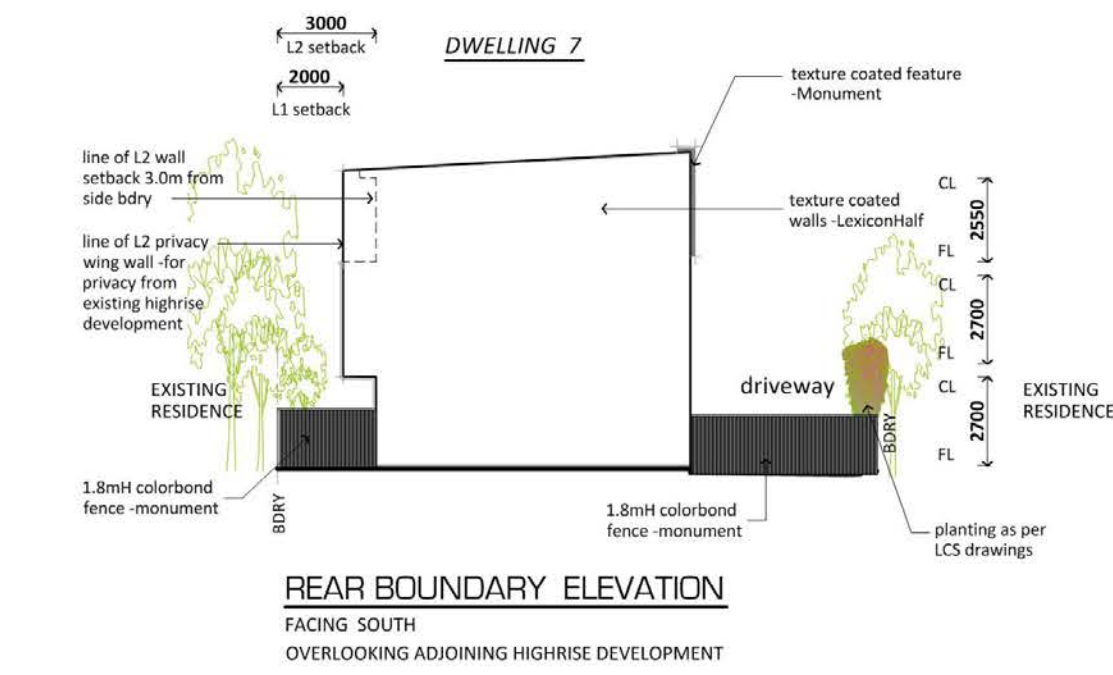
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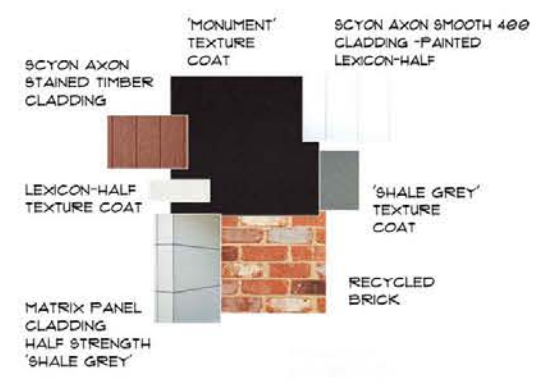
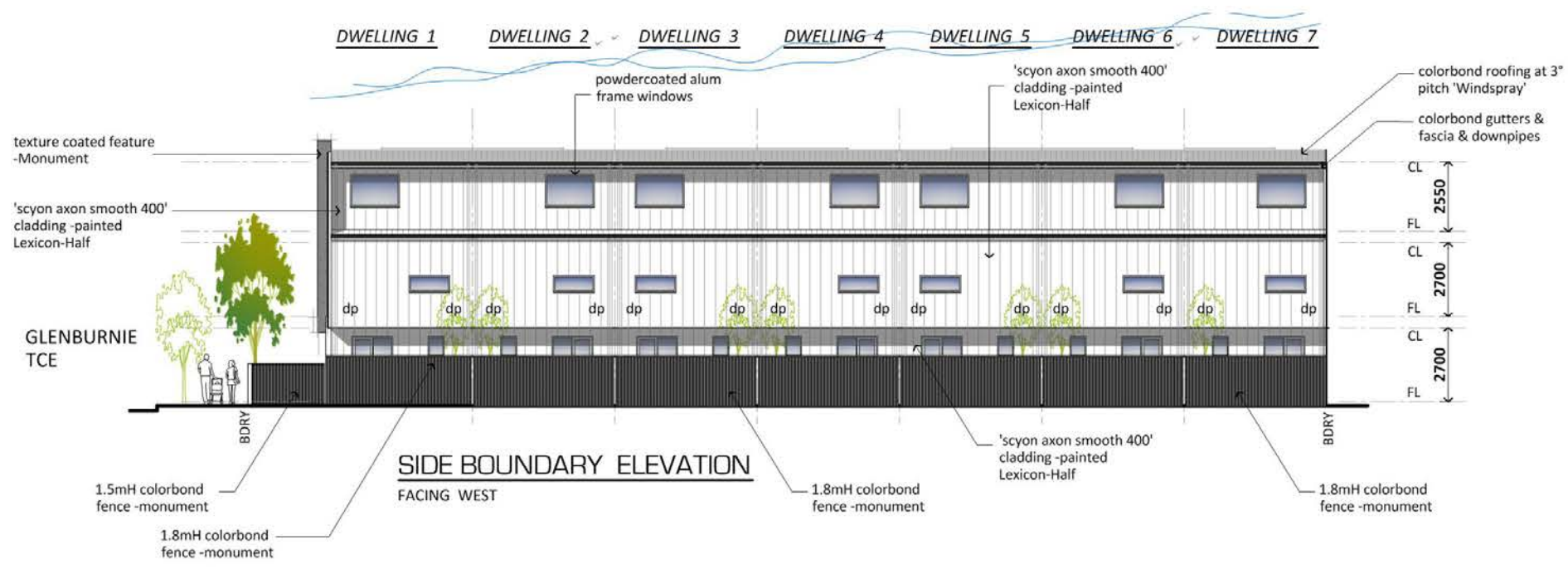
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EXTERNAL FINISHES & COLOUR SCHEDULE	
FEATURE BRICK WALLS	RECYCLED BRICK FINISH.
GENERAL EXTERNAL WALLS	APPLIED TEXTURE COAT FINISH. - 'DULUX LEXICON-HALF'
LIGHTWEIGHT WALLS	'AXON SMOOTH 400' CLADDING. - 'DULUX LEXICON-HALF'
FEATURE BLADE WALLS	'MATRIX' CLADDING. - 'DULUX SHALE GREY-HALF STRENGTH'
UPPER LEVEL FEATURE TEXTURE WALLS	APPLIED TEXTURE COAT FINISH. - 'COLORBOND MONUMENT'
UPPER LEVEL LIGHTWEIGHT WALLS	'AXON GRAINED' CLADDING. - 'TIMBER FINISH'
GUTTERS	COLORBOND QUAD GUTTERS - 'COLORBOND MONUMENT'
FASCIAS	METAL FASCIA. - PAINT FINISH MATCH WALL COLOUR
DOWNPIPES	-PAINT FINISH MATCH WALL COLOUR
WINDOWS	ALUMINIUM FRAMED - 'POWDERCOAT BLACK'
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GARAGE DOORS	PANEL LIFT DOORS - 'COLORBOND MONUMENT'
ROOFING	COLORBOND ROOFING. - 'COLORBOND WINDSPRAY'



PROPOSED ELEVATIONS

SCALE 1:200

0 2 4 6 8 10 M

scale bar 1:200

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Proposed Residential Development at 7 Glenburnie Tce, Plympton for ADGS

PLANNING CONSENT STAGE

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Page 25

NOTES:

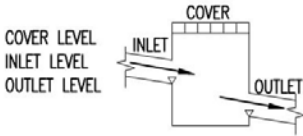
1. All downpipe connections are to be Ø90 uPVC and are to be provided with cleaning eyes.
2. All Stormwater pipes shall be Ø90 UNO.
3. All Stormwater pipes shall be laid as per AS 3500 to achieve minimum cover and grade U.N.O. If cover can not be achieved encase pipe in 100 thick concrete.
4. Sumps, gratings & MH's shall be 300sq (UNO) with walls & floors of 100 thick concrete, reinforced with SL72 fabric central + an approved cover/grate & frame.
5. Where sumps/grates or the like are cast into a concrete slab, provide 2-N12 crack control bars at the corners of such cast-in items. Bars are to be 1000 long and tied to the top layer of slab reinforcement.
6. Bedding and back-filling around stormwater pipes shall conform to AS 3725-1989.
7. Bedding sand for stormwater pipes shall be coarse, free flowing pit sand, with a plasticity index less than 5. The material shall be clean with 100% passing the 6.7mm sieve and not greater than 10% passing a 0.075mm sieve. It shall have a minimum compacted depth of 75mm.
8. Boundary Locations are based on fences/stakes only. It is recommended that an identification survey be done to establish true boundaries.
9. Provide 40mm thick lagging to all pipe penetrations through footing beams.
10. Waterproofing of neighbouring boundary walls is required if the finish floor level of the proposed building and/or site is higher than the finish floor level of neighbouring buildings.



LEGEND

- uPVC SEWER PIPE
- SEAL — Ø100 uPVC STORMWATER SEALED SYSTEM
- uPVC STORMWATER PIPE
- PUMP — HDPE PUMP CHAMBER DISCHARGE PIPE
- +100.456 EXISTING SURFACE SPOT LEVEL
- +W199.46 EXISTING WATER TABLE LEVEL
- +TK99.61 EXISTING TOP OF KERB LEVEL
- +RW99.61 TOP OF RETAINING WALL LEVEL
- +PL99.61 NEW PAVEMENT LEVEL
- +LL99.61 LAWN/LANDSCAPE LEVEL
- Ⓢ STORMWATER INSPECTION POINT
- ⓖ STORMWATER Ø90 PVC GRATE
- 300sq GRATED SUMP (U.N.O)
- ☒ STORMWATER JUNCTION BOX
- dp Ø90 DOWNPIPE
- SE SPREADER DRAIN
- Ⓢ ETSA PIT/CABLE
- TELSTRA SERVICES
- STOBIE POLE
- G GAS METER
- ⊗ WATER METER
- EXISTING TREE
- "ACO" DRAIN K200s WITH GALVANISED GRATE (150kN)
- PAVING
- 100 KERB AND GUTTER
- CONCRETE SPOON DRAIN
- RETAINING WALL
- LANDSCAPING/LAWN AREA

CL = 119.92  
IL = 119.52  
OL = 119.47



D	13.8.19	Issued for approval.
C	10.7.19	Changes due to new architectural drawings.
B	9.7.19	Changes due to new architectural drawings.
A	8.7.19	Minor changes.
Amend	Date	Description

SCA

PTY LTD

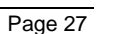
ENGINEERS

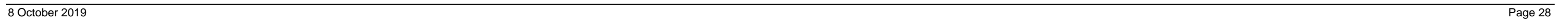
SUITE 3, 76 OSMOND TERRACE NORWOOD SA 5067  
T : 08 8331 0126 E : office@scaengineering.com.au

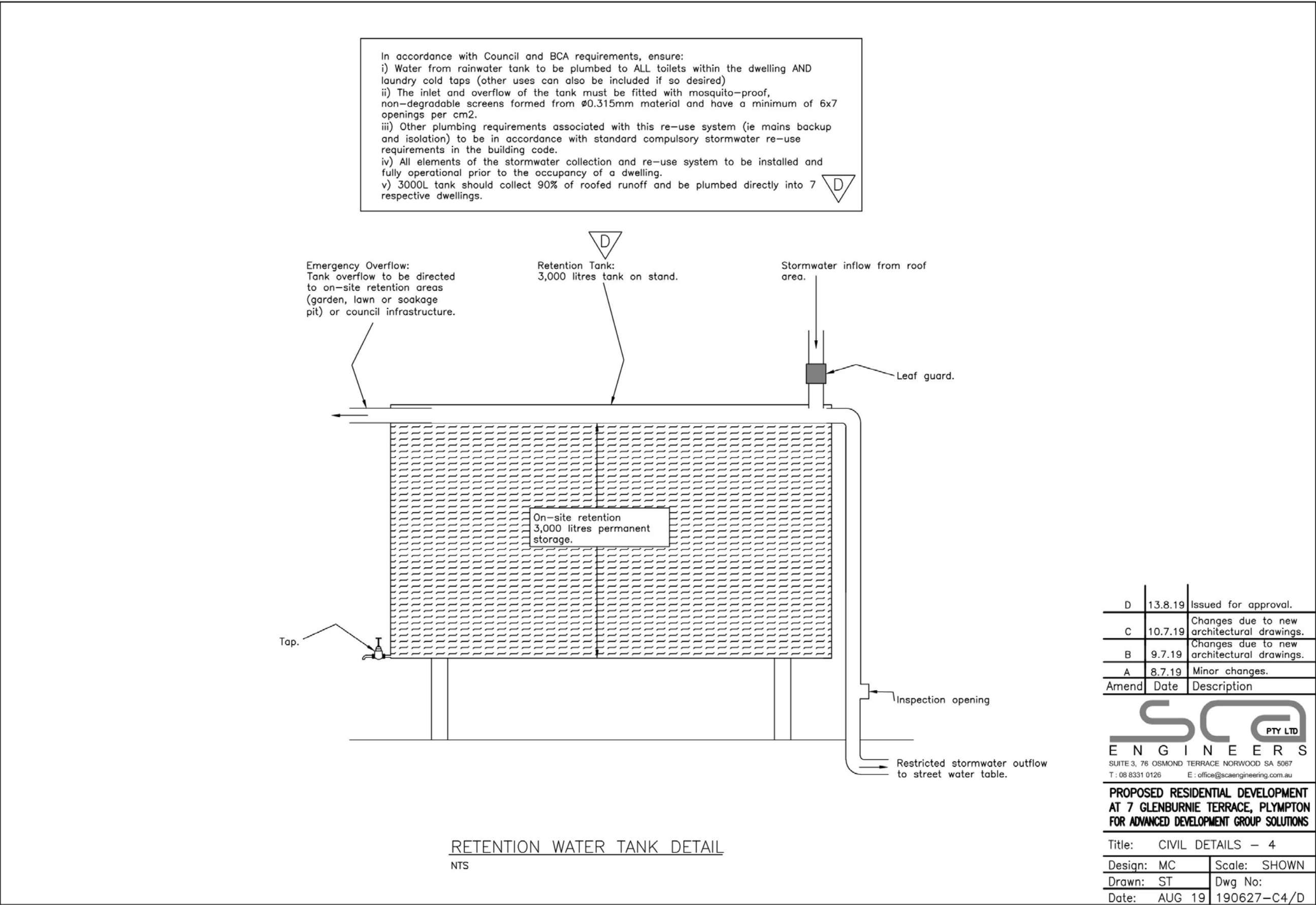
PROPOSED RESIDENTIAL DEVELOPMENT  
AT 7 GLENBURNIE TERRACE, PLYMPTON  
FOR ADVANCED DEVELOPMENT GROUP SOLUTIONS

Title: CIVIL DETAILS – 1	
Design: MC	Scale: SHOWN
Drawn: ST	Dwg No:
Date: AUG 19	190627-C1/D













22 July 2019

Advanced Development Group Solutions Pty Ltd  
Phuong Pham  
C/- StudioeD3

**Re: Covering letter for proposed landscape design for 7 Glenburnie Terrace, Plympton SA 5038**

To whom it may concern,

The purpose of this letter is to provide Landscape Architectural Design advice for the proposed residential development at 7 Glenburnie Terrace, Plympton SA 5038 and accompanies the amended landscape concept LS.049.19 dated 22.07.2019

This landscape design incorporates low maintenance trees and plants that provide maximum amenity within garden beds. Species have been selected for their proven success in these types of projects within the Adelaide metropolitan area.

Landscape treatments adjacent the driveway have a combination of *Dianella 'Tas Red'* and *Anigozanthos 'Amber Velvet'* which create uniformity and interest at ground level with low maintenance foliage and long lasting seasonal colour. This planting is punctuated with upright *Prunus* which will assist with some shade on the driveway area and provide vertical height for the driveway planting.

The western landscape to the dwellings aims to screen off and filter views to the apartment development to the rear by using *Syzgium australe 'Pinnacle'* screening plants and other complimentary species.

The front landscape aims to soften the transition between the development and the streetscape with attractive, low planting. The upright *Zelkova 'Musashino'* provides shade and height to help filter the built form. A bike parking zone has also been incorporated into the front landscape.

Correct amelioration of the existing soil and organic loam will assist with the establishment of the landscape. An automatically controlled irrigation system with inline drippers is also important for the landscape.

Overall the plant species selected and sizing of plants will provide vital greenspace for this development and will help soften the built form and provide a good quality landscape for the residents of the development.

Please don't hesitate to contact me for further information

Yours faithfully,

A handwritten signature in black ink, appearing to read 'Steve Kindstrom', with a long, sweeping horizontal line extending to the right.

Steve Kindstrom  
Registered Landscape Architect #3222  
skindstrom@lcslandscapes.com.au  
Mobile: 0448 075 035

3 Creswell Road  
Largs North SA 5016

PO Box 3246  
Port Adelaide SA 5015

tel 8249 9799  
fax 8249 9744

enquiry@lcslandscapes.com.au  
www.lcslandscapes.com.au

Landscape Construction Services Pty / Builders Licence: BLD175870 / ABN: 88 102 505 180







GLENBURNIE TERRACE ELEVATION  
SCALE 1:100 AT A1



LANDSCAPE ELEVATION - COMMON DRIVEWAY  
SCALE 1:100 AT A1

PLANTING SCHEDULE

CODE	SPECIES	COMMON NAME	POT SIZE	INDICATIVE SIZE (Height x Spread)
LOW/ MEDIUM SHRUBS, GRASSES & GROUND COVERS				
1	Anigozanthos 'Amber Velvet'	Kangaroo Paw	140mm	600mm x 400mm
2	Buxus japonica	Japanese Box	140mm	600mm x 600mm
3	Dianella 'Tas Red'	Flax Lily	140mm	500mm x 500mm
4	Liriope 'Evergreen giant'	Lily turf	140mm	500mm x 500mm
5	Nandina domestica 'Moonbay'	Heavenly Bamboo	140mm	600mm x 700mm
6	Abelia x grandiflora 'Little Gold'	Glossy Abelia	140mm	600mm x 800mm
TALL SHRUBS/ SCREENING				
7	Magnolia grandiflora 'Little Gem'	Little Gem	300mm	5m x 3m
8	Syzygium australe 'Pinnacle'	Lilly Pilly	200mm	6m x 1m
TREES				
9	Lagerstroemia indica 'Diamonds in the dark'	Crape Myrtle	45L	3m x 2.5m
10	Prunus cerasifera 'Oaiville Crimson Spire'	Ornamental upright plum	45L	4m x 1m
11	Zelkova serrata 'Musashino'	Musashino Zelkova	45L	10m x 3m





**Ref: 18341|BNW**

15 July 2019

Miss Phuong Pham  
Advanced Development Group Solutions Pty Ltd  
By Email: phuongpha@gmail.com

Dear Phuong,

**PROPOSED RESIDENTIAL DEVELOPMENT  
7 GLENBURNIE TERRACE, PLYMPTON**

I refer to the proposed residential development at 7 Glenburnie Terrace, Plympton. As requested, I have undertaken a compliance review of the updated proposal. This letter summarises the assessment undertaken.

The proposal comprises the demolition of the existing dwelling and the construction of seven dwellings on the subject site. Each dwelling will have a garage with direct access via a common driveway. A further independently accessible visitor space will also be provided, with direct access via the common driveway. The common driveway will be accessed via a two-way crossover on Glenburnie Terrace.

The garaged parking spaces, visitor parking spaces and the associated driveway will meet the requirements of the Australian/New Zealand Standard for "Parking Facilities – Part 1: Off-street parking" (AS/NZS 2890.1:2004) in that:

- garages will have door opening widths of 2.8 m (which is above the minimum 2.4 m width);
- the visitor parking space will be 2.4 m wide and 5.4 m in length. Adjacent landscaping within 0.3 m of the turn path shall be low lying;
- the common driveway (adjacent garages and visitor spaces) will provide a minimum width of 6.2 m where turns into/out of garages and the visitor space are required. This exceeds the 6.0 m width required by AS/NZS 2890.1:2004 associated with a 2.8 m door opening.

Given that the dimensional requirements of the Australian Standard are satisfied (and, in fact, exceeded) for door opening and apron widths, a turn path assessment should not be required in order to determine appropriate manoeuvrability. Nevertheless, a turn path assessment has been undertaken in order to demonstrate that single turns into and out



of (single reverse before driving forward out) can be achieved as typically required by City of West Torrens.

The attached turn path assessment has been based upon the turning requirements of the B85th percentile design vehicle (domestic car). The assessment has been undertaken using AutoCAD Vehicle Tracking software. The assessment indicates that access can be achieved in accordance with Council's typical requirements (and in excess of the requirements of the Australian Standard).

In summary, the proposal will exceed the requirements of the relevant Australian Standard in regard to garage door opening widths and driveway/apron widths. A turn path assessment has been undertaken to verify that ingress and egress movements can be undertaken to all parking spaces in single entry and exit manoeuvres as required by Council. The turn path assessment identifies that access can be achieved and that all vehicles will be able to enter/exit the site in a forward direction.

Please feel free to contact me on (08) 7078 1801 should you require any additional information.

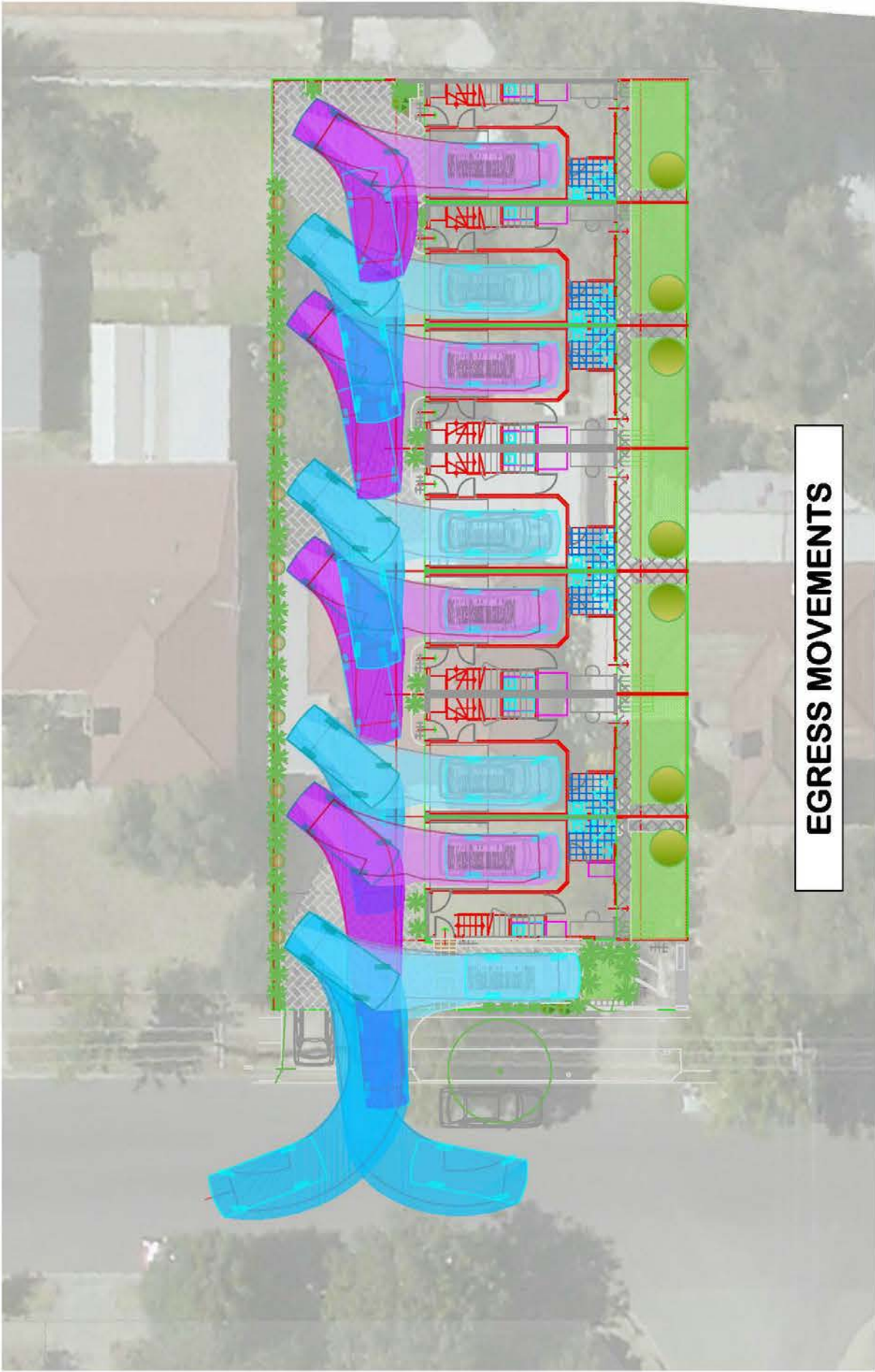
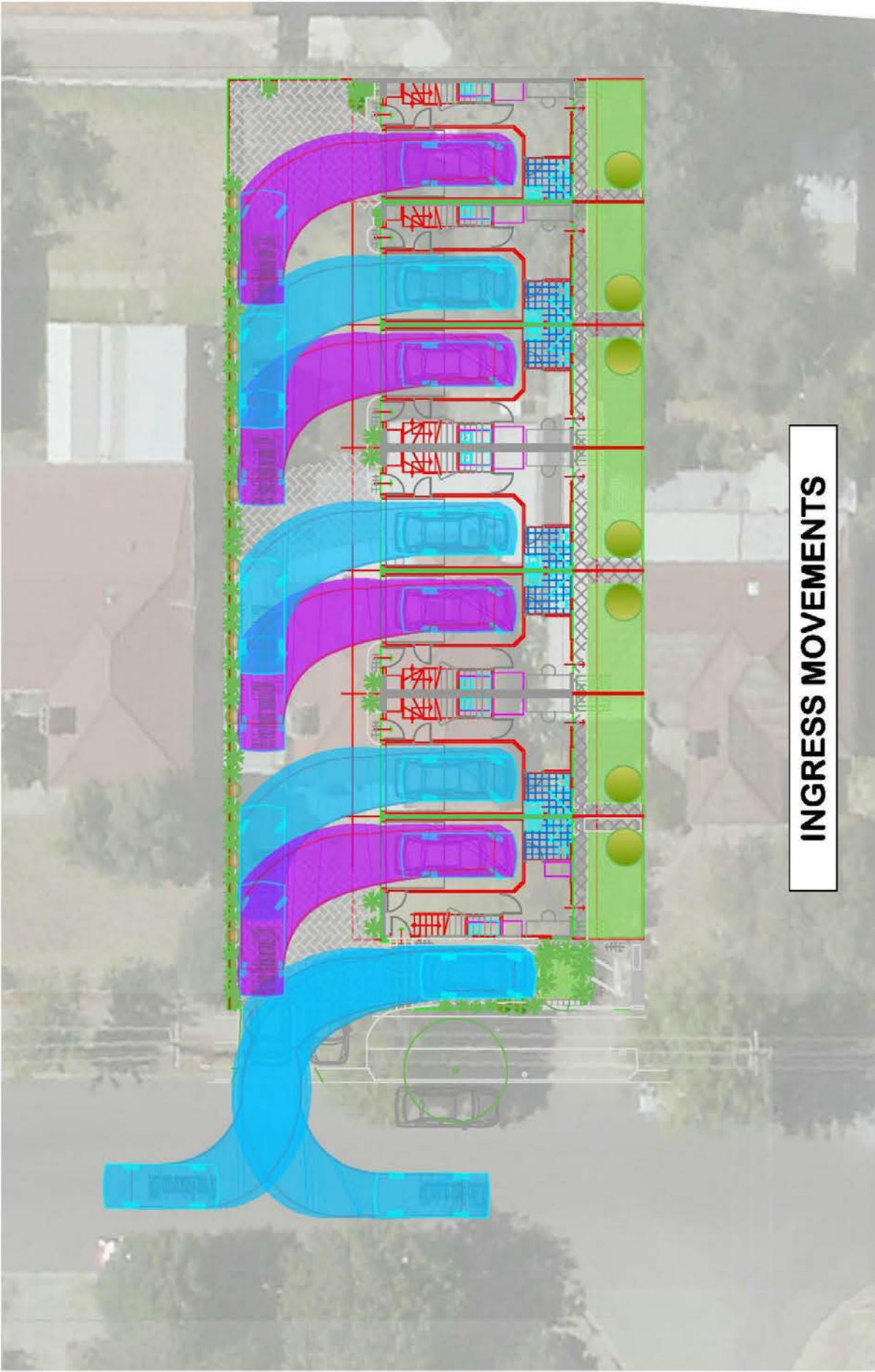
Yours sincerely,

A handwritten signature in blue ink, appearing to read "Ben Wilson", with a stylized flourish at the end.

**BEN WILSON**  
Director, CIRQA Pty Ltd

Encl. – Turn path assessment undertaken by CIRQA (C19225\_01D SH01)





DRAWING AMENDMENTS			
VER	CHK	DATE	DESCRIPTION
B	JJB	09/07/2019	AMENDED PLANS FOR SUBMISSION
D	JJB	11/07/2019	
E	JJB	12/07/2019	
F	JJB	15/07/2019	
G	JJB	15/07/2019	FOR SUBMISSION
CIRQA CONSULTING 18/07/2019 3:30 PM			



**PROPOSED DEVELOPMENT**  
**7 GLENBURNIE TERRACE, PLYMPTON**  
**TURN PATH ASSESSMENT**  
PROJECT # 19225 SHEET # 01\_SH01

This drawing is a concept plan only and is subject to the provision of detailed survey information (by others) and the preparation of detailed design. The drawing is not suitable for construction purposes. The information and data identified within this drawing are the property of CIRQA Pty Ltd and copyright. This drawing and the information contained therein is for the use of the authorized Client noted below. The drawing may not be used, copied, reproduced or modified in whole or in part for any purpose other than for which it was supplied by CIRQA Pty Ltd. CIRQA Pty Ltd accepts no responsibility or liability to any other party who may use or rely upon this drawing or the information contained therein.





25 July 2019

Attention: Rachel Knuckey & Council Assessment Panel Members  
City of West Torrens  
165 Sir Donald Bradman Drive  
HILTON SA 5033

Dear Rachel & Members

**Re: Proposed Residential Development at 7 Glenburnie Terrace, Plympton SA 5038, consisting of Residential Flat Building comprising of 7 x 3 storey dwellings.**

The purpose of this document is to summarise the development proposal & its suitability with the Council's Development Plan requirements as a supporting statement, together with the enclosed documents and drawings:

- 1) Completed Development Application Form and Powerline Clearance Declaration Form
- 2) Certificate of Title
- 3) Architectural drawings prepared by 'studio eD3 – building design'
  - > Proposed Site Plan, Overall Floor Plans, Dwelling Type Floor Plans & Elevations - Drawing Numbers 1922 PD.01 – PD.08 – Dated 24.07.19.
- 4) Engineering documentation prepared by SCA Engineers
  - > Survey layout Plan, Civil Layout Plan, Combination Detention/Retention Water Tank Detail and Stormwater Calculations & Specifications –190627 – Dated July 2019.
- 5) Landscape documentation prepared by LCS Landscapes
  - > Landscape Concept Design, Elevations & Planting Palette and Statement Letter – Dated 22 July 2019.
- 6) Vehicle Movement Plans prepared by CIRQA Traffic Consultants
  - > Ingress layout Plan, Egress layout Plan and Statement Letter – Dated 15 July 2019
- 7) A3 size 3D external perspective prepared by 'realizestudio'

A description of the proposal, its land and locality, and an appraisal of the proposed developments performance against the relevant planning provisions, is set out below.

### Subject Land and Locality

The subject land has a frontage to Glenburnie Terrace of 17.68 metres and a depth of approximately 39.57 metres, a total area of approximately 699 square metres and is a rectangular-shaped allotment. The site is located on the southern side of Glenburnie Terrace with an approximate north-south orientation. Currently a detached dwelling with outbuildings are situated on the subject land and is relatively flat. Its Certificate of Title is listed as Volume 5143 Folio 616 comprising allotment 178 in Deposited Plan 3690 and there are no easements attached to the title.

The locality comprises a mix of residential detached dwellings, semi-detached dwellings with adjoining high rise residential buildings. The era of housing is typically from the 1920s-1950s and frequently comprises of pockets of newer medium to high density development nearby constructed recently and is dominated by an existing 9 storey red brick residential building and a 4 storey cream brick residential building at the rear of the subject land.

Of relevance, the subject land is within Urban Corridor Zone – Boulevard Policy Area 34. This zone is envisaged to contain a mix of medium density, high density residential development, together with retail & community land uses. Its intended that buildings of 3 storeys or more, will be the predominant built form. The subject land is within 50 metres of Anzac Highway being transport corridor for Adelaide and within 60 metres of Gray Street that is well served by public transport.

### Proposal

The proposal is for a three storey Residential Flat Building comprising of 7 dwellings each with a garage under the main roof.

The proposed development will have a contemporary appearance with styling of articulated facades with feature cubed elements, to the street and northern elevation that is characterised by a mix of materials, texture & finishes including recycled face brick at ground level and various cladding types throughout.

The development would involve the demolition of existing buildings & structures to accommodate the proposal.

Vehicle access to all dwellings is from Glenburnie Terrace in the form of a common driveway, which is positioned to avoid street tree removal & would use an existing yet modified crossover. Refer to attached CIRQA drawing SH02 for turn path assessment.

Landscaping will be undertaken throughout the site, particularly along Glenburnie Terrace frontage and the common driveway to reinforce the existing nature of the streetscape. Refer to enclosed LCS Landscapes drawings 1 and 2 for detailed landscape layout and schedule.

### Procedural Matters

As per the West Torrens Development Plan (consolidated 12 July 2018) the subject land is situated within the Urban Corridor Zone – Boulevard Policy Area 34.

Residential Flat Buildings are 'complying development' (Urban Corridor Zone PDC #1 and PA34 PDC #2).

The proposal is for a 'a residential flat building of 3 storeys comprising 7 dwellings' and therefore is a 'Category 2' form of development.

### Planning Assessment

In our view, the relevant planning considerations are as follows:

- Land Use and Density
- Site Areas and Frontage
- High Quality Living Environment
- Setbacks
- Building Height
- Private Open Space
- Overshadowing
- Vehicle Parking
- Design and Appearance

The table below outlines the assessment for consistency of the prescriptive requirements of the Development Plan, for the proposed development.

<b>Development Plan Provisions</b>	<b>7 Glenburnie Tce Development</b>	<b>Assessment</b>
Land Use (PDC 1) and Density (PDC 5)	Residential Flat Building 7 Dwellings, the average Density for this Zone is 5 dwellings (PA 34 = 7, PA 35 = 4.9, PA 36 = 3.1, combined = 15 dwellings) 15 dwellings / 3 zones = average Density of 5 dwellings	Satisfies Satisfies
Site Areas and Frontage	Average Site Area 100m <sup>2</sup> (no minimum site area requirement) 17.68m (no minimum site area requirement)	Satisfies Satisfies
High Quality Living Environment	2 bed dwellings, each are a min 109m <sup>2</sup> Living area	Satisfies
Setbacks Front (PDC 17) Rear (PDC 19) Side (PDC 19)	2.93 metres (minimum 2.00 metres) 0.00 metres (no minimum rear setback) East bdry min 6.5 metres, West bdry 3.0 metres @ GF, East bdry min 5.3 metres, West bdry 2.0 metres @ L1, East bdry min 5.3 metres, West bdry 3.0 metres @ L2, (no minimum setback req'd for first 2 floors, 3.0m setback req'd for above 2 storeys)	Satisfies Satisfies Satisfies
Building Height (PDC 13)	max 10.2m total building height	Satisfies
Private Open Space (Res PDC 19)	Ground floor, each dwelling 16m <sup>2</sup> in a 3.0m wide private courtyard, Level 1 Living area, each dwelling 10m <sup>2</sup> in a 2.0m wide balcony facing East - total 26m <sup>2</sup> /dwelling (min requirement 24m <sup>2</sup> )	Satisfies
Overshadowing (PDC 16)	Building is orientated along North- South axis & setback min 5.3m from East bdry, therefore winter sunlight available to adjacent dwelling. (min 2 hours of sunlight between 9.00am & 3.00pm on 21 June to ground level of existing adjacent dwelling)	Satisfies
Vehicle Parking (PDC 20)	8 on-site vehicle parks, including 1 on-site visitor park & 1 additional on-street visitor park. (minimum requirement is 1 vehicle parks per 2 bed dwelling plus an additional 0.25 vehicle parks per dwelling) 1.0 x 7( 2 bed dwellings) + 0.25 x 7 (visitor) = 8.75 vehicle parks	Satisfies Merit Assessment



### Design and Appearance

It is noted that generally the building is in keeping with the Council objectives of the area and greater design principles.

- Context

*Setbacks*, it is noted that the proposed building has setbacks that are either equal to or greater than those required under *Setbacks from Road Frontages & Other Setbacks* items 17 & 19 under the *Urban Corridor Zone*. The side setbacks are progressive as the building increases in height and with the balconies facing North-East, they assist in reducing the perceived mass of the development.

- Scale

It is noted that the current design reflects a height which is under the maximum 8 storeys and or 32.5m, as prescribed in *Building Height* item 13 under *Urban Corridor Zone*.

- Built Form and Aesthetics

The proposed dwellings will have a contemporary styled appearance and feature a cubed, articulated facades to Glenburnie Terrace and eastern elevation that is characterised by a mix of materials, texture & finishes including recycled face brick at ground level and various cladding types throughout. Refer to Appendix 1 – External Design Palette Concept.

- Density

The proposed design reflects an appropriate density.

- Amenity

The orientation of the site provides an opportunity to achieve east & partial north facing living areas and balconies, with overhang eaves and screening walls.

Balconies are typically 10m<sup>2</sup> in areas in addition with the ground level 3.0m wide Courtyards which is in line with *PDC item 19* under *Residential Development* requirements for a dwelling with a site area less than 300m<sup>2</sup>.

Internal living areas are approximately 109m<sup>2</sup>. The orientation of the living areas with the east-north-east facing balconies provide suitable indoor and outdoor living enhancing the quality of liveable amenity to the site.

- Domestic Storage

The Development Plan calls for a minimum of 8m<sup>3</sup> of storage per dwelling, *PDC item 25* under *Medium to High Rise Development (3 or more storeys)*.

The proposal indicates a total of 60.4m<sup>3</sup> of storage, which is made up of 8.2m<sup>3</sup> for dwelling 1 and 8.7m<sup>3</sup> for dwellings 2-7, in the form of 4 linen cupboards and under stair storage per dwelling. These storage areas are indicated on the floor plans.



- Landscaping

The Development Plan calls for a minimum of 7% of the site area to be comprised of deep soil area *PDC item 23 under Medium to High Rise Development (3 or more storeys)*, this equates to a minimum of 49m<sup>2</sup>. This area should have a minimum dimension of 3m.

The proposal indicates landscape deep soil zone areas of approximately 82m<sup>2</sup>

The Development Plan also calls for a minimum of 10% of the site area should be landscaped *PDC item 4 under Landscaping, Fencing and Walls*, this equates to a minimum of 69m<sup>2</sup>.

The proposal indicates a total of landscape areas of approximately 97m<sup>2</sup>.

Refer to attached LCS Landscapes statement letter and drawings dated 22 July 2019 for species schedule and planting.

- Car Parking and Traffic

Based on 7 x two-bedroom dwellings, 7 on-site dwelling vehicle parking spaces should be provided in line with *PDC item 20 under the Urban Corridor Zone and Table WeTo/6 Off Street Vehicle Parking requirements for Designated Areas*.

7 car parking spaces which are covered, have been provided on site, plus an additional 1 visitor park has been provided on-site. In addition, excluding the driveway, the proposal would retain approx. 11.5 metres of the overall frontage as upright kerb. This length is capable of retaining at least 1 on-street vehicle parking space.

- Overshadowing

The proposed 3 storey building is along North - South axis with a varying setback of 5.3 to 6.5m from the east boundary and 2.0m to 3.0m from the west boundary which adjoins neighbouring dwellings. Being a minimum of 5.3m from the east boundary, it is a suitable assessment that winter sunlight is available to the adjacent eastern dwelling and provides in excess of the minimum 2 hours of sunlight between 9.00am and 3.00pm on 21 June to the ground level, as prescribed in *PDC item 16 under the Urban Corridor Zone*.

- Waste Management

It is intended that a 'Reduced Shared Council 3 Bin Waste Service' and would be provided to the subject site. Based on the '*City of West Torrens Developers Waste Management Guide for new residential development*', that a total of 12 shared bins is required, with either 9 or 8 bins to be serviced weekly from the roadside kerb of the property.

The proposal indicates 2 storage areas for 12 shared bins within the site. The allocated bin storage areas are located to be accessible to all residents and does not conflict with vehicle manoeuvring.

- Bicycle Parking

The Development Plan calls for certain types of development within the Urban Corridor Zone to provide a number of bicycle parks on the site *Table WeTo/7 Off-street Bicycle Parking Requirements for Urban Corridor Zone*, the minimum requirement is 3 bicycle parks.

The proposal indicates 3 bicycle parks at the front of the development and can facilitate 4 bicycle parks if required.

- Noise

It is intended that all dwellings will comply with the 'Ministers specification SA 78B, Construction requirements for control of external sound' for the facades that are facing towards Anzac Highway. Methods such as thickened glazing, higher density insulation to floors and walls will limit airbourne sound to the surrounding areas.

#### **Conclusion**

The size and scale of the development is in line with the proposed future development of the area and the ground floor materials assist in achieving consistency in the existing street scape context.

It is our opinion that the dwellings amenity is suitable and will provide a higher quality lifestyle for it users and is sympathetic to the existing locality, for a medium density residential development in this location.

It has been designed with the Council Wide Principles in mind, and sustainability as a focus.

Kind Regards,



**Mark Sturru**  
**Director,**  
**studio eD3 pty ltd**

### Appendix 1 –Design Concept



7 Glenburnie Terrace, Plympton – Street View

#### Design Concept Principles:

To create a solid textured ground floor podium by way of varied texture using recycled bricks to enhance its connection with the area.

This gives way to an Australian vernacular theme above ground floor utilising a texture finish and open balconies to the first floor and timber look cladding facades with feature cubed articulation to the second floor, reinforcing separated spaces.

Overall the building utilises natural tones and textures to identify its location and enhance the existing amenity.

## Appendix 1 –cont'd



7 Glenburnie Terrace – Street View – Materials Palette

**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

DEVELOPMENT No. 211/633/2019  
 PROPERTY ADDRESS: 7 Glenburnie Terrace, PLYMPTON SA 5038

YOUR FULL NAME	Matthew Morgan Rechner & Jane Ann Roads
YOUR ADDRESS	6 Glenburnie Tce, Plympton 5038
YOUR PHONE No	[REDACTED]
YOUR EMAIL	[REDACTED]
NATURE OF INTEREST	Adjoining resident / land owners. <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION	Please refer attached document
MY REPRESENTATIONS WOULD BE OVERCOME BY <small>(state action sought)</small>	As per attached document - increase height of balcony balustrade wall to 1700mm opaque glass - reduce bulk & scale to 2 story - increase visitor carparking on-site

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission:

I DO NOT WISH TO BE HEARD ☐


I DESIRE TO BE HEARD PERSONALLY ☒

I DESIRE TO BE REPRESENTED BY \_\_\_\_\_ ☐

(PLEASE SPECIFY)

SIGNED

DATE

23/8/19 

Responsible Officer: Jordan Leverington  
 Ends: Tuesday 3 September 2019

If space insufficient, please attach sheets



**We, Matthew Rechner and Jane Roads are the owners of 6 Glenburnie Tce, Plympton and strongly oppose the proposed 3 story development at 7 Glenburnie Tce Plympton.**

Our property is directly across the road from the proposed 3 story redevelopment. Given our immediate proximity to the site and the obvious and undeniable negative impacts this will have on our street amenity, our quality of life, and that of our neighbours, we trust the below information is given due consideration it deserves in the assessment process.

A number of issues associated with the proposal are of serious concern to us and our neighbours and we welcome the opportunity to appear personally at the CAP hearing to present our objections formally.

#### **Scale, setback and overlooking**

Our dwelling is located in the Residential Zone to the immediate North of the proposed development and strong consideration must be given to the impacts this will have. We are fully aware of the changes in zoning that gives consideration to this type of development however this proposal is one street back from ANZAC Highway and in a residential back street with single story dwellings, set back up to 10+m from the footpath. Having this 10.2m high 3 story development will significantly change the streetscape and amenity with overlooking into adjoining properties a huge issue impacting our quality of life, not to mention property values. The existing dwelling is set back approximately 7m from the street and is consistent with other dwellings in the street. The proposed development is 2.9m but given the bulk and scale of the proposal, this will have an overpowering impact on the streetscape.

#### **Overlooking / visual privacy**

The negative impacts of overlooking into our property, particularly our private and secure front yard cannot be overstated. **The plans viewed did not indicate the inclusion of fixed opaque glazing to 1700mm, and in fact the first floor balcony has clear glass balustrade to 1000mm.** The proposed development must include fixed, opaque glazing to 1.7m, in order to minimise opportunities for overlooking into neighbouring private open spaces, and appropriately minimises opportunities for overlooking to achieve PDC 27 outcomes.

**The bulk, height and scale** of the proposed redevelopment is in no way compatible with the predominant single storey character of detached dwellings in Glenburnie Tce.

No consideration has been given to the existing street scape or amenity of the street and impacted properties.

The plans show the proposal to be set back 2.9m from footpath on ground floor which is unacceptable and a considerable reduction from the setback of the existing dwelling. The proposed design does not even step back on the upper floors, giving it even more bulk and scale in the residential setting.

#### **On-site Car parking.**

The proposed new double cross over point on Glenburnie Tce, will remove on-street parking (probably 2 spaces). The current cross over is a single cross over and is close to the Netherby St intersection.

7 car parking spaces for the 7 x 2 bedroom dwellings is inadequate and given the size of proposed development, and that all of units are 2 bedroom, it is not unreasonable to assume each bedroom should be allocated a car space.

As has been proven the case time and time again, occupants of these types of dwellings are typically in a shared house arrangement, non-related occupants with their own vehicles. This will again add increased pressure on the local street network for on-street parking if additional off street parking is not catered for. 1 off street visitor park is insufficient for 7 dwellings. The "allocation" of a designated on street visitor carpark is not valid as there are no rights or permits given to privatise on street parking for sole use of visitors.

#### **On street parking**

No consideration has been given to the other high density developments approved in close proximity (16 dwellings Crn Anzac Highway and Anzac Highway, 32 dwellings #1 Glenburnie Tce) to this site and the combined, accumulated impact these will all have on the on-street parking demand in Glenburnie Tce.

Gray Street, both sides is no parking, Anzac Highway is no parking, and the closest available on-street parking is the eastern end of Glenburnie Tce.

There is increasing demand for on-street parking capacity on Glenburnie Tce however there is no extra capacity as demand fluctuates and will only increase with other infill developments underway.

#### **Waste Collection**

This is another impact that this development will have on the amenity of our street with 8 or 9 bins to be serviced weekly. This will create an unsightly bank of bins curbside, make on street parking even more difficult and waste collection difficult, trying to empty these bins amongst the parked cars.

**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

City of West Torrens

04 SEP 2019

City Development

DEVELOPMENT No. 211/633/2019  
 PROPERTY ADDRESS: 7 Glenburnie Terrace, PLYMPTON SA 5038

YOUR FULL NAME	KAREN KLINGBERG												
YOUR ADDRESS	14 GLENBURNIE TCE, PLYMPTON												
YOUR PHONE No													
YOUR EMAIL													
NATURE OF INTEREST	LAND IN THE VICINITY <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>												
REASON/S FOR REPRESENTATION													
ATTACHED													
<div style="border: 1px solid black; padding: 5px; width: fit-content; margin: auto;"> <p align="center">RECEIVED CSU WTCC</p> <table border="1"> <tr> <td>AM</td> <td>8</td> <td>9</td> <td>10</td> <td>11</td> <td>12</td> </tr> <tr> <td>PM</td> <td>1</td> <td>2</td> <td>3</td> <td>4</td> <td>5</td> </tr> </table> <p align="center">3 SEP 2019</p> </div>		AM	8	9	10	11	12	PM	1	2	3	4	5
AM	8	9	10	11	12								
PM	1	2	3	4	5								
MY REPRESENTATIONS WOULD BE OVERCOME BY <small>(state action sought)</small>													
ATTACHED													

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission:

I DO NOT WISH TO BE HEARD ☐

I DESIRE TO BE HEARD PERSONALLY ☐

I DESIRE TO BE REPRESENTED BY TBC ☒  
(PLEASE SPECIFY)

SIGNED

DATE

Karen Klingberg  
2.9.19

Responsible Officer: Jordan Leverington  
 Ends: Tuesday 3 September 2019

If space insufficient, please attach sheets

To whom it may concern

The site at 7 Glenburnie Terrace, Plympton is located in the Urban Corridor Zone (Boulevard Policy Area 34). The northern side of the Glenburnie Terrace is located in the Residential Zone (Medium Density Policy Area 18).

### **REASONS FOR REPRESENTATION**

#### **BUILDING HEIGHT**

The proposed development is three storeys in height.

The Desired Character statement for the Urban Corridor Zone also states:

**...As one of the key zones in the City of West Torrens where there will be transformation in built form, new buildings will be recognised for their design excellence. These buildings will establish an interesting pedestrian environment and human-scale at ground level through careful building articulation and fenestration, verandas, balconies, canopies and landscaping. In general, the greatest height, mass and intensity of development will be focussed at the main road frontage. Buildings of 3 or more storeys will be the predominant built form. It is for these reasons that dwellings other than detached dwellings will be the predominant form of residential development... (Underlining added)**

It is acknowledged that this focuses for the Zone is on buildings of 3 or more storeys. We contend that it is inappropriate, however, to interpret 3 storeys as a minimum building height. This is particularly the case because Objective 5 of the Urban Corridor Zone seeks:

**Objective 5     A built form that provides a transition down in scale and intensity at the zone boundary to maintain the amenity of residential properties located within adjoining zones. (Underlining added)**

As stated previously, the northern side of Glenburnie Terrace opposite the subject site is located in the Medium Density Policy Area 18 of the Residential Zone. Within this Policy Area, buildings of up to three storeys in height are anticipated. At present, however, all existing buildings on the northern side of Glenburnie Terrace are single storey detached dwellings. This is significant to the assessment of this application.

A core function of development assessment is to weigh the provisions of the Development Plan against the existing circumstances of each locality.

While the transformational nature of the Urban Corridor Zone policy is acknowledged, we contend that it is premature to approve the proposed three storey development located just under 3m from the street boundary given the nature of existing low-scale development at this zone interface. Such an outcome does not provide the appropriate "transition down in scale and intensity at the zone boundary to maintain the amenity of residential properties located with adjoining zones" sought by Objective 5 quoted above.

We contend that two storey development would more appropriately achieve the transition down in scale to the existing single storey detached dwellings opposite in Glenburnie Terrace in a way that the proposed three storey development does not.



### **DENSITY OF DEVELOPMENT**

The subject site has a total area of 699m<sup>2</sup>. A total of 7 dwellings are proposed. This means that the average density per dwelling is approximately 100m<sup>2</sup> per dwelling. This equates to a density of 100 dwellings per hectare.

The Desired Character statement for the Urban Corridor Zone states that:

**The zone will contain an innovative mix of medium density (45-70 dwellings per hectare) and high density (70-200) dwellings per hectare...**

While it is acknowledged that Principle of Development Control 5 in the Urban Corridor Zone states that development in the Boulevard Policy Area 34 should have a minimum net density of 100 dwellings per hectare, we contend that a net density of 100 dwellings per hectare is too dense for a site located at the interface with the Residential Zone. A more medium density development would satisfy the Zone's stated intent to accommodate some medium density development at the same time as addressing object 5 of the Urban Corridor zone.

**Objective 5      A built form that provides a transition down in scale and intensity at the zone boundary to maintain the amenity of residential properties located within adjoining zones. (Underlining added)**

### **ON-SITE PARKING**

Table WeTo/6 – Off street parking Vehicle Requirements

Table - 3 stipulates 1 carpark for each 2 bedroom dwelling and 1.25 parks for a 3+ bedroom dwelling. In addition to this it requests .25 parks per dwelling for visitor parking. Based on a 2 bedroom assessment the proposed development has a shortfall of one parking space. I note the current proposal shows the lower level/s as having a 'study' with an attached en-suite. To this end the properties can be marketed and occupied as 3 bedroom dwellings and subsequently should be assessed with the 3+ bedroom parking provisions in mind. Using the 3+ bedroom criteria the proposal currently has a parking shortfall of 2.5 spaces.

### **MY REPRESENTATIONS WOULD BE OVERCOME BY**

Removing the first dwelling at the front of the property and allocating this space to visitor parking will achieve a more uniform setback within the current streetscape, this approach will go some way to maintaining the amenity of the adjoining properties/zone outlined in objective 5. An increased setback will provide significant reduction in the dominance of a 3 story building at street level. Providing 3 visitor parks will address the shortage in off-street parking in what can be occupied as a 3 bedroom/tenant dwelling. Reducing the development to 6 dwellings will also decrease the density bringing the proposal more in line with the medium density provisions, which I can only assume were intended to address objective 5 of the Urban Corridor Zone.



9 September 2019

Jordan Leverington  
Senior Development Officer - Planning  
City West Torrens Council

By email: [jleverington@wtcc.sa.gov.au](mailto:jleverington@wtcc.sa.gov.au)

Dear Jordan,

**Response to representations for development application 211/633/2019 at 7 Glenburnie Terrace, PLYMPTON SA 5038 for Combined Land division - Community Title; SCAP No. 211/C067/19; Create six (6) additional allotments and common property; demolition of existing dwellings and construction of a three storey residential flat building containing 7 dwellings**

## **1.0 Introduction**

Adelaide Planning and Development Solutions (APDS) have been engaged by the applicant to provide a response to the representations received following the public notification.

In preparing this response, I confirm that I have visited the subject land and locality, had regard to the representations received and the West Torrens Council Development Plan Consolidated 12 July 2018.

This response should be considered in addition to supporting information which was provided as part of the application documentation which considers the majority of the matters raised by the representors.

The following response will address the reasons raised in the representations below.

For the reasons I will detail below, I am of the view that proposal results in a development which warrants Development Plan Consent in its current form.

ABN 55 289 434 618  
Planning & Development Solutions Pty Ltd | Town Planning Specialists | Planning Private Certifiers  
e: [mark@adelaideplanning.com.au](mailto:mark@adelaideplanning.com.au) | ph: 0499933311 | w: [www.adelaideplanning.com.au](http://www.adelaideplanning.com.au)



## 2.0 Representation

Two representations were received during the public notification period. One representor wishes to be heard by the Council Assessment Panel.

The table provides details of the name of the representor, their address, whether they wish to be heard and whether they are in favour of the proposal or not supportive of the proposal.

Name of representor	Address of representor	Wishes to be heard by CAP	Supportive or not of proposal
Matthew Rechener and Jane Roads	6 Glenburnie Terrace, Plympton	Yes	Not in support
Karen Kungberg	14 Glenburnie Terrace, Plympton	Unclear if wishes to be heard	Not in support

## 3.0 Consideration of representations

Having reviewed the representation, the concerns raised in the representation specifically relate to:

- Building height, bulk and scale and setbacks of proposal
- Density of development
- Overlooking and visual privacy
- Onsite and on street parking
- Waste collection

We respond accordingly.



- **Building height, bulk and scale and setbacks of proposal**

The proposal seeks the construction of a three storey residential flat building on an allotment which is contained within the Urban Corridor Zone – Boulevard Policy Area 34.

The Urban Corridor zone seeks a mix of medium to high density residential development, with a built form of 3 storeys or more which acts as a transition down in scale and intensity at the zone boundary to maintain the amenity of residential properties in adjoining zones. Two storey development as desired by the representors is not envisaged within the Policy Area.

The proposed three storey residential flat building will have a maximum building height 10.2 metres and will provide a well-designed and suitably sited development with a form which is envisaged within the Zone and Policy Area which is consistent with the development on the adjoining allotments facing Anzac Highway which range from three to nine stories in height.

Whilst we note the development will be the first example of three storey development along Glenburnie Terrace, there are numerous examples within the Urban Corridor Zone – Boulevard Policy Area 34 in the wider locality which are resulting in a changing character of the locality. Our experience indicates that the aging properties will be progressively redeveloped as the value of the land outstrips the value of the older housing stock developed on them. Therefore, the future character will continue to emerge through redevelopment as owner's lever off the increasing land value.

It should be noted that the current design reflects a height which is under the maximum 8 storeys and or 32.5m, as prescribed in Building Height in PDC 13 of the Urban Corridor Zone. The proposal will provide a suitable transition from the Urban Corridor Zone – Boulevard Policy Area 34 to the adjoining residential properties on the opposite side of Glenburnie Terrace which are located within the Residential Medium Density Policy Area 18.

The proposed use of a range of building materials and articulated built form results in a well-designed and suitably designed residential flat building with building facades which present an appropriate built form both to the street and internally. The proposed dwellings will have a contemporary styled appearance and feature a cubed, articulated facades to Glenburnie Terrace and eastern elevation that is characterised by a mix of materials, texture & finishes





including recycled red brick at ground level and various cladding types throughout. **Appendix 1– External Design Palette Concept** of the application documents provides further details on the materials proposed.

In relation to building setbacks, as previously outlined, the proposal provides the following setbacks which are in keeping with Principles of Development Control 17 and 19 of the Urban Corridor Zone – Boulevard Policy Area 34.

Development Plan Provisions	Proposed	Complies with PDC
Front Setback (PDC 17) 2m.	2.93 metres (minimum 2.00 metres)	Complies
Rear setback (PDC 19) 0m	0.00 metres (no minimum rear setback)	Complies
Side setback (PDC 19) - no minimum setback first 2 floors, 3.0m setback above 2 stories	Ground Floor - East boundary min 6.5 metres, West boundary 3.0 metres  Level 1 East boundary min 5.3 metres, West boundary 2.0 metres  Level 2 - East boundary min 5.3 metres, West boundary 3.0 metres	Complies

It is our opinion that the proposal presents and appropriate built form on the subject land which will provide a suitable transition from the Urban Corridor Zone – Boulevard Policy Area 34 to the adjoining residential properties on the opposite side of Glenburnie Terrace which are located within the Residential Medium Density Policy Area 18 in keeping with the intent of the Zone and Policy Area provisions.



- **Density of development**

The Development Plan seeks an innovative mix of medium density (45-70 dwellings per hectare) and high density (70-200 dwellings per hectare) residential development; and residential development that creates a medium-to-high density urban environment incorporating residential flat buildings and dwellings in mixed-use buildings, and not lower density residential development such as detached dwellings. PDC 5 provides guidance on the minimum density requirements for development within the Urban Corridor Zone – Boulevard Policy Area 34 with a minimum net residential allotment density of 100 dwellings per hectare net.

The proposal seeks the development of 7 dwellings on an allotment with an area of 699 square metres or a density of 100 dwellings per hectare which is in keeping with the form and density of development envisaged on the subject land within the Urban Corridor Zone – Boulevard Policy Area 34.

- **Overlooking and visual privacy**

Principle of Development Control 27 of the General Section - Residential Development indicates:

*Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.*

The proposal is for a three storey residential flat building, which does not require screening devices to balconies for visual privacy purposes to be provided.

Therefore, the balcony arrangements are appropriate in the context of the Development Plan and will not have a detrimental impact to visual privacy on the Glenburnie Terrace properties.



- **Off Street and on street parking**

Table WeTo/6 of The Development Plan envisages a parking rate of 1 car parking spaces per 2 bedroom dwelling and an additional 0.25 car parking spaces per dwelling for residential flat buildings (RFBs) in Urban Corridor Zone – Boulevard Policy 34. This results in theoretical demand in this instance of 8.75 parking spaces to be provided or 9 spaces.

Further considerations in the Development Plan in relation to parking in this instance are

*Principle of Development Control 16 of the General Section – Land Division indicates:*

*The design of the land division should provide space sufficient for on-street visitor car parking for the number and size of allotments, taking account of:*

- (a) the size of proposed allotments and sites and opportunities for on-site parking*
- (b) the availability and frequency of public and community transport*
- (c) on-street parking demand likely to be generated by nearby uses.*

Further, Principle of Development Control 44 of the General Section - Transportation and Access indicates on-site vehicle parking should be provided having regard to:

- (a) the number, nature and size of proposed dwellings*
- (b) proximity to centre facilities, public and community transport within walking distance of the dwellings*
- (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons.*

This proposal provides 8 car parking spaces within the development and a further 2 on-street car parking spaces. The proposal provides 1 parking space for each unit and a further visitor parking space to the front of the site resulting in 8 parking spaces onsite and a further dedicated area for 3 bicycle parking spaces. Excluding the driveway, there will be approximately 12.0 metres kerb space, which equates to parking for 2 cars on Glenburnie Terrace directly in front of the subject land should the yellow line be removed from Glenburnie Terrace which we are formally requesting this instance.





It is also important to note that demand for visitors car parking is typically on weekends and at evening time on weekdays. The subject land is located within 70 metres of Gray Street that is well served by public transport and 120 metres from Anzac Highway which is a main transport corridor providing a range of public transport options. Further, the site is located within walking distance to a Neighbourhood Centre Zone that accommodates a variety of established shops.

As aforementioned, the site is in close proximity to a range of public transport options which will allow for alternate forms of transport to the future residents. Further, there is adequate parking on street in the immediate locality should this be required. Again, each unit is provided with parking space as well as bicycle parking provisions which will adequately service the future residents of the subject land in addition to a range of public transport options and services within walking distance.

Therefore, it is considered that the proposed car-parking arrangements on the site are appropriate in the context of the wider provisions Development Plan and should not have a detrimental impact to existing on-street car parking on Glenburnie Terrace. On this basis, the slight shortfall in this instance is not considered fatal to the proposal.

- **Waste collection**

The proposal will promote waste minimisation and reuse and recycling with each unit providing adequate areas for a dedicated waste storage area at ground level which will allow for onsite collection and sorting of recycling materials. Waste will be collected by Council's waste collection service which is consistent with other residential development in the immediate and wider locality.

### **3.0 Conclusion**

The proposed development will provide a reasonable expansion of the existing Urban Corridor Zone and the proposed development on an existing residential allotment will reflect the development in the existing locality in keeping with the intent of the Boulevard Policy Area 34 of the Urban Corridor Zone provisions. The proposal has been designed to meet the quantitative and qualitative provisions as they relate to medium density development in the Urban Corridor Zone.





Having regard to all the relevant provisions of the Development Plan, for the reasons aforementioned, it is my opinion, that the application represents is appropriate in the context of the Boulevard Policy Area 34 of the Urban Corridor Zone, the general provisions of the West Torrens Council Development Plan, and the unique circumstances of the subject land and locality.

For all of the above reasons, we consider the proposal to demonstrate sound performance against the pertinent Development Plan standards. Accordingly, planning consent in this instance is warranted.

Please confirm when this proposal will be considered by the Council Assessment Panel and the date and time of the meeting. A representative shall attend at this meeting in support of the proposal.

Should you have any queries or require any further information or clarification with any components of this response, please do not hesitate to contact me by email at [mark@adelaideplanning.com.au](mailto:mark@adelaideplanning.com.au)

Yours Sincerely,

Mark Kwiatkowski MPIA CPP

Principal Urban Planner

**Adelaide Planning & Development Solutions - Town Planning Specialists | Planning Private  
Certifiers**

## Preliminary Traffic, Flooding & Stormwater Assessment

**Development Application No: 211/633/2019**

**Assessing Officer:** Jordan Leverington  
**Site Address:** 7 Glenburnie Terrace, PLYMPTON SA 5038  
**Certificate of Title:** CT-5143/616  
**Description of Development** Combined Land division - Community Title; SCAP No. 211/C067/19; Create six (6) additional allotments and common property; demolition of existing dwellings and construction of a three-storey residential flat building containing 7 dwellings

### TO THE TECHNICAL OFFICER - CITY ASSETS

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Jordan Leverington      DATE    7 August, 2019**



## Memo

To Jordan Leverington  
From Richard Tan  
Date 07-Aug-2019  
Subject 211/633/2019, 7 Glenburnie Terrace, PLYMPTON SA 5038

Jordan Leverington,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

Note: As this DA has changed from land division application to combined development application, the following comments provided should be read separate from previous assessment dated 10/07/2019 (Objective ID A2333069).

### 1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

- 1.1 In accordance with the provided 'Site Layout Plan' (SCA, Ref: 190627-C3/3, dated 10/7/2019), the FFLs of the proposed development (17.50 minimum) have been assessed as satisfying minimum requirements (17.45) in consideration of street and/or flood level information.

### 2.0 Verge Interaction

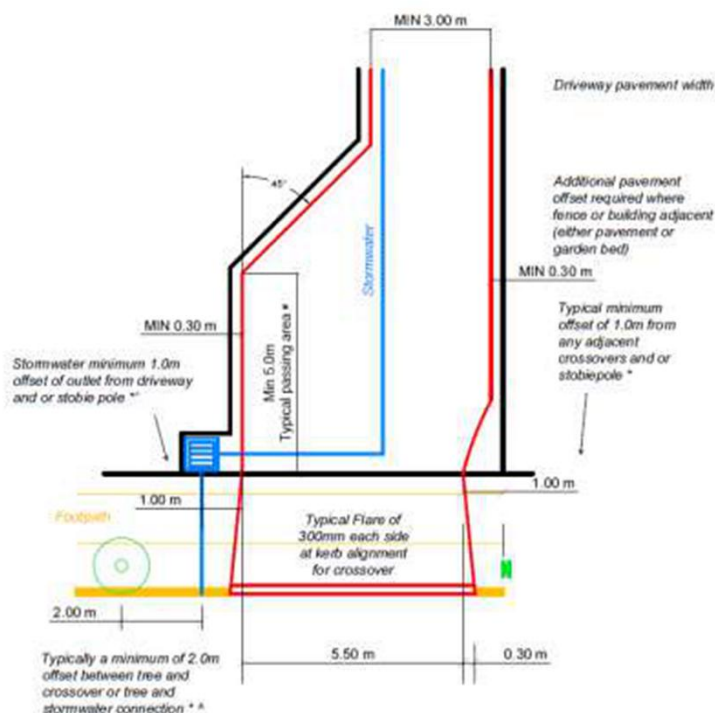
- 2.1 Verge interaction has been assessed as acceptable in accordance with the site layout shown in 'Site Layout Plan' (SCA, Ref: 190627-C3/3, dated 10/7/2019)

### 3.0 Traffic Comments

- 3.1 The proposed driveway corridor to the site is slightly different from Council's standard, but still provides similar function, with a minimum of 5.5m wide pavement width (+ 300mm offset from fences/walls/boundary) for the first 5.0m into the site to permit the passing of entering and exiting traffic. Hence it has been assessed as satisfying minimum requirements.
- 3.2 The proposed crossover should include 0.3m flaring on each side to allow better access to/from the site. Please refer to the attached sketch for a typical layout.



### ACCESS ARRANGEMENT SERVICING REAR CAR PARK OFF LOW VOLUME ROAD



#### NOTES:

- \* Distance as measured along alignment of front property boundary
- ^ Must be deemed to comply by Council's Technical Officer (Amenity)
- # No aboveground structure(s) such as letterboxes, service meters or similar are to be installed within the common driveway entrance and passing area.
- Stormwater connection through the road verge area to be constructed of shape and material to satisfy Council's standard requirements.
  - 100 x 50 x 2mm RHS Galvanised Steel or
  - 125 x 75 x 2mm RHS Galvanised Steel or
  - Multiples of the above.

**It is recommended that 0.3m flaring should be included as part of the crossover for easier access.**

- 3.3 One parking spaces has been proposed to be located directly off of the 5.5m by 5.0m passing entry within the property.

Although often supported by City Development, City Assets does not support this arrangement and considers this to be potentially dangerous due to vehicles accessing these spaces having to enter and exit the property from the wrong side of the common driveway (ie driving of the right side of the carriageway rather than the typical left.)





**Further determination of the requirement for further consideration of this design element is left to the discretion of the planning officer.**

- 3.4 Traffic manoeuvrability has been assessed as acceptable in accordance with the provided Traffic Assessment Report (CIRQA, Ref: 18341-BNW, dated 15/07/2019)
- 3.5 The garage dimension as indicated in 'Floor Plan - Ground' (DC, Ref: Page 6/13, Rev A, dated 09/07/2019) has been assessed as satisfying minimum requirements.
- 3.6 The garage dimension as measured in Proposed Site Plan' (Studio eD3, Ref: 1922-PD-01, dated 24/07/2019) has been assessed as satisfying minimum requirements.

#### **4.0 Waste Management**

- 4.1 Due to the nature of this application being a commercial development, it is recommended that further assessment from Council's Waste Management Team is required. However, it should be noted that the remaining public verge space is 10.6m wide, which is likely too narrow for 14 bins presentation.

**It is recommended that further assessment from Council's Waste Management Team is required.**

#### **5.0 Stormwater**

- 5.1 Stormwater detention calculation has been provided. The pre-development peak discharge rate has been calculated using a 1 in 5 year ARI storm events. Based on internal calculation using Council's stormwater standard (1 in 20 year ARI storm, 10min duration and 0.25 runoff coefficient), the proposed pre-development peak discharge rate is higher than requirements, which will not be accepted.

I have also noted that there is contradiction in the provided stormwater management plan and the description in the calculation sheet. The calculation sheet has indicated that an underground detention tank with orifice will be installed on site, however, that has not been reflected in the provided civil plan. The provided combination tank details and the sketch does not match each other.

It should be noted that if there is intention of surface/pipe storage (detention), then surface ponding is not permitted in a 1 in 5 ARI storm events.



For this scale and nature of proposed development, Council's City Assets Department would consider acceptable an alternate approach to the provision of conventional stormwater detention calculations and implementation.

This alternate solution would provide improved sustainable supply to water to the ultimate homeowner and at the same time, collect and use the majority of the roof stormwater generated by the properties.

In this alternate stormwater management proposal, the following arrangements should be notated for each dwelling within the development.

- Installation of a 3,000 litre rainwater tank (no detention element).
- Rainwater tank plumbed to deliver recycled water all toilets and laundry cold water outlet. (Can also be connected to Hot Water Service if desired).
- A minimum of 90% of the dwelling roof area is to have its stormwater runoff directed to the rainwater tank.
- Mains water backup, pump and plumbing arrangements as typically required to support such an installation are to be compliant with the standard Building Code requirements associated with a compulsory rainwater tank installation.
- The stormwater collection and re-use system is to be installed and operational prior to occupancy of the dwelling.

In association with a development where the applicant has nominated this approach, it is recommended that a condition similar to the following be included with any approval;

- Prior to occupancy of a dwelling, the 3000 litre stormwater collection & reuse tank and associated plumbing to service all toilets and laundry is to be installed and operational.

Should the applicant not desire to utilise the above alternate arrangement for stormwater management, then the applicant would be requested to demonstrate through satisfactory calculations and design for conventional stormwater detention. These works to limit the peak discharge rate for the site critical 20 year ARI storm event to equivalent to a predevelopment arrangement with a 0.25 runoff coefficient.



**It is recommended that revised plans and supporting calculations (if necessary) clearly and accurately indicating satisfaction of the above criteria be provided to Council.**

- 5.2 Council typically requests the implementation of stormwater quality measures for development of this nature to address the removal of stormwater pollutants from the stormwater flow exiting the site.

Given the size of the site, it is recommended that some basic stormwater quality product should be installed to remove the oil, grease and litter from stormwater runoff prior to discharge offsite.

**An indication of how the water quality requirements are to be met should be provided on revised site plans prior to the finalisation of the planning assessment for this development.**

Regards  
Richard Tan  
Civil Engineer



## Waste Management Assessment

**Development Application No:** 211/633/2019

**Assessing Officer:** Jordan Leverington

**Site Address:** 7 Glenburnie Terrace, PLYMPTON SA 5038

**Certificate of Title:** CT-5143/616

**Description of Development** Combined Land division - Community Title; SCAP No. 211/C067/19; Create six (6) additional allotments and common property; demolition of existing dwellings and construction of a three-storey residential flat building containing 7 dwellings

### TO TEAM LEADER WASTE MANAGEMENT - REGULATORY SERVICES

Please provide your comments in relation to:

☐ Any aspect that you feel needs further attention or detail

.....  
.....





## Memo

To Jordan Leverington  
From Nick Teoh  
Date 08-Aug-2019  
Subject 211/633/2019 7 Glenburnie Terrace, PLYMPTON SA 5038

Dear Jordan Leverington

The following Waste Management comments are provided with regards to the assessment of the above develop application:

### Waste Management

1. Waste Collection System  
Waste Management is prepared to provide the following waste service through a shared bin service:
  - 4 x 140L general waste - weekly service
  - 4 x 240L comingled recycling - fortnightly service
  - 2 x 240L organics - fortnightly service
2. Bin Presentation and Collection  
It is anticipated that at peak, 8 bins will be presented for collection with an estimated 7.96m of verge required for presentation.

Kind regards

**Nick Teoh**  
**Team Leader Waste Management**



Contact Planning Services  
Telephone 7109 7016  
Email [dlcptipdclearanceletters@sa.gov.au](mailto:dlcptipdclearanceletters@sa.gov.au)

State Commission  
Assessment Panel

16 July 2019

Level 5  
50 Flinders Street  
Adelaide SA 5000

City Manager  
City of West Torrens  
165 Sir Donald Bradman Dr.  
HILTON SA 5033

GPO Box 1815  
Adelaide SA 5001

Dear Sir

08 7109 7061

**Re: Proposed Development Application No. 211/C067/19 (ID 65477)  
for Land Division (*Community Title Plan*) by Advanced Development Group Solutions**

Further to my letter dated 4 July 2019 and to assist the Council in reaching a decision on this application, copies of consultation agency reports received by the State Commission Assessment Panel (SCAP) are available for your consideration.

**IT IS REQUESTED PURSUANT TO SECTION 33 (1) (c) OF THE *DEVELOPMENT ACT 1993* THAT THE COUNCIL INCLUDE IN ITS DEVELOPMENT APPROVAL THE FOLLOWING REQUIREMENTS OF THE SCAP.**

1. The financial requirements of the S A Water Corporation shall be met for the provision of water supply and sewerage services. (S A Water H0087471)

SA Water advises on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non-standard fees.

The developer must inform potential purchasers of the community lots in regards to the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

2. Payment of \$43,518.00 into the Planning and Development Fund (6 allotment/s @ \$7,253.00 /allotment). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Department of Planning, Transport and Infrastructure marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, by cheque or credit card, at Level 5, 50 Flinders Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

**IT IS ALSO REQUIRED THAT COUNCIL PROVIDE THE SCAP WITH:**

- a) the date on which any existing building(s) on the site were erected (if known);
- b) the postal address of the site; and
- c) a copy of its Decision Notification Form (via EDALA) pursuant to Regulations 60 (4) (b) ii and 44 respectively.

**IT IS RECOMMENDED THAT THIS INFORMATION BE INCORPORATED INTO COUNCIL'S ADVICE WHEN REPORTING THAT THEIR REQUIREMENTS (IF ANY) HAVE BEEN FULLY SATISFIED.**

Yours faithfully,

A handwritten signature in blue ink, appearing to read 'Biljana B.'.

Biljana Prokic  
**LAND DIVISION COORDINATOR – PLANNING SERVICES**  
as delegate of the  
**STATE COMMISSION ASSESSMENT PANEL**

## 6.2 34-48 Lipsett Terrace, BROOKLYN PARK

Application No 211/646/2019

### DEVELOPMENT APPLICATION DETAILS

DESCRIPTION OF DEVELOPMENT	Installation of 1 wall mounted sign, 1 double sided pylon sign, and relocation of existing freestanding advertisement
APPLICANT	Tonkin Schutz Design Build
LODGEMENT DATE	5 July 2019
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 20
APPLICATION TYPE	Non-complying
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> <li>• Nil</li> </ul> External <ul style="list-style-type: none"> <li>• Nil</li> </ul>
DEVELOPMENT PLAN VERSION	12 July 2018
DELEGATION	<ul style="list-style-type: none"> <li>• The relevant application proposes a non-complying form of development and the application is to be determined after a full merit assessment against the Development Plan, except <b>where</b> the relevant development application proposes a change of use to office in a Commercial Zone.</li> </ul>
RECOMMENDATION	Support with conditions
AUTHOR	Phil Smith

### SUBJECT LAND AND LOCALITY

The subject land is formally described as Allotment 200 in Community Plan 41407 in the area named Brooklyn Park, Hundred of Adelaide, Volume 6211 Folio 969, more commonly known as 34-48 Lipsett Terrace, Brooklyn Park. The subject site is irregular in shape with a 100.6 metre (m) wide frontage to Lipsett Terrace and a site area of 7,638 square metres (m<sup>2</sup>).

There are no easements, encumbrances or Land Management Agreements registered on the Certificate of Title and no regulated trees on the subject site or on adjoining land that would be affected by the development.

The site is relatively flat and currently contains one contiguous two storey school building (Emmaus Christian College) in the northern half of site and a second smaller two storey building located adjacent to Sir Donald Bradman Drive (Adelaide West Uniting Church). The site also contains associated car parking and landscaping. There are three freestanding signs and one wall mounted sign which are associated with the church use and located to face Sir Donald Bradman Drive.

Small directional signage and one freestanding sign are located adjacent to Lipsett Terrace to identify the school use.

The locality predominantly consists of residential land uses in all directions leading from the subject site. Opposite the site and further to the east is Brooklyn Park Private Kindergarten and John Bosco School, which both have a number of signs identifying their respective land uses along Lipsett Terrace.

The site directly opposite the subject site to the north (25-29 Lipsett Terrace) is to be redeveloped with new dwellings approved under DA 211/157/2018, which are oriented away from Lipsett Terrace. The rear yards will face the street, with a 1.8m high fence interfacing Lipsett Terrace.

The site and locality are shown in the following figures, and on the aerial imagery and maps below.



Figure 1: View towards the western end of the subject site





**Figure 2: View towards the eastern end of the subject site**



**Figure 3: Close up view of existing directional sign at the western end of the subject site**





Figure 4: View of the main entry of the school



Figure 5: Close up view of proposed location for existing sign





**Figure 6: View directly across the street from the subject site**



**Figure 7: View directly across the street from the subject site**

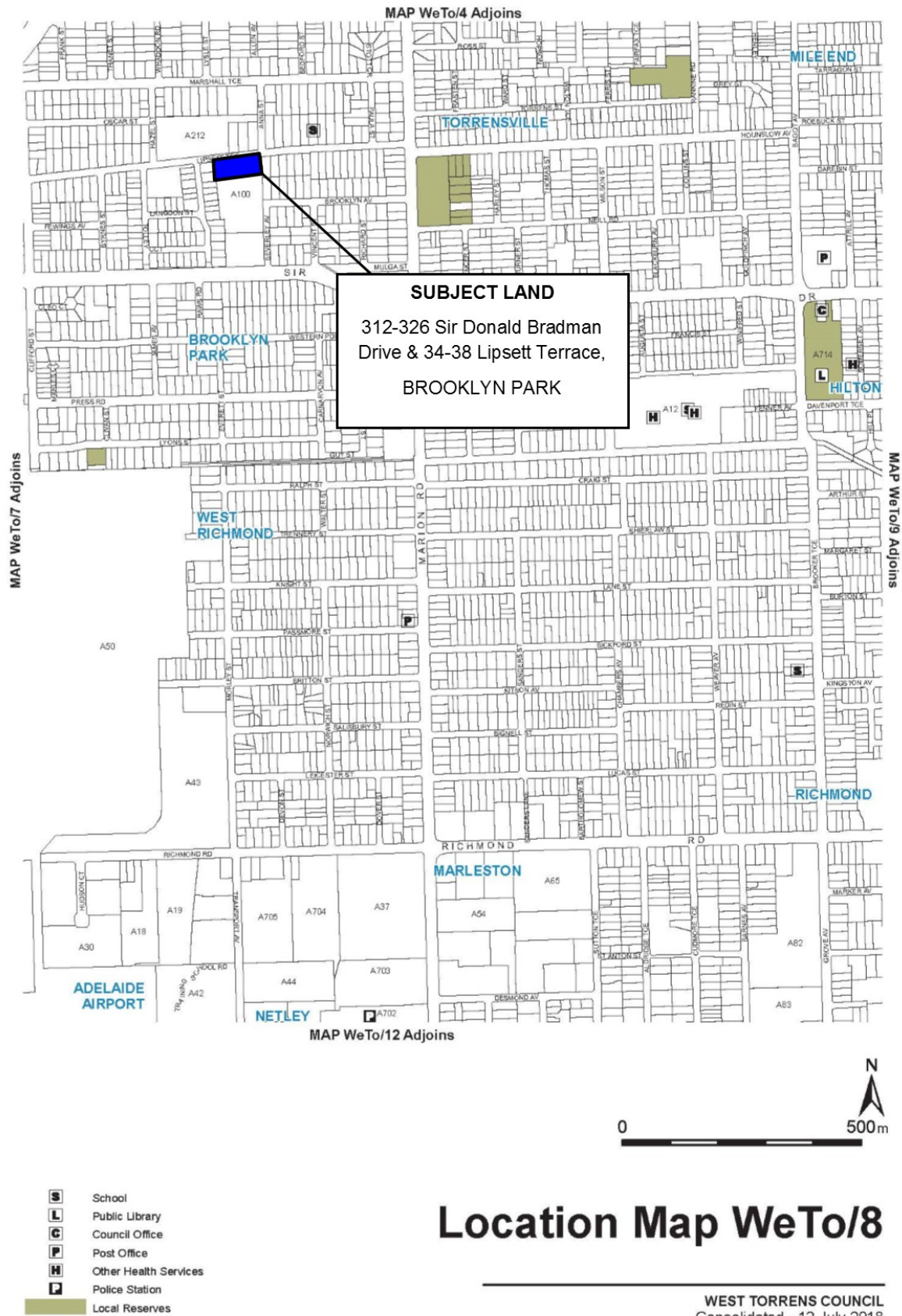


Figure 8: View of kindergarten across from and to the east of the subject site









**RELEVANT APPLICATIONS**

DA Number	Description of Development	Decision	Decision Date
211/696/1999	Free standing sign	Development Approval Granted	24 August 1999

**PROPOSAL**

The applicant is seeking Development Plan consent for:

- installation of 1 wall mounted sign which is the main entry sign measuring 0.9m<sup>2</sup> in area;
- installation of 1 double sided pylon sign measuring 7.65m<sup>2</sup> in area;
- relocation of an existing freestanding advertisement so that it will be attached to the front fence measuring 2.88m<sup>2</sup> in area; and
- rebadging of the existing directional sign measuring 1.44m<sup>2</sup> in area.

The relevant plans and documents are contained in **Attachment 2**.

**NON-COMPLYING**

The Procedural Matters section of the Residential Zone captures all advertisements as non-complying development.

The applicant has not provided a Statement of Effect pursuant to Regulation 17(6) of the *Development Regulations 2008* (refer **Attachment 2**) as Council considers that the proposed development is of a minor nature and would not unreasonably impact upon any adjoining resident for reasons that will be described later in the report.

Should the CAP resolve to approve the application, the concurrence of the State Commission Assessment Panel is required. Alternatively, should the CAP refuse the application, no appeal rights are afforded to the applicant. As the administration resolved, under delegation, to proceed with an assessment of the proposal, the application is now presented to the Panel for a decision.

**PUBLIC NOTIFICATION**

The application is considered to be a Category 1 form of development pursuant to Schedule 9, Part 1, (3)(b) of the *Development Regulations 2008*. This clause involves three elements which are discussed below:

- **The proposal involves the construction of a building to be used as ancillary to/associated with an existing building.**  
The proposed signage falls within the definition of a building (which includes a structure) and will be used to advertise the school occupying the subject land.
- **The proposal will facilitate the better enjoyment of the purpose for which the existing building is being used.**  
The proposed signage will assist in identifying the school and attracting future students to support the continued use of the site. This is considered to facilitate the better enjoyment of the purpose for which the existing building is being used.



- **The proposal constitutes, in the opinion of the relevant authority, development of a minor nature only.**

The proposed signage is considered to be relatively minor in scale taking into account the existing signage being replaced and the size of other existing advertisements in the locality.

The additional signage has an approximate advertisement area of 8.6m<sup>2</sup> which is mostly taken up by the pylon sign. In addition, the proposed signage is to be non-illuminated.

As the proposal is Category 1, public notification was not required to be undertaken.

## RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the Residential Zone and, more specifically, Low Density Policy Area 20 as described in the West Torrens Council Development Plan.

The relevant Desired Character statements and associated Objectives and Principles of Development Control (PDC) are as follows:

<b>Residential Zone - Desired Character</b>	
<i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i>	
<i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i>	
<i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i>	
<i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i>	
<b>Objectives</b>	<b>4</b>
<b>Principles of Development Control</b>	<b>1, 2, 5</b>

<b>Low Density Policy Area 20 - Desired Character</b>	
<p><i>Allotments in the policy area will be at low density, accommodating predominantly detached dwellings and some other dwellings types such as semi-detached and group dwellings. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battle-axe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.</i></p>	
<i>Objective</i>	<i>1</i>
<i>Principles of Development Control</i>	<i>1</i>

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

## **ASSESSMENT**

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### **Land Use, Desired Character and Existing Character**

The proposed signage will be associated with an existing and lawful school. Whilst advertisements (signage) are not specifically listed as an envisaged form of development, schools are envisaged within the zone and policy area. It is considered that signage is an integral part of an educational establishment and therefore an appropriate form of development.

It is noted within the Procedural Matters of the Residential Zone that any form of advertisement in the Zone is non-complying in order to discourage signs in general and avoid proliferation of advertisements within the urban environment. Whilst the signs are non-complying, it is considered that the locality supports the scale of signage proposed as a result of the existing school land use. Furthermore, the proposal comprises shifting or rebadging of existing signage or new signage that identifies the main entry into the school. The remaining sign proposed assists in identifying the site to passing motorists and pedestrians from a short distance away from the site. Given the scale and appearance of existing signage within the locality, and that the largest sign (pylon sign) is 7.65m<sup>2</sup>, it is unlikely that the proposed signage will negatively impact on the existing character of the locality.

### **Design and Appearance**

It is considered that the proposed signs are designed well, being of a uniform, neat and professional appearance with a limited colour palette (navy, gold and white). The content of the signs relate to the legitimate use of the land. The signs do not extend higher than the top of the wall and are designed to conceal their supporting hoarding. Accordingly, the proposed signs are considered to be in keeping with the existing character of the locality and will result in a better design outcome than a number of existing signs within the locality. Taking into consideration the above, the proposed signs satisfy Objectives 1 and 3 and Principles of Development Control (PDCs) 1, 2, 4, 5, 6, 7, 10, and 11 of the Advertisements module.

## Amenity

The amenity of adjoining properties is unlikely to be negatively impacted by the proposed signs as they are non-illuminated and will largely be unseen by occupants of the adjoining properties on the northern side of Lipsett Terrace, as these dwellings are rear-facing to Lipsett Terrace. Due to the lengthy frontage width of the subject site, dwellings to the east and west of the school will have minimal, if any, direct views of the signage, thus satisfying Objective 3 and PDC 5(c) of the Advertising module.

Furthermore, the proposed signs are not too dissimilar to the signage located at the Brooklyn Park Private Kindergarten and John Bosco School further along Lipsett Terrace, both of which are sited directly opposite dwellings that face Lipsett Terrace. For this reason, the proposed signage is considered to satisfy PDCs 1 and 2 of the Advertisements module.

## Safety

The proposed signage is unlikely to cause a hazard to road users as it is non-illuminated. Accordingly, Objective 2 and PDCs 2(d) and 14 of the Advertisements module are satisfied.

## SUMMARY

The proposal involves the installation of 1 wall mounted sign, 1 double sided pylon sign, and relocation of an existing freestanding advertisement. Despite its non-complying status, the signs are considered to be an appropriate form of development taking into account the context of the locality, particularly in terms of existing signage. The signs are of a high design standard and integrate well with the existing building to which they are to be attached. The proposed signage is non-illuminated thus will not cause nuisance to adjoining land owners or a hazard to road users.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is not considered to be seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and warrants Development Plan Consent.

## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/646/2019 by Tonkin Schutz Design Build to install 2 flag poles, 4 signs, including updating main entry sign at 34-48 Lipsett Terrace, Brooklyn Park and 312-326 Sir Donald Bradman Drive, Brooklyn Park (CTs 6211/970 & 6211/969) subject to the concurrence of the State Commission Assessment Panel and the following conditions of consent:

### Development Plan Consent Conditions

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this Application except where varied by any condition(s) listed below.

*Reason: To ensure the proposal is developed in accordance with plans and documents lodged with Council.*

2. The content of the signage approved herein shall relate to the legitimate use of the land at all times and shall not be used for third party advertising.

*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*



3. Lighting associated with the signs shall be of an intensity to not cause light spill nuisance to adjacent occupiers, or cause a distraction to drivers on adjacent public roads.

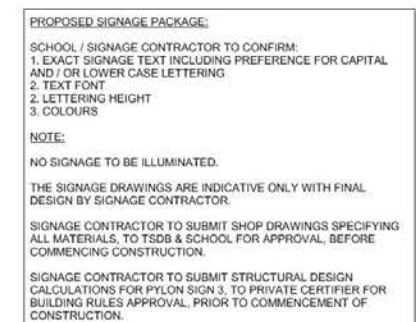
*Reason: To ensure the approved signage does not cause undue disturbance, annoyance or inconvenience to adjoining land users and motorists.*

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### **Attachments**

1. **Relevant Development Plan Provisions**
2. **Application Plans**

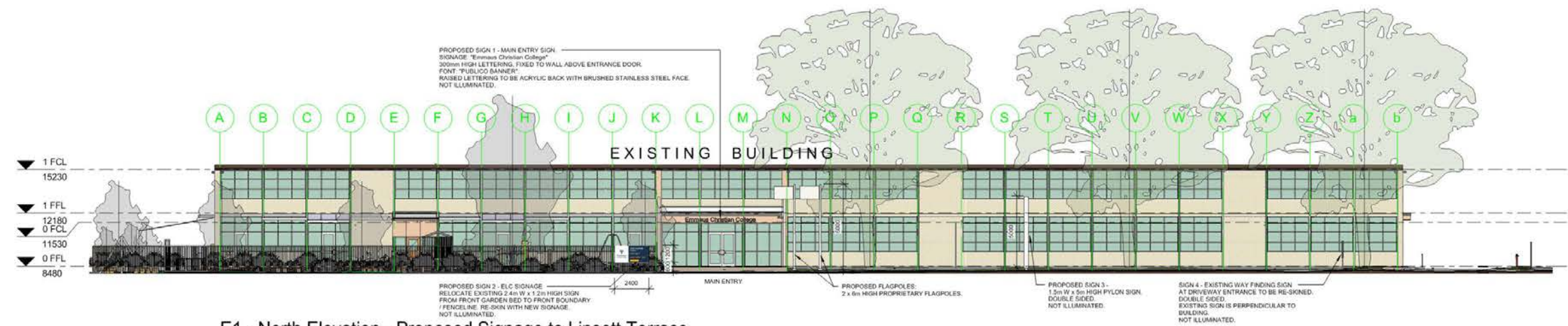
<b>General Section</b>		
Advertisements	<i>Objectives</i>	1, 2 & 3
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 9, 10, 11, 14, 15, 16, 17, 18, 19, 20, 21 & 22
Design and Appearance	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 8, 10, 11, 12, 13, 14, 15, 16
Orderly and Sustainable Development	<i>Objectives</i>	1, 2, 3, 4
	<i>Principles of Development Control</i>	1



Site Plan - Proposed Signage Package  
1 : 200

SCALE As indicated (Ⓐ A1) or 1:400(Ⓐ A3)  
SGN-01.A





E1 - North Elevation - Proposed Signage to Lipsett Terrace  
1 : 150



E2 West Elevation - Proposed Signage  
1 : 100



Lipsett Tce 3D / Street View of Signage  
1 : 1

**PROPOSED SIGNAGE PACKAGE:**

SCHOOL / SIGNAGE CONTRACTOR TO CONFIRM:

1. EXACT SIGNAGE TEXT INCLUDING PREFERENCE FOR CAPITAL AND / OR LOWER CASE LETTERING
2. TEXT FONT
3. LETTERING HEIGHT
3. COLOURS

**NOTE:**

NO SIGNAGE TO BE ILLUMINATED.

THE SIGNAGE DRAWINGS ARE INDICATIVE ONLY WITH FINAL DESIGN BY SIGNAGE CONTRACTOR.

SIGNAGE CONTRACTOR TO SUBMIT SHOP DRAWINGS SPECIFYING ALL MATERIALS, TO TSDB & SCHOOL FOR APPROVAL, BEFORE COMMENCING CONSTRUCTION.

SIGNAGE CONTRACTOR TO SUBMIT STRUCTURAL DESIGN CALCULATIONS FOR PYLON SIGN 3, TO PRIVATE CERTIFIER FOR BUILDING RULES APPROVAL, PRIOR TO COMMENCEMENT OF CONSTRUCTION.

Proposed ELC & Primary School Refurbishment  
Emmaus Christian College - 34-38 Lipsett Terrace, Brooklyn Park, S.A.

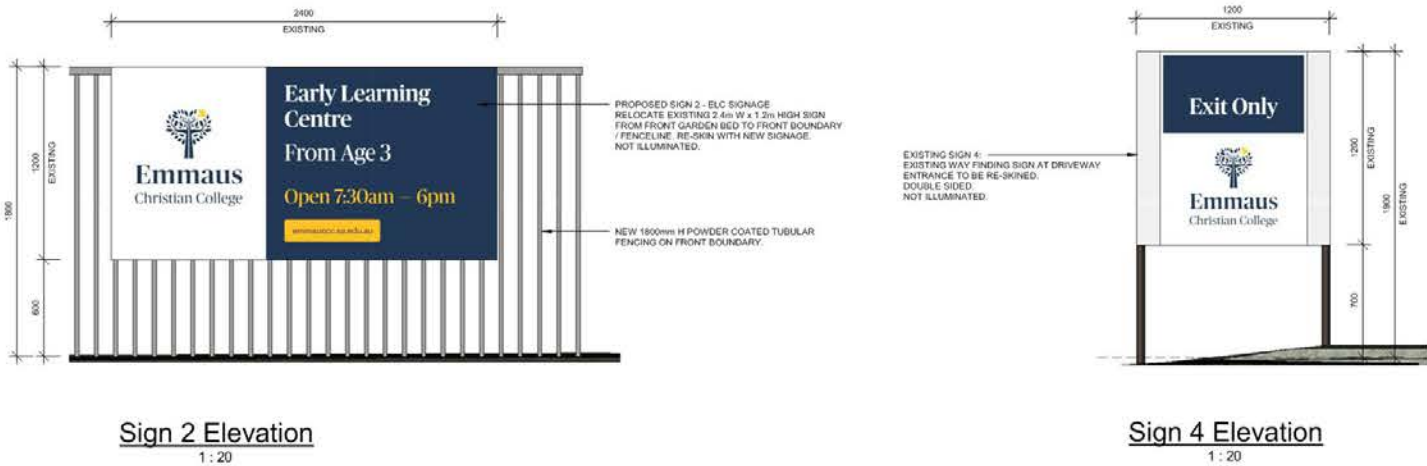
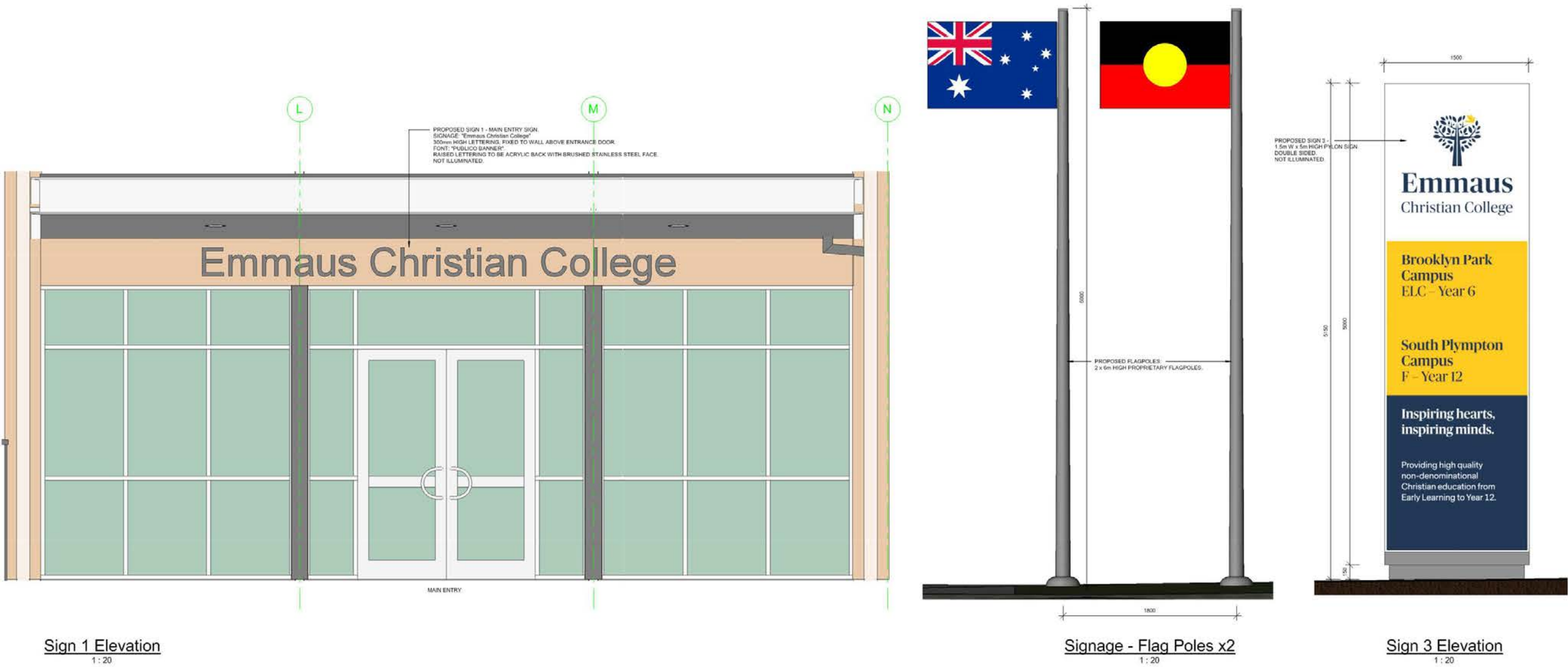
PROJECT NUMBER  
0259  
4/07/2019 4:01:24 PM

**TONKIN  
SCHUTZ** design  
build  
16-22 Erindale Ave, Edmondstown S.A. 5039  
Ph: (08) 8277 0111  
Fax: (08) 8277 2255  
Commercial - Industrial - Domestic  
© copyright

**SIGNAGE PACKAGE SHEET 2**

SCALE: As indicated ( @ A1) or 1:200 ( @ A3)  
**SGN-02.A**





**PROPOSED SIGNAGE PACKAGE:**

SCHOOL / SIGNAGE CONTRACTOR TO CONFIRM:

1. EXACT SIGNAGE TEXT INCLUDING PREFERENCE FOR CAPITAL AND / OR LOWER CASE LETTERING
2. TEXT FONT
3. LETTERING HEIGHT
3. COLOURS

**NOTE:**

NO SIGNAGE TO BE ILLUMINATED.

THE SIGNAGE DRAWINGS ARE INDICATIVE ONLY WITH FINAL DESIGN BY SIGNAGE CONTRACTOR.

SIGNAGE CONTRACTOR TO SUBMIT SHOP DRAWINGS SPECIFYING ALL MATERIALS, TO TSDB & SCHOOL FOR APPROVAL, BEFORE COMMENCING CONSTRUCTION.

SIGNAGE CONTRACTOR TO SUBMIT STRUCTURAL DESIGN CALCULATIONS FOR PYLON SIGN 3, TO PRIVATE CERTIFIER FOR BUILDING RULES APPROVAL, PRIOR TO COMMENCEMENT OF CONSTRUCTION.

### 6.3 9 Weston Street, WEST BEACH

Application No 211/613/2019

#### DEVELOPMENT APPLICATION DETAILS

DESCRIPTION OF DEVELOPMENT	Land Division - Torrens Title; SCAP No. 211/D065/19; Create one (1) additional allotment
APPLICANT	RJ & RB Enterprises Pty Ltd
LODGEMENT DATE	25 June 2019
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 21
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> <li>Nil</li> </ul> External <ul style="list-style-type: none"> <li>State Commission Assessment Panel (SCAP)</li> <li>South Australian Water Corporation (SA Water)</li> </ul>
DEVELOPMENT PLAN VERSION	Consolidated 12 July 2018
DELEGATION	<ul style="list-style-type: none"> <li>The relevant application proposes a merit form of development which does not meet the minimum site area requirements in the relevant Zone or Policy Area by 7.5% or more.</li> </ul>
RECOMMENDATION	Support with conditions
AUTHOR	Sonia Gallarello

#### BACKGROUND

This application commenced as a combined application for land division and two x two storey detached dwellings. A further information letter was sent to the applicant requesting that they address a number of items including an improvement to the front elevation of the two proposed dwellings. At the time of writing, this issue remains unresolved. It was suggested to the applicant that an amendment to remove the dwelling component of the application be made with Council to assess the land division component only. Should the land division be approved, the dwelling applications could then be lodged subsequently and separately. This approach was accepted by the applicant.

#### SUBJECT LAND AND LOCALITY

The subject land is formally described as Allotment 86 in Deposited Plan 6945 in the area named West Beach, Hundred of Adelaide, Volume 5357 Folio 386, more commonly known as 9 Weston Street. The subject site is rectangular in shape with a 19.8 metre (m) wide frontage to Weston Street and a site area of 755 square metres (m<sup>2</sup>).

It is noted that there are no easements, encumbrances or Land Management Agreements on the Certificate of Title and there are no regulated trees on the subject site or on adjoining land that would be affected by the development.

The site is relatively flat and currently contains a single storey detached dwelling and outbuildings.



The locality largely consists of single storey detached dwellings established around 1960 with some more recent subdivided allotments with new single or two storey detached dwellings established from 2000. To the rear is a sporting oval and facilities used for various sports including hockey.

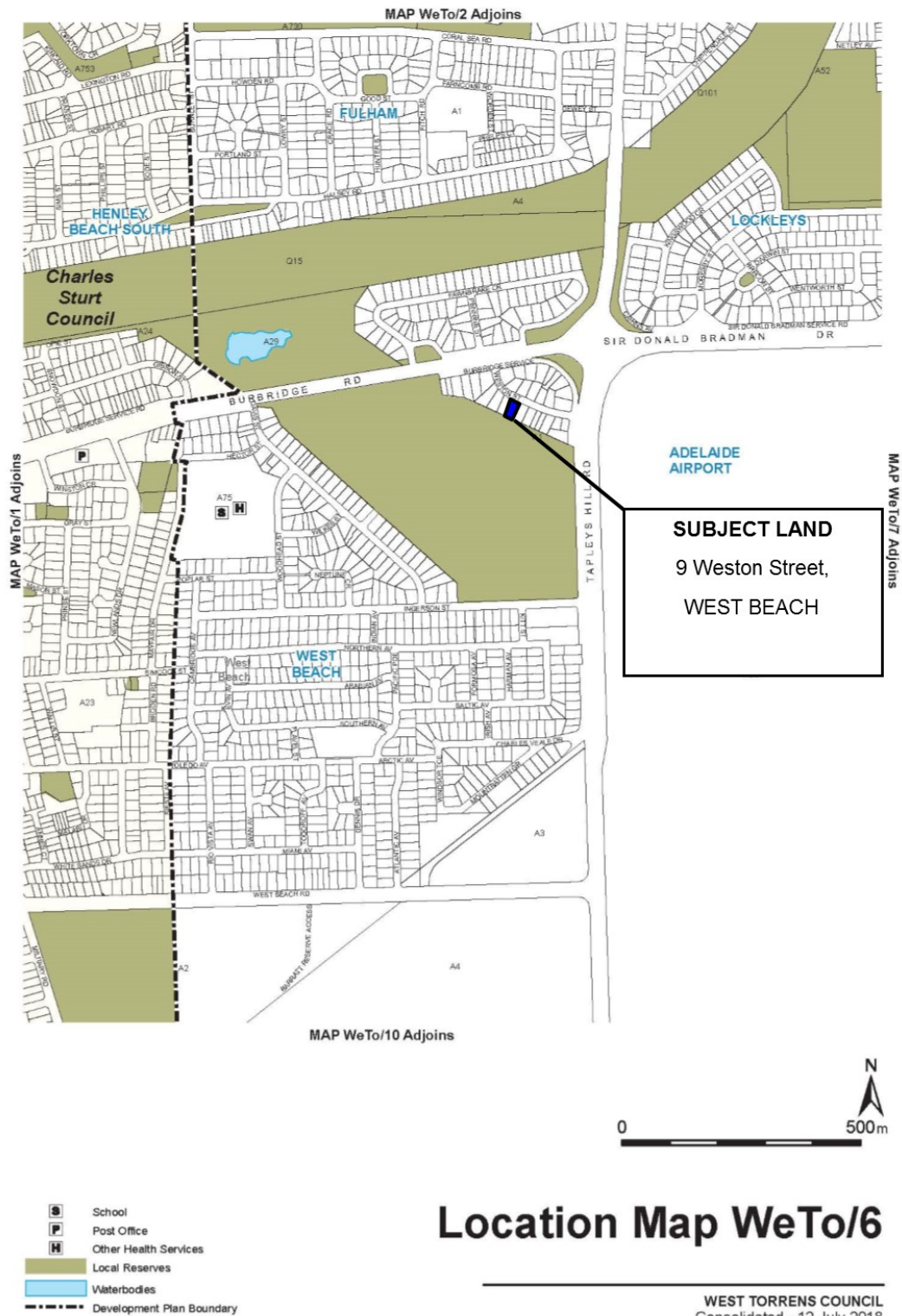
While the subject site is not within 400m of a Centre zone, it is close to the Adelaide airport land which has a variety of different convenience shops. The site is also close to public transport available on Burbridge Road and Tapleys Hill Road.

The amenity of the locality is considered to be of moderate quality with a few recent examples of development similar to that proposed. Allotment sizes vary from around 340m<sup>2</sup> up to around 800m<sup>2</sup>. The newer dwellings tend to be double storey with a prominent double garage.

The subject land sits slightly outside of the ANEF 20 contour and within a height restricted area to above 12 metres.

The subject land and locality are shown on the aerial imagery and maps below.







## RELEVANT APPLICATIONS

Address	DA Number	Description of Development	Decision	Decision Date	Allotment sizes
2 Weston Street	211/1256/2018	Land division - one allotment into two	Approved	December 10, 2018	336m <sup>2</sup> & 335m <sup>2</sup>
2 & 4 Weston Street	211/1519/2017	Two dwellings	Approved	January 31, 2019	
8 Weston Street	211/742/2018	Two storey dwelling	Approved	September 25, 2018	
8 Weston Street	211/1043/2016	Land Division - one allotment into two	Approved	October 27, 2017	399m <sup>2</sup> & 409m <sup>2</sup>
15 Weston Street	211/939/1999	Land Division - one allotment into two	Approved	November 15, 1999	408m <sup>2</sup> & 415m <sup>2</sup>
15 Weston Street	211/177/2001	Single storey detached dwelling	Approved	April 3, 2001	
15A Weston Street	211/811/2000	Two storey detached dwelling	Approved	September 7, 2000	

The dwellings at 2 and 4 Weston Street and subsequent land division were approved as residential code development issued by a private certifier with full approval issued by Council. The other applications were assessed as merit forms of development.

## PROPOSAL

As discussed, this application initially sought approval for a combined development. The application has subsequently been amended seeking Land Division Consent, Development Plan Consent and Development Approval for a Torrens Title land division application creating two allotments (one additional) with the following dimensions:

Allotment	Size	Frontage
300	377m <sup>2</sup>	9.9m
301	378m <sup>2</sup>	9.9m

In terms of driveway options, there is an existing crossover on the eastern side of the site, a street tree slightly east of the centre of the site and a stobie pole toward the west. This provides a number of options for driveway locations that can minimise the impact on verge infrastructure.

The relevant plans for the land division component are contained in **Attachment 2**. It was not deemed necessary to require an indicative land use/dwelling plan.

## EXTERNAL REFERRALS

Department	Comments
SCAP	No concerns were raised by the SCAP and standard conditions have been recommended should the application be supported.
SA Water	SA Water raised no concerns with the proposal and have recommended standard conditions should the application be supported.

A copy of the relevant referral response is contained in **Attachment 3**.

## RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the Residential Zone and, more specifically, Low Density Policy Area 21, as described in the West Torrens Council Development Plan.

The relevant Desired Character statements are as follows:

Residential Zone - Desired Character	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objectives	4
Principles of Development Control	5

<b>Low Density Policy Area 21 - Desired Character</b>	
<p><i>This policy area will have a low density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).</i></p> <p><i>There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage. In the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park, where the consistent allotment pattern is a significant positive feature of the locality, subdivision will reinforce the existing allotment pattern.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings. Buildings in the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park will be complementary to existing dwellings through the incorporation of design features such as pitched roofs, eaves and variation in the texture of building materials.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.</i></p>	
Objectives	1
Principles of Development Control	1, 6

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

## QUANTITATIVE STANDARDS

The proposal is assessed for consistency with the quantitative requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
ALLOTMENT AREA <i>Low Density Policy Area 21</i> <i>PDC 6</i>	420m <sup>2</sup> (min.)	377m <sup>2</sup> (lot 300) 378m <sup>2</sup> (lot 301)  <b>Does Not Satisfy</b>
ALLOTMENT FRONTAGE <i>Low Density Policy Area 21</i> <i>PDC 6</i>	12m (min.)	9.9m (lot 300) 9.9m (lot 301)  <b>Does Not Satisfy</b>



## ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### Desired character

The desired character statement for the Zone and Policy Area seeks low density residential development. Given that the proposed allotments, if developed with a single dwelling on each, would yield a net density of 27 dwellings per hectare, the proposal is considered to achieve the density sought.

### Allotment area and frontage

Both the allotment areas and frontage widths fall short of the requirement specified in PDC 6 of Low Density Policy Area 21. However, the proposal does meet the intent of the policy area by providing two low density allotments of sufficient size and frontage width to accommodate associated low density housing.

As outlined in the table above, other divisions have occurred within the locality and along Weston Street that have a similar size and frontage to the proposed development. As a result, the development would not be out of character with the prevailing streetscape.

While the subject land is not within 400m of a Centre Zone (allowing for reduced allotment sizes and frontage widths), it is nonetheless within a one kilometre radius of four supermarkets / bulky good stores including Foodland, Woolworths, IKEA and ALDI. The subject land is located around 600m from ALDI which is highly accessible. An element of practicality should be applied in this instance as the subject land has a higher degree of accessibility to facilities usually found in centre zones than a lot of land located within the 400m Centre Zone buffers.

## SUMMARY

The proposed development proposes two low density sized allotments in an area that has high accessibility to shops (on airport land), public open space and public transport.

Having considered all the relevant provisions of the Development Plan, the proposal is not considered to be seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and warrants Development Plan Consent, Land Division Consent and Development Approval.

## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent, Land Division Consent and Development Approval for Application No. 211/613/2019 by RJ & RB Enterprises Pty Ltd to undertake a Land Division - Torrens Title; SCAP No. 211/D065/19; Create one (1) additional allotment at 9 Weston Street, West Beach (CT 5357/386) subject to the following conditions of consent:

### Development Plan Consent Conditions:

1. The development shall be undertaken, completed and maintained in accordance with the following plans and information detailed in this application except where varied by any condition listed below:
  - a) Survey Plan by Elite Land Solutions, File Ref ELS 084-19 Proposal, Revision A

*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*

**Land Division Consent Conditions:  
SCAP Requirements**

2. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

On receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non -standard fees.

On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

*Reason: To satisfy the requirements of the South Australian Water Corporation.*

3. Payment of \$7253 into the Planning and Development Fund (1 allotment @ \$7253/allotment). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Department of Planning, Transport and Infrastructure and marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Level 5, 50 Flinders Street, Adelaide.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

4. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

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**Attachments**

1. **Relevant Development Plan provisions**
2. **Proposal plan**
3. **External referrals**

**ASSESSMENT TABLE – 12 JULY 2018**

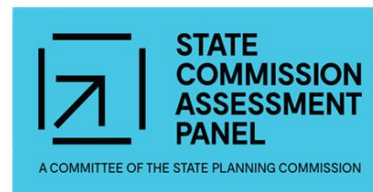
<u>General Section</u>		
<i>Land Division</i>	<i>Objectives</i>	<i>1, 2, 3, 4</i>
	<i>Principles of Development Control</i>	<i>1, 2, 5, 6, 8</i>





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Contact Planning Services  
Telephone 7109 7016  
Email [dldptipdc Clearanceletters@sa.gov.au](mailto:dldptipdc Clearanceletters@sa.gov.au)



18 July 2019

The Chief Executive Officer  
City of West Torrens

Dear Sir/Madam

**Re: Proposed Application No.** 211/D065/19 (ID 65431)  
**for Land Division by** RJ & RB Enterprises Pty. Ltd.

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 27 June 2019, I advise that the State Commission Assessment Panel (SCAP) has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the State Commission Assessment Panel has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.  
On receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non -standard fees.  
On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.
2. Payment of \$7253 into the Planning and Development Fund (1 allotment(s) @ \$7253/allotment).  
Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Department of Planning, Transport and Infrastructure and marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Level 5, 50 Flinders Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

**PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.**

Yours faithfully

A handwritten signature in blue ink that reads "Prokic B." The signature is written in a cursive style with a large 'P' and a distinct 'B'.

Biljana Prokic  
**Land Division Coordinator - Planning Services**  
as delegate of  
**STATE COMMISSION ASSESSMENT PANEL**



18 July 2019

Our Ref: H0087144

The Chairman  
State Commission Assessment Panel  
50 Flinders St  
ADELAIDE SA 5000

Dear Sir/Madam

**PROPOSED LAND DIVISION APPLICATION NO: 211/D065/19 AT WEST BEACH**

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

On receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non -standard fees.

On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

Yours faithfully

CAROL CARY  
for **MANAGER LAND DEVELOPMENT & CONNECTIONS**

SA Water  
Level 6, 250 Victoria Square  
ADELAIDE SA 5000  
Ph (08) 7424 1119  
Inquiries CAROL CARY  
Telephone 7424 1119



## **7 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER**

Nil

## 8 SUMMARY OF COURT APPEALS

### 8.1 Summary of ERD Court matters, items determined by SCAP/Minister/Governor and deferred CAP items - October 2019

#### Brief

This report presents information in relation to:

1. any planning appeals before the Environment, Resources and Development (ERD) Court;
2. any matters being determined by the State Commission Assessment Panel (SCAP);
3. any matters determined by the Minister of Planning (Section 49);
4. any matters determined by the Governor of South Australia (Section 46); and
5. any deferred items previously considered by the Council Assessment Panel.

#### Development Application appeals before the ERD Court

Nil

#### Matters pending determination by SCAP

Reason for referral	DA number	Address	Description of development
Schedule 10	211/M030/18	192 ANZAC Highway, GLANDORE	Eight-storey RFB, 40 dwellings & removal of regulated tree
Schedule 10	211/M015/19	1 Glenburnie Terrace, PLYMPTON	Six-storey RFB, 32 dwellings & associated car parking
Schedule 10	211/M018/19	6 Ebor Avenue, MILE END	Mixed use building comprising ground floor shop & residential apartments

#### Matters pending determination by the Minister of Planning

Reason for referral	DA number	Address	Description of development
Section 49	211/V007/12 V3	Lot 2 in FP 1000, West Beach Road WEST BEACH	Variation - removal of east-west internal road

#### Matters pending determination by the Governor of South Australia

Nil

#### Deferred CAP Items

Nil

#### Conclusion

This report is current as at 26 September 2019.

#### RECOMMENDATION

The Council Assessment Panel receive and note the information.

#### Attachments

Nil

**9 OTHER BUSINESS**

Nil

**10 MEETING CLOSE**