

CITY OF WEST TORRENS



# Notice of Panel Meeting

Notice is Hereby Given that a Meeting of the  
**COUNCIL ASSESSMENT PANEL**

will be held in the George Robertson Room, Civic Centre  
165 Sir Donald Bradman Drive, Hilton

on

**TUESDAY, 11 JUNE 2019  
at 5.00pm**

**Donna Ferretti  
Assessment Manager**

## **City of West Torrens Disclaimer**

### **Council Assessment Panel**

Please note that the contents of this Council Assessment Panel Agenda have yet to be considered and deliberated by the Council Assessment Panel therefore the recommendations may be adjusted or changed by the Council Assessment Panel in the process of making the formal Council Assessment Panel decision.

**Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.**

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**1 MEETING OPENED****1.1 Evacuation Procedures****2 PRESENT****3 APOLOGIES****4 CONFIRMATION OF MINUTES****RECOMMENDATION**

That the Minutes of the meeting of the Council Assessment Panel held on 14 May 2019 be confirmed as a true and correct record.

**5 DISCLOSURE STATEMENTS**

In accordance with section 7 of the *Assessment Panel Members – Code of Conduct* the following information should be considered by Council Assessment Panel members prior to a meeting:

A member of a Council Assessment Panel who has a direct or indirect personal or pecuniary interest in a matter before the Council Assessment Panel (other than an indirect interest that exists in common with a substantial class of persons) –

- a. must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the panel; and
- b. must not take part in any hearings conducted by the panel, or in any deliberations or decision of the panel, on the matter and must be absent from the meeting when any deliberations are taking place or decision is being made.

If an interest has been declared by any member of the panel, the Assessment Manager will record the nature of the interest in the minutes of meeting.

## 6 REPORTS OF THE ASSESSMENT MANAGER

### 6.1 5 Wainhouse Street, TORRENSVILLE

Application No 211/1349/2018

Appearing before the Panel will be:

Representors: **Maria Fachin** of 6 Huntriss Street, Torrensville wishes to appear in support of the representation.

**Dr Will Cusworth and Kate Jefferis** of 8 Huntriss Street, Torrensville wish to appear in support of the representation.

Applicant: **T Kelly** of Future Urban Group on behalf of **E Politis** wishes to appear in response to the representations.

#### DEVELOPMENT APPLICATION DETAILS

DESCRIPTION OF DEVELOPMENT	Demolition of existing dwelling and associated ancillary domestic structures and construction of one single storey detached dwelling and a two-storey residential flat building comprising two dwellings
APPLICANT	E Politis
LODGEMENT DATE	19 December 2018
ZONE	Residential
POLICY AREA	Torrensville East Conservation Policy Area 33
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 2
REFERRALS	Internal <ul style="list-style-type: none"> <li>• City Assets</li> <li>• Heritage Advisor</li> </ul> External <ul style="list-style-type: none"> <li>• Nil</li> </ul>
DEVELOPMENT PLAN VERSION	Consolidated 12 July 2018
DELEGATION	<ul style="list-style-type: none"> <li>• The relevant application proposes one or more new dwellings in Residential Zone Conservation Policy Areas 29-33.</li> <li>• The relevant application is for a merit, Category 2 or Category 3 form of development, representations have been received and one or more representors wish to be heard on their representation.</li> </ul>
RECOMMENDATION	Support with conditions
AUTHOR	Josh Banks

## SUBJECT LAND AND LOCALITY

The subject land is formally described as Allotment 92 in Deposited Plan 1721 in the area named Torrensville Hundred of Adelaide, Volume 6084 Folio 927, more commonly known as 5 Wainhouse Street, Torrensville. The subject site is rectangular in shape with a 19.81 metre (m) wide frontage to Wainhouse Street and a site area of approximately 906 square metres (m<sup>2</sup>).

It is noted that there are no encumbrances or Land Management Agreements on the Certificate of Title and there are no regulated trees on the subject site or on adjoining land that would be affected by the development.

The site currently contains a bungalow style dwelling, with ancillary domestic structures. Topographically the land and surrounds appear relatively flat.

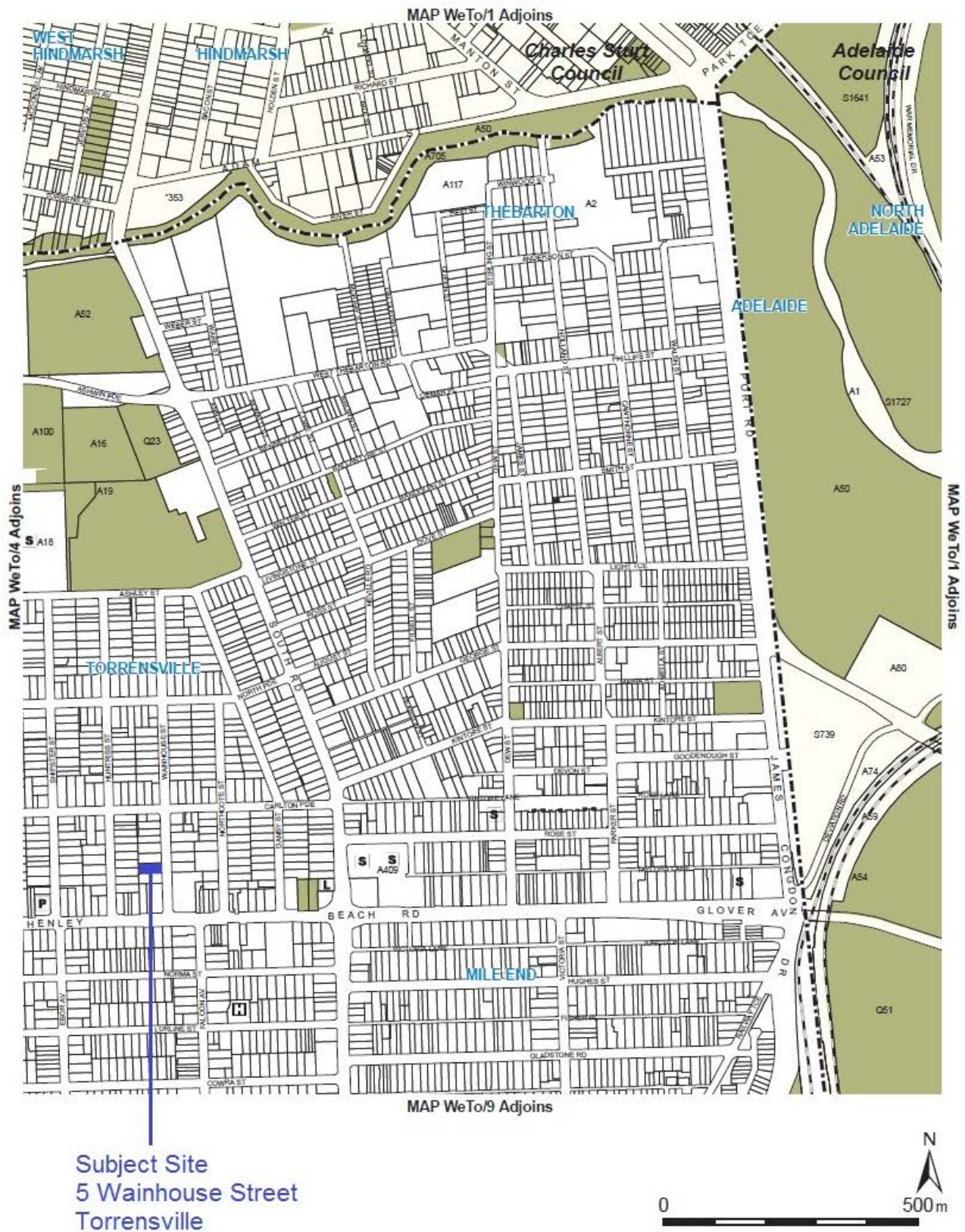
The locality consists of a mix of character residential and commercial uses. The areas to the north of the subject site are predominantly residential. The site is situated approximately 80m north of Henley Beach Road - an arterial road - and an activity centre which features a range of retail and commercial uses including shops, cafes, restaurants, and a shopping centre approximately 350m to the west. There are bus stops on both sides of Henley Beach Road located some 150m from the subject site.

The allotment pattern contains mostly regularly-shaped allotments although a wide variation in frontage widths is evident from the street. There is a notable example of a battle-axe configuration at 14 Wainhouse Street, with further interruptions of the pattern noted in the wider locality (e.g. 5, 12 and 16 Northcote Street, and 7 Huntriss Street). The various allotments to the south of the subject site that make up the retail and commercial sites at the southern end of Wainhouse Street have also disturbed the prevailing allotment pattern.

Council's records indicate that the majority of dwellings in the locality were established between 1900 and 1930, with a small number being established within the last two decades. Some dwellings have undergone additions or alterations but the character of these dwellings remains fairly consistent. Pitched roofs and front verandahs are prominent features, and most dwellings are set back consistently from the street with well-landscaped gardens.

The amenity of the locality is considered to be fair, with mature street vegetation lending strong positive contributions to the streetscape character. The nature and appearance of the commercial buildings - in particular the West Adelaide Telephone Exchange building and car park directly opposite the subject land - detract from the streetscape character and amenity giving the locality a more commercial 'feel'.

The subject land and locality are shown in the following imagery and maps below.



## Location Map WeTo/5

WEST TORRENS COUNCIL  
Consolidated - 12 July 2018













### Historical Context

The City of West Torrens commissioned a Local Character Study in 2010 to identify areas of importance as being notable and worthy of preservation or enhancement. It is noted that the subject land, or indeed Wainhouse Street, was not included within the selected Torrensville area of this report.

Council then undertook a Housing Diversity Development Plan Amendment (adopted in 2015) which resulted in the introduction of new Residential Character areas and the subsequent rezoning of the subject land from District Centre to Residential, as shown for comparative purposes below:



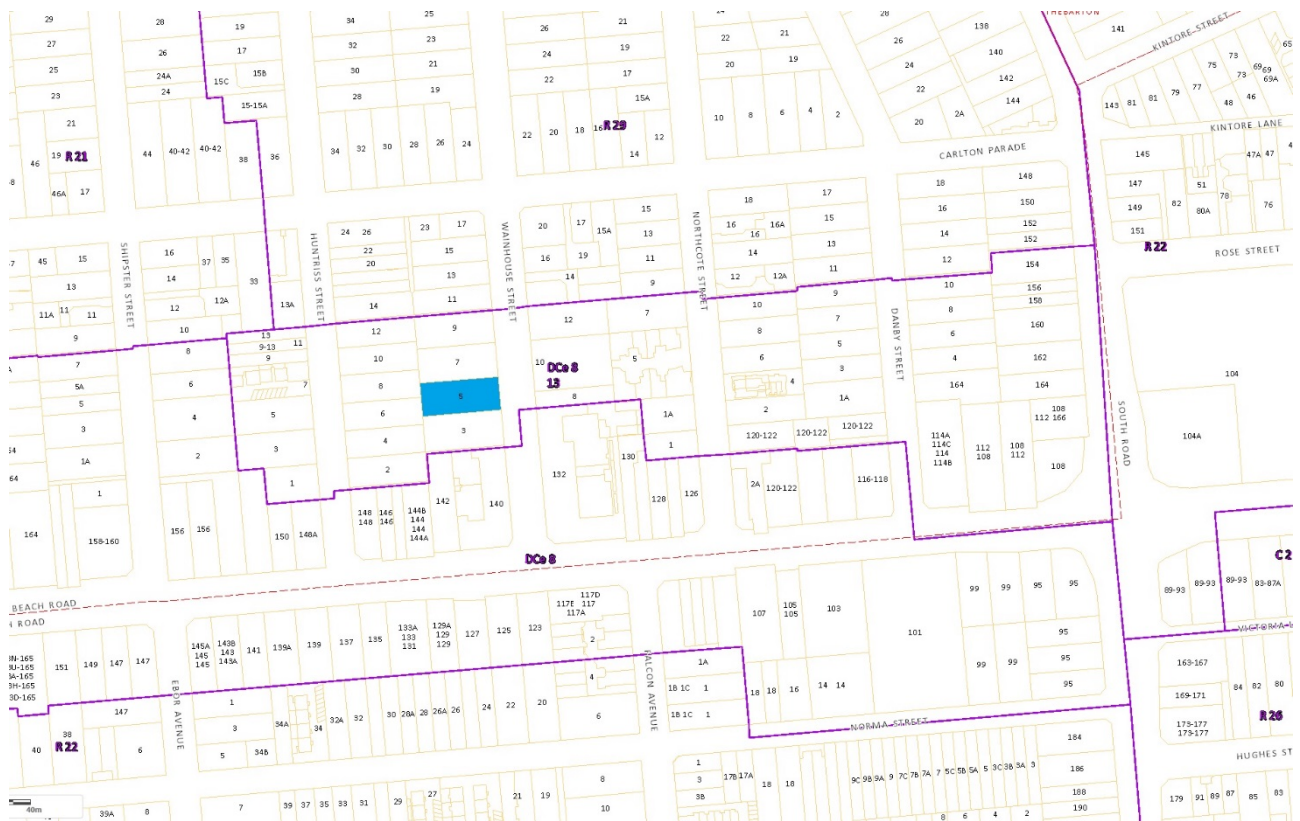


Figure 1: Subject Land formerly within District Centre Zone

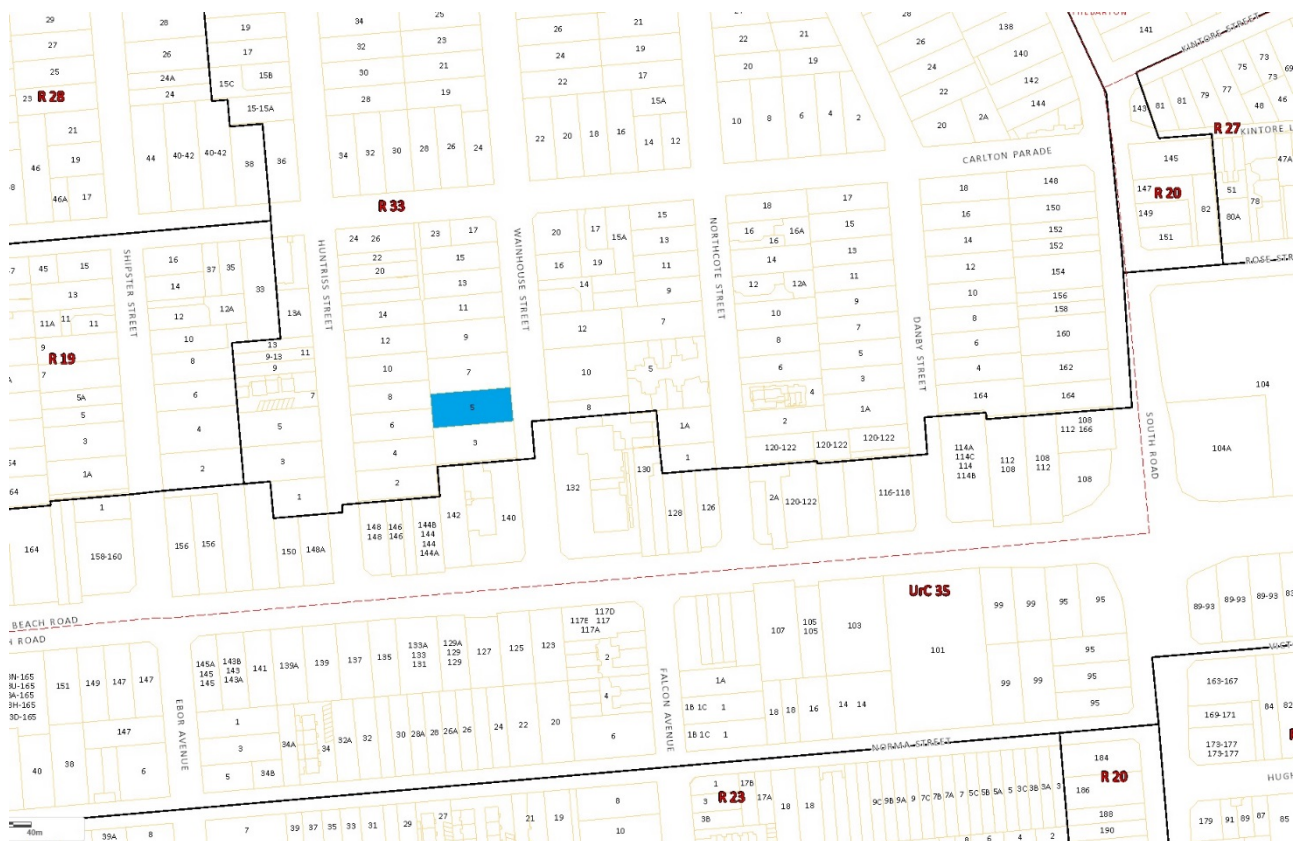


Figure 2: Subject Land as currently zoned - Residential



## PROPOSAL

The proposal is to demolish the existing dwelling and all associated structures, and construct one detached dwelling to the front and two dwellings to the rear of the site, including associated vehicle manoeuvring and landscaping areas.

Dwelling 1 will be a single-storey bungalow-style dwelling that will have direct street frontage on a site of approximately 345m<sup>2</sup>. It will comprise three bedrooms, a single garage with a new driveway to the street and north-facing private open space.

Dwellings 2 and 3 will be in the form of a two-storey residential flat building, each featuring three bedrooms, a garage and carport, with living areas to the ground floor and bedrooms to the upper floor. The upper floor is set back from side boundaries with the exception of weatherboard-clad access staircases to each side, with raked roofs to the single storey portions. The site areas average approximately 280m<sup>2</sup> for each dwelling including the common area.

The proposal also includes detailed landscaping to the site, an open pergola over a section of the shared driveway handle for access to the rear dwellings, and domestic storage sheds and 3000 litre rainwater tanks for each dwelling.

The relevant plans and supporting documents are contained in **Attachment 2**.

## PUBLIC NOTIFICATION

The application is a Category 2 form of development pursuant to Schedule 9 of the *Development Regulations 2008* or Procedural Matters section of the Residential Zone.

<b>Properties notified</b>	Eleven properties were notified during the public notification process.
<b>Representations</b>	Two representations were received.
<b>Persons wishing to be heard</b>	Two representors wish to be heard: <ul style="list-style-type: none"> <li>• Maria Fachin</li> <li>• Dr Will Cusworth and Kate Jefferis</li> </ul>
<b>Summary of representations</b>	Concerns were raised regarding the following matters: <ul style="list-style-type: none"> <li>• Homes located too close to land boundary</li> <li>• Second storey windows overlooking neighbouring yards</li> <li>• High density housing</li> <li>• Loss of character home (which Torrensville is known for)</li> <li>• Disregard for Heritage and Conservation Area provisions</li> <li>• Two-storey development inappropriate for site</li> <li>• Lack of frontage &amp; imposing overshadowing of neighbouring land to rear</li> <li>• Overlooking and overshadowing</li> <li>• Loss of visual amenity.</li> </ul>

<b>Applicant's response to representations</b>	<p>Summary of applicant's response:</p> <ul style="list-style-type: none"> <li>• Expert evidence from two heritage experts lies in favour of the proposed development</li> <li>• Two-storey development is common within the locality - the Telephone Exchange building, immediately adjacent the site, and the Spartan building 20m further south</li> <li>• Residential Development PDC 27 is met as the upper level rear-facing windows feature fixed obscured glazing to 1.7m above finished floor level</li> <li>• Residential Development PDCs 10, 11 and 12 are met as no shadow will be cast over 6 and 8 Huntriss Street after 12:30pm on 21 June</li> <li>• Proposed development would be an improvement and allows for strategic landscaping, and existing garage at 8 Huntriss Street would limit direct views of the development</li> <li>• Proposal is not high or medium density, as it achieves a net density of 33 dwellings per hectare. Medium density defined as 40-67 dwellings per hectare.</li> </ul>
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A copy of the representations and the applicant's response is contained in **Attachment 3**.

#### INTERNAL REFERRALS

Department	Comments
<b>City Assets</b>	<ul style="list-style-type: none"> <li>• Proposed Finished Floor Levels (FFL) acceptable</li> <li>• Recommend letterboxes be relocated</li> <li>• Stormwater connection acceptable</li> <li>• Vehicular manoeuvrability acceptable, but one visitor space shortfall noted</li> <li>• Bin placement on verge acceptable</li> <li>• 3000L tanks should collect 90% of roofed runoff and be plumbed directly into respective dwellings.</li> </ul>
<b>City Operations</b>	<ul style="list-style-type: none"> <li>• Widening of existing driveway and construction of new driveway supportable, with no impact on existing street tree.</li> </ul>
<b>Heritage Advisor</b>	<ul style="list-style-type: none"> <li>• Proposal demonstrates careful consideration of historic streetscape character. Front replacement dwelling a satisfactory replacement for the Contributory Place.</li> <li>• Rear development acceptable in consideration of screening offered by the replacement dwelling and the adjacent structures constructed to the side boundaries.</li> </ul>

A copy of the relevant referral responses is contained in **Attachment 4**.

## RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the Residential Zone and, more specifically, Torrensville East Conservation Policy Area 33 as described in the West Torrens Council Development Plan. The subject land is also within a nominated Historic Conservation Area and the dwelling is listed as a Contributory Heritage Item.

The relevant Desired Character statements are as follows:

Residential Zone - Desired Character	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objectives	2 & 4
Principles of Development Control	1, 5, 7, 8, 9, 10, 11, 12, 13, 14 & 17

Torrensville East Conservation Policy Area 33 - Desired Character	
<p><i>The provisions of the Historic Conservation Area apply to this policy area.</i></p> <p><i>The policy area will contain predominantly detached dwellings and limited semi-detached dwellings.</i></p> <p><i>Allotments are at low to very low density and are generally deep, with wide frontages to main streets. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.</i></p> <p><i>Streetscape character elements including low front fencing, landscaping space in front yards, regular street trees and on-street visitor car parking will be supported by limiting vehicle cross-overs.</i></p> <p><i>There will be a unity of built-form, where all new development is complementary to historic buildings rather than dominating or detracting from them. There will be predominantly one storey buildings, with some two storey buildings where the upper level is contained within the roof space in a manner that is complementary to the single storey character of nearby buildings. New dwellings will incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as stone, bricks, limited rendered masonry and corrugated iron/steel.</i></p>	

*Alterations and additions will be primarily located at the rear of existing dwellings so that they have minimal impact on the streetscape. Setbacks will be complementary to the boundary setbacks of existing buildings in the policy area.*

*There will be no garaging/carports forward of the main facade of buildings.*

*Historic front fencing, such as post and rail with woven wire and timber picket fences, will be preserved and new front fencing will be complementary in form and materials, facilitating views into front yards and of building facades.*

Objectives	1
Principles of Development Control	1, 2, 3 & 4

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

### QUANTITATIVE STANDARDS

The proposal is assessed for consistency with the quantitative requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
PRIMARY STREET SETBACK <i>Torrensville East Conservation Policy Area 33</i> <i>PDC 4</i>	Align with the average of the setback of the buildings situated on allotments on either side (5m)	6m (façade) 3.4m (verandah portico)  <b>Does not Satisfy</b>
SIDE SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	0/1m (min. ground floor) 2m (min. upper floor)	Dwelling 1 - 0m/1m Dwellings 2 & 3 - 0m/0m (ground) 0m/0m (upper)  <b>Does not Satisfy</b>
REAR SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	3m (min. ground floor) 8m (min. upper floor)	Dwelling 1 - 0m Dwellings 2 & 3 - 5m (ground) 5m (upper)  <b>Partially Satisfies</b>
INTERNAL FLOOR AREA <i>Residential Development</i> <i>PDC 9</i>	100m <sup>2</sup> (min.)	Dwelling 1 - 140m <sup>2</sup> Dwellings 2 & 3 - 124m <sup>2</sup>  <b>Satisfies</b>

<b>PRIVATE OPEN SPACE</b> <i>Residential Development</i> <b>PDC 19</b>	<p>300-500m<sup>2</sup> (dwg 1)          60m<sup>2</sup> (min. area),          4m (min. dimension)          16m<sup>2</sup> (min. directly accessible from a habitable room).</p> <p>&lt;300m<sup>2</sup> (dwgs 2&amp;3)          24m<sup>2</sup> (min. area)          3m (min. dimension)          16m<sup>2</sup> (min. directly accessible from a habitable room)</p>	<p>46m<sup>2</sup> (area)          6.4m (dimension)          46m<sup>2</sup> (accessed from habitable room)</p> <p><b>Does not Satisfy</b></p> <p>40.5m<sup>2</sup> (total area)          5m (dimension)          35m<sup>2</sup> (accessed from habitable room)</p> <p><b>Satisfies</b></p>
<b>LANDSCAPING</b> <i>Landscaping, Fences and Walls</i> <b>PDC 4</b>	10% of the site (min).	20.3% (184 m <sup>2</sup> )  <b>Satisfies</b>
<b>CAR PARKING SPACES</b> <i>Transportation and Access</i> <b>PDC 34</b>	<p><u>Dwelling 1</u>          2 spaces (1 covered)</p> <p><u>Dwellings 2 &amp; 3</u>          4 spaces (2 covered) + an additional 1 visitor space</p>	<p>Dwelling 1 - 2 spaces</p> <p>Dwellings 2 &amp; 3 - 4 spaces          No visitor spaces</p> <p><b>Does Not Satisfy</b></p>

## ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### Heritage Conservation

The applicant included a heritage review of the site by Bruce Harry, which concluded that the demolition and replacement of the Contributory-listed dwelling was justified in this instance. The replacement would be *"of sympathetic scale, built form and siting, could occur without meaningful detriment to the overall historic character of the Torrensville East Conservation Policy Area 33"*, and that the *"two storey development to the rear would be feasible, with minimal, if any, visual impact on the character of the streetscape in the locality"*.

Council's consulting heritage advisor, Douglas Alexander of Flightpath Architects, concluded similarly that the front replacement dwelling is suitable and demonstrates careful consideration of historic streetscape character through visual interest, proportions and composition, scale and bulk. He also concluded that the rear development, from a heritage context, is acceptable in consideration of the screening that would be offered by the front dwelling.

On this basis, favourable consideration has been given to the proposed demolition of the Contributory Heritage-listed dwelling as the proposed replacement would be suitable in terms of built form outcomes given that it would not compromise the heritage value or historic character of the area.

The Historic Conservation Area module Objectives 1, 2 and 3 are considered addressed as the development is considered to conserve and enhance the historic character of the locality as described above. Principles of Development Control (PDCs) 1, 5 and 6 are also considered addressed as the development is complementary to and supports the desired character of the area. These aspects are discussed in greater detail below.

### **Desired Character**

The provisions of the Historic Conservation Area module, the Residential Zone, and the Torrensville East Conservation Policy Area 33 are relevant here, and contain a number of provisions that relate to various desired planning outcomes.

The proposed dwellings take design cues from existing historic built forms without replicating them, and feature well thought out materials, colours, roof forms and façade articulation. As previously discussed, this proposal has been reviewed by two independent heritage consultants who both conclude that the development can occur in a sympathetic manner and without detriment to the overall historic character of the locality.

The unity of built form is considered met as the new dwelling at the front will be complementary to the surrounding historical built form. Whilst the rear dwellings are not single-storey and are not designed with upper storeys contained within roof spaces, they do feature building elements and detailing common to other older structures, and maintain compatible vertical and horizontal building alignments. The overall height of the proposed dwellings is also similar to surrounding built form heights. Furthermore, these rear dwellings will be located in such a way to not be notable when viewed from the street. Not only will they be positioned behind Dwelling 1, but will feature a meaningfully landscaped area directly in front of Dwelling 2 and a pergola over the driveway which will aid in softening the appearance of the dwellings at the rear.

The Residential Zone talks about, *inter alia*, a diversity of housing options, a range of allotment sizes to support anticipated dwelling types in each policy area, and residential flat buildings being common near centres and in policy areas where the desired density is higher. From this aspect, the proposal satisfies these desired outcomes by being located within a short distance to Henley Beach Road, an activity centre and public transport route (and an Urban Corridor Zone). Residential Zone Objective 3 further encourages higher density residential development in these areas.

Conversely, whilst these aspects of the desired character of the Torrensville East Conservation Policy Area 33 are addressed, the desired character for the Residential Zone specifically states that residential flat buildings will not be undertaken in Historic Conservation Areas.

Upon reviewing the provisions of the Historic Conservation Area module, there is a consistent theme of preserving, enhancing and complementing both areas of historical significance and the historic character of buildings that offer positive contributions. As noted above, both heritage consultants have viewed the proposal favourably and opine that the locality is already compromised somewhat. The site is located on the southern edge of the policy area and Historic Conservation overlay and, in addition to the nearby commercial built forms to the south, a two-storey residential flat building is immediately adjacent the site as well as a visually prominent telephone exchange building directly opposite the site.

The site is, in effect, located in a transitional position between lower-amenity built forms to the south, and better examples of the desired character of the policy area to the north. On this basis it seems reasonable for a relatively small-scale residential development of this nature to be located on this site. Accordingly, the majority of the desired character outcomes for the policy area, the Historic Conservation Area, and the Zone are considered to be met.

### **Pattern of Division and Density**

As previously discussed, the allotment pattern within the locality consists of a mix of allotment sizes and layouts. The internal pattern of division will not be retained through this development, and therefore PDC 22 of the Residential Zone and PDC 13 of the Historic Conservation Area module are not completely met. However, this discrepancy is not great enough to warrant non-support of the proposal as the outcome to the street would be compatible with the immediate locality insofar as it would not be an unusual addition nor would it be of significant detriment to the existing streetscape character.

The proposal results in a net density of 33 dwellings per hectare, which is less than the definition of medium density (40-67 dwellings per hectare) noted in the Development Plan. In this regard the development is considered appropriate. Further consideration has been given to the location of the site in close proximity to an activity centre and to the Urban Corridor Zone. Residential Zone Objectives 1, 2, 3 and 4 are therefore considered met as the proposal offers some housing choice and an increase in density in a location that is close to a major public transport route in Henley Beach Road.

### **Built Form**

Residential Zone PDC 6 in isolation has been addressed as the proposal is limited to two storeys, and has a maximum vertical side wall height of 5.7m. PDC 17 of the Residential Zone and the Desired Character statement for the policy area does not exclude two storey dwellings. It is also noted that the Desired Character uses the term '*complementary*' in terms of new development which suggests that new development could demonstrate some differences rather than being imitations of older existing built forms.

Whilst the proposal is not single-storey, it is considered appropriately sympathetic as the dwellings are of similar heights, scale and proportions, and are constructed of similar materials that not only complement, but reinforce the character and design elements of existing buildings. This satisfies Residential Zone PDC 20.

The proposal has also been designed with emphasis placed on environmental sustainability principles, with over 20% of the site being landscaped and north-facing areas of private open space (POS). On balance, the design is considered to achieve a good outcome in terms of solar efficiency by way of siting and orientation, configuration of living and private open space areas, and by minimising upper level roofed areas for the dwellings at the rear. In this regard, Objective 1 and PDC 1 of the Design and Appearance module are considered to be met.

### **Private Open Space**

The amount of POS for Dwelling 1 is approximately 46m<sup>2</sup>. An additional 7m<sup>2</sup> is also shown on the site plan however it is excluded from the calculation as it does not feature a minimum dimension of 4 metres. This is marginally short of the quantitative amount noted in PDC 19 of the Residential Development module. Notwithstanding this shortfall, the positioning and configuration of the POS of all three dwellings is considered to be very good with respect to the proposed site area, and the usability of the POS is considered commensurate to the size of each dwelling, providing an adequate area for outdoor entertaining, clothes drying and the like.

### **Overshadowing**

A desktop analysis of the proposal shows that the proposed dwellings at the rear will not overshadow 8 Huntriss Street, but will partially overshadow portions of the rear yard of 6 Huntriss Street in the early morning during winter. The Development Plan seeks at least 3 hours of direct sunlight to habitable room windows and private open space of adjoining land. This will be achieved for these neighbouring allotments.

It is acknowledged that the dwellings will partially overshadow adjoining land located immediately to the south at 3 Wainhouse Street, however the overshadowing will be over an existing outbuilding on the southern boundary. Any shadowing cast will only strike the existing two-storey building in the late afternoon and is, as a result, considered to satisfactorily meet PDC 14 of the Residential Zone, PDC 3 of the Energy Efficiency module, and PDCs 10-13 of the Residential Development module.

### **Setbacks**

The front setback of Dwelling 1 will be marginally forward of the average of the adjoining dwellings, however quantitative shortfall aside, the proposal is consistent with a number of other built form examples in the immediate locality. Due to its complementary single-storey character design with open verandah element and façade in line with adjacent dwellings and meaningfully landscaped front yard, it will not impair the amenity of the streetscape character and will not result in a negative contribution to the locality. In this regard PDC 4 of the Torrensville East Conservation Policy Area 33 is considered met.

Whilst the proposed dwellings at the rear are not set back 8m from the rear boundary, consideration has been given to both visual and overshadowing impacts to adjacent land. The upper level windows facing to the rear feature fixed obscured glazing to 1.7m above the finished floor level, which effectively mitigates casual overlooking and satisfies Residential Development PDC 27. The proposed roof height of 6.8m does not exceed that of many of the nearby existing dwellings to any significant degree, and it is also noted that the existing dwellings at 6 and 8 Huntriss Street are positioned approximately 10 and 23 metres respectively from the shared rear boundary. This will result in a minimum separation distance of 15m between the new dwellings and the existing dwellings behind the subject site. On this basis the proposal is not considered to result in an unreasonable visual impact to adjoining owners or occupiers of the adjoining dwellings.

The side setbacks for the proposed rear dwellings, being on boundary for portions of 6.2m in length have been proposed as they will immediately abut unrelieved walls of existing buildings on both sides of the subject land. There is no benefit to be gained in having the proposal located off these respective boundaries, as additional setbacks would incur no practical outcome to any visual or overshadowing impacts.

### **Car Parking**

PDC 34 of the Transportation and Access module seeks the provision of one visitor car parking space, in addition to two spaces for each dwelling. The shortfall of one space is acceptable due to the proximity of the subject site to public transport routes within short walking distances. The Administration also accepts the applicant's opinion in this instance that landscaped areas are more preferable in some circumstances to hard-standing parking areas in order to offset heat island impacts.

### **SUMMARY**

The proposed development demonstrates due regard of the site and locality characteristics together with consideration of the intent of the Development Plan provisions. Notwithstanding a number of quantitative shortfalls and qualitative provisions not being explicitly met, the intent of both the Historic Conservation Area and Residential Conservation Policy Area is considered to be appropriately met. The reasoning and opinions of both the applicant's and Council's heritage consultants strengthens this determination from a heritage perspective.

The proposed dwellings have been designed and sited in a manner that complements the single-storey character of nearby buildings and addresses the intent of the Desired Character of Policy Area 33. The two-storey dwellings at the rear are reasonable given the context of the site and the proposed siting behind a replacement dwelling fronting the street.



Significant consideration has been given to the proposal in light of all relevant Development Plan provisions, together with the representations received. As discussed, the proposal does not meet all of these provisions, however upon weighing up the merits of the proposal against these provisions, the proposal demonstrates sufficient merit to warrant support. The proposal is therefore not considered to be seriously at variance with the Development Plan.

On fine balance, the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and therefore the granting of Development Plan Consent is warranted.

## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/1349/2018 by E Politis to undertake the demolition of existing dwelling and associated ancillary domestic structures and construction of one single storey detached dwelling and a two-storey residential flat building comprising two dwellings at 5 Wainhouse Street, Torrensville (CTs 6084/926 & 6084/927) subject to the following conditions of consent:

### Development Plan Consent Conditions:

1. The development shall be undertaken, completed and maintained in accordance with the plans and information detailed in this application except where varied by any conditions listed below.  
*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*
2. Prior to the occupation or use of the development, all stormwater design and construction shall be to the satisfaction of Council to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
  - a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create insanitary or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; or
  - e) Flow across footpaths or public ways.*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*
3. All external materials, surface finishes and colours shall be consistent with the information detailed in this application and shall be maintained in a good condition at all times to the reasonable satisfaction of Council.  
*Reason: To ensure a high standard of materials and finishes are used in the final presentation of the building.*
4. Prior to the occupation or use of the development, all driveways, parking and vehicle manoeuvring areas shall be constructed and surfaced with concrete, bitumen or paving, and shall be drained and maintained in a good condition at all times to the reasonable satisfaction of Council.  
*Reason: To provide safe and convenient parking and manoeuvring areas for users of the development.*

5. The establishment of all landscaping shall occur no later than the next available planting season after substantial completion of the development. Such landscaping shall be maintained in good health and condition to the reasonable satisfaction of Council at all times. Any dead or diseased plants or trees shall be replaced with a suitable species.

*Reason: To provide amenity for the occupants of the development and those of adjacent properties.*

6. Prior to the occupation or use of the development, the upper storey windows on the rear (western) elevations of Dwellings 2 and 3 shall be fitted with fixed obscure glass (not film coated) or raised sills to a minimum height of 1.7 metres above the upper floor level to minimise the potential for overlooking of adjoining properties. The glazing in these windows shall be maintained in good condition at all times to the reasonable satisfaction of Council.

*Reason: To minimise the impact on privacy to residents of adjoining dwellings.*

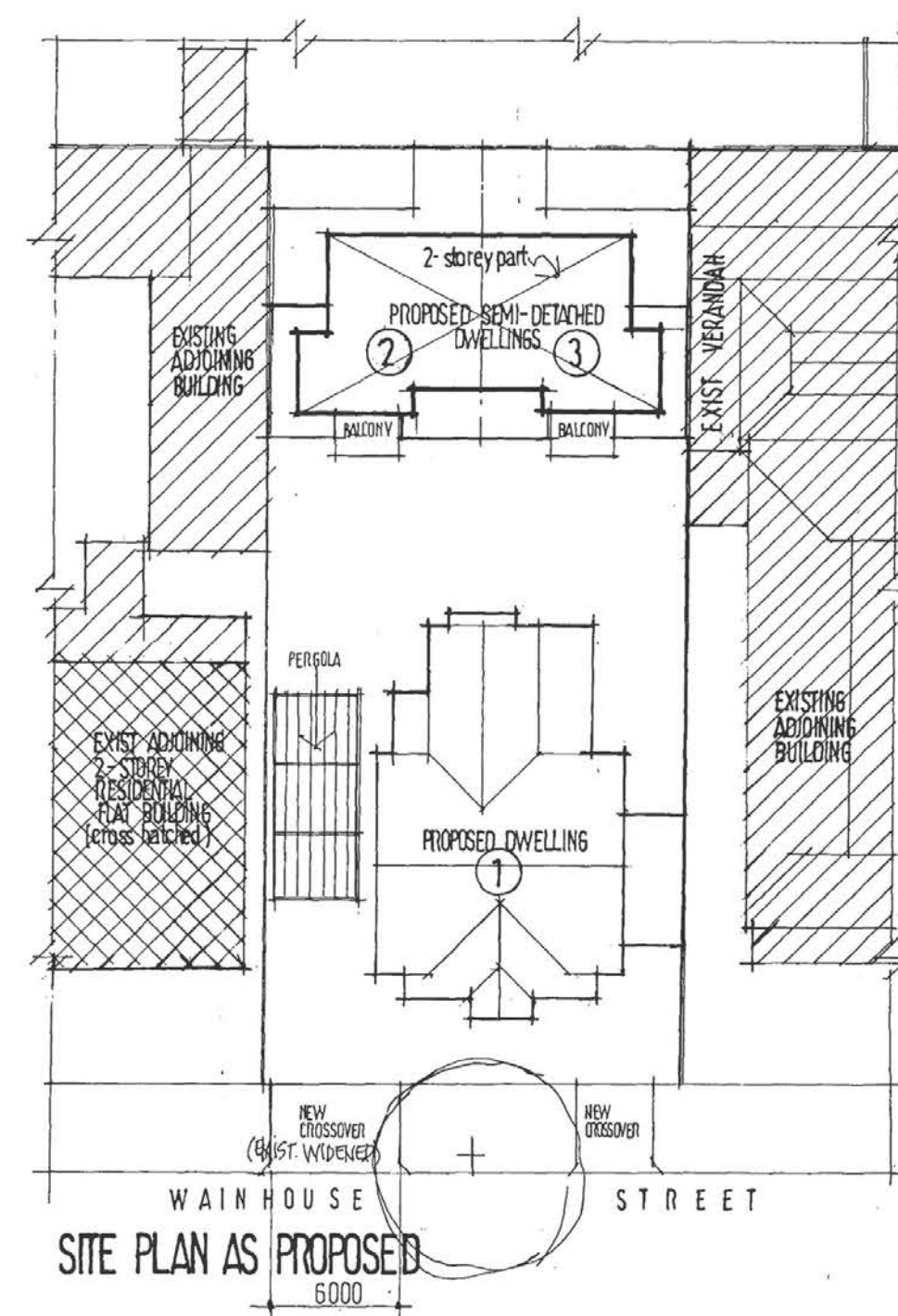
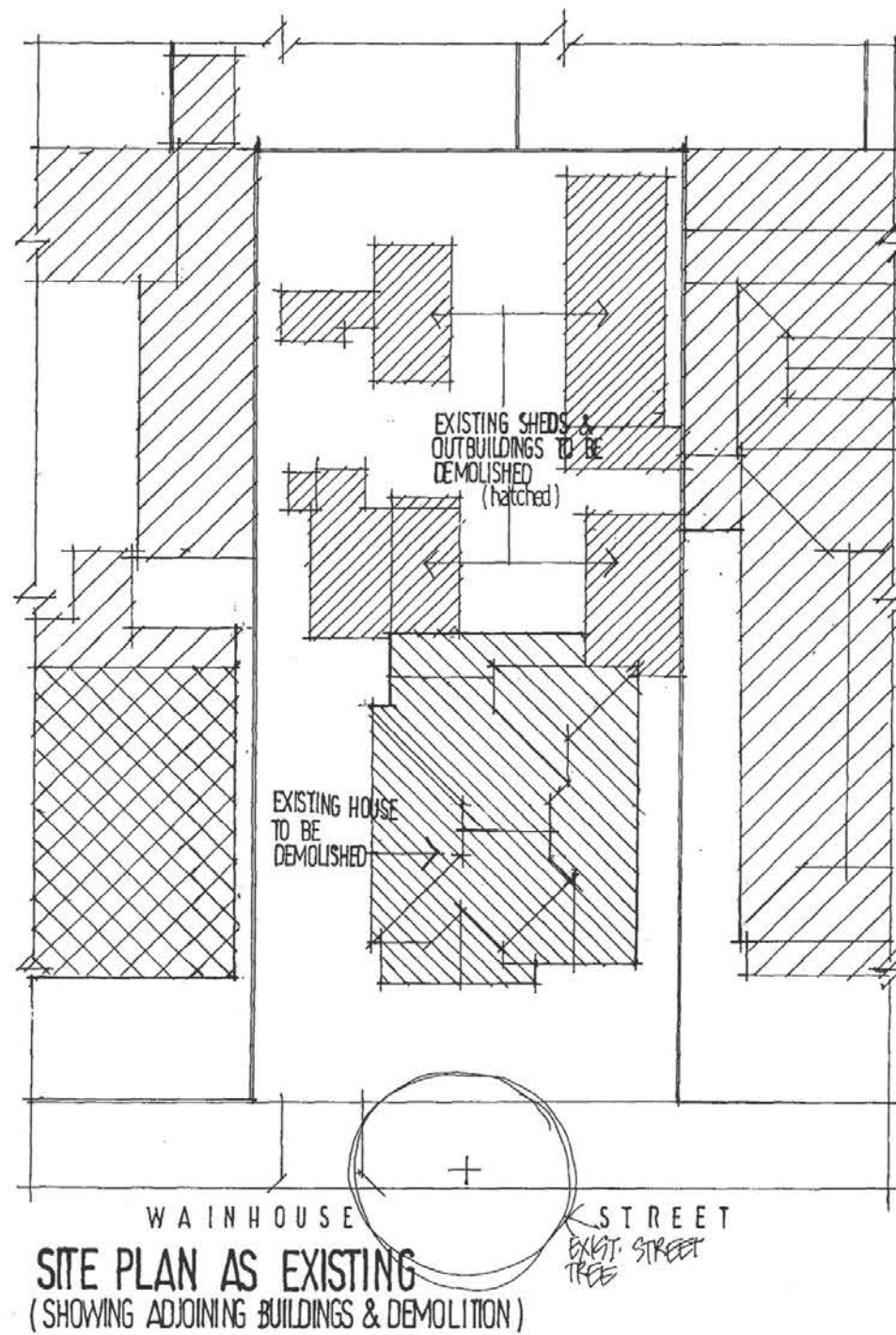
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## Attachments

1. **Additional Development Plan Provisions**
2. **Plans and Associated Documentation**
3. **Representations and Response**
4. **Internal Referral Responses**

**Relevant Development Plan Provisions**

<u>General Section</u>		
<i>Crime Prevention</i>	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 6, 7 & 8
<i>Design and Appearance</i>	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 9, 10, 11, 12, 13, 14, 15, 18, 19, 20 & 21
<i>Energy Efficiency</i>	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1 & 2
<i>Landscaping, Fences and Walls</i>	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 4 & 6
<i>Residential Development</i>	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 16, 18, 19, 20, 21, 27 & 31
<i>Transportation and Access</i>	<i>Objectives</i>	2
	<i>Principles of Development Control</i>	1, 8, 11, 23, 24, 30, 34, 35, 36 & 44

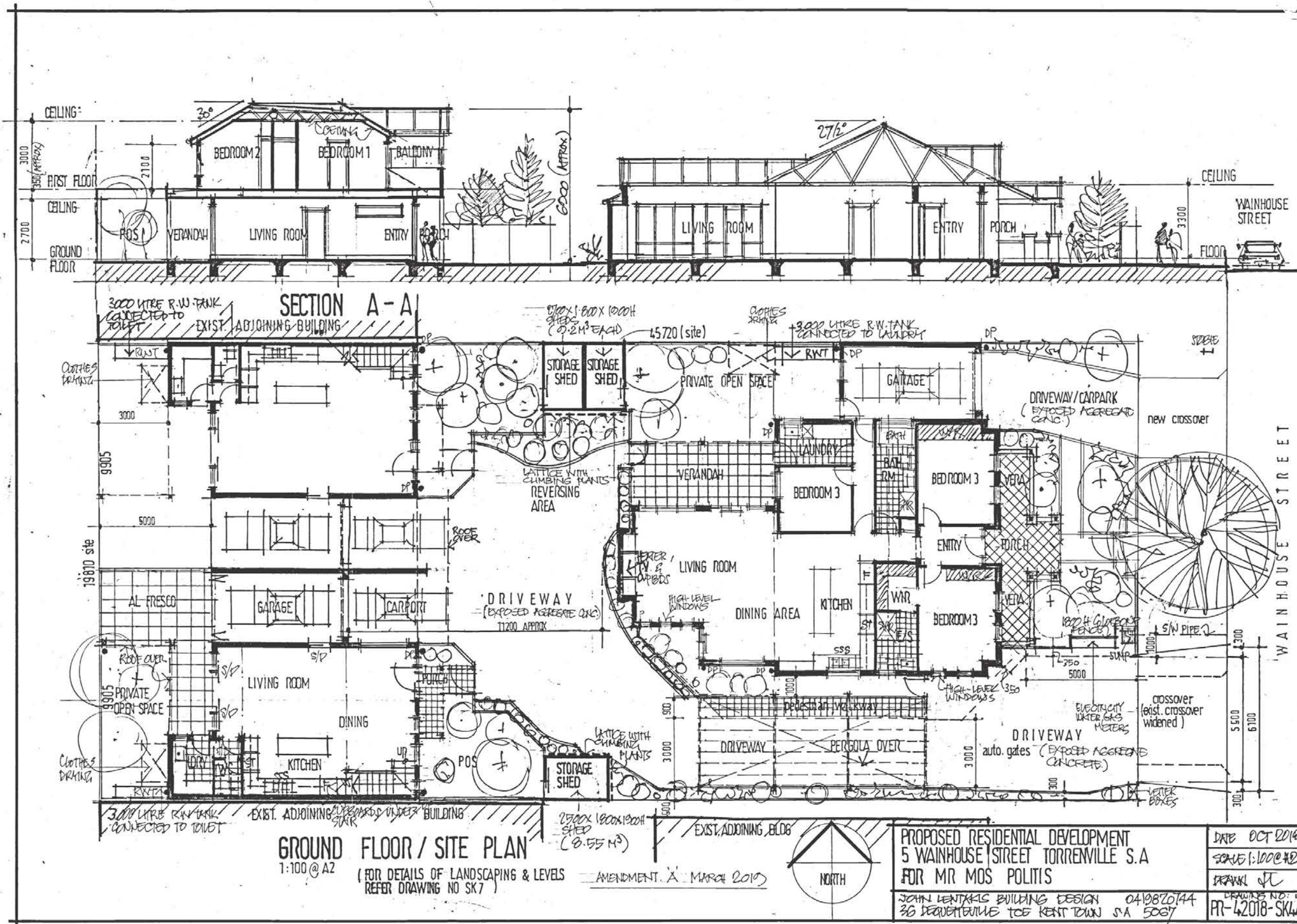


PROPOSED RESIDENTIAL DEVELOPMENT  
5 WAINHOUSE STREET TORRENSVILLE S.A  
FOR MR MOS POLITIS

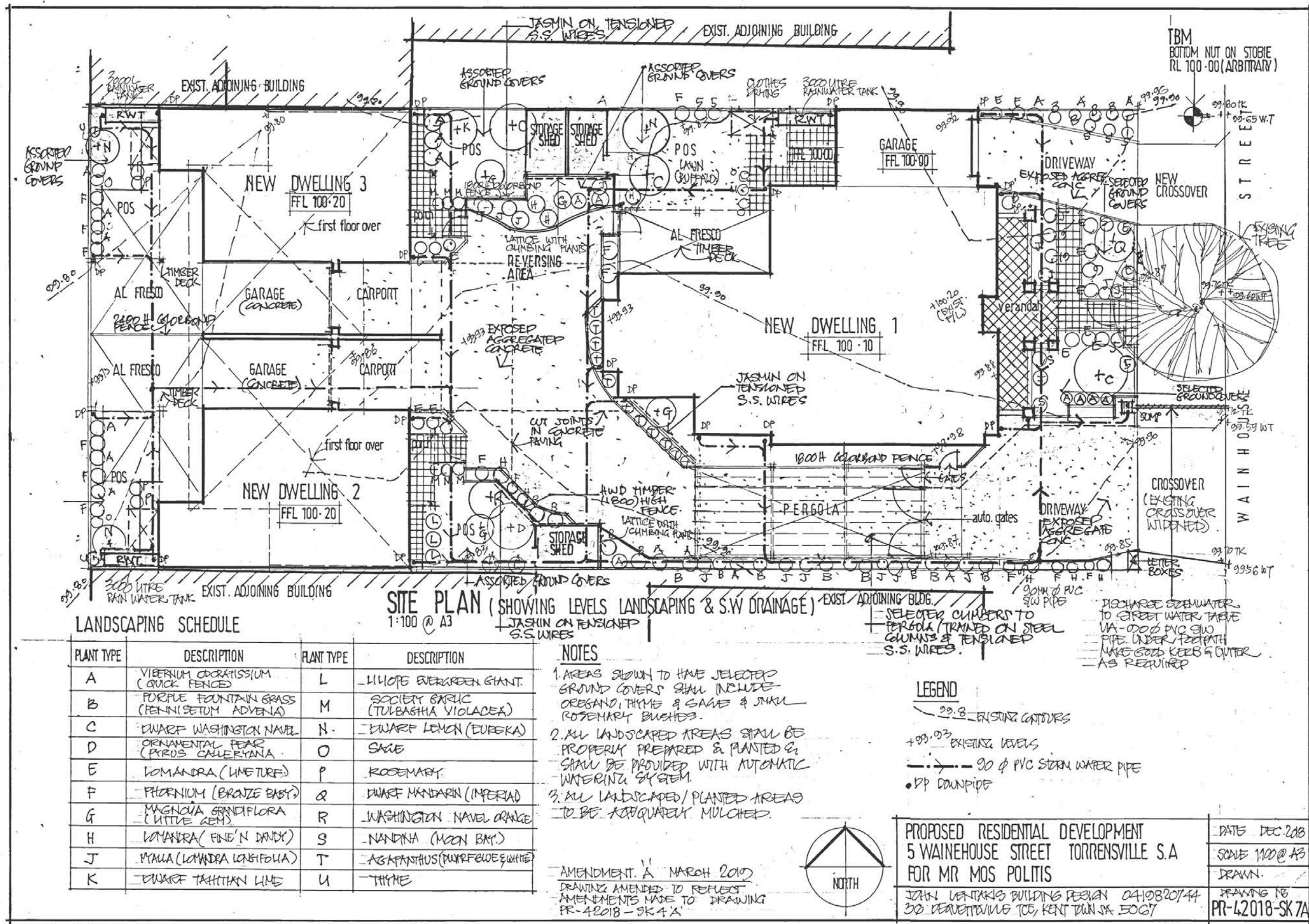
JOHN LEKATAS BUILDING DESIGN 04082044  
36 DEVEREUXE TCE. KENT TOWN SA 5067

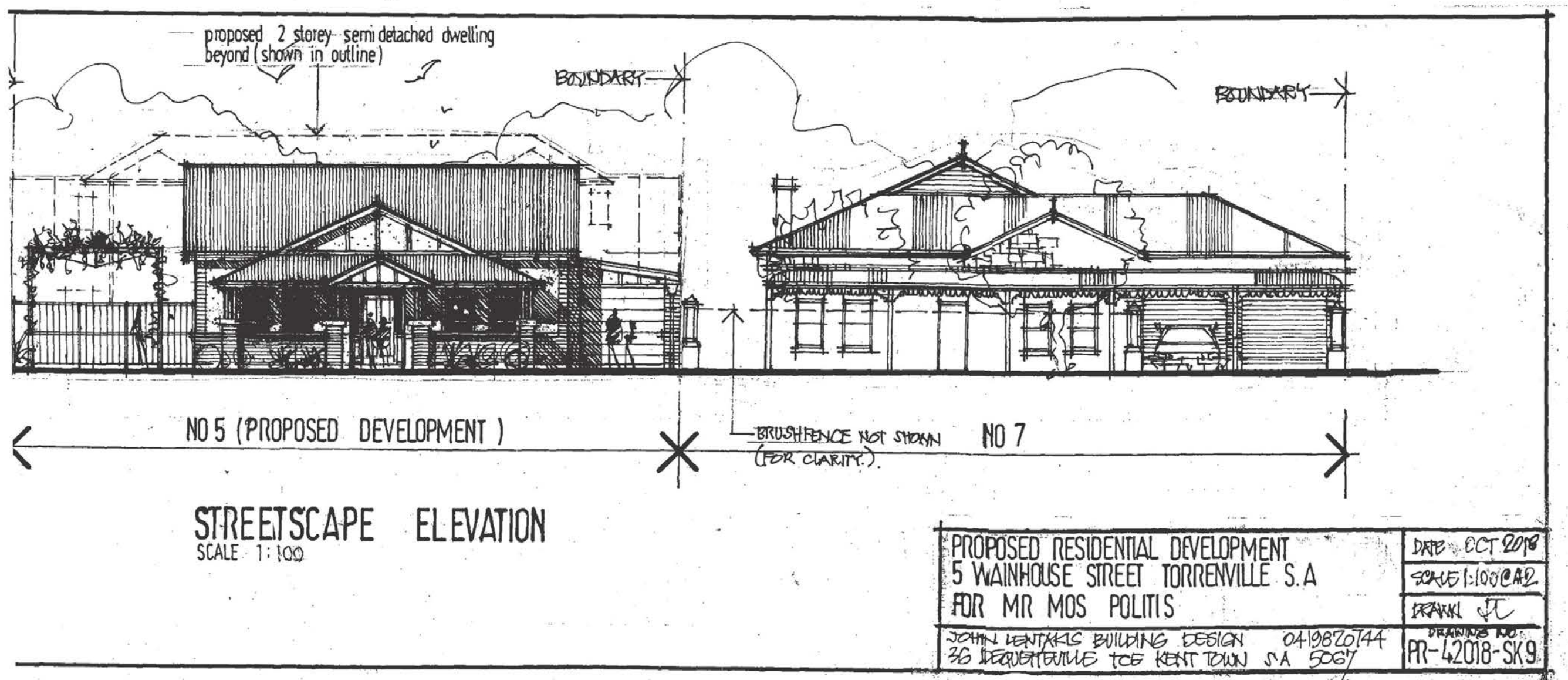
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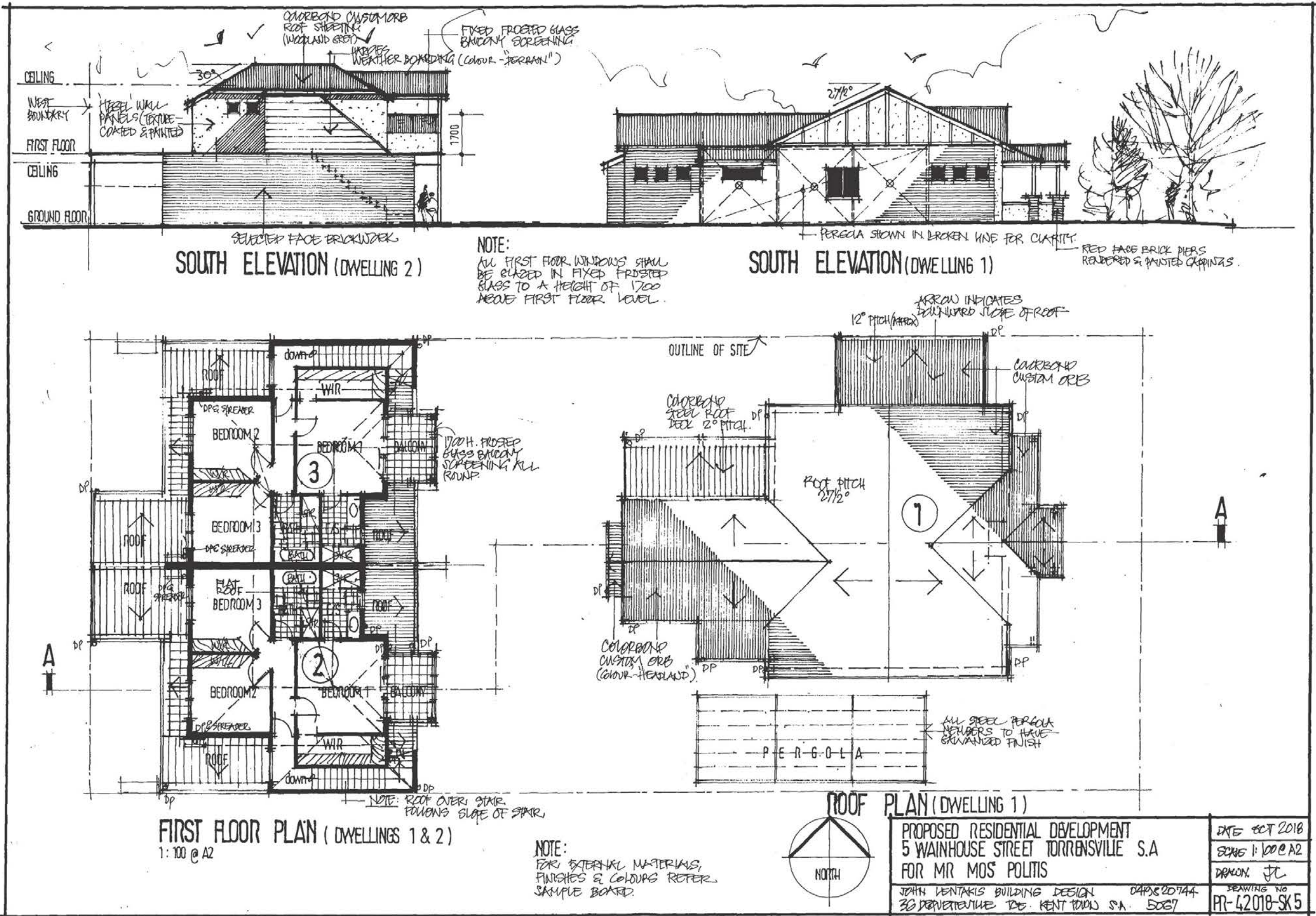




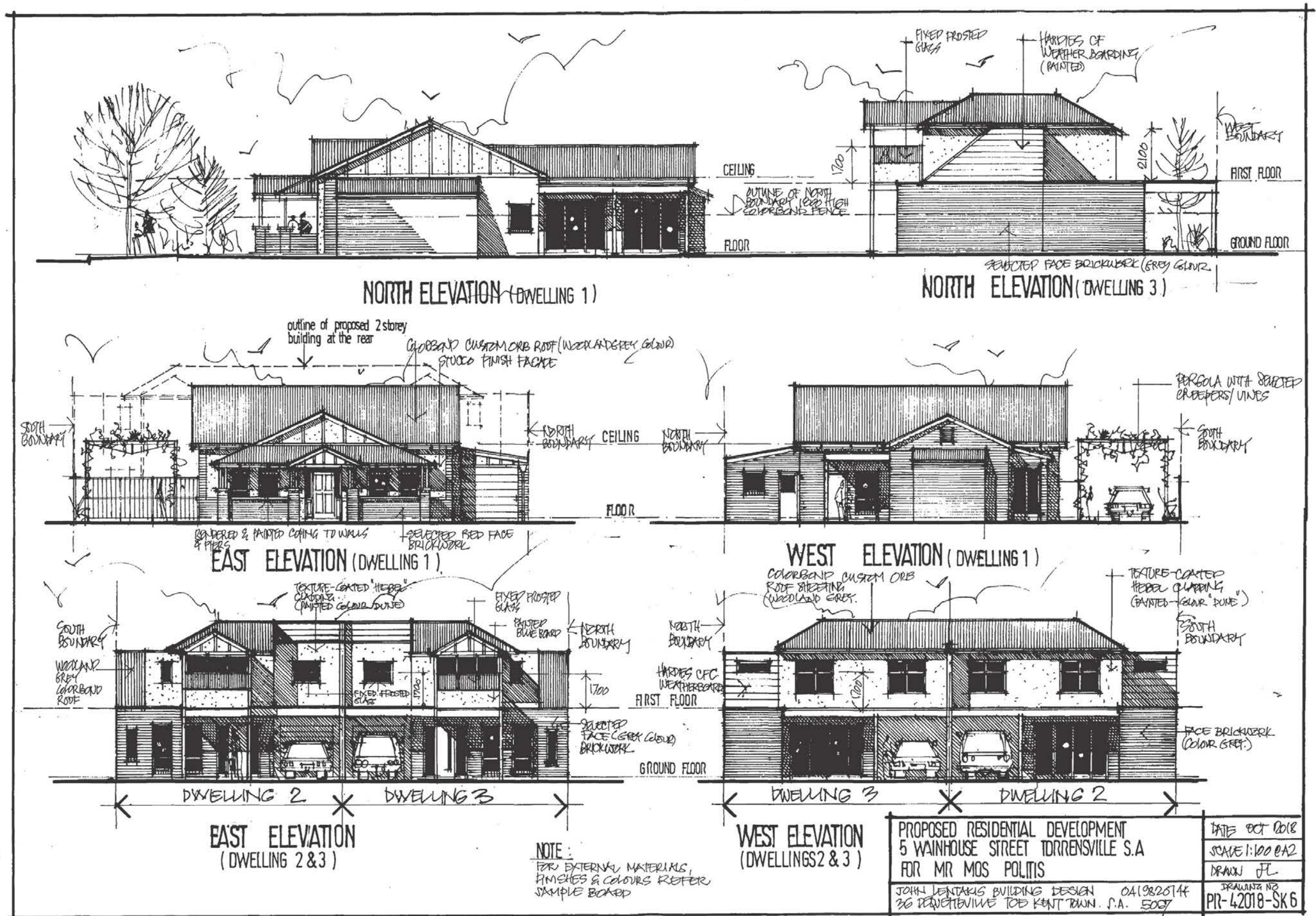




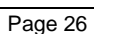




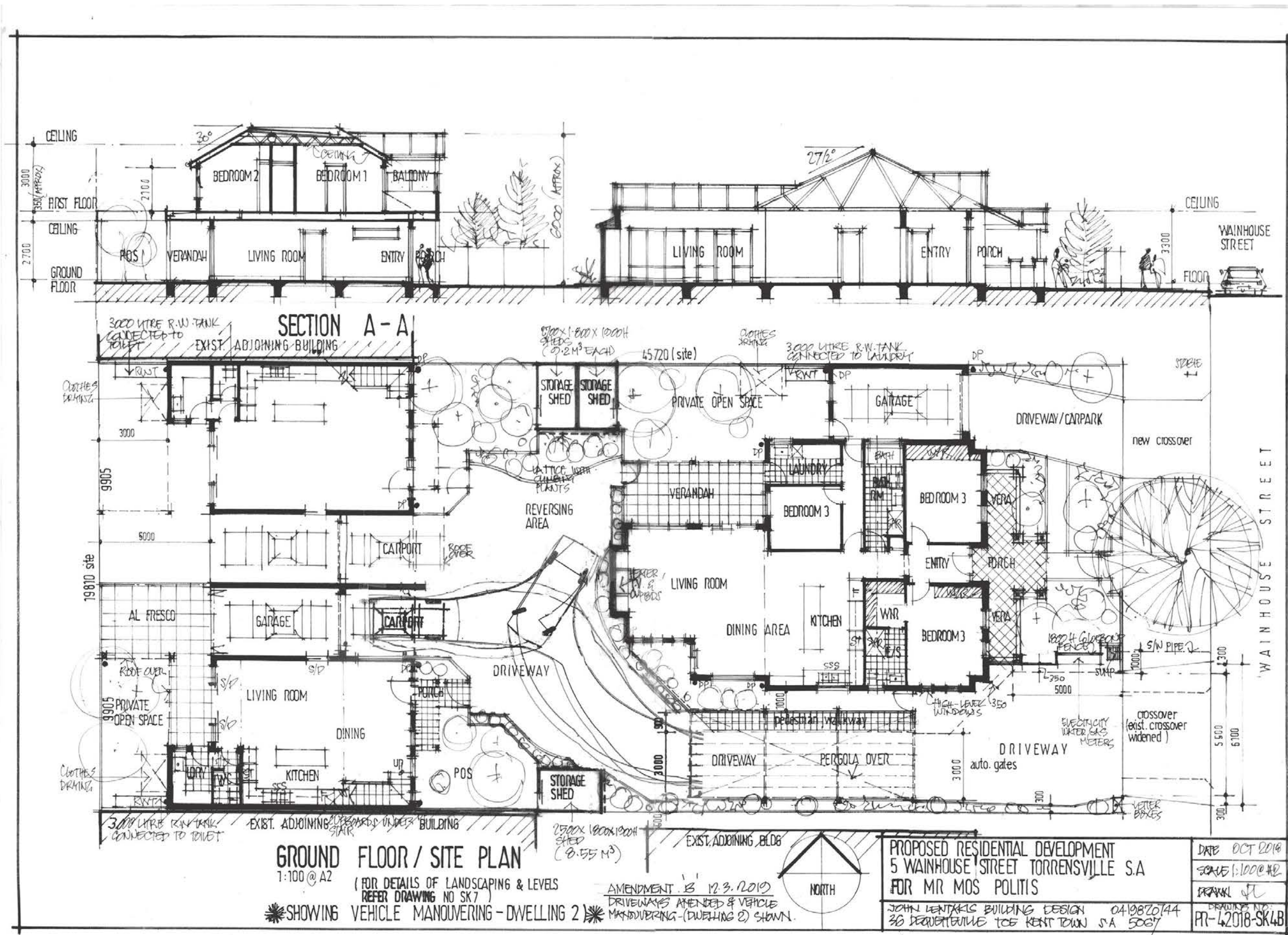




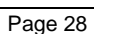














### Owner's Perspective of 5 Wainhouse Street, Torrensville

My parents purchased the property in 1954 after they married. My sister and I were born soon after and lived in the house until our late 20s. My mother still lives in the house, some 65 years later. Her grandchildren have lived in the house for extended periods of time, and today, her great-grandchildren have sleepovers.

As a child I remember the changing face of Wainhouse Street with its many developments. There was the square and bulky 2-storey Telstra addition. There was the bungalow opposite our house demolished for Telstra carparking. There were the villas/bungalows both east and west of Wainhouse Street demolished for the new Centrelink and Spartan Electrical buildings. And there was the bungalow next door, at 3 Wainhouse Street which morphed into a 2-storey block of flats over a 10-year period.

Due to past Council planning rules and regulations, the immediate amenity and streetscape has diminished with the development of the ugly 2-storey Telstra building, the ugly 2-storey flats and the commercialisation of the southern side with the Centrelink and Spartan Electrical buildings. We have had to endure a direct view from the next-door flat's balcony overlooking our kitchen/dining room. We have become accustomed to the spike in traffic as a result of the encroachment and commercialisation of the street. And we have become accustomed to noise from the increasing number planes flying overhead as we lie on a flightpath.

We hope to overcome these issues with the new development. My Architect has over 50 years' experience. With the help of a planning consultant, my architect has addressed the overlooking issue into the rear back yards of the adjoining properties by utilising pragmatic and best practice design. This development is not an 'off-the-shelf' design. Much thought has been put into the design of the exterior, to complement and enhance the area. And my architect with the help of both the planning consultant and a heritage architect has taken into consideration all aspects of the streetscape and adjoining properties with the final design.

My mother, sister and I have decided to redevelop the site, not as some random hit-and-run developer focused on a 'quick-buck', but as a close-knit family wishing to maximise the potential of this great location. We wish to replace the house with a new single-storey house at the front for my mother, so she may live the remaining years of her life in comfort, with minimal maintenance, and enjoy better thermal and sound properties. Further my sister and I wish to build a townhouse each, to be used by our families, so they too can enjoy this precinct. We hope to create an asset for the area, and in 35 years' time, we hope our family to have established roots at 5 Wainhouse Street for 100 years.



## PLANNING REPORT

### 5 WAINHOUSE STREET, TORRENSVILLE

CONSTRUCTION OF ONE SINGLE STOREY AND TWO, TWO STOREY DWELLINGS

Prepared for:  
Moss Politis

Date:  
18/12/2018



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#### Document Control

Revision	Description	Author	Date
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V2	Final	TK	18/12/2018

REF 0344 | 19 December 2018





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## 1. INTRODUCTION

The proponent seeks Development Plan Consent to demolish all existing structures, and erect and complete three dwellings together with extensive landscaping and associated car parking.

We have undertaken an inspection of the site and locality and assessed the proposal against the relevant provisions of the West Torrens Council Development Plan (consolidated version 12 July 2018). We have formed the opinion that the proposal warrants favourable consideration.

We understand that the following documents have been submitted with the application:

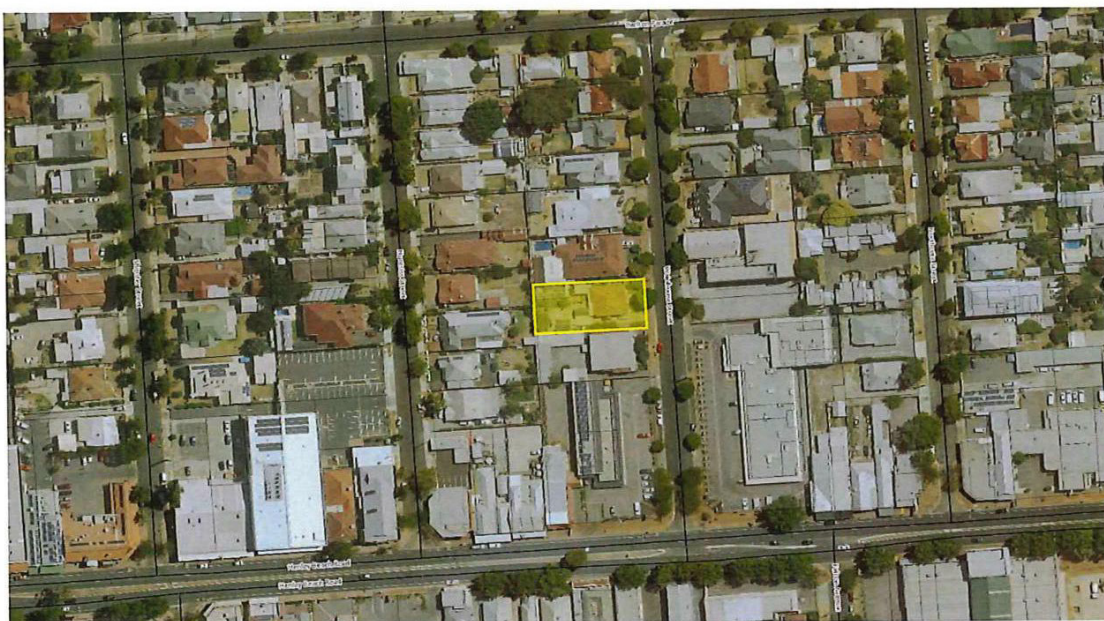
- signed Development Application Form;
- signed Electricity Regulation Form;
- Certificate of Title;
- plans and elevations prepared by John Lentakis; and
- Heritage Report prepared by Bruce Harry.



## 2. SITE AND LOCALITY

The subject land is located on the western side of Wainhouse Street approximately 80 metres north of its intersection with Henley Beach Road, has a frontage of 19.81 metres to Wainhouse Street, a depth of 45.72 metres and an area of approximately 906 square metres. A single storey asymmetrical bungalow style dwelling, used as such, and associated outbuildings are existing on the land.

**Figure 2.1** *Site and Locality.*



The land is relatively flat and access is provided by way of an existing driveway/crossover within approximately 2.0 metres of the southern boundary. There are no regulated trees on or within proximity of the subject land. There is a mature street tree approximately mid-block and a stobie pole adjacent the northern boundary of the site.

The locality has a mixed character.

Immediately adjoining the subject land to the south is a two storey residential flat building, reportedly being a former bungalow that was remodelled in the 1960's, which now includes boundary to boundary development. Further south along this same side of Wainhouse Street in the direction of Henley Beach Road is the Spartan Electrical showroom.

Directly opposite the Spartan showroom, on the eastern corner of Wainhouse Street and Henley Beach Road, is a predominantly single storey commercial complex including Centrelink, Medicare and employment agencies. A two storey building, built boundary to boundary, occupies the north eastern corner of this site. Further north along this same side of Wainhouse Street in the direction of Carlton Parade is the West Adelaide Telephone Exchange building and car park. This is a somewhat nondescript development presenting predominantly unrelieved walls to the streetscape.

Immediately adjoining the subject land to the north is a reproduction style dwelling, circa 1980's, which includes boundary to boundary development.

The areas generally to the north of the subject site are used for residential purposes.

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### 3. THE PROPOSAL

Following demolition of all existing structures it is proposed to erect and complete three dwellings together with extensive landscaping and associated car parking.

The development is described as follows.

#### 3.1 STREET FRONTING DWELLING

A single storey simplified bungalow style with a floor to ceiling height of 3.3 metres to allow for natural ventilation and infiltration of daylight and comprising 3 bedrooms, main with W.I.R. and en suite, open living, dining and kitchen area and laundry.

#### 3.2 REAR DWELLINGS

Two (2), 2 storey dwellings each comprising open living, dining and kitchen area and laundry at ground level with 3 bedrooms, main with W.I.R. and en suite, at first floor level.

#### 3.3 MATERIALS AND FINISHES

Red brick with stucco façade, Hardies weatherboard, grey Colourbond roof and painted woodwork, the palette to reflect colours of existing houses in the street.

A pergola or trellis type structure, designed to support and display climbing and trailing plants, is proposed to the front of the site adjacent to the street facing dwelling and over the proposed driveway.



#### 4. PLANNING ASSESSMENT

The subject land is within the Residential Zone and more particularly Torrensville East Conservation Policy Area 33 under the West Torrens Council Development Plan consolidated 12 July 2018 within which the proposal is neither complying nor non-complying and, accordingly, is for consideration on its merits.

The following provisions of the Development Plan have been taken into consideration in the preparation of this report.

##### GENERAL SECTION

###### *Building Near Airfields*

Objective: 1

PDC's: 1, 3, 4, 6 and 7.

###### *Crime Prevention*

Objective: 1

PDC's: 1, 2, 3, 6, 7, and 8.

###### *Design and Appearance*

Objectives: 1 and 2.

PDC's: 1, 2, 3, 9, 10, 12, 13, 14, 15, 20 and 21.

###### *Energy Efficiency*

Objectives: 1 and 2.

PDC's: 1, 2 and 3.

###### *Historic Conservation Area*

Objectives: 1, 2, 3, 4, 5, 6, 7, 9, 10, 12 and 13.

###### *Land Division*

Objectives: 1, 2, 3 and 4.

PDC's: 1, 2, 4, 5, 6, 7, 8 and 12.

###### *Landscaping, Fences and Walls*

Objectives: 1 and 2.

PDC's: 1, 2, 3, 4 and 6.

###### *Orderly and Sustainable Development*

Objectives: 1, 2, 3, 4 and 5.

PDC's: 1, 3, 5 and 7.





### *Residential Development*

Objectives: 1, 2 and 4.

PDC's: 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 14, 15, 16, 18, 19, 20, 21, 27, 28, 29, 30 and 31.

### *Transportation and Access*

Objective: 2.

PDC's: 1, 2, 8, 10, 11, 20, 21, 23, 24, 30, 32, 34, 35, 36, 40, 41, 43 and 44.

### *Waste*

Objectives: 1 and 2.

PDC's: 1, 2, 3, 7, 10, 11 and 13.

## **RESIDENTIAL ZONE**

Objectives: 1, 2, 3 and 4.

PDC's: 1, 4, 5, 6, 7, 8, 10, 12 and 13.

## **TORRENSVILLE EAST CONSERVATION POLICY AREA 33**

Objective: 1

PDC's: 1 and 2.

In this instance it is considered that Heritage Conservation, both in the broad and specific sense, Land Use and Zoning, Design and Appearance, Orderly and Sustainable Development, Residential Development and Transportation and Access are the primary issues to be addressed.

### **4.1 HERITAGE CONSERVATION**

From the outset it is important to set this matter in context and equally as important to keep in mind that the Development Plan is a practical document calling for practical application. It is well established that the provisions of the Development Plan are advisory and not mandatory; they are not a set of hard and fast rules.

Section 23 (3a) of the Development Act, 1993, as amended, provides that Development Plans may set out objectives and principles which describe the characteristics and other aspects of the natural or constructed environment that are desired within the community in order to provide clear directions with respect to development in the relevant area.

There is an accepted hierarchy of heritage listings in South Australia. State and Local Heritage Places are clearly defined under the Heritage Places Act 1993 and the Development Act 1993, as amended, however the creation of Heritage Conservation Zones and Policy areas or the listing of contributory items is not so clear.

Contributory item is not defined in the Development Act, 1993, as amended, the Development Regulations 2008 or the West Torrens Council Development Plan.



As noted in the attached Heritage Review prepared by Bruce Harry, a well respected and appropriately qualified expert in this field, the 2001 "Planning Bulletin Heritage" produced jointly by Planning SA and Heritage South Australia states that Contributory Items *"are not a near equivalent to a Local Heritage Place, but are surviving examples of the particular period and its character."* Their contribution to the character of their locality is generally limited to their visibility from the public domain.

For this purpose, it is interesting to note the decision of the Full Court of the Environment, Resources and Development Court in the matter of *13 Hutt Street v Adelaide City Council* (2010) in reference to 'townscape' a similar concept to contributory items. The following extracts are considered most pertinent to the present application.

*"The term 'townscape' is nowhere defined in the Development Plan. We take it to mean a view or vista of a particular stretch or expanse within a town, as seen by a person at or near street level. A townscape would include the buildings near and distant with the vegetation (if any) and street furniture and other structures located within the expanse.....one building alone would not comprise a townscape."*

*".....'townscape' is used in the Development Plan to mean a vista at street level, comprised of buildings (their scale and built-form, fenestration, decoration etc.) and the relationship thus created between them, with the street, with landscaping and vegetation, and with street furniture. It is not necessarily the view observed from any particular location in the street, or from the premises directly opposite any particular group of buildings. Thus historic character comprise a vista that includes buildings of a certain age that makes them historic. A cohesive townscape might be a vista where the existing view of buildings and the interplay between them gives rise to a sense of unity."*

(emphasis added)

The designation of a building as a contributory item has a number of potential consequences. For example, building work which may not otherwise require planning consent now requires consent, some building work that may not otherwise constitute development now requires approval and a Development Plan may list demolition of a contributory item as non-complying.

Turning to the specific. Table WeTo/3 identifies No. 5 Wainhouse Street as a contributory item. This requires any assessment to take into account the provisions for Historic Conservation Areas and more specifically Torrens East Conservation Policy Area 33. The weight to be applied to such provisions must take into account the generality of its description, the absence in the Plan of its history or intent of listing and the present realities of the building and its historical and factual context.

As noted above a comprehensive Heritage Review, prepared by Bruce Harry, a well respected and appropriately qualified expert in this field accompanies the application.

This review concludes that, in the particular circumstances of this case, No. 5 Wainhouse Street:

- is, in reality, a common place example of a bungalow style dwelling that was mass produced across metropolitan Adelaide between the end of World War 1 and the onset of the Depression in 1929;
- being sited between a two storey residential flat building (remodelled former bungalow) and reproduction style dwelling and adjacent commercial buildings, does not have a unifying presence;
- does not have any significant attributes that would justify its preservation as a heritage place in its own right;





- does not contribute positively to the historic character of the area;
- replacement by a contemporary dwelling of sympathetic scale, built form and siting, could occur without meaningful detriment to the overall historic character of the Torrensville East Conservation Policy Area 33; and
- a two storey development to the rear would be feasible, with minimal, if any, visual impact on the character of the streetscape in the locality.

#### 4.2 LAND USE AND ZONING

It is established law that the zoning intentions and existing character of a locality are to be properly balanced and a proposed development must be judged in its historical and factual context. A conclusion that a proposed development is not consistent with particular zoning provisions is not necessarily decisive of the matter. For example, the existing character of a locality may mean that certain provisions have limited relevance.

In this instance it is our considered professional opinion that the particular circumstances of this land merits flexibility in approach and the way in which the Policy Area principles are applied and that there are reasonable grounds to support a departure from the Policy Area provisions in that:

- the disparate nature of the adjoining and adjacent built form and land uses is not conducive to a sense of unity;
- the existing building does not contribute positively to the heritage character of the area;
- the proposed development has been consciously designed to respect its setting and make a positive contribution to the amenity of the locality;
- the proposed development takes its design cues from the existing historic built forms without replicating historic detailing;
- the design maintains compatible vertical and horizontal building alignments and solid to void relationships;
- landscaping, which will include the planting of drought tolerant species, including locally indigenous species where appropriate, is an integral component of the development;
- the subject land is, in effect, isolated ('wedged') between a nondescript two storey residential flat building (remodelled former bungalow) and reproduction dwelling both of which are built boundary to boundary;
- the proposed development presents as a detached dwelling to the streetscape;
- the subject land is within 20 metres of the Urban Corridor Zone; and
- Residential Zone Objective 3 encourages medium and high density residential development in areas in close proximity to activity centres, public and community transport and public open spaces; noting that Henley Beach Road, being an activity centre and public transport route is approximately 80 metres to the south of the subject land and Thebarton Oval and Kings Reserve are approximately 500 metres to the north.



#### 4.3 DESIGN AND APPEARANCE

As noted above, the proposed development has been consciously designed to respect its setting and presents a high design standard and appearance that responds to and reinforces the positive aspects, which in this case are somewhat limited, of the local environment and built form.

For this purpose, it is noted that:

- the street facing dwelling is of a simplified bungalow style which has regard to building height, mass and proportion, external materials and finishes, roof form and pitch and façade articulation and detailing. The development adopts a streetscape character that enhances and reinforces the historic character sought and is a careful design which emphasises subtle differences in expression whilst avoiding undue repetition of style and external appearance;
- the street facing dwelling has been designed to maximise its northerly aspect and the living rooms of all dwellings have an external outlook;
- entries to all dwellings are clearly visible from the street or access way and readily identifiable to visitors;
- each dwelling is provided with a high quality living environment with internal floor areas well in excess of those recommended;
- the proposal appropriately mitigates against any unreasonable detrimental impact of overlooking and overshadowing;
- the proposed side boundary walls have maximum vertical heights of less than 6 metres, maximum lengths of 6 metres and immediately abut walls of existing buildings on both the adjoining sites, which generally respects the relevant provisions of the Development Plan. In this respect, the proposed development does not materially manifest itself in terms of visual built form impacts or loss of reasonably expected residential amenity;
- all relevant crime prevention measures have been adopted;
- appropriate energy efficiency measures, insulation, window placement, lighting, cross ventilation, solar access and the like will be incorporated into the final detailed design; and
- the proposed pergola over the driveway will create a point of interest and soften the appearance of the two storey dwellings to the rear of the site.

The design has evolved from a genuine attempt to improve living conditions whilst realising the potential of the site and in due consideration of orientation, layout, daylight access, visual and acoustic privacy, natural ventilation and provision of deep soil zones.

Good design responds and contributes to its context. Understanding the local context is a key step in the process of establishing a robust urban response that supports change in a particular situation. The present proposal respects this approach.





#### 4.4 ORDERLY AND SUSTAINABLE DEVELOPMENT

The Development Plan recognises that demographic change will see increased demands for development opportunities at various densities to address such issues as increased housing demand, efficient and effective use of infrastructure, population change and an increased mix in the range and number of dwelling types available, noting that the Plan calls for increased dwelling densities in close proximity to centres, public transport routes and public open space.

The present proposal respects this policy intent and contributes to the ongoing development of the City in an orderly and sustainable manner in that:

- it creates a safe, convenient and pleasant environment in which to live and socialise;
- it maximises the efficient and effective provision of infrastructure and services;
- it does not jeopardise the continuance of adjoining authorised land uses;
- it does not prejudice the attainment of the provisions of the Development Plan in general;
- it does not prejudice the development of the zone or policy area for their intended purposes; and
- it provides for the development of otherwise underutilised land in an efficient and coordinated manner without hindering the orderly development of adjacent lands.

#### 4.5 RESIDENTIAL DEVELOPMENT

As noted above the Development Plan recognises that demographic change will see increased demands in the range and number of dwelling types available and increased densities in close proximity to centres, public transport routes and public open space.

Objective 1, General Section, *Residential Development*, of the Plan calls for safe, convenient, pleasant and healthy living environments that meet the full range of needs and preferences of the community.

The existing house is dark and gloomy and somewhat depressing and is not conducive to a safe, pleasant and healthy living environment. Further, it is currently overlooked by the adjoining two storey residential flat building and is within an area affected by aircraft noise. The replacement development will rectify these issues. The proposed dwellings take full advantage of the northerly aspect and will incorporate the necessary noise attenuation measures.

The following table sets out the proposals' consistency with the relevant quantitative standards.

DEVELOPMENT PLAN PROVISION	STANDARD	PROPOSED
Allotment Area	Not specified	Average of 300 square metres
Allotment Frontage	Not specified	19.81 metres
Site Coverage	Not specified	40.22%
Primary Street Setback	4.6 metres	6 metres
Side Setback	1 and 2 metres	On the boundary and between 3.3 and 4.25 metres
Rear Setback	3 and 8 metres	On the boundary and between 3 and 4.25 metres
Building Height	Single storey recommended	Single and two storey



Internal Floor Area	100 square metres	Street fronting dwelling: 154.72 square metres Rear dwellings: 128 square metres
Private Open Space	24 square metres	Street fronting dwelling: 54 square metres Rear dwellings: 44 square metres
Landscaping	10 percent	21.73%
Car Parking	7 spaces	6 spaces

**Table 4.1** Comparison of proposal against relevant quantitative provisions for Residential Development.

#### 4.5.2 Site Coverage

The proposed development is mid-range medium density (33 dwellings per hectare). By comparison Medium Density Policy Areas 18 and 19 provide for a site coverage of 70% and 60% respectively. The present proposal is considerably below these recommended minima.

#### 4.5.3 Side and Rear Setbacks

The stated intent of dwelling setbacks from side and rear boundaries is to minimise the visual impacts from adjoining properties and minimise overshadowing. In this instance the side boundary walls of the rear dwellings, being part of a deliberate design exercise to minimise building height and present a well-articulated and interesting built form and limited in length to 6 metres, are immediately abutting unrelieved walls of existing buildings on the adjoining sites to the north and south of the subject land. It is also noted that these existing buildings are built to their respective rear boundaries and whilst it is acknowledged that the recommended side and rear boundary setbacks are not met, we say that, in this instance, the setbacks proposed respect the intent of the relevant Principles in that any likely visual and overshadowing impacts are appropriately minimised.

#### 4.5.4 Building Height

General Section, Residential Development, PDC 2 recommends that buildings on battleaxe allotments that do not have a frontage to a public road should be single storey and designed to maintain the privacy of adjoining properties. Frontage is defined as a strip or extent of land abutting on a street or water and in this instance the rear dwellings have a frontage, albeit shared, to the public road. As noted above the proposed development presents as a detached dwelling to the streetscape and any likely visual, overshadowing and residential amenity impacts have been appropriately addressed.

#### 4.5.5 Internal Floor Area, Private Open Space and Landscaping

The proposed development far exceeds the recommended minima which contributes in no small measure to the creation of a safe, convenient, pleasant and healthy-living environment.

#### 4.5.6 Car Parking

See Transportation and Access below

As noted elsewhere in this report the subject land is within comfortable walking distance to high frequency public transport and centre type activities and, accordingly, is ideally located for medium density residential development.



#### 4.6 TRANSPORTATION AND ACCESS

General Section, *Transportation and Access*, PDC 34 provides that development should provide off-street vehicle parking to meet anticipated demand in accordance with Table WeTo/2 – Off-street Vehicle Parking Requirements.

Based on Table WeTo/2 the parking rate has been calculated at 7 spaces.

In this instance a total of 6 spaces are proposed which represents a shortfall of 1 space.

It is not uncommon however to reduce the parking requirement having regard to proximity to transport services, provision of bicycle parks and where on-street parking is also available. In fact, General Section, *Transportation and Access*, PDC 44 provides that vehicle parking for residential development should be provided having regard to, *inter alia*, proximity to centre facilities and public and community transport within walking distance of the dwellings. As noted above the subject site is within 80 metres of Henley Beach Road which is a designated public transport route and includes centre type facilities.

Whilst it may be possible to provide the additional space on site this would need to be at the expense of landscaped areas. In these circumstances we respectfully suggest that the proposed parking arrangement provides a practical solution to the vehicle parking requirements without the need for large expanses of visually undesirable hard standing areas.





## 5. CONCLUSION

When assessed against certain provisions of the West Torrens Council Development Plan and more particularly Torrensville East Conservation Policy Area 33 factors emerge that may speak against the proposal, and a narrow reading may suggest that the proposal is at odds with such provisions. Whilst we readily acknowledge that the proposed development does not necessarily meet the letter of these provisions we say that, in the particular circumstances of this case, it respects the spirit and intent.

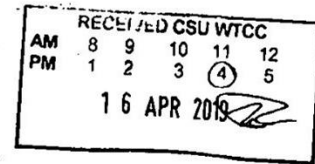
In *Town of Gawler v Impact Investments* the Supreme Court had reason to consider the correct legal approach to the assessment of a development that failed to comply with a clear policy intent. The Court noted the earlier decision of *Angaston District Council v Hamilton* where it was stated that ‘*...one would normally expect a planning authority, having proper regard to the Plan, to respect what it says unless as a matter of planning judgement, there is some good reason to justify a different conclusion.*’ (emphasis added)

In this instance we say that the site and locality characteristics provide, in the circumstances, sound reasons for departing from the policy intent in the Plan. That is to say that.....’ *as a matter of planning judgement there is some good reason to justify a different conclusion.*’

Having due regard to the nature of the site and its historical and factual context it is considered that the proposed development is not seriously at variance with the provisions of the West Torrens Council Development Plan and is a reasonable form of development that:

- is orderly and sustainable;
- is well designed and sited and responds in an appropriate manner to its setting and locality;
- will not detract from the value and setting of surrounding development;
- has due regard to its neighbours;
- will enhance the visual amenity of the locality; and
- adopts appropriate energy efficiency and crime prevention measures.

Accordingly, we consider that the proposal has sufficient merit to warrant consent.



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

**RECEIVED - CWT IM**  
**17 APR 2019**

DEVELOPMENT No. 211/1349/2018  
 PROPERTY ADDRESS: 5 Wainhouse Street, TORRENSVILLE SA 5031

YOUR FULL NAME	MARIA FACHIN	
YOUR ADDRESS	6 Huntriss St., Torrensville 5031	
YOUR PHONE No	[REDACTED]	
YOUR EMAIL	[REDACTED]	
NATURE OF INTEREST	Neighbour <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>	City of West Torrens
REASON/S FOR REPRESENTATION	17 APR 2019 City Development	
* Homes located too close to land boundary * Second story windows overlooking backyard of neighbours * High density housing (3 rowettes on the current block) * Loss of character home		
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought) * Torrensville known for its character homes, we are at risk of losing this. * Current home NOT to be demolished		

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission:

I DO NOT WISH TO BE HEARD ☐

I DESIRE TO BE HEARD PERSONALLY ☐

I DESIRE TO BE REPRESENTED BY

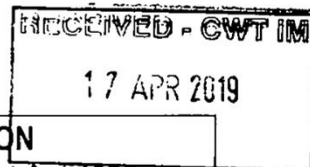
Will Cusworth  
(PLEASE SPECIFY)



SIGNED

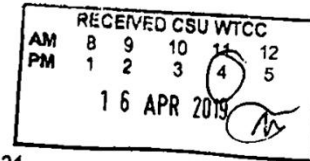
DATE 15/4/19

Responsible Officer: Josh Banks  
 Ends: Thursday 18 April 2019



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033



DEVELOPMENT No. 211/1349/2018  
 PROPERTY ADDRESS: 5 Wainhouse Street, TORRENSVILLE SA 5031

YOUR FULL NAME	Dr Will Cusumano & Ms Kate Jerris	
YOUR ADDRESS	8 MUNTRISS ST TORRENSVILLE SA	
YOUR PHONE No	[REDACTED]	
YOUR EMAIL	[REDACTED]	
NATURE OF INTEREST	ADJOINING RESIDENT <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>	
REASON/S FOR REPRESENTATION		
<ul style="list-style-type: none"> <li>- CONCERN WITH DISREGARD FOR HERITAGE &amp; CONSERVATION AREA POLICIES &amp; DEVELOPMENT PLAN OBJECTIVES</li> <li>- 2 STORY DEVELOPMENT INAPPROPRIATE FOR SITE WITH INADEQUATE BOUNDARY CLEARANCE, LACK OF FRONTAGE &amp; IMPOSING OVERSHADOWING OF OUR BLOCK</li> </ul> <p align="center">(SEE ATTACHED FOR DETAILED RESPONSE)</p>		
MY REPRESENTATIONS WOULD BE OVERCOME BY <small>(state action sought)</small>		
<ul style="list-style-type: none"> <li>- RETAINING CONTRIBUTORY ITEM OF EXISTING DWELLING</li> <li>- SITE DEVELOPMENT TO COMPLY WITH CONSERVATION AREA, DEVELOPMENT PLAN &amp; BUILDING RECOMMENDATION POLICIES</li> </ul> <p align="center">(SEE ATTACHED)</p>		

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission:

I DO NOT WISH TO BE HEARD ☐

I DESIRE TO BE HEARD PERSONALLY ☒

I DESIRE TO BE REPRESENTED BY ☐

(PLEASE SPECIFY)

SIGNED

DATE

14/4/19

Responsible Officer: Josh Banks  
 Ends: Thursday 18 April 2019

If space insufficient, please attach sheets



Statement of Representation Re: **Dev No. 211/1349/2018** for  
Property Address: **5 Wainhouse St, TORRENSVILLE SA 5031**

---

WA CUSWORTH and KM Jefferis  
8 Huntriss St, TORRENSVILLE SA 5031

### REASONS FOR REPRESENTATION

The submitted application shows a disregard for the intent of the Council Development Plan, for the Heritage and Conservation Area applicable to the property at 5 Wainhouse St, Torrensville, and it downplays the significance of the Contributory Item that the existing dwelling represents. It also fails to comprehensively consider the impact upon adjoining properties that a two storey development at this site would have.

*With regard to the Council Development Plan and compliance with Heritage and Conservation Area Objectives:*

- The relevant Conservation Area makes up a comparatively small proportion of the suburb of Torrensville, placing a greater onus on the preservation of this property as a Contributory Item in order to maintain the historic character of the area.
- While the submitted development plan may be considered acceptable within other areas of Torrensville not bound by Heritage and Conservation Area objectives and Development Control, we believe that this submission with plans to demolish a Contributory Item, and construct a two storey development is totally inappropriate for a Conservation Area.
- Existing Telstra infrastructure and flat block buildings adjacent the property represent **exceptions** to the character of the area, and should not be used to justify a departure from Conservation Objectives, or to downplay the relevance and significance of this Contributory Item.
- Reference in the submission to the existing Contributory Item as being a "common place" example of "mass produced" bungalow architecture should be justification enough as to why this property should be preserved within the Conservation Area. In our view, this should only be reinforced by it's proximity to commercial (Telstra), flat-block and reproduction constructions, which given due consideration, should weight more heavily toward protecting and conserving the Contributory Item, as the only well-preserved, original-style bungalow construction in the immediate vicinity, and the only construction able to contribute positively to the historic character of the area.
- A broader, more-considered viewing of the Wainhouse streetscape beyond adjoining properties (as outlined above), should only confirm the placement of this Contributory Item within a Conservation Area, and supports its relevance to the historic character of the area with a "...sense of unity..."
- Plans to utilise coloured cladding and weatherboard elements for structures at the rear of the property are not consistent with the Objectives of the Development Plan, nor is the inclusion of a two storey development. The impact of such construction on a Conservation Area with established architectural design and materials solidly in the first decades of last century should not be downplayed.

- With regard to the above point, the submitted plan will impact heavily on the outlook from our property, with loss of a significant period dwelling, including its terracotta tiled roofline, pitched gables, and associated fruit trees, glass house, and grape vines, which from our perspective maintain the historic character of the area from within our own dwelling and yard. This would be replaced by boundary to boundary construction of a two storey dwelling overshadowing our yard and with modern construction materials.
- More pressingly, the impact of a two storey construction, though minimised in the submission as having little impact from the street scape of 5 Wainhouse St, should be given due consideration from the westerly Huntriss St aspect. In this context, a two storey construction will significantly overshadow our yard and the existing single story brick garage to the rear of our property, and would be readily visible from the front of our property, and street frontage of Huntriss St ie to the public. This would include unimpeded, direct line of sight down our southerly aspect driveway to the proposed two storey clad constructions.

With consideration of the above points, we **reject** the assertions of the submission in **Section 4.1** that the existing Contributory Item;

1. "...does not contribute positively to the historic character of the area", and could be replaced by a modern construction "...without meaningful detriment to the overall historic character of the Torrensville East Conservation Pollicy Area 33", and that;
2. "...a two storey development would have "...minimal., if any, visual impact on the character of the streetscape in the locality."

Drawing attention to **Section 4.2** of the submission, we similarly **reject** statements of "considered professional opinion" used to substantiate a departure from policy provisions by the exercising of "flexibility in approach". We believe these show a blatant disregard for the Development Plan Objectives, and relevance of the Conservation Area in question.

*In failing to comprehensively consider the impact upon adjoining properties that a two storey development at this site would have:*

- The planned construction would lead to total loss of privacy in our back yard, with *at least* 2 bedrooms directly overlooking our yard and with direct line of sight into the eastern portion of our dwelling. This includes line of sight into our toilet and bathroom facilities, as well as the dressing area and change station for our 2 infant children. Such a planning submission is totally unacceptable in our view. This does not show a "due regard for its neighbours" as stated in the closing paragraph.
- **Section 4.5.3** concedes that an appropriate rear boundary setback has not been met, but suggests that "...any likely visual and overshadowing impact are appropriately minimised." We reject this statement and maintain that a two storey construction would have a serious and sustained impact on the outlook from our dwelling and yard, would lead to permanent loss of privacy, and would mark a significant loss of historic character to the area noticeable to us on a daily basis. Conceding an inappropriate rear boundary setback only compounds this impact.
- **Section 4.5.4** concedes that General Section, Residential Development, PDC2 recommends any building on such a site should be limited to single storey due to lack of frontage to a

**public** road. We maintain that this is the case, despite rubbery arguments to the contrary, and would request that such development requirements ie restriction to a single story building be maintained.

- Having recently moved to Torrensville (Jan 2019) from a CBD situated townhouse, it was the preserved character of the suburb that drew us to Torrensville, as well as the lure of open yard space for our young children. We are familiar with high density living, party walls, and overshadowing of neighbours with which we lived for eleven years. With that in mind, and with reference to **Section 4.3** (sixth bullet point), we **reject** the suggestion that the "...proposed dwelling does not materially manifest itself in terms of visual built form impacts or loss of reasonably expected residential amenity". Our residence (8 Huntriss St) was purchased with knowledge of the Conservation Area and Contributory Item policies that apply to it, as well as to adjoining properties. It therefore remains a reasonable expectation that we would be able to preserve it's surrounds for ourselves and our children, and preserve the "reasonably expected residential amenity" that our yard and its vista provides.

In conclusion, and drawing attention to **Section 5** of the submission, it is readily conceded that "...factors emerge that may speak against the proposal..." and that "...the proposed submission does not necessarily meet the letter of these provisions..." (re: applicable Development Plan and Conservations Area policies). **We would uphold that this is indeed the case, and would reject suggestions or requests that any leniency should be granted.**

Similarly, **we reject the notion that the submission "...respects the spirit and intent..." of said policies** and maintain the opinion that the submission is biased in its representations, shows disregard to appropriate Heritage and Conservation Area and Development Plan provisions, and **fails to provide sound justification and "...good reason to justify a different conclusion"**.

#### **MY REPRESENTATIONS WOULD BE OVERCOME BY**

- Compliance with Heritage and Conservation Area policy, Development Plan Objectives and protection of the existing Contributory Item in the truest sense.
- Modification of the submission to afford the Contributory Item it's due regard in the development plan ie retention of the existing dwelling.
- Consideration of the true impact a two storey construction represents at this location, with suitable modification of the plans to afford privacy, for which options include but are not limited to; restriction to a single storey development (preferred) in line with development plan recommendations, appropriate rear boundary setback, and/or relocation of first storey bedroom windows so as not to overlook our property and yard, and at the very least, modification of window plans to afford privacy by restricting sightline by consideration of window height or window pane material (eg requirement for frosted panes which in our understanding would be **mandatory** for the existing proposal)

Signed;



Dr Will Cusworth



Ms Kate Jefferis



REF: 0344 – 5 Wainhouse St, Torrensville

16 May 2019

Mr Josh Banks  
Senior Development Officer – APPS  
City of West Torrens  
165 Sir Donald Bradman Drive  
HILTON SA 5033



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89 King William Street  
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E: [info@futureurbangroup.com](mailto:info@futureurbangroup.com)  
ABN: 34 452 110 398

Dear Josh,

**RESPONSE TO REPRESENTATIONS RECEIVED IN RELATION TO DEVELOPMENT APPLICATION  
211/1349/2018 AT 5 WAINHOUSE STREET, TORRENSVILLE.**

We have been engaged by the applicant, Emmanuel Politis, to provide a response to the representations received in respect of the above application.

Council has advised that two representations were received namely:

- Dr. Will Cusworth & Ms. Kate Jefferis (8 Huntriss Street, TORRENSVILLE); and
- Maria Fachin (6 Huntriss Street, TORRENSVILLE).

The concerns raised may be summarised as follows:

- disregard for the Heritage and Conservation provisions of the Development Plan;
- two-storey development inappropriate;
- overlooking;
- overshadowing;
- loss of visual amenity; and
- high density housing.

We reiterate that, from the outset, it is important to keep in mind that the Development Plan is a practical document calling for practical application. It is established law that the provisions of a Development Plan are advisory and not mandatory; they are not a set of hard and fast rules. The task of the relevant authority is to weigh-up the 'pros' and 'cons', having due regard to any guidance provided by the Plan and make a planning judgement as to whether or not the proposal accords with the overall intent and purposes of the Plan.

For this purpose, and as noted in *Hickinbotham Blue Gum Pty Ltd v Corporation of City of Campbelltown (1981)*:

*"There is scarcely any planning proposal, however good, to which some legitimate objection cannot be taken, but that does not mean that it has to be refused. On the contrary, the duty of the Planning Appeal Board is to look at the proposal as a whole, to consider all evidence, including the opinion of expert planners, in support of the proposal as a desirable and sensible form of land use and development, to weigh the legitimate objections and criticisms, with due regard to the public interest, but not ignoring private interests if both such interests can be satisfactorily reconciled, and to determine in the end whether the balance lies in favour of granting or refusing approval."*

REF 0344 | 17 May 2019





## HERITAGE AND CONSERVATION

The weight of expert evidence being, Bruce Harry and Council's independent heritage expert Douglas Alexander, both well respected and highly qualified experts in this field, lies in favour of the proposed development.

## TWO-STOREY DEVELOPMENT INAPPROPRIATE

Two-storey development is not alien to the built form of the locality and, in fact, two-storey development is common in the immediate locality. Immediately adjoining the subject land to the south is a two-storey residential flat building, approximately 20 metres further south is the two-storey Spartan building, built to the boundary, and directly opposite is the two-storey Telephone Exchange building.

It is also noted that the Urban Corridor Zone, more particularly High Street Policy Area 35, which allows for building heights of 4 storeys, is diagonally opposite and approximately 20 metres to the south of the subject land.

## OVERLOOKING

Principle of Development Control (PDC) 27, General Section, Residential Development, of the West Torrens Council Development Plan states:

*PDC 27 Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.*

The present proposal includes fixed obscure glazing, 1.7 metres above finished floor level, to the upper level windows and, accordingly, PDC 27 is met.

## OVERSHADOWING

PDC's 10, 11 and 12, General Section, Residential Development, provide that:

*PDC 10 The design and location of buildings should ensure that direct winter sunlight is available to adjacent dwellings, with particular consideration given to:*

- a) windows of habitable rooms, particularly living areas*
- b) ground-level private open space*
- c) upper-level private balconies that provide the primary open space area for an dwelling*
- d) access to solar energy.*

*PDC 11 Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.*



*PDC 12 Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:*

- a) half of the existing ground-level open space*
- b) 35 square metres of the existing ground-level open space (with at least one of the areas dimensions measuring 2.5 metres).*

In consideration of the relative position and path of travel of the sun no shadow will be cast by the proposed development on No's 6-8 Huntriss Street after approximately 12. 30 p.m. That is to say that the proposed development meets all the above provisions.

#### LOSS OF VISUAL AMENITY

The existing dwelling on No. 8 Huntriss Street is setback approximately 23 metres from the rear boundary of the subject land and No. 6 approximately 10 metres. The windows to the rear of No. 8 Huntriss Street do not appear to service living areas.

The topography of the area is relatively flat and the easterly outlook currently enjoyed by Nos. 6-8 Huntriss Street is somewhat limited to the blank walls of existing boundary development, boundary fencing, the roofs of existing buildings and trees/ ancillary domestic structures associated with the subject land which are not in the best state of repair.

The ground floor of the proposed development, excluding the al fresco, is set back 3 metres from the rear boundary. This allows for strategic and intensive landscaping which will soften the appearance and reduce the visual impact. In these circumstances, we say that the proposed development will be a significant improvement to the existing situation and enhance the visual attractiveness of the locality.

Further, the suggestion that the public would have an unimpeded and direct line of sight to the proposed development down the southerly aspect driveway of No. 8 Huntriss Street is not quite accurate as the existing garage at No. 8 obscures such views.

#### HIGH DENSITY HOUSING

The proposed development achieves a net density of 33 dwellings per hectare, which is below the stated medium density provisions of the Development Plan, being between 40 and 67 dwellings per hectare.

As previously noted, Residential Zone Objective 3 encourages increased dwelling densities in close proximity to centres, public transport routes and public open spaces.

In this instance, Henley Beach Road, being an activity centre and public transport route is approximately 80 metres to the south of the subject land and Thebarton Oval and Kings Reserve are approximately 500 metres to the north. That is to say that the present proposal respects this policy intent.

Again, and as noted above, the Urban Corridor Zone, more particularly High Street Policy Area 35, which allows for minimum net residential densities of 70 dwellings per hectare and building heights of 4 storeys, is diagonally opposite and approximately 20 metres to the south of the subject land.





## CONCLUSION

Having given due consideration to the representations received we stand by our original submission that having due regard to the nature of the site and its historical and factual context the proposed development is a reasonable form of development that:

- is orderly and sustainable;
- is well designed and responds in an appropriate manner to its setting and locality;
- will not detract from the value and setting of surrounding development;
- has due regard to its neighbours;
- will enhance the visual amenity of the locality;
- adopts appropriate energy efficiency and crime prevention measures; and
- includes a materials and finishes palette that blends in a coherent manner with the surrounding development.

Should you wish to discuss further any of the matters raised above, do not hesitate to call our office on (08) 8221 5511.

Yours Sincerely

A handwritten signature in blue ink that reads 'A. Kelly'.

Tony Kelly  
Principal



## Memo

To Josh Banks  
From Richard Tan  
Date 21-May-2019  
Subject 211/1349/2018, 5 Wainhouse Street, TORRENSVILLE SA 5031

Josh Banks,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

Note: Internal check with AutoCAD has indicated difficulty in getting the accurate scale for the hand sketch plan. It is assumed that the dimension as indicated in the plan is accurate.

### 1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

- 1.1 This has been previously accepted in City Asset's assessment dated 10/01/2019.

### 2.0 Verge Interaction

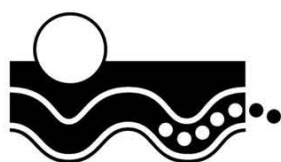
- 2.1 The common driveway area and crossover has been assessed as satisfying minimum requirements.
- 2.2 It is also important to ensure that the functionality of this driveway entrance and passing area is not compromised by the ultimate installation of letterboxes, above ground service metres or similar.

Provided plans has indicated that letterboxes is located within the common driveway area, within the 300mm landscaping area.

**It is recommended that the letterboxes should be relocated to outside the common driveway area.**

**It is also recommended that any approval associated with this development included a condition of similar wording to the following;**

**"No aboveground structure(s) such as letterboxes, service meters or similar are to be installed within the common driveway entrance and passing area."**



City of **West Torrens**

Between the City and the Sea

**2.3** Offset distance for stormwater connection through the verge area to remaining verge features has been assessed as satisfying minimum requirements.

**2.4** No further assessment provided. Item still considered outstanding.

It should also be nominated for the stormwater connection through the road verge area to be constructed of shape and material to satisfy Council's standard requirements

- 100 x 50 x 2mm RHS Galvanised Steel or
- 125 x 75 x 2mm RHS Galvanised Steel or
- Multiples of the above.
- No connection through bus stop hard stand (if applicable).

**It is recommended that revised plans clearly and accurately indicating satisfaction of the above criteria be provided to Council.**

### **3.0 Traffic Requirements**

**3.1** This has been previously accepted in City Asset's assessment dated 10/01/2019.

**3.2** This has been previously accepted in City Asset's assessment dated 10/01/2019.

**3.3** This has been previously accepted in City Asset's assessment dated 10/01/2019.

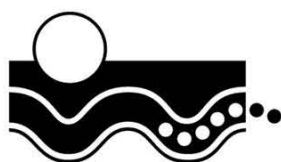
**3.4** It is noted that with current driveway crossover proposal, number of on-street car parking space will be reduced from two to one.

**3.5** Traffic manoeuvrability has been previously accepted in City Asset's assessment dated 26/03/2019.

**3.6** No further assessment provided. Item still considered outstanding.

It is understood that for group dwellings it is recommended that there should be a provision of an on-site visitor car parking spaces at a rate of 0.25 spaces per dwelling. As there are 2 proposed dwellings there would be the expectation of one on site visitor car park. The current proposal does not allow for this. It is considered **critical** that this extra car park be provided due to the loss of off-street car parking as a result of the two proposed crossovers.





City of **West Torrens**

Between the City and the Sea

**It is recommended that revised plans showing the provision of this additional visitor parking space be provided to Council.**

#### **4.0 Waste Management**

**4.1** This has been previously accepted in City Asset's assessment dated 10/01/2019.

#### **5.0 Stormwater Management (3 or more higher density, row and group dwelling) (ROOF AREA MUST BE 50% OF SITE OR MORE)**

**5.1** The applicant has provided a 3000L rainwater tank. Without any supporting calculation, it is assumed that the applicant has adopted Council's 'Alternate' stormwater detention approach. However, I noted that the provided stormwater plan has indicated that less than 90% of the roof runoff has been directed to the rainwater tank.

It is recommended that should the applicant wish to adopt Council's 'Alternate' stormwater detention approach, the plan should be updated to include all criteria as listed in the following:

For this scale and nature of proposed development, Council's City Assets Department would consider acceptable an alternate approach to the provision of conventional stormwater detention calculations and implementation.

This alternate solution would provide improved sustainable supply to water to the ultimate homeowner and at the same time, collect and use the majority of the roof stormwater generated by the properties.

In this alternate stormwater management proposal, the following arrangements should be notated for each dwelling within the development.

- Installation of a 3,000 litre rainwater tank (no detention element).
- Rainwater tank plumbed to deliver recycled water all toilets and laundry cold water outlet. (Can also be connected to Hot Water Service if desired).
- A minimum of 90% of the dwelling roof area is to have its stormwater runoff directed to the rainwater tank.
- Mains water backup, pump and plumbing arrangements as typically required to support such an installation are to be compliant with the standard Building Code requirements associated with a compulsory rainwater tank installation.



- The stormwater collection and re-use system is to be installed and operational prior to occupancy of the dwelling.

In association with a development where the applicant has nominated this approach, it is recommended that a condition similar to the following be included with any approval;

- Prior to occupancy of a dwelling, the 3000 litre stormwater collection & reuse tank and associated plumbing to service all toilets and laundry is to be installed and operational.

Should the applicant not desire to utilise the above alternate arrangement for stormwater management, then the applicant would be requested to demonstrate through satisfactory calculations and design for conventional stormwater detention. These works to limit the peak discharge rate for the site critical 20 year ARI storm event to equivalent to a predevelopment arrangement with a 0.25 runoff coefficient.

**It is recommended that revised plans and supporting calculations (if necessary) clearly and accurately indicating satisfaction of the above criteria be provided to Council.**

- 5.2** I noted that the stormwater for Dwelling 1 has been connected to the stormwater connection of Dwelling 2 and 3. If Dwelling 1 has a Torrens title then stormwater easement should be indicated on plan.

**It is recommended that further clarification regarding the land title should be provided.**

Regards,  
Richard Tan  
Civil Engineer

## Arboricultural Assessment of Street Trees

Development Application No: 211/1349/2018

**REFERRAL DUE DATE:** 30 June 2019

**Assessing Officer:** Josh Banks  
**Site Address:** 5 Wainhouse Street, TORRENSVILLE SA 5031  
**Certificate of Title:** CT-6084/926, CT-6084/927  
**Description of Development** Demolition of existing dwelling and associated ancillary domestic structures and construction of one single storey detached dwelling and a two-storey residential flat building comprising two dwellings

### TO THE TECHNICAL OFFICER – CITY ASSETS

**Please provide your comments in relation to:**

- ☐ The removal of or impact upon the Street Tree
- ☐ Species of Tree:
- ☐ Your advice is also sought on other aspects of the proposal as follows:

PLANNING OFFICER - Josh Banks

DATE 20 May 2019

### FROM THE TECHNICAL OFFICER

**I have examined the plans as requested and provide comments as follow.**

As with all development applications it must be proven beyond reasonable doubt that all alternatives have been explored so not to hinder the progress of any street tree(s).

Any proposed development that does not consider "AS4970 Protection of Trees on Development Sites", is likely to require revision until all plans accurately correspond with the specific tree information detailed in this standard.

Verge interaction must consider all services that cross council land including stormwater outlets (and other) which will need to be maintained a minimum of 2.0m from any existing street tree (unless existing or otherwise negotiated). All services must be indicated /documented on appropriate plans for Council assessment and approval.

**A site investigation together with the information provided has revealed that the proposed widening of the existing crossover on the southern side of the street tree will only have minor impact on the existing *Lophostomen confertus* (brush box) street tree.**



There is 10.3m from the southern property boundary to the buttress root section of the existing street tree. This will allow for a 2.0m offset from the street tree and accommodate a 1.0m offset from the proposed stormwater location.

For the proposed crossover location on the northern side of the existing street tree, there is 7.8m from the street tree to the northern property boundary and a stobie pole located 0.3m inside this boundary.

City Operations will support a crossover in this location with a 2.0m offset from the street tree.

As a result of the proposed development on Wainhouse Street, City Operations in this instance will support this proposal in its current form.

Please note, under no circumstances is any individuals other than council staff permitted to interfere with a street tree. If pruning etc. is required, council must be notified via the appropriate customer request, and council staff will perform all works associated with the community asset.

Final crossover locations will be confirmed once appropriate documentation has been received from the applicant and they have submitted "an application to construct a vehicle crossing place(s) across council land".

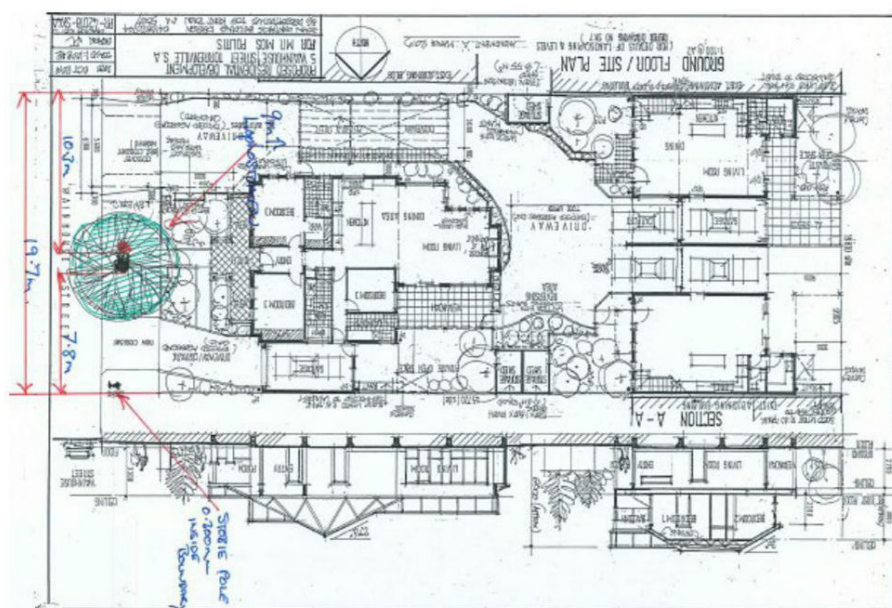
**Rick Holmes**  
**Technical Support Officer Arboriculture**  
**165 Sir Donald Bradman Drive**  
**Hilton SA 5033**

**Telephone: 8416 6333**

**Fax: 8443 5709**

**Email: rholmes@wtcc.sa.gov.au**

**DATE: 23/05/2019**



## City of West Torrens Heritage Advisor Comment

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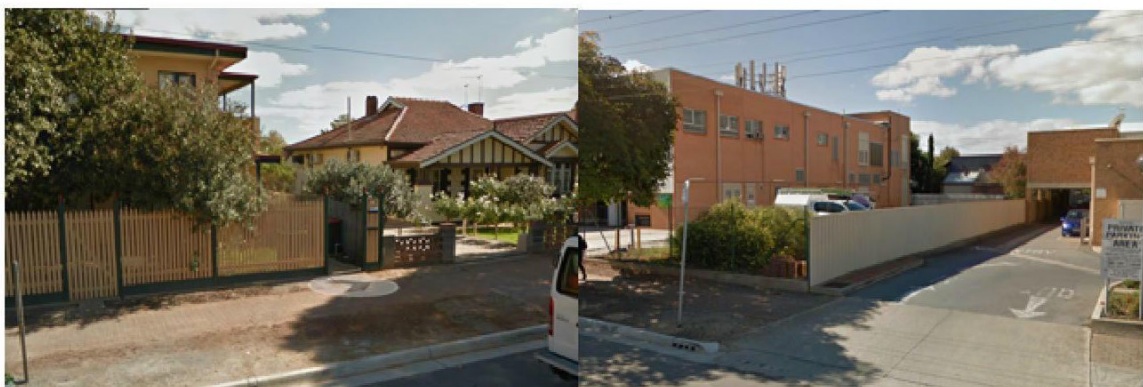
**Planning Application No.:** 211/1349/2018  
**Location:** 5 Wainhouse Street, Torrensville  
**Zone:** Residential  
**Policy Area:** Torrensville East Conservation Policy Area 33  
**Heritage Status:** Contributory  
**Proposal:** Demolition of existing dwelling and associated ancillary domestic structures and construction of one single storey detached dwelling and a two-storey residential flat building comprising two dwellings  
**To:** Brendan Fewster  
**Date:** 09/01/19

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## City of West Torrens Heritage Advisor Comment



### **Description:**

The proposal is to:

- Demolish a Contributory Place, a detached, single storey bungalow style dwelling with driveway to the south;



## City of West Torrens Heritage Advisor Comment

- Demolition of various outbuildings;
- Construct a replacement single storey detached dwelling facing Wainhouse Street and single width garage constructed to the north boundary;
- The new dwelling appears slightly further forward than the existing dwelling;
- Widen the existing driveway;
- Construct a new driveway on the north side;
- Construct two semi-detached dwellings, of two storey construction with the single storey portion constructed to the north and south boundary with the first floor set back, apart from a weatherboard clad access staircase to each side, with a raking roof portion above the single storey portion;
- Construct a pergola over the existing driveway.

The replacement dwelling facing the street will have a pitched main roof with its ridge running parallel to the street and a symmetrical bungalow style verandah.

The new two storey dwellings will have a traditional "M" main roof with projecting gables over the first floor verandah.

The material palette includes:

- Brick walls;
- Corrugated roofing;
- Weatherboard cladding.

The proposed colours to the development are:

- Traditional colours to the single storey building;
- Modern colours to the two additional dwellings.

I note there are no details of:

- land division;
- front fencing;
- the front set back to the existing dwelling is not dimensioned.

Having inspected the Subject Land I observed that the Contributory Place:

- Had been altered internally and externally;
- Was in reasonable structural condition.

I also observed the Subject Site to be on the southern edge of the Policy Area, proximate to commercial development on Henley Beach Road and there to be:

- Modern residential flat building to the south;
- A modern, telecommunications building opposite.

The immediate locality is therefore not pristine in character.

I have therefore considered the following Development Plan Provisions:

### **Torrensville East Conservation Policy Area 33**

#### **OBJECTIVES: 1**

#### **DESIRED CHARACTER**

#### **PRINCIPLES OF DEVELOPMENT CONTROL: 1,2,3,4,5**

#### **Historic Conservation Area**

## City of West Torrens Heritage Advisor Comment

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**OBJECTIVES: 1,2,3,4**

**PRINCIPLES OF DEVELOPMENT CONTROL: 1,2,3,4, 5,6, 7, 8, 9,10, 13**

**Residential Zone**

**OBJECTIVES: 1,2,3,4**

**DESIRED CHARACTER**

**PRINCIPLES OF DEVELOPMENT CONTROL: 1,5,6,7,8,10,12,13**

### **Assessment:**

The Desired Character of Torrensville East Conservation Policy Area 33 Objective 1 and PDC 2, Historic Conservation Area PDC 1 and Residential Zone Objective 4 and PDC 5, seek development of predominantly detached dwellings and limited semi-detached dwellings. Wide frontages to the street are contemplated.

Desired character of the Policy Area does not seek to increase vehicle crossovers because on-street visitor car parking is supported by limiting vehicle cross-overs.

Desired Character also considers streetscape character such as fencing and street trees, which appear retained in this proposal.

Importantly it seeks unity of built-form, where all new development is complementary to historic buildings rather than dominating or detracting from them. It seeks predominantly one storey buildings, with some two storey buildings where the upper level is contained within the roof space in a manner that is complementary to the single storey character of nearby buildings and new dwellings that incorporate building elements common to older structures such as pitched roofs, verandas and simple detailing, as well as building materials such as bricks, limited rendered masonry and corrugated iron/steel.

Considering the infill development in a similar vein to alterations and additions the rear location ensures they have minimal impact on the streetscape. However, the rear development does not reflect the side or rear setbacks of the existing dwelling and the front dwelling is slightly further forward. Consideration of the surrounding context is of importance.

This application does not provide details of front fencing proposed or land division. The density is somewhat increased by the proposal, while the wide frontage is considered achieved. Notably the new garage is not forward of the main facade.

The proposal is single storey facing the street satisfying PDC 3 and the front setback, while forward, is considered to satisfy PDC 4.

PDC 5 seeks a land division application on the basis that the accompanying application is for dwellings that are consistent with the desired character statement for the policy area. In that respect the built form is considered to satisfy Desired Character, especially in consideration of the site location and context.

While the conservation of the area is lessened by the demolition of a Contributory Place and Objective 4 and PDC 2 and PDC 3 not satisfied, the replacement dwelling is considered to provide a similar contribution satisfying Objective 1 and a development that demonstrates enhancement of the historic character of the area as sought by Objective 2.

The built form, especially facing the street is complementary satisfying Objective 3. The design advice of PDC 3 and PDC 4 are adopted in locating the new development to the rear, retaining the prominence of and screening offered by the street facing single storey character, despite not retaining the Contributory Place.

## City of West Torrens Heritage Advisor Comment

PDC 5 (c) is considered satisfied by the replacement dwelling that supports the desired character for the Historic Conservation Area and a proposal that has incorporated the guidance of PDC 6. The potential for a generous front garden and fence remain and therefore PDC 7 and PDC 9 should be easily satisfied. The design of the infill development adopts the guidance of PDC 8, through the setting back of the upper storey and its partial incorporation within the roof space.

Detail is required on fencing to also satisfy PDC 10. Unfortunately, the pattern internally of the allotment will not be retained but the scale of the original allotment boundaries will be retained. PDC 13, while not fully satisfied is not considered offended.

The proposal satisfies the Residential Zone Objectives 1 to 3 by offering choice and increased density closer to a public transport route.

Residential Zone PDC 6 is accepting of two storeys and the maximum height of the development and the set backs from side boundaries, while located to the rear, does not detract from the desired streetscape character, because the generous spaces to the side boundaries have been preserved with the new front dwelling, as contemplated in PDC 7. The front setback appears consistent with PDC 8.

The balcony screening appears to mitigate the concerns of PDC 10. The proposal adopts lesser setbacks than contemplated in PDC 11, but the adjacent structures on neighbouring allotments suggest this is not a major concern, especially with the considerations of PDC 12 and PDC 13.

### **Conclusion:**

The proposal demonstrates careful consideration of historic streetscape character through the visual interest, proportions and composition, scale and bulk of the front replacement dwelling, which provides a satisfactory replacement for the Contributory Place.

The rear development is acceptable in consideration of the screening offered by the replacement dwelling and the adjacent structures constructed to the side boundaries.

The surrounding altered context in the immediate locality provides the opportunity for this development to occur.

Heritage support subject to:

- Details of front and side fencing.

**Douglas Alexander**



**6.2 80-84 Sir Donald Bradman Drive, HILTON**

Application No 211/1287/2018

**DEVELOPMENT APPLICATION DETAILS**

DESCRIPTION OF DEVELOPMENT	Construction of a one and two-storey facility, incorporating office, training room, commercial kitchen, store and 3.1m high acoustic barrier, advertising and ancillary carpark	
APPLICANT	Meals on Wheels SA Inc.	
LODGEMENT DATE	30/11/2018	
ZONE	Residential Zone	Commercial Zone
POLICY AREA	Cowandilla / Mile End West Character Policy Area 23	Arterial Roads Policy Area 1
Precinct	N/A	Precinct 3 Sir Donald Bradman Drive (Mile End)
APPLICATION TYPE	Merit	
PUBLIC NOTIFICATION	Category 3	
REFERRALS	Internal <ul style="list-style-type: none"> <li>• City Assets</li> <li>• City Operations</li> <li>• Environmental Health</li> </ul> External <ul style="list-style-type: none"> <li>• Department of Planning, Transport and Infrastructure (DPTI)</li> </ul>	
DEVELOPMENT PLAN VERSION	Consolidated 12 July 2018	
DELEGATION	<ul style="list-style-type: none"> <li>• Where the Chief Executive Officer or Assessment Manager form the opinion that the relevant application warrants consideration and determination by the CAP.</li> </ul>	
RECOMMENDATION	Support with conditions	
AUTHOR	Jordan Leverington	

**SUBJECT LAND AND LOCALITY**

The subject land is formally described as:

- Allotment 53 Deposited Plan 48953 (CT Volume 5725 Folio 70).
- Allotment 54 Deposited Plan 48953 (CT Volume 5725 Folio 71).
- Allotment 55 Deposited Plan 48953 (CT Volume 5725 Folio 72).
- Allotment 35 Deposited Plan 2574 (CT Volume 5773 Folio 544).
- Allotment 36, 37, 38 & 39 Deposited Plan 2574 (CT Volume 5875 Folio 402).

This land is located in the area named Hilton, Hundred of Adelaide, more commonly known as 80-84 Sir Donald Bradman Drive, Hilton. The subject site is irregular in shape with a 52 metre (m) wide frontage to Sir Donald Bradman Drive, a secondary frontage to Pearson Street of 109m and a site area of 5167 square metres (m<sup>2</sup>). It is noted that there are no easements, encumbrances or Land Management Agreements on the Certificates of Title and there are no regulated trees on the subject site or on adjoining land that would be affected by the development.

The site is currently vacant, having recently had all existing structures and vegetation removed. The site is relatively flat with the southern end of the allotment subject to up to 0.1m of flood water during a 1 in 100 year Annual Rain Interval (ARI) event.

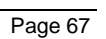
The locality is mixed use in nature comprised of low density residential development as well as commercial development along Sir Donald Bradman Drive. Residential development is generally in the form of post-war single storey detached dwellings at low density.

Commercial development in the locality is single and double storey in nature and either built up to the front boundary along Sir Donald Bradman Drive or set back with a car park to the front of the site. The commercial land uses vary from shops and restaurants to light industry and bulky goods.

Sir Donald Bradman Drive is an arterial road with an estimated 29,800 vehicles per day

The amenity of the locality is considered to be low to medium due it being located under the flight path, as well as the traffic impact along Sir Donald Bradman Drive and existing commercial development.

The subject land and locality are shown on the aerial imagery and maps below.







## Location Map WeTo/9

 School  
 Railways  
 Local Reserves  
 Development Plan Boundary

## RELEVANT APPLICATIONS

DA Number	Description of Development	Decision	Decision Date
211/175/2019	Demolition of existing and associated structures	Development approved	26/02/2019

## PROPOSAL

The proposed application seeks to build the South Australian head office for Meals on Wheels. This facility will incorporate:

- A commercial kitchen;
- Office;
- Training rooms;
- Store;
- Loading area; and
- Car parks.

The facility will accommodate between 55 - 70 staff and visitors during operating hours between 5:00am and 11:00pm Monday to Friday. The office and training areas of the facility are to be located adjacent Sir Donald Bradman Drive where the building is to be two storeys in height.

There are two car parks proposed, the largest of which is located at the northern end of the site and can accommodate 53 vehicles. The smaller car park, at the southern end of the site, can accommodate a further 11 vehicles. The peripheries of the car parks will be landscaped.

The loading area is to be located on the eastern side of the building and will be used by Medium Rigid Vehicles (MRV). Local deliveries will also be undertaken by commercial vans to be stored adjacent the northern façade of the building when not in use.

The existing brick wall along portions of the eastern boundary of the site will be retained up to a height of 3.1m. On portions of the boundary where this wall does not exist, a 3.1m high Colorbond® fence will be built to form an acoustic barrier to the adjacent dwellings.

General and food waste will be collected daily, whilst comingled recycling will be collected at least twice a week. All waste will be collected by a private contractor.

The relevant plans and documents are contained in **Attachment 2**.

## INTERNAL REFERRALS

Department	Comments
City Assets	<ul style="list-style-type: none"> <li>• Car parking numbers and vehicle movements have been determined as satisfactory.</li> <li>• The largest vehicle to be accommodated on-site should not exceed a Medium Rigid Vehicle (MRV) as defined in the Australian Standard 2890.2.</li> <li>• All commercial and service vehicles must enter via the Pearson Street crossover.</li> <li>• The car park access via Sir Donald Bradman Drive must only be used for visitor parking and not commercial vehicles.</li> <li>• The crossover on Sir Donald Bradman Drive needs to be widened in order to accommodate simultaneous movement of vehicles.</li> <li>• Unused existing crossovers on Pearson Street will be reinstated to upright kerb.</li> </ul>

	<ul style="list-style-type: none"> <li>Stormwater collection and reuse concept is acceptable and detailed design should be in accordance with this concept.</li> <li>Peak stormwater discharge from the site shall not exceed 22 litres per second.</li> <li>The subject site is prone to a low level of flooding, but the proposed FFL is satisfactory to mitigate inundation.</li> </ul>
<b>City Operations</b>	<ul style="list-style-type: none"> <li>Some pruning of a Platanus x acerifolia (London Plane tree) will be necessary to allow construction of the proposed building. These works will be undertaken by Council staff.</li> </ul>
<b>Environmental Health</b>	<ul style="list-style-type: none"> <li>Generally supportive of the proposal but wishes to arrange a pre-opening inspection to ensure food safety standards have been achieved.</li> </ul>

### EXTERNAL REFERRALS

Department	Comments
DPTI	<ul style="list-style-type: none"> <li>Does not object in principle to the proposed development.</li> <li>Council should ensure there is sufficient parking on site.</li> <li>The Metropolitan Adelaide Road Widening Plan shows that there is a possible requirement for a 4.5m deep strip of land to be acquired for road widening purposes. Consent to build within this area needs to be obtained from the Commissioner of Highways.</li> </ul>

A copy of the relevant referral response is contained in **Attachment 3**.

### RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the following Zones, Policy Areas and precincts as described in the West Torrens Council Development Plan:

- Residential Zone, Cowandilla / Mile End West Character Policy Area 23
- Commercial Zone, Arterial Roads Policy Area 1
- Commercial Zone, Arterial Roads Policy Area 1, Precinct 3, Sir Donald Bradman Drive (Mile End)



The relevant Desired Character statements are as follows:

<b>Residential Zone - Desired Character</b>	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objective	4
Principles of Development Control	1, 3, 5, 12, 14, 17 & 18

<b>Cowandilla/Mile End West Character Policy Area 23 - Desired Character</b>	
<p><i>The policy area will contain predominantly detached dwelling and semi-detached dwellings. There will also be some small-scale non-residential activities such as offices, shops and consulting rooms in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will vary in size from low density to very low density and are generally deep, with narrow frontages to main streets. Subdivision will reinforce the existing allotment pattern which is a significant positive feature of the policy area.</i></p> <p><i>There will be unity of built-form, particularly as viewed from the street, where all new development is complementary to the key character elements of Victorian-era villas, cottages, inter-war bungalows, Spanish mission and Dutch colonial-style dwellings, rather than dominating or detracting from them. Key elements of this character include pitched roofs, verandahs/porticos and masonry building materials. There will be predominantly one storey buildings, with some two storey buildings designed in a manner that is complementary to the single storey character of nearby buildings. Setbacks will be complementary to the boundary setbacks of older dwelling in the policy area, preserving considerable space in private yards for landscaping.</i></p> <p><i>There will be no garages/carports forward of the main façade of buildings. Fencing forward of dwellings will be low to provide views of built-form that define the character of the policy area. Any driveway crossovers will be carefully designed and located to ensure the preservation of street trees which have an important positive impact on the streetscape.</i></p>	
Objective	1
Principles of Development Control	1 & 2

<b>Arterial Roads Policy Area 1 - Desired Character</b>	
<p><i>This policy area will accommodate a wide range of commercial and light industrial uses.</i></p> <p><i>It is envisaged that the appearance of commercial development within the policy area will be improved through the redevelopment and upgrading of existing development sites.</i></p> <p><i>Development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities.</i></p>	
Objective	1
Principles of Development Control	1, 2, 3, 7 & 8

<b>Precinct 3: Sir Donald Bradman Drive (Mile End) - Desired Character</b>	
<p><i>This precinct will accommodate bulky goods outlets, light industry, service industry and warehouses. Residential developments in the form of two and three storey residential flat buildings or dwellings above office and consulting room developments are envisaged in the area west of the South Road intersection.</i></p> <p><i>Development facing Sir Donald Bradman Drive and South Road will be of high quality and well landscaped. Large scale development up to three storeys is envisaged east of the South Road intersection, reducing to smaller scale development west of the intersection.</i></p> <p><i>That part of the precinct between the Hilton Bridge and South Road will accommodate high quality bulky goods outlets development and upper level office space.</i></p> <p><i>Extensive landscaping will be undertaken in the setback areas near the eastern boundary of the precinct incorporating substantial trees which will grow to form prominent features in the eastern part of the precinct, particularly as viewed from the Hilton Bridge.</i></p>	
Principles of Development Control	26 & 27

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

## LEGAL ADVICE

As previously described, the subject site crosses two Zones, two Policy Areas and a Precinct. Previous case law has directed that the proposal must be assessed against **all** the relevant provisions of the Development Plan. But as these Zones, Policy Areas and Precinct have different Desired Character statements, non-complying triggers and public notification requirements, a number of issues have arisen through the assessment process. Of particular note is that some aspects of the proposal are non-complying in the Residential Zone, but not in the Commercial Zone.

The applicant originally proposed that the application was an integrated development and, as such, none of the individual aspects of the development could be separated out and individually assessed. In addition, since integrated development is not listed as non-complying, the proposal should be assessed as merit. This conflicted with previous legal advice Council had received when considering recent applications for petrol stations. This advice indicated that the Courts were no longer supporting the notion of integrated development and that each component part of a development proposal should be considered when assessing the application.

Administration sought further legal advice in order to determine the correct approach to categorisation and processing of this particular application. The advice suggested that the application should be assessed on merit as those aspects of the development that would trigger the non-complying status were not to be built in the Residential Zone. Given that the advertising, office and industrial components of the proposal are all to be built in the Commercial Zone, the non-complying process is not triggered. The legal advice also reinforced the fact that the application could not be considered as an integrated development.

## QUANTITATIVE STANDARDS

The proposal is assessed for consistency with the quantitative requirements of the Development Plan as outlined in the table below:

The parcel of land within the Residential Zone is to be used for the northern car park, landscaping and the acoustic fence. As there are no quantitative provisions within the Zone that relate to these aspects, no assessment of these aspects can be undertaken. Accordingly, the following provisions apply to the Commercial Zone only.

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
GROSS LEASABLE AREA <i>Commercial Zone</i> <i>PDC 4</i>	Consulting room or office (not greater than 30% of gross leasable floor area)  GLFA = 2443m <sup>2</sup>	30% (741m <sup>2</sup> )  <b>Satisfies</b>
PRIMARY STREET SETBACK <i>Commercial Zone</i> <i>PDC 8</i>	3m	3.5m  <b>Satisfies</b>
BUILDING HEIGHT <i>Commercial Zone</i> <i>PDC 32 &amp; 33</i>	2 storeys or 8.5m (max.)	2 storeys / 11m  <b>Does Not Satisfy</b>
CAR PARKING SPACES <i>Transportation and Access</i> <i>PDC 34</i>	46 spaces required	64 spaces provided  <b>Satisfies</b>

## ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### Land Use

The subject site was previously used by Rossi Boots for manufacturing and factory direct sales. This industrial land use pre-dates planning authorisations with aerial imaging showing a factory had been built on the site between 1949 and 1959.



The proposal is formed of several different land uses combined into a mixed use development. These land uses include light industry, office and car parking. None of these land uses are envisaged within the Residential Zone, whereas light industry and offices ancillary to a light industry are envisaged within the Commercial Zone and in Precinct 3 Sir Donald Bradman Drive (Mile End). Given that the light industry and office uses are located entirely within the Commercial Zone, the proposed land use is considered appropriate for the site.

### **Desired Character**

The Desired Character of the Commercial Zone, Arterial Roads Policy Area 1 and Precinct 3 Sir Donald Bradman Drive (Mile End) envisages light industry to take place although as noted, it is not envisaged in the Residential Zone. Light industry is defined in the Development Regulations 2008 as:

***light industry** means an industry where the process carried on, the materials and machinery used, the transport of materials, goods or commodities to and from the land on or in which (wholly or in part) the industry is conducted and the scale of the industry does not—*  
*(a) detrimentally affect the amenity of the locality or the amenity within the vicinity of the locality by reason of the establishment or the bulk of any building or structure, the emission of noise, vibration, smell, fumes, smoke, vapour, steam, soot, ash, dust, waste water, waste products, grit, oil, spilled light, or otherwise howsoever; or*  
*(b) directly or indirectly, cause dangerous or congested traffic conditions in any nearby road;*

The proposed development is considered to satisfy this definition whereas it is arguable whether the previous industrial use did.

The office component of the proposal will allow Meals on Wheels to incorporate the kitchen and head office on the same site. Offices are also envisaged within the Commercial Zone, Arterial Roads Policy Area 1 and Precinct 3 Sir Donald Bradman Drive (Mile End). The Desired Character for Precinct 3 specifically states that offices are envisaged along Sir Donald Bradman Drive west of the South Road intersection which is consistent with the subject site.

The Desired Character of the Residential Zone encourages residential land uses and small scale non-residential land uses which serve the local community. The proposed northern car park to be located within the Residential Zone is considered to be a small scale non-residential land use, however it will not serve the local community as it is a secured car park only accessible by staff. While this does not strictly meet the intent of the desired character, it is not considered to be fatal to the application.

### **Interface between land uses**

The Development Plan recognises the potential for issues to arise between different land uses especially when industry adjoins residential uses. However, in this instance industrial and residential land uses have existed side by side since the mid 1900's. Perhaps it is for this reason that no representations were received against the application during the public notification period.

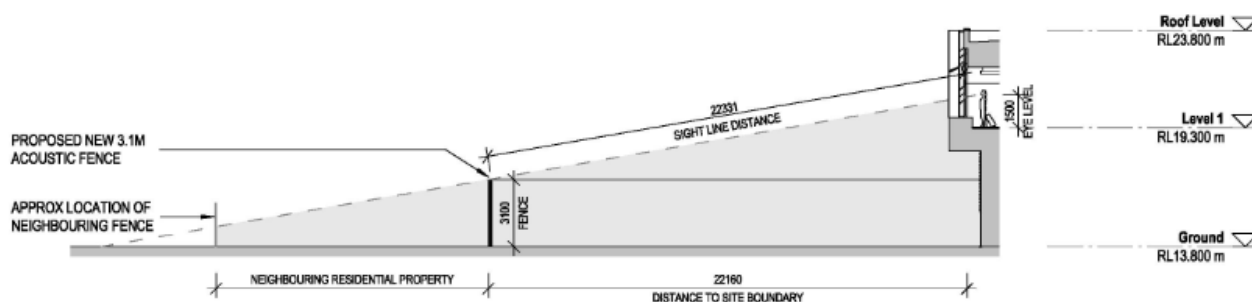
Whilst the proposed land use is similar in nature to the existing land use, the wholesale redevelopment of the site will provide for a more modern, useable and improved design in that the impacts often associated with light industry will be mitigated. For example, loading and unloading of materials was previously undertaken by Rossi on Pearson Street using roller doors placed on the western façade of the building. The proposed development incorporates a purpose-built vehicle loading bay and a suitable area for on-site vehicle movements to ensure that such movements do not impact on the local traffic network.

The proposal will nonetheless generate noise and odour which could cause nuisance to adjoining residents. However, the applicant has included methods to mitigate or eliminate the potential nuisance. Noise will be generated by staff and delivery vehicles attending the site from 5:00am and then leaving again up to 11pm. It will also be generated by plant equipment associated with the kitchen. In order to reduce the impact from these noise sources, acoustic treatments have been proposed at the northern and eastern boundaries of the subject site and on the building itself to protect the amenity of adjoining residential properties. The previous factory was built to the eastern boundary so the wall has been retained to not only reduce noise but to also retain the status quo visually. Where the boundary wall did not exist, a new 3.1m high Colorbond® fence will be built.

The applicant has provided an acoustic report written by WSP engineers that highlights the potential noise emitters such as delivery vehicles, forklift, staff cars, plant equipment, refrigeration and air conditioning units. The report was originally drafted with an assumption that semi-trailers would be conducting deliveries and a 2.8m high boundary fence would be in place. However, the application has been changed to limit the size of vehicles to an MRV and to increase the boundary wall height to 3.1m. The Acoustic report concludes that the proposed development will comply with the EPA's Environmental Protection (Noise) Policy 2009.

The roof mounted plant equipment will also be acoustically treated by a 1m high parapet wall while the kitchen exhaust fan will be fitted with an Air and Odour Management HC series hood. These hoods use stainless steel grease filters, electrostatic filtration, ozone generators and activated carbon modules to reduce the emission of odours. This equipment was tested on a McDonald's restaurant in Sydney and was found to remove between 69% and 87% of all odours produced by the kitchen. The roof mounted location of the kitchen exhaust will also help disperse any odours that are emitted before they are able to travel to adjoining residential properties.

Administration were initially concerned about the potential for overlooking from the first floor offices into the rear yard of 2 Mallen Street. However the applicant has provided a diagram demonstrating that overlooking from these offices is unlikely (see Figure 1 below or a larger copy within Attachment 2). This diagram demonstrates the proposed development will not result in unreasonable overlooking of the adjacent residential properties.



**Figure 1: Overlooking diagram**

As previously described the hours of operation will be between 5am and 11pm Monday to Friday. This has the potential to conflict with residential land uses, but due to the proximity of Sir Donald Bradman Drive this impact is considered to be minimal. The proposed hours of operation exceed those of the previous industry land use, however there were no conditions that restricted these operating hours. Issues that could arise with the extended operating hours are noise, odour and traffic impacts. The noise and odour issues are considered to be satisfactorily resolved by methods described above.

Traffic impacts are considered to be relatively minor given that the majority of staff attending the site will be working in the office component of the development during normal business hours. There will be approximately 20 kitchen staff that will attend the site outside of these hours which is unlikely to cause traffic impacts, especially as the site is well serviced by high frequency public (bus) transport along Sir Donald Bradman Drive.

Due to the hours of operation, external lighting will be necessary around the car park and loading area. The final lighting design has yet to be determined, but the applicant has provided a preliminary scheme outlining that external lighting will be via Light Emitting Diodes (LED), controlled by timers and shielded from sensitive receivers in accordance with Australian Standard 4282 - Control of the Obtrusive Effects of Outdoor lighting. A condition has been added to the staff recommendation in order to control light spill to adjoining properties and the public road network.

### **Built Form**

The proposed building is a single structure formed of two distinct parts. The two storey component adjacent Sir Donald Bradman Drive accommodates the office, meeting rooms, training rooms and storage. The single storey component accommodates the food preparation, storage and loading areas.

The external surfaces of the building have been carefully chosen to integrate the past and present. The ground level southern and western facades will be predominantly red face brick reflecting the external surfaces of the previous Rossi building and matching the brick wall being retained along the eastern boundary of the allotment. The upper level will be predominantly clad in a white metal shroud and will incorporate considerable glazing overlooking Sir Donald Bradman Drive and the southern car park.

The maximum height of the building is 11m which exceeds the 8.5m maximum height described in PDC 33 of the Commercial Zone. The Policy Area provisions do not provide any further guidance as to what the 8.5m maximum height limit is seeking to achieve although staff consider it seeks to moderate the visual bulk and scale of development as well as protect adjacent residential properties from potential overlooking and overshadowing impacts. It is considered that the overlooking and overshadowing aspects have been satisfactorily resolved. The impact from the 1.5m exceedance in height is considered to be mitigated by the 20m setback from the closest residential property boundary.

PDC 14 of the Design and Appearance section of the Development Plan calls for buildings to be designed in order to avoid extensive areas of uninterrupted walls facing public areas. The western façade of the building is 64m long with windows only being incorporated within the first 5m back from Sir Donald Bradman Drive. The remainder of the wall is made up of face brick and Danapalon (polycarbonate sheeting) in order to offset what would otherwise be a blank and uninteresting wall. The architect has also incorporated some articulation, varying heights and shapes. This is considered to be an improvement over the western façade of the previous building, as shown in Figure 2 below. It should also be noted that the dwellings on the western side of Pearson Street do not face the subject site and as such will not be detrimentally impacted. The council verge in this location is destined for major upgrades (discussed below) which will further improve the amenity of the site when viewed from Pearson Street.





**Figure 2: Looking east at the subject site from Pearson Street (Source: Google street view)**

### **Parking and Access**

A traffic impact statement has been prepared by WSP engineers on behalf of the applicant. As noted, the largest vehicle accessing the site will be a Medium Rigid Vehicle (MRV). Delivery trucks will enter solely via the southern Pearson Street crossover and exit via Pearson Street or Sir Donald Bradman Drive. These movements have been considered and endorsed by both Council's traffic engineers as well as the Department of Planning, Transport and Infrastructure (DPTI). DPTI's advice includes seven conditions and an advisory note which have been added to the staff recommendation.

The proposal includes four parking spaces at the rear of the proposed building which will be used to store delivery vans when not in use. These parks are reserved and will not be available for staff or visitors.

Staff parking is located at the northern end of the site and is accessed via a double-width crossover. Access to this car park was revised from the initial design in order to improve driver visibility. The initial proposed location resulted in obstructed views for vehicles exiting the site due to a large electrical transformer. This change is considered to satisfy Objective 2 and PDC 9 of the Transport and Access section of the Development Plan which seeks safe and convenient access for anticipated modes of transport.

Visitors will utilise the car park at the southern end of the site using the crossover on Sir Donald Bradman Drive. All car parks are of a sufficient size and allow convenient movements in accordance with Australian Standard 2890. The provision of 64 car parking spaces has been accepted as an appropriate amount that will cater for the proposed development. In addition to the car parking there are nine bicycle parks and easy access to public transport that traverses Sir Donald Bradman Drive and South Road.

On street parking will be increased as the current restrictions along Pearson Street are to be removed as all loading and unloading will now be undertaken on-site.

## Waste Management

A comprehensive waste management plan has been produced by WSP engineers and outlines the demand and servicing of the proposed development. In accordance with PDC 5 of the Waste section of the Development Plan, there is an external 29m<sup>2</sup> bin storage area located at the northern end of the building which will accommodate the following:

- 2 x 1,100L general waste bins;
- 6 x 1,100L commingled recyclables; and
- 4 x 240L food waste bins;

The majority of waste produced by the proposal is from food production, with 1400 to 2800 meals to be made each day. Waste will also be generated from the office and training areas but at a smaller scale.

Due to the volume of waste created, as well as the potential for odour and attraction of pests, waste will be collected regularly. All waste will be collected by a private contractor with general and food waste being collected daily and commingled recyclables collected a minimum of twice a week. The kitchen grease trap will be serviced on an as-needs basis but at a minimum of every 3 months. The contractor will pump out the grease into a tanker and deposit it to a licensed liquid recovery and treatment centre. This is considered to satisfy Objective 1 of the Waste section of the Development Plan which seeks waste to be minimised where possible and disposed of in an environmentally sensitive manner.

Waste collection vehicles will enter the site via Pearson Road and park adjacent the bin storage area. Bins will then be moved from the storage area in order to be deposited into the truck. Due to the potential noise arising from this activity, waste collection will be limited to between 7am and 7pm weekdays.

## Landscaping

There is a wide selection of plantings proposed that include ground covered shrubs, bushes, hedges and trees. The species have been selected to be both visually attractive whilst also being able to thrive in the local climate. PDC 2 of the Landscaping, Fences and Wall section of the Development Plan supports drought tolerant plants and suggests locally indigenous species where appropriate. The proposal includes a number of plants indigenous to Australia but only one that is locally indigenous, *Myoporum parvifolium* (Creeping Boobialla).

The proposed landscaping has been dispersed across the site which helps break up expanses of hard paved surfaces. The northern car park will have 18 trees around the periphery which will provide a green buffer between the proposed use and adjoining residential uses as well as shade for the car park.

Landscaping is also proposed along the Sir Donald Bradman Road frontage and within a court yard between the single and two storey parts of the building.

The Development Plan calls for a minimum of 10% of the site to be landscaped, which equates to an area of 517m<sup>2</sup>. The proposal provides 449m<sup>2</sup> or 8.7% of the site area. The deficiency is not considered fatal to the application due to its quality and because this is a significant increase in landscaping compared to the current situation. It should be noted that the landscaping proposed is also significantly more than that provided by other non-residential development in the locality.

Council's Assets department has agreed to enter into a verge upgrade project with Meals on Wheels to undertake extensive works to the Pearson Street frontage. These works involve:

- Removal and reinstatement of existing redundant concrete driveway inverts, being returned to kerb and water table;
- Removal of existing concrete footpath along the length of the site;
- Supply and lay new brick paved footpath along site frontage (to match into existing brick paving at Sir Donald Bradman Drive end);
- Remove existing street trees (except Plane Tree);
- Establish 15 new trees along site frontage;
- Supply and install new timber edge tree beds around each tree, including mulch infill finish;
- Establish new low level plantings around the base of each new tree;
- Supply and install compacted verge treatment (x6) between tree beds;
- Supply and install irrigation to all new land and connect to an applicant provided supply point; and
- Construction of new concrete inverts and new driveway crossing places to the site.

These new street trees will provide significant screening of the western facade of the proposed building, whilst also substantially improving the amenity of the locality and reducing urban heat loads.

### **Stormwater Management**

The proposed development will produce a large amount of stormwater runoff from both the structure itself as well as the hard paved areas of the loading bay and car parks. Council actively encourages the on-site storage and reuse of this water to not only reduce the impact on Council infrastructure, but also for its environmental benefits. Simple solutions such as plumbing rainwater to the toilet cisterns or irrigation systems can have a dramatic impact on water usage.

The applicants have provided a conceptual stormwater management plan drafted by WSP Engineers. City Assets have considered the report and generally accept its solutions, however it should be noted that detailed design has not yet been undertaken. If the final design accords with the conceptual plan then it will be appropriate. A condition to reflect this has been added to the staff recommendation.

### **SUMMARY**

The proposed development will replace an existing industrial land use but construct an entirely new structure and car park. The proposed built form will modernise the way the site can operate by allowing loading and unloading to occur on-site rather than within the public road network. There are some minor deficiencies in terms of overall building height and amount of landscaping, however they are considered minor and not fatal to the application. The application includes treatments to minimise the noise and odour impact to neighbours which are considered to be an improvement on the current situation. No representations were received during the Category 3 public notification process which indicates that the local community is not concerned by this proposal.

Having considered all the relevant provisions of the Development Plan, the proposal is not considered to be seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and warrants Development Plan Consent.



## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/1287/2018 by Meals on Wheels to undertake the construction of a one and two-storey facility, incorporating office, training room, commercial kitchen, store and 3.1m high acoustic barrier, advertising and ancillary carpark at 80-84 Sir Donald Bradman Drive (CTs 5773/544, 5875/402, 5725/70, 5725/71 & 5725/72) subject to the following conditions of consent:

### Development Plan Consent Conditions:

1. The development shall be undertaken, completed and maintained in accordance with the plans and information detailed in this application except where varied by any conditions listed below.  
*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*
2. The establishment of all landscaping shall occur no later than the next available planting season after substantial completion of the development. Such landscaping shall be maintained in good health and condition to the reasonable satisfaction of Council at all times. Any dead or diseased plants or trees shall be replaced with a suitable species.  
*Reason: To provide amenity for the occupants of the development and those of adjacent properties.*
3. The maximum size of service vehicles accessing the site, including the refuse collection vehicle, shall be limited to a Medium Rigid Vehicle (MRV).  
*Reason: To ensure the ongoing use and safety of vehicle parking and manoeuvring areas.*
4. The driveways, parking and vehicle manoeuvring areas shall not be used for the storage or display of materials or goods, including waste products and refuse.  
*Reason: To ensure the ongoing use and safety of vehicle parking and manoeuvring areas.*
5. The loading and unloading of goods and merchandise shall be carried out on the subject land and is not permitted to be carried out in the street.  
*Reason: To maintain the flow of traffic and ensure the ongoing safety of the street for travellers.*
6. Prior to the occupation or use of the development, all stormwater design and construction shall be to the satisfaction of Council to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
  - a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create insanitary or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; or
  - e) Flow across footpaths or public ways.*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*

7. During construction, stormwater from the site shall be managed to ensure that it does not cause nuisance to any adjoining property until the site is stabilised. Temporary drainage measures shall be installed as soon as the roof is constructed to ensure debris, litter, sediment, fuels and oil products from the construction site do not enter Council's stormwater system, neighbouring properties or the road network.

*Reason: To provide adequate protection against the possibility of stormwater inundation to neighbouring properties.*

8. The hours of operation of the land use approved herein shall be limited to 5am to 11pm - Monday to Friday;

*Reason: To ensure that the development does not unreasonably diminish the amenity of residents of adjoining properties.*

9. A 3.1 metre boundary fence, as shown in North Fencing Diagram shall be constructed of Colorbond® steel and be airtight at all junctions including with the ground. The fence shall be installed in accordance with the acoustic report prepared by WSP dated March 2019.

*Reason: To ensure the proposal is established in accordance with the plans and documents lodged with Council.*

10. All external lights on the subject site shall be directed, screened and of such limited intensity that overspill of light into nearby premises is avoided and no nuisance or loss of amenity is caused to any person beyond the site, including passing motorists.

*Reason: To ensure that the proposed lighting does not cause undue disturbance, annoyance or inconvenience to the general public, adjoining landowners, users, motorists.*

11. The Sir Donald Bradman Drive access driveway should be clearly signed to indicate "Visitor Car Park Access Only - No Commercial Vehicles".

*Reason: To provide safe and convenient parking and access for users of the development.*

12. Detailed design of Stormwater Detention measures are to be consistent with the WSP 'DRAINS' model as provided to ensure;

- Overall peak stormwater discharge from the site is to be limited to a maximum of 38 litres per second.
- Peak stormwater discharge from the visitor car park catchment is to be limited to 22 litres per second, with a 12,000 litre stormwater detention storage capacity.
- Peak stormwater discharge from the staff car park catchment is to be limited to 16 litres per second, with a 47,000 litre stormwater detention storage capacity.

*Reason: To ensure the proposal is established in accordance with the plans and documents lodged with Council.*

13. Detailed design of Stormwater Quality Improvement measures are to be consistent with the WSP Stormwater Management Plan dated November 2018.

*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*

14. A backflow prevention device must be installed on the private stormwater system, within the private site, to prevent the potential of surcharge from the public stormwater system entering the private site stormwater system.

*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*

15. Waste collection shall not occur before 7am or after 7pm.

*Reason: To provide amenity for the occupants of the development and those of adjacent properties.*

**Conditions imposed upon recommendation of DPTI**

16. All access shall be located in general accordance with the site plan provided by JPE Design Studio, Drawing Number. A-1-01, Revision D, dated 20 March 2019.  
*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*
17. The Sir Donald Bradman Drive access shall cater for two-way passenger vehicles and Medium Rigid Vehicle exit movements only.  
*Reason: To maintain the flow of traffic and ensure the ongoing safety of the street for travellers.*
18. The Sir Donald Bradman Drive crossover shall be suitably flared from the property boundary to the kerb line to facilitate simultaneous two-way vehicular movements while keeping a minimum of 2 metre separation from the street tree on both sides of the crossover.  
*Reason: To maintain the flow of traffic and ensure the ongoing safety of the street for travellers.*
19. All vehicles shall enter and exit the site in a forward direction.  
*Reason: To maintain the flow of traffic and ensure the ongoing safety of the street for travellers.*
20. All commercial vehicle parking facilities shall be designed in accordance with AS/NZS 2890.2:2018.  
*Reason: To ensure the ongoing use and safety of vehicle parking and manoeuvring areas.*
21. Clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in AS/NZS 2890.1:2004, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.  
*Reason: To maintain the flow of traffic and ensure the ongoing safety of the street for travellers.*
22. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of Sir Donald Bradman Drive. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.  
*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*

**Note imposed upon recommendation of DPTI**

The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Sir Donald Bradman Drive frontage of this site, for future upgrading of the Sir Donald Bradman Drive/South Road intersection. The consent of the Commissioner of Highways under the Metropolitan Adelaide Road Widening Plan Act 1972 is required to all building works on or within 6 metres of the possible requirement. The attached consent form should be completed by the applicant and forwarded to DPTI via email (dpti.luc@sa.gov.au), together with a copy of the approved plans.

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**Attachments**

1. **Objectives and Principles of Development Control**
2. **Plans and associated information**
3. **Referrals**



**Relevant Development Plan Provisions**

<u>General Section</u>		
Advertisements	Objectives	1, 2 & 3
	Principles of Development Control	1, 2, 4, 5, 6, 7, 8, 10, 11, 12, 13, 14, 15 & 16
Crime Prevention	Objective	1
	Principles of Development Control	1, 2, 3, 7 & 10
Design and Appearance	Objective	1
	Principles of Development Control	1, 2, 3, 4, 9, 10, 11, 13, 14, 15, 16, 19, 20, 21, 22 & 23
Energy Efficiency	Objectives	1 & 2
	Principles of Development Control	1, 2, & 3
Hazards	Objectives	1, 2 & 4
	Principles of Development Control	1, 2, 3, 4, 5, 6 & 7
Industrial Development	Objectives	1, 2 & 5
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8 & 9
Interface between Land Uses	Objectives	1, 2 & 3
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 12 & 13
Landscaping, Fences and Walls	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4 & 6
Orderly and Sustainable Development	Objectives	1, 2, 3 & 4
	Principles of Development Control	1, 3, 5 & 7
Residential Development	Objectives	1, 3 & 4
	Principles of Development Control	10, 11, 12, 27, 28, 29 & 31
Transportation and Access	Objective	2, 4 & 5
	Principles of Development Control	1, 2, 5, 8, 9, 10, 11, 12, 13, 14, 18, 20, 21, 22, 23, 24, 26, 27, 28, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42 & 43
Waste	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4, 5, 7 & 8



JPE Design Studio



Planning Updates  
25.03.2019

12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



## Contact Information

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### Appendix

Title Information  
Proof of Title



12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



## 1.0 Introduction & Development Summary

This document has been prepared as part of a Planning Stage development application for the proposed Meals on Wheels Development on the subject site at 80-84 Sir Donald Bradman Drive, Hilton.

The project team comprises of:

**Meals on Wheels**  
Client

**JPE Design Studio**  
Architecture / Interior Design and Landscape Architecture

**Future Urban Group**  
Planning Consultant

**WSP**  
Structural, Civil & Services Engineers

**Chris Sale Consulting**  
Cost Consultants

**Trento Fuller**  
Private Certifiers

**The Mack Group**  
Kitchen Consultants



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2.0 Site Context & Analysis

Site Context Plan - Macro





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2.0 Site Context

Site Context & Locality Plan



-  Residential (All Single Storey)
-  Commercial - 1 Storey
-  Commercial - 2 Storey

Address: 80 - 84 Sir Donald Bradman Drive, Hilton, Adelaide.

Area: Approx 5167m<sup>2</sup>.

The built form in the vicinity of the site is predominantly single storey residential with retail and commercial buildings along Sir Donald Bradman Drive.

Immediate neighbours include Chandler Macleod to the East and Battery World on the West across Pearson Street. Other notable businesses in the area include the Hilton Hotel, Hilton Plaza and Hamra Centre Library.



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2.0 Site Context

Site Context & Locality Plan





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2.0 Site Context

Existing Site Context Photographs



1. West - Pearson Street View



2. Pearson Street Existing Carpark



3. South - Sir Donald Bradman Drive View



4. Existing Transformer on Pearson Street



5. Rear of Site



6. Rear of Site



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2.0 Site Context

Neighbouring Site Context Photographs



7. Eastern View from Site



8. Immediate neighbour to site  
- Battery World



9. Neighbour across from site



10. Neighbour across from site



11. Immediate neighbour

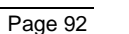


12. Western view towards





## Existing Site Survey





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3.0 Design Response

Brief & Form Development



Design Theme 1

Office / Home



Design Theme 2

Kitchen / Office Connection



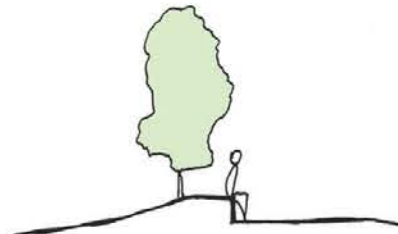
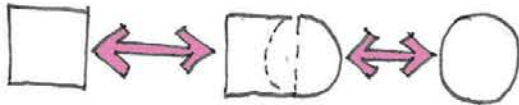
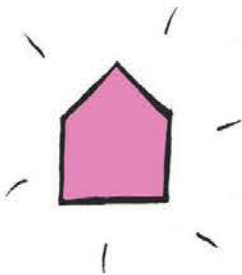
Design Theme 3

Texture / Composition



Design Theme 4

Green Integration



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3.0 Design Response

Streetscape & Long Distance View Study



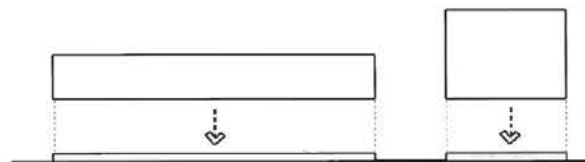


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### 3.0 Design Response

#### Form Analysis



#### Continuous Ground Plane

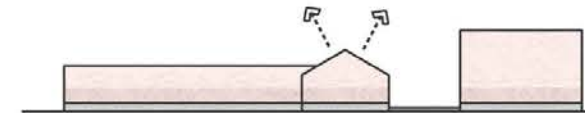
Opportunity to lift the building to create a continuous active ground plane. Create an inviting architecture in line with Meals on Wheels' focus on community service and engagement whilst addressing the site's close proximity to Sir Donald Bradman Drive and South Road.

Both office and kitchen need to be open yet sheltered to ensure harmony between a quality and environmentally connected working environment with the organizations functional optimization of a site located next to a main arterial thoroughfare.



#### Architectural Amalgamation

Creating a visually coherent architecture across two very different functional briefs to ensure the office spaces are able to functionally and visually co exist on our site. Where possible they should compliment one another.



#### Pitched Roof Kitchen - 'Central Hub' of Production & Team Connection

It is important to ensure a level of visual and physical connectivity and integration is achieved between both the kitchen and office working teams as well as within the departments. Shared indoor and outdoor spaces act to create a common ground for harboring a sense of connection.



#### Courtyard Link & Site Treatment

Providing a central courtyard link between the offices and kitchens, creating a visual connection

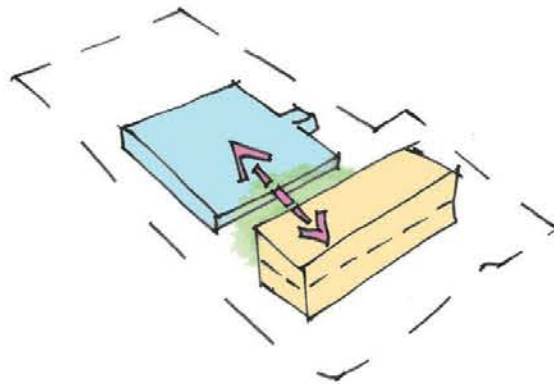
The site straddles a frontage to a major arterial road yet backs onto a residential area. The site also has a requirement for the entirety of the ground floor to be raised nominally 300mm. The aim should be to create a flowing and coherent site in it's entirety, suitable for both visiting pedestrians and vehicle traffic to safely coexist.

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### 3.0 Design Response

#### Form Analysis



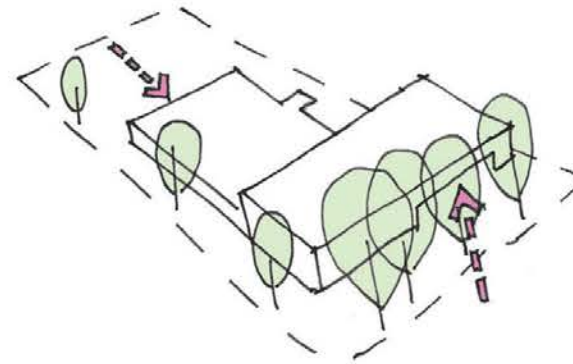
#### Split Functions

Meals on Wheels' offices are located primarily on level 01 with the exception of those departments needing to be within close proximity to the main kitchen.

A large communal hub inclusive of staff kitchenette, reception and waiting/ transitional spaces are located on ground with connection to the internal courtyard between the kitchen and office.

#### Landscape Band & Link

The ground level communal spaces and open offices are designed to have a direct visual and physical connection to the central landscape band, facilitating a line of sight to the kitchen building and into the kitchen itself.

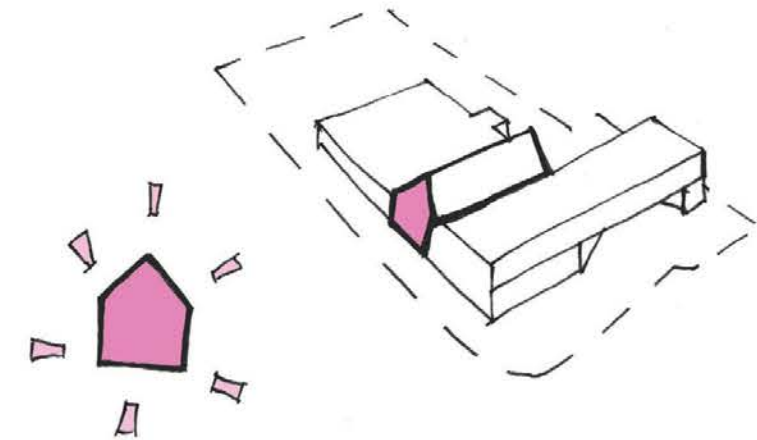


#### Lifting the Entry

Lifting the Southern entry from Sir Donald Bradman drive for vehicle accessibility creates an entry statement to the site from Sir Donald Bradman Drive.

#### Street Presence

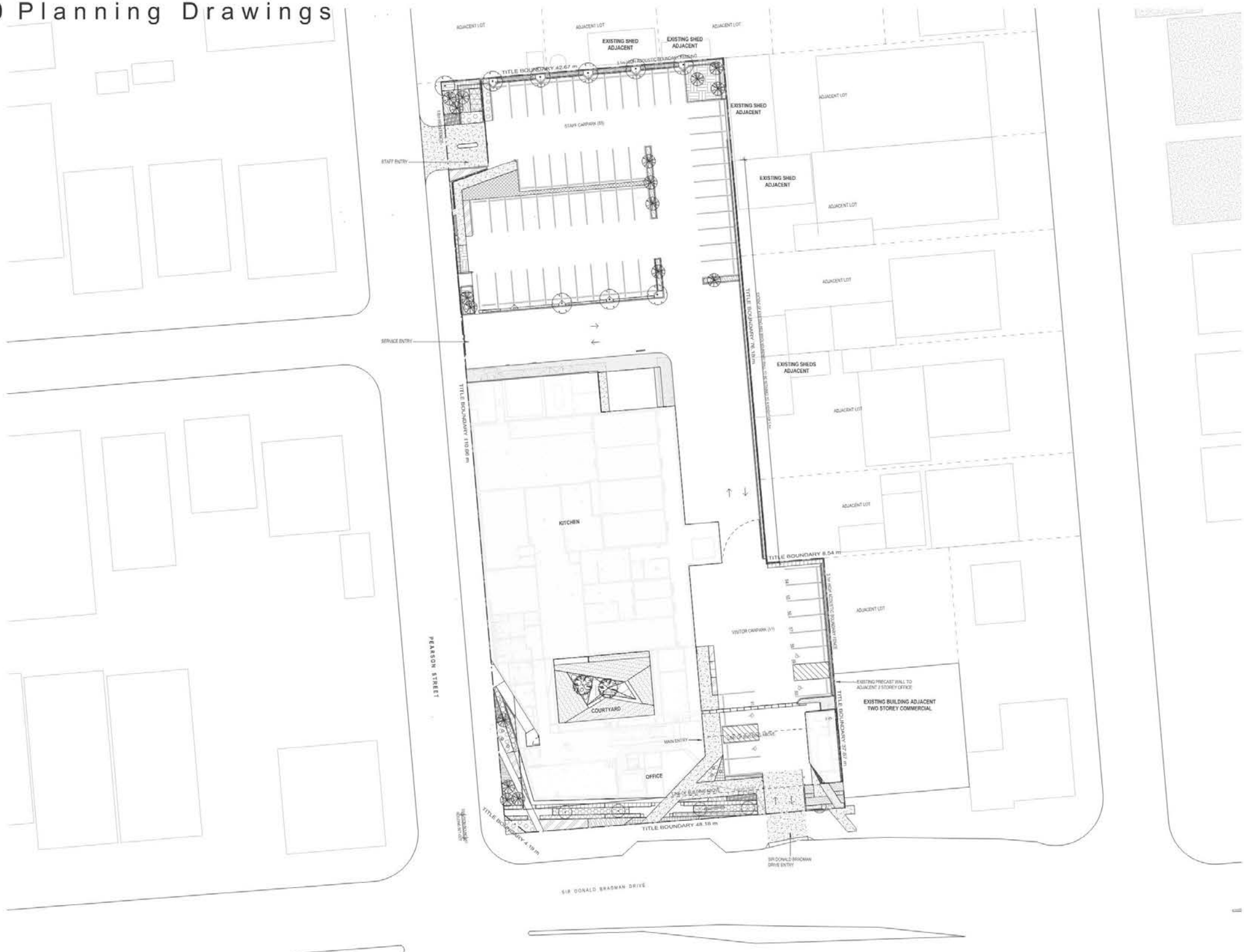
Activation from both Sir Donald Bradman Drive and Pearson Street, by green space and visual connections to and from the building. Creating the pitched roof form softens the visual impact of the kitchen block, creating an attractive interface to Pearson Street.



#### Response to Site

Connecting to design language of surrounding buildings, respecting and tying into the materiality of surrounding architecture.

4.0 Planning Drawings



Revisions:

Rev	Description	Date
A	Preliminary For Information	13.02.19
B	DWG Issue	05.03.19
C	Preliminary For Information	08.03.19
D	Preliminary For Information	20.03.19

**General Notes**

The following notes shall be used in conjunction with all Architectural, Landscape Architectural, Structural, Civil and Services Drawings, schedules and specifications relating to this project.

Refer to the Survey drawing to locate and confirm all existing services, poles, poles, easements, etc and the line prior to the commencement of work.

Do not scale off drawings.

Builder to confirm service of building, check all dimensions on site, and confirm any variations from the drawings as necessary with the Architect before commencement of work.

Contractor to visit the site and thoroughly inspect all areas and prepare a Demolition Management Plan for approval prior to the commencement of demolition work.

Contractor to undertake detailed demolition where required to ensure items to be retained are not damaged.

Contractor is to verify all items to be demolished and removed from site as required as the prior to the commencement of work.

Refer to engineers drawings for removal and relocation of services.

Building site conditions based on survey information. Confirm all details on site prior to commencement of any work.

Carriageway removal or items to be handed back to the proprietor or relevant authority.

Ensure security of all adjoining buildings is maintained at all times where boundary lines are involved for removal.

Clear away or relocate from site except those noted for reuse.

Coordinate with builder for location of building compound and handling.

Remove all obsolete existing in ground infrastructure. Refer to civil/structural and services engineers.

Protect existing adjacent buildings at all times.

Hoarding, fencing, lighting and temporary access ways shall comply with the SCA, The Occupational Health, Safety and Welfare Act and Regulations and the relevant council authorities.

Make good of surfaces damaged or disturbed by work.

Confirm boundary location prior to commencement of work by a registered surveyor.

Ensure water is controlled on site during demolition to maintain dust control.

Existing street trees to be retained and protected during demolition.

**Disclaimer**

Although the guarantees contained within have informed our documentation and consultation processes, JPE makes no claim to the accuracy or completeness of this information when viewed in isolation. All models must be viewed in conjunction with the most current survey documentation and dimensional survey must be read directly from the Construction Drawings.

All geometry and associated data within any model which has been issued under these provisions are intended to be supporting information only. The model data is in no way intended to supplement for any drawing, schedule, specification or survey which JPE Design Studio might otherwise be responsible for.

JPE Design Studio makes no claims or guarantees with regard to the dimensional information or level of accuracy of the model. This disclaimer prevails over any other contracts, sub-contracts, sub-consultant or any other information from using the model as anything other than a visual reference.

**MEALS ON WHEELS**

**JPE**  
JPE Design Studio

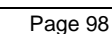
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Street SA 5042  
400 127 007 129 243

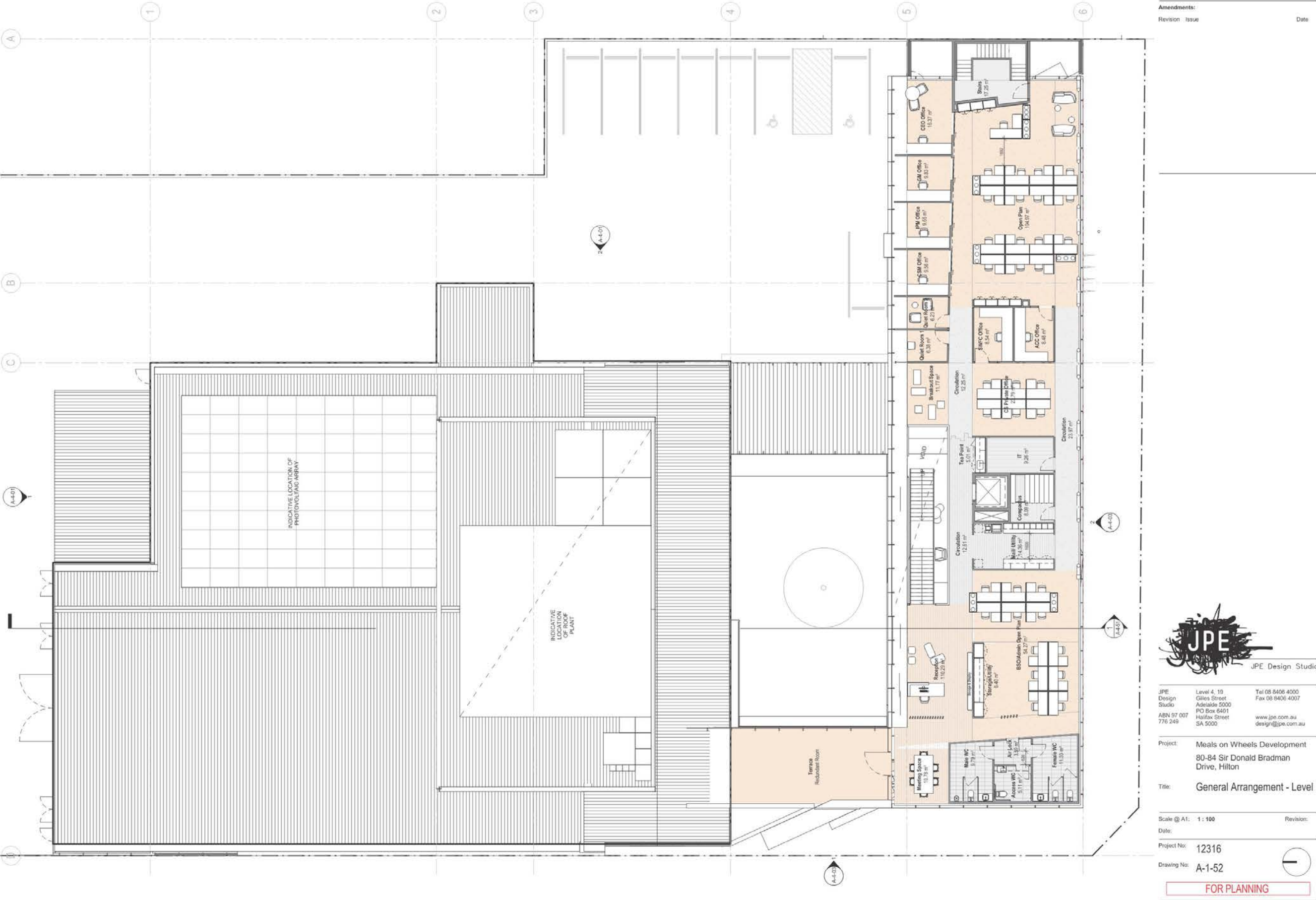
Project: Meals on Wheels Development  
80-84 Sir Donald Bradman Drive, Hilton  
Title: Site Plan

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Date: 20/03/2019 10:24:47 AM  
Project No: 12316  
Drawing No: A-1-01

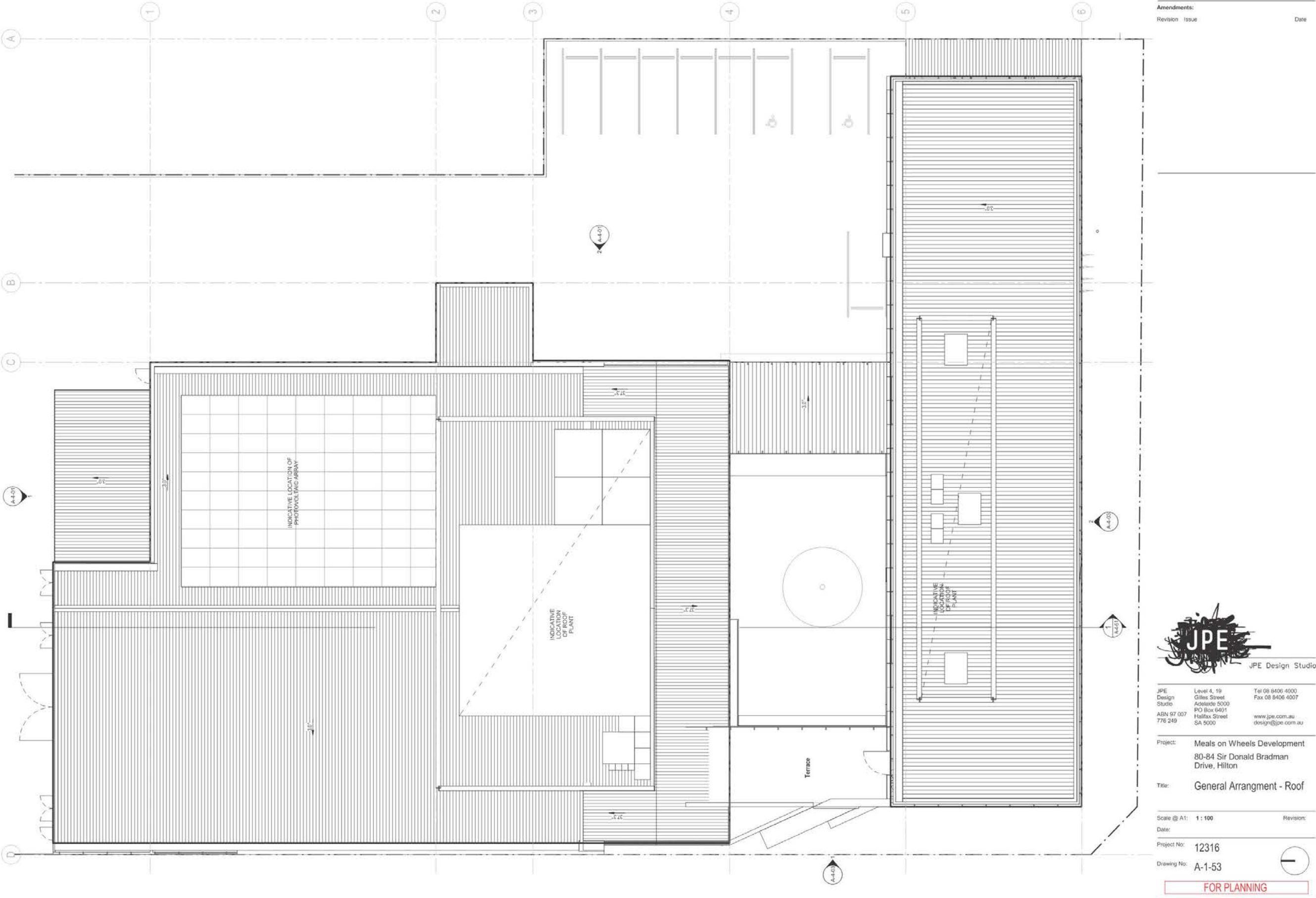
PRELIMINARY ISSUE

















4.0 Planning Drawings

Planting Palette



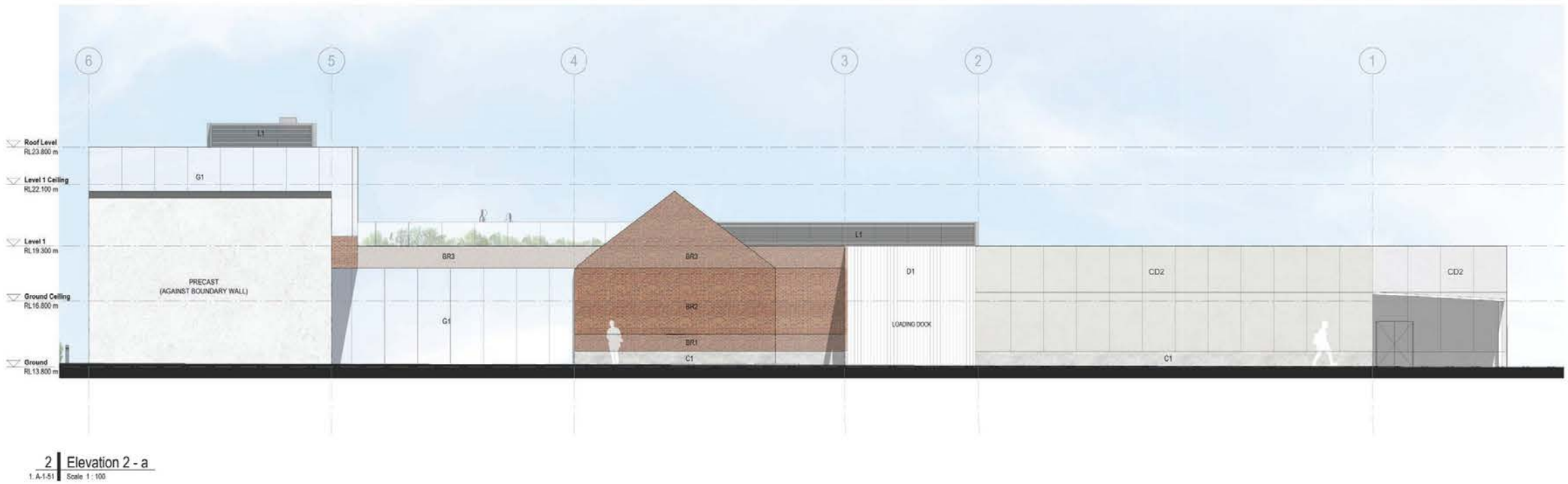
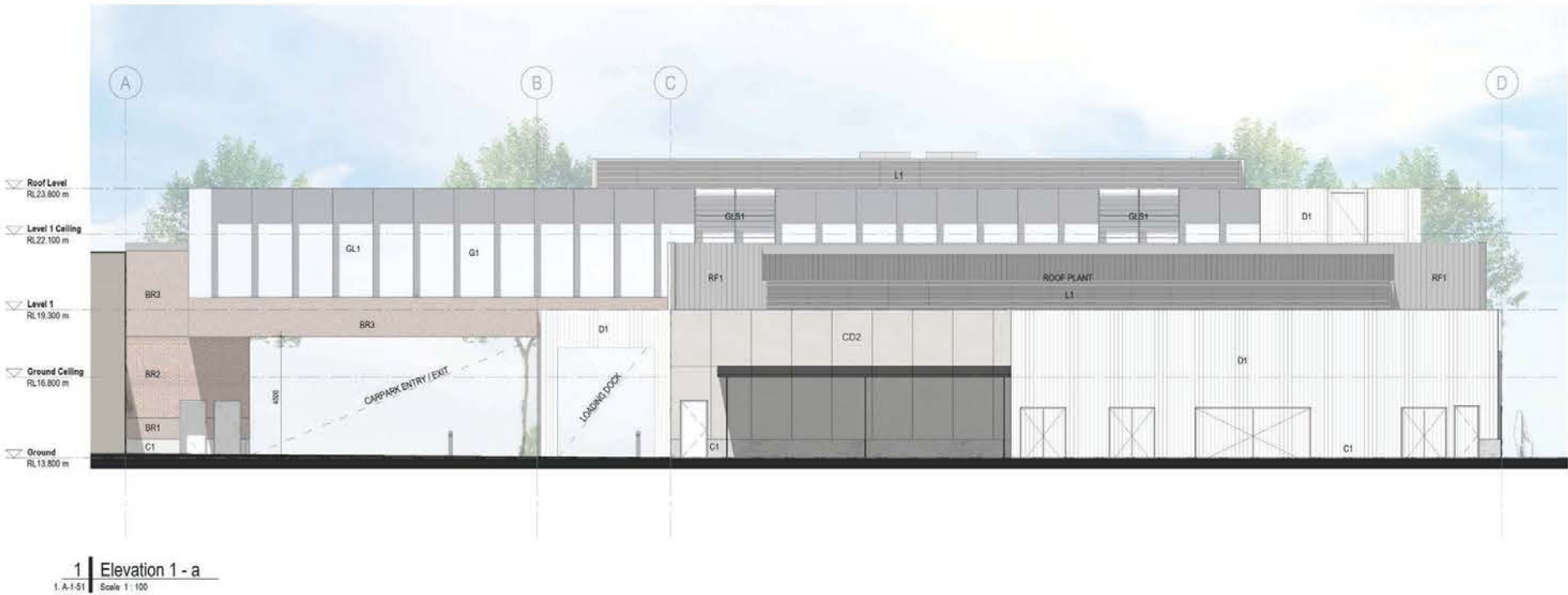


Amendments:

Revision	Issue	Date
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**LEGEND**

G1 - GLAZING  
BR1 - BRICK TYPE 1  
BR2 - BRICK TYPE 2  
BR3 - BRICK TYPE 3  
C1 - CONCRETE  
D1 - OPAQUE POLYCARBONATE CLADDING  
RF1 - ROOFING



JPE Design Studio  
Level 4, 19  
Giles Street  
Adelaide 5000  
PO Box 6401  
Halifax Street  
SA 5000  
Tel 08 8406 4000  
Fax 08 8406 4007  
www.jpe.com.au  
design@jpe.com.au

Project: Meals on Wheels Development  
80-84 Sir Donald Bradman  
Drive, Hilton

Title: Elevations Overall - North & East

Scale @ A1: 1: 100 Revision:

Date:

Project No: 12316

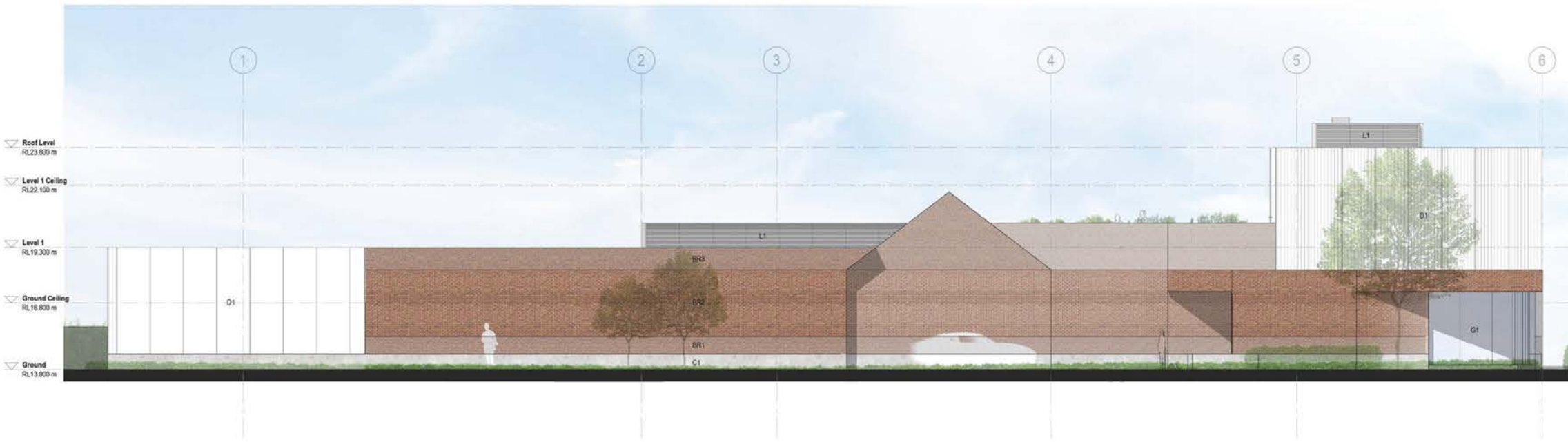
Drawing No: A-4-01

FOR PLANNING





1 | Elevation 3 - a  
1. A-1-51 Scale 1:100



2 | Elevation 4 - a  
1. A-1-51 Scale 1:100

Amendments:

Revision	Issue	Date
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LEGEND

- G1 - GLAZING
- BR1 - BRICK TYPE 1
- BR2 - BRICK TYPE 2
- BR3 - BRICK TYPE 3
- C1 - CONCRETE
- D1 - OPAQUE POLYCARBONATE CLADDING
- RF1 - ROOFING
- S1 - SIGNAGE



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design@jpe.com.au

Project: Meals on Wheels Development  
80-84 Sir Donald Bradman  
Drive, Hilton

Title: Elevations Overall - South & West

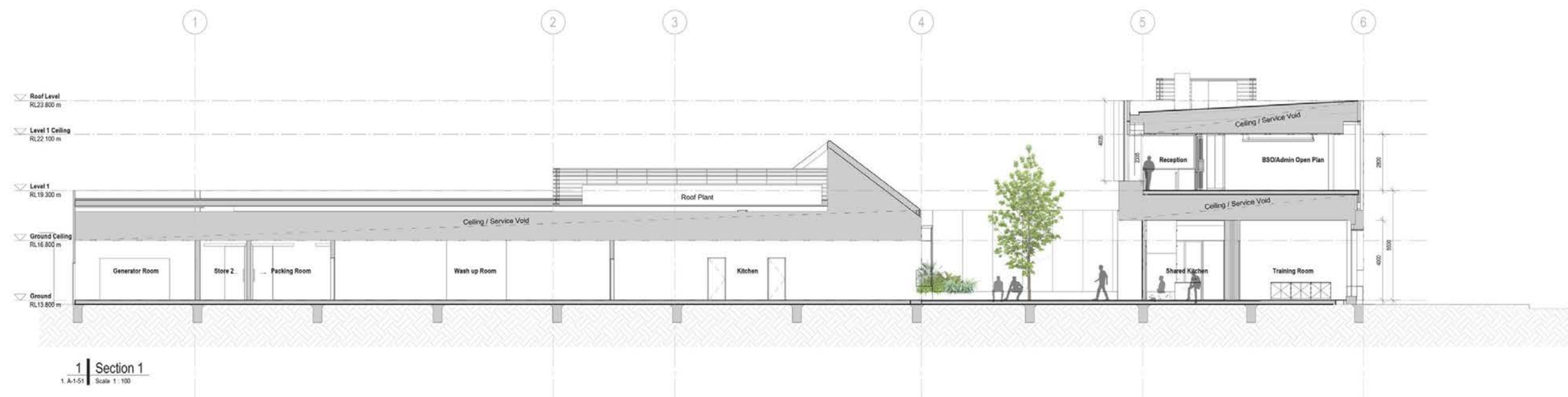
Scale @ A1: 1:100 Revision:

Date:

Project No: 12316

Drawing No: A-4-02

FOR PLANNING



Amendments:		
Revision	Issue	Date



JPE Design Studio

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Level 4, 19 Gtton Street Adelaide 5000

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Project:

Meals on Wheels Development  
80-84 Sir Donald Bradman Drive, Hilton

Title:

Building Section A

Scale @ A1:

1 : 100

Date:

Revision:

Project No:

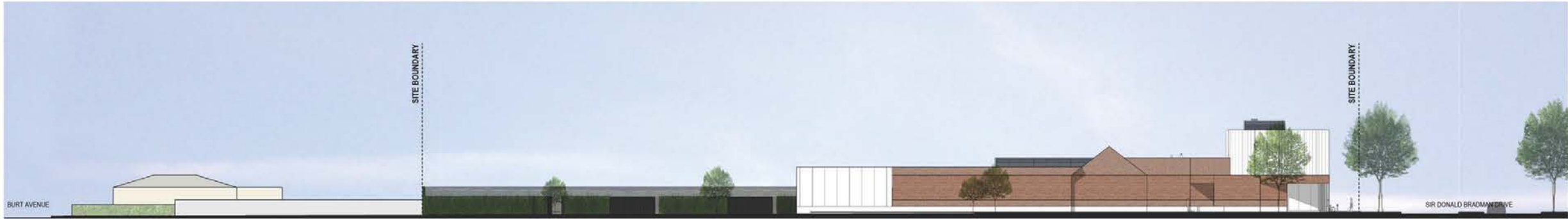
12316

Drawing No:

A-4-51

FOR PLANNING

12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



Pearson Street Elevation



Sir Donald Bradman Drive Elevation




















12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



5.0 External Materials

Proposed External Materials

Proposed External Materials

G1	Glazing						
D1	Danpalon	G 1	D 1	BR 1	BR 2	BR 3	C 1
BR1	Brick						
BR2	Feature Brick						
BR3	Brick						
C1	Concrete						
RF1	Roof Sheetting						
GL1	Glazed Louvres (manually operated)	RF 1	GL 1	GLS 1	CD 2	F 1	F 2
GLS1	White Metal Shroud						
CD2	Fibre Cement Cladding - Expressed Joints						
F1	Black Steel Fence - 2.4m						
F2	Acoustic Fence - 3.1m						
PV1	Herringbone Paving						
BT	Bitumen	PV 1	BT	GR 1	L 1	S 1 - Signage Precedent Options	
GR1	Compacted Gravel						
L1	Roof Plant Room Screening						
S1	Signage						

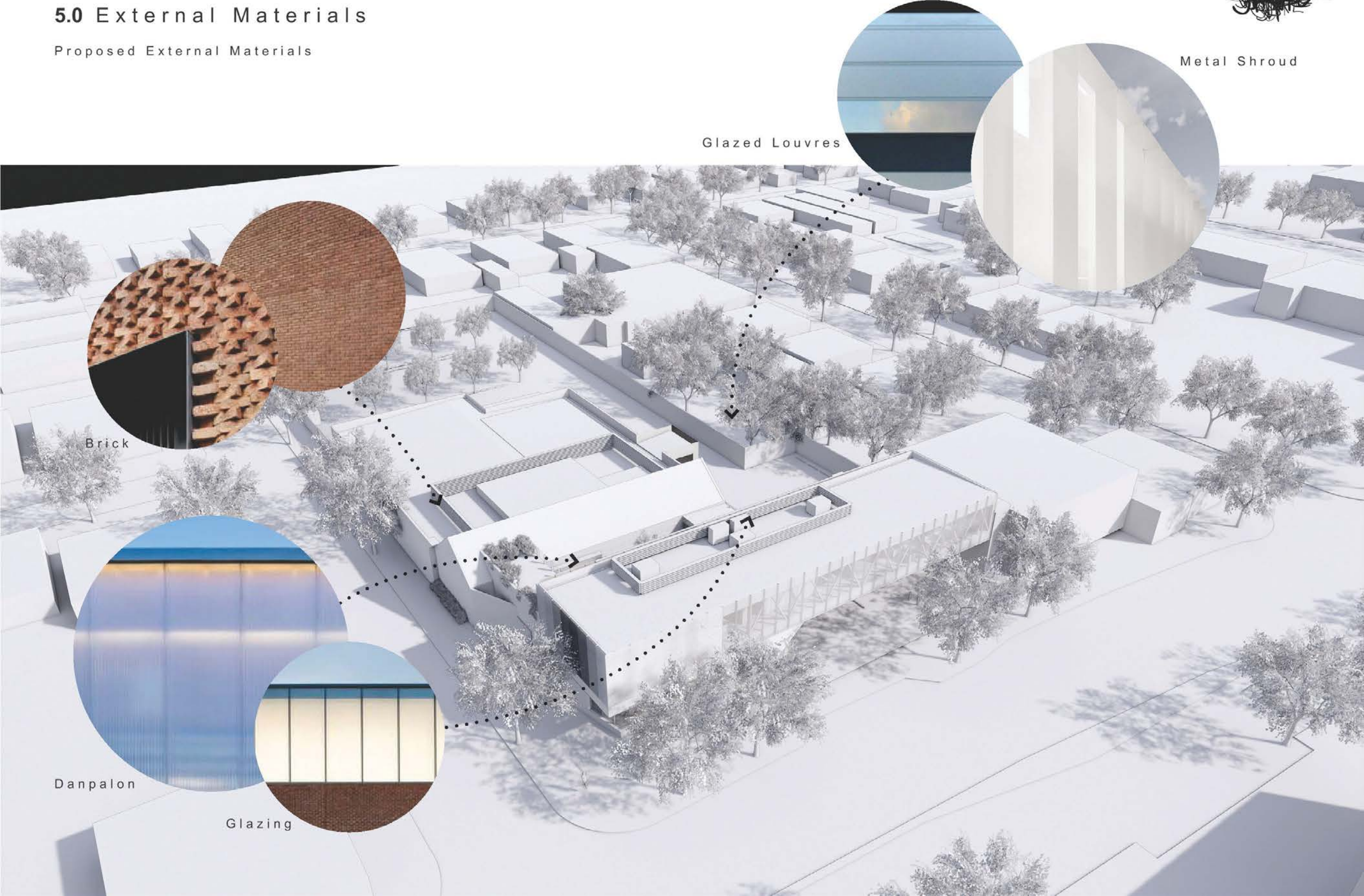


12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



5.0 External Materials

Proposed External Materials





12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



## 6.0 Model Views & Perspectives

Perspective 1





12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



## 6.0 Model Views & Perspectives

Perspective 2





12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



6.0 Model Views & Perspectives

Perspective 2



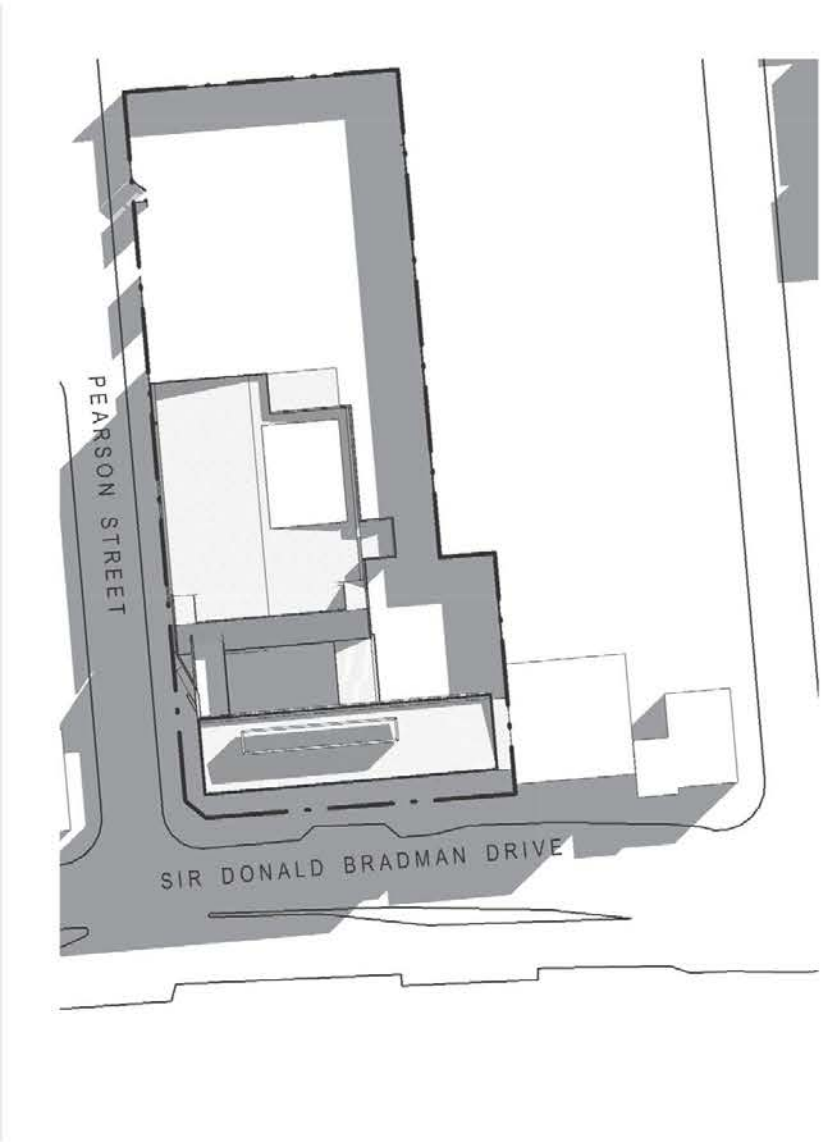


12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION

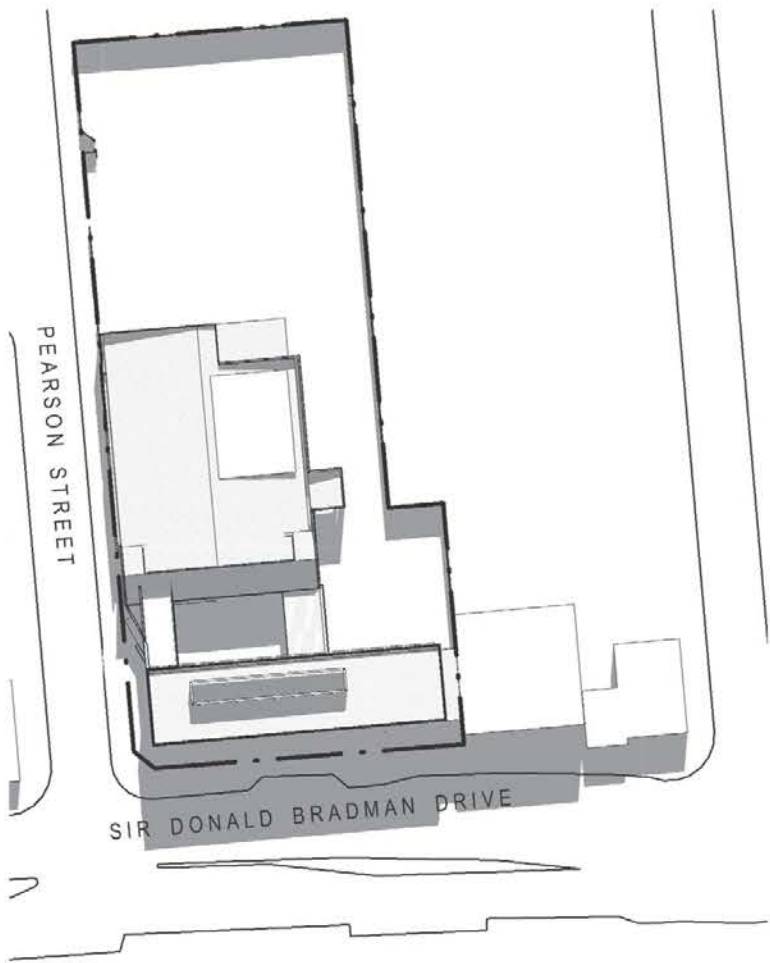


7.0 Shadow Diagrams

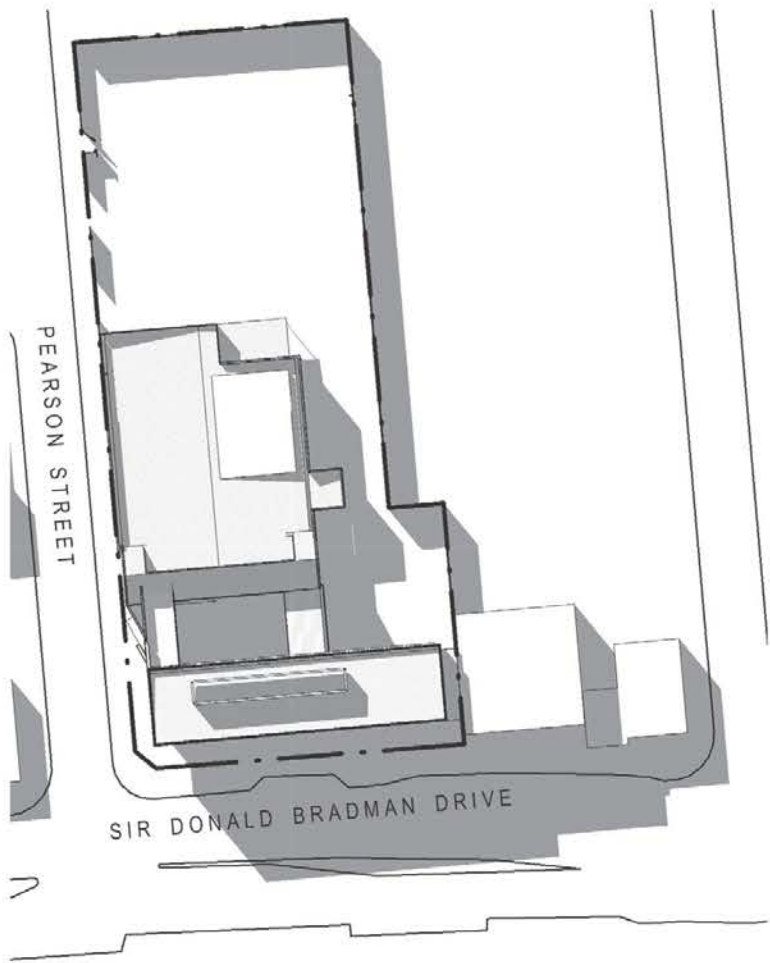
Proposed Shadows



Proposed Shadow 21 June 9am



Proposed Shadow 21 June 12pm



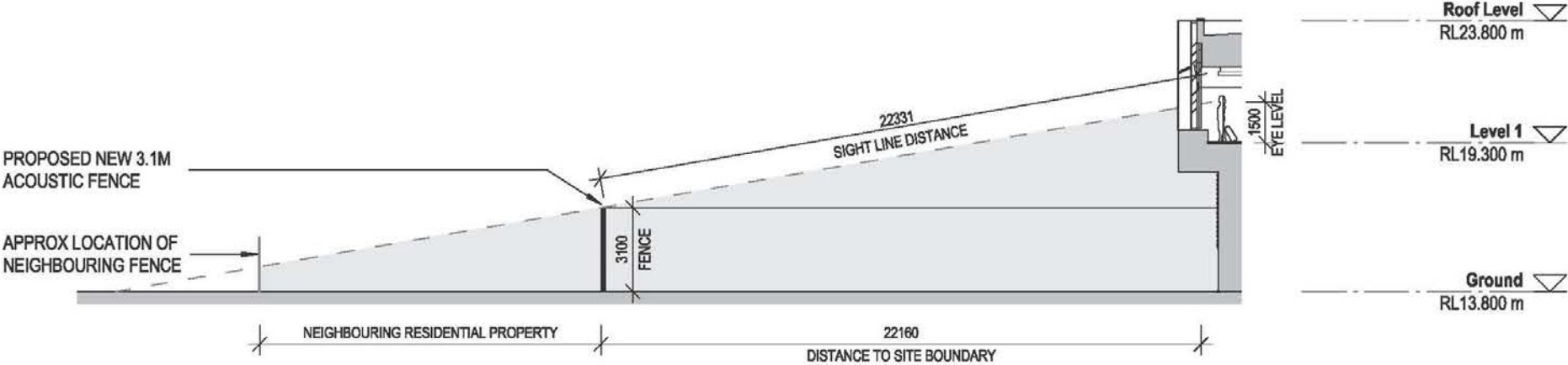
Proposed Shadow 21 June 3pm



12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION

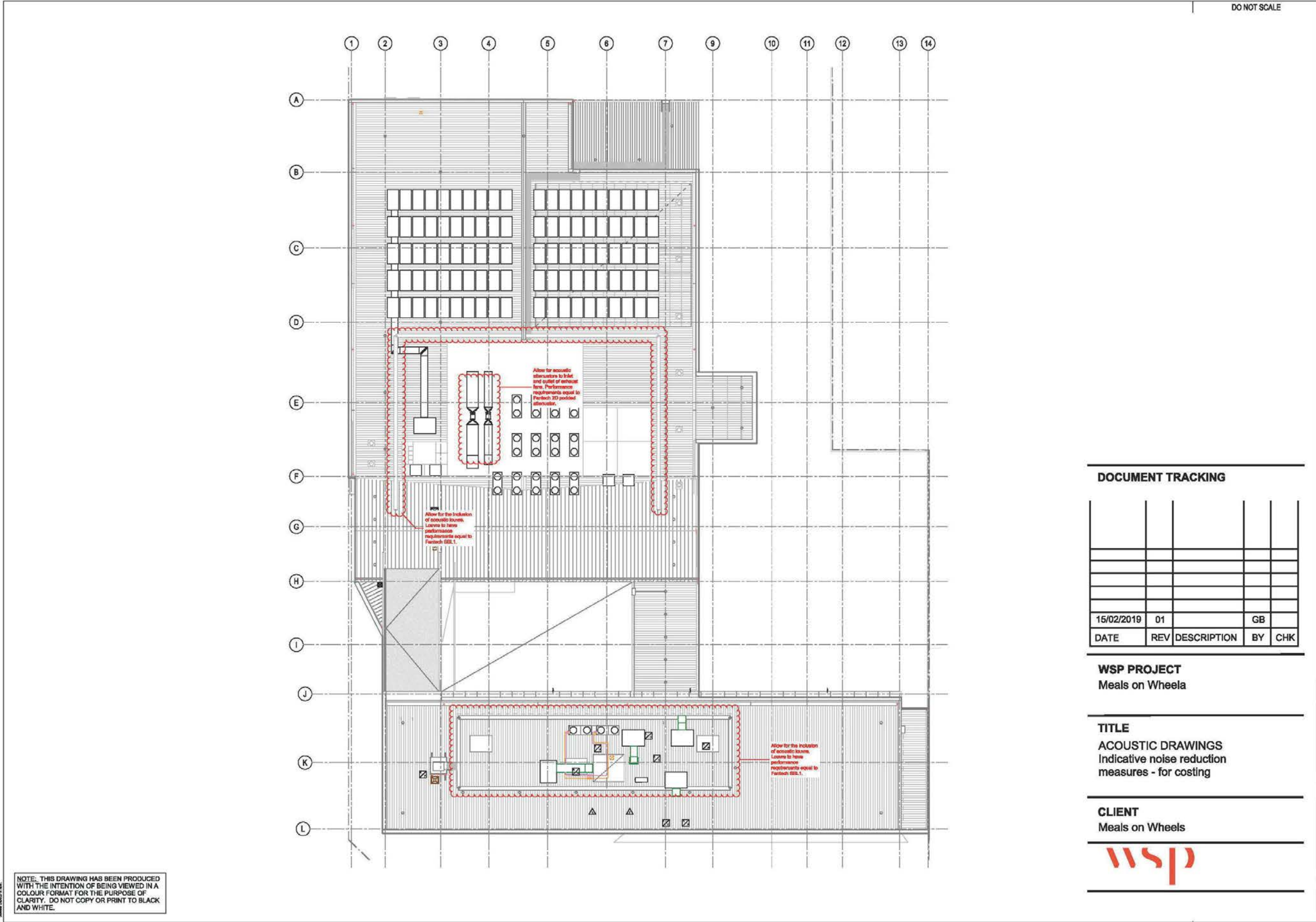


Appendix

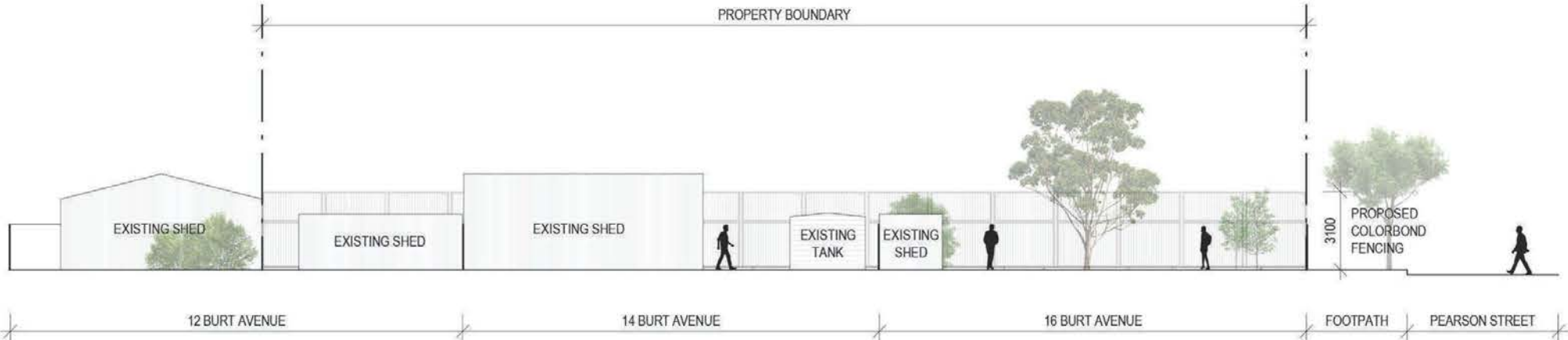


SK-001  
Scale: 1 : 200 @A3  
Sight Line Diagram  
Meals on Wheels Development









SK-003

Scale: 1 : 200 @A3

North Fencing Diagram  
Meals on Wheels Development





Our ref: PS106499-ELE-LTR-001a

18 March 2019

Milly Nott  
Future Urban Group  
89 King William Street  
ADELAIDE SA 5001

Dear Madam

As requested we provide the following summary of the proposed external carpark lighting within the Meals on Wheels Development, with particular reference to the light spill assessment, minimisation and control.

**Project Description**

The site is located at 80 – 84 Sir Donald Bradman Dr, Hilton, SA 5033 and covers: -

- Allotments 36 – 39, and 53 – 55, located within Arterial Roads Policy Area 1 of the Commercial Zone, with Allotments 53, 54 and 55 also being located within Precinct 3 Sir Donald Bradman Drive (Mile End) and
- The northern-most allotment (being Allotment 35) is located within the Cowandilla/Mile End West Character Policy Area 23 of the Residential Zone .

The site is abutted by adjoining single storey residential developments to the North and the majority of the Eastern boundaries. One two-storey commercial developments abuts the proposed site adjacent the Sir Donald Bradman Road frontage. (refer Appendix 1 for reference)

The proposed architectural works feature a new 3.1m high solid fence to the Northern and Eastern boundary.

The primary operating hours of the site will be from 5am until 11pm.

**Proposed External Lighting Systems**

Artificial lighting is proposed for the site's carpark and internal roads to provide a safe and functional external space for operational activities and also to provide a visual deterrent to vandalism and crime.

The proposed lighting system will incorporate: -

- LED flood lights mounted to (maximum) 6m tall free standing lighting columns.
- LED flood lights and battens mounted to building structure (walls, soffits) etc
- Decorative building façade and landscaping lighting incorporating LED luminaires.

Lighting control will be provided via time clock and photoelectric cell control. This will allow operation of lighting on pre-determined time schedules, whilst also preventing lighting from operating when there is sufficient natural daylight.

Level 1, 1 King William Street  
Adelaide SA 5000  
GPO Box 398  
Adelaide SA 5001

Tel: +61 8 8405 4300  
Fax: +61 8 8405 4301  
www.wsp.com

WSP Australia Pty Limited ABN 80 078 004 798

**Light Spill Control**

The assessment of light spill is governed by Australian Standard 4282 – Control of the Obtrusive Effects of Outdoor Lighting – 2019.

Based on the site arrangements, configuration and proposed works the following parameters are deemed applicable to this site:-

- A3 – Medium District Brightness (suburban areas in towns and cities)
- Curfewed Hours
- Assessment of the spill light will be taken at a 10m set back from the adjoining boundaries as per AS4282 Clause 3.3.1.3 c)

Based on the above parameters the applicable maximum compliance limits are:-

- Vertical illuminance levels ( $E_v$ ): 2 lx
- Threshold increment (TI): 20%
- Maximum luminous intensities per luminaire: 2,500 cd
- Upward Light Ratio: 0.02

These criteria will be utilised during the development and finalisation of the lighting design to confirm compliance with the Standard .

**Summary**

We confirm that the external lighting systems associated with the proposed site will be designed to comply with AS4282:2019, specifically the applicable maximum compliance limits as detailed above.

We trust the above is satisfactory and would be pleased to further advise as required.

Yours faithfully

A handwritten signature in black ink, appearing to read 'G. Hall'.

Gavin Hall  
Associate, Senior Electrical Engineer





## APPENDIX 1. DEVELOPMENT LOCATION

12316 - MEALS ON WHEELS DEVELOPMENT - DA PLANNING APPLICATION



## 2.0 Site Context

### Site Context & Locality Plan



- Residential (All Single Storey)
- Commercial - 1 Storey
- Commercial - 2 Storey

Address: 80 - 84 Sir Donald Bradman Drive, Hilton, Adelaide.

Area: Approx 5167m<sup>2</sup>.

The built form in the vicinity of the site is predominantly single storey residential with retail and commercial buildings along Sir Donald Bradman Drive.

Immediate neighbours include Chandler Macleod to the East and Battery World on the West across Pearson Street. Other notable businesses in the area include the Hilton Hotel, Hilton Plaza and Hamra Centre Library.

## Preliminary Traffic, Flooding & Stormwater Assessment

**Development Application No: 211/1287/2018**

<b>Assessing Officer:</b>	Jordan Leverington
<b>Site Address:</b>	80-84 Sir Donald Bradman Drive, HILTON SA 5033
<b>Certificate of Title:</b>	CT-5773/544, CT-5875/402, CT-5725/70, CT-5725/71, CT-5725/72
<b>Description of Development</b>	Construction of a one and two-storey facility, incorporating office, training room, commercial kitchen store and 3.1m high acoustic barrier, advertising and ancillary carpark

### TO THE TECHNICAL OFFICER - CITY ASSETS

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Jordan Leverington      DATE      24 May, 2019**





## Memo

To Jordan Leverington  
From Andrew King  
Date 24-May-2019  
Subject 211/1287/2018, 80-84 Sir Donald Bradman Drive, HILTON SA 5033

Jordan Leverington,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

### **1.0 Traffic and Parking**

#### *1.1 Parking Provision*

The scale of parking proposed to be provided by this application has been reviewed by Council's Traffic Consultant and considered sufficient for the proposed distribution of activities on this site.

The proposed design nominates separate areas for staff and visitor parking, both being of reasonable scale. Some potential conflict occurs between site servicing and the visitor parking area, however it would be recommended that this be managed through the restriction of operating hours associated with the southern loading dock (as outlined below in recommendations).

It is noted that biking provision are proposed to be provided at multiple locations within the site to service both staff and visitors at appropriate locations.

#### *1.2 Traffic Movement*

The southern staff carpark has its own access driveway connection to Pearson Street, with secure gates over the access. The gates have been recessed into the site to provide appropriate storage of a vehicle on site whilst waiting for the gates to open, without obstruction of the public footpath or road.

Aisles and parking space dimensions within the carparking area have been assessed to satisfy standard requirements.

Commercial vehicles accessing the site for general operations and servicing have been indicated by the applicant to be restricted to a maximum MRV scale vehicle.

All commercial vehicle entry access to the site has been nominated to be via the Pearson Street driveway. No commercial vehicle entry to the site via the Sir



Donald Bradman Drive access. Commercial vehicle will predominately exit via Pearson Street, however the site has been designed with suitable flexibility to permit some commercial vehicle exiting of the site via the Sir Donald Bradman Drive access.

Commercial vehicles utilising the southern loading dock on the eastern side of the building are required to undertake manoeuvring which potentially conflicts with the visitor car parking area. It has been recommended that this can be addressed through the controlling of the hours of utilisation of this loading dock.

Based on the limitation of MRV vehicle access to the site, it is considered that that the full range of turning movements at the intersection of Pearson Street and Sir Donald Bradman Drive can be suitably accommodated. DPTI have similar commented on the satisfaction of this.

Based on the current movements accommodated at the intersection of Pearson Street and Sir Donald Bradman Drive, as well as how the streets to the north of the development are arranged, it is considered that the proposed development will have little to no impact on the areas and streets to the north of the site.

***It is recommended that Conditions similar to the following be included with any approval for this development;***

- ***The maximum scale vehicle to access the site shall be limited to an MRV (Medium Rigid Vehicle) as described within AS 2890.2***
- ***All commercial and service vehicles entering the site must do so via the Pearson Street access driveway.***
- ***The Sir Donald Bradman Drive access driveway should be clearly signed to indicate "Visitor Carpark Access Only - No Commercial Vehicles"***
- ***The hours of use of the southern loading dock on the eastern side of the building shall be limited to outside of the standard operating hours of the office element of the site.***

## **2.0 Verge Interaction**

### **2.1 Sir Donald Bradman Dive Access**

There is an existing access driveway to Sir Donald Bradman Drive which has a memorial Plane Tree on the road verge on either side of the crossing place. To accommodate the simultaneous exiting of a service vehicle and entry of a domestic vehicle, some widening of the crossing place is necessary.

A design for the widening of the crossing place has been developed in consultation with Council (Assets and Works Departments) and DPTI, ultimately providing a clear 2.0m minimum offset from the street trees and sufficient manoeuvrability.



It is noted that DPTI recommended conditions nominate a minimum 1.0m offset from the street trees within their feedback for this development, however it has been directly confirmed that with them that they are also satisfied with the 2.0m minimum offset from the trees as developed for this location.

## 2.2 *Pearson Street Frontage*

The proposed two new driveways to Pearson Street both conflict with existing street trees. The site also has numerous existing crossing places which would standardly be required to be removed and reinstated in association with the development. There are only a few existing street trees long this frontage of the development of varying condition and value, and at inconsistent spacing.

Council has developed and presented to the applicant an alternative to upscale the Pearson Street frontage of the development, as a jointly funded project between the Developer and Council. These works would include replacement of the existing concrete footpath with brick paving, reinstatement of all redundant crossing places, removal of the majority of the existing trees and replacement with approximately 15 new trees and areas of irrigated garden bed, with rubble verges to the balance of space.

The applicant's representatives have indicated interest in taking up this upscaling of Pearson Street, but have yet to formally acknowledge commitment to such.

Council would be satisfied with the removal of the street trees necessary to accommodate the new accesses to the development in good faith that the applicant ultimately enters into agreement for the upscaling of the Pearson Street road frontage.

## 3.0 **Stormwater Management**

### 3.1 *Stormwater Harvest and Re-Use*

City Assets Department strongly encourage the collection and re-use of stormwater within new developments and believes that developments of this nature provide an excellent opportunity for the active re-use of stormwater within.

Information provided by the applicant's consultant has suggested that 'treatment' of collected stormwater is require prior to re-use within toilets and as such the cost of re-use for this purpose is not considered justifiable. Council if of the opinion that this is not correct with many development of similar nature adopting re-use of stormwater for this purpose, most without complicated 'treatment'.

Reference is made to the 'potential' of the provision of a stormwater harvesting storage for use in association with irrigation of the site and/or Pearson Street





road verge landscape. No detail of the scale or effectiveness of this collection and supply has been provided within documentation.

It is noted that harvesting water for irrigation of small areas purposed only, it typically not a highly efficient use of a stormwater storage and typically fail to provide other sustainability benefits that can be achieved through 'active' use of collected stormwater.

### 3.2 Stormwater Quality Improvement

The applicant's consultants have nominated a stormwater quality improvement train within the development which includes Biofiltration trenches within carparking areas and proprietary devices to treat runoff from non-carpark areas. Industry standard MUSIC modelling of the performance of these measures against the desired targets has been provided and are considered to provide a reasonable approach.

Design information in relation to this has been provided in a conceptual manner to date and will require further detailing.

***It is recommended that Conditions similar to the following be included with any approval for this development;***

- ***Detail design of Stormwater Quality Improvement measure are to be consistent with the WSP Stormwater Management Plan dated November 2018.***

### 3.3 Stormwater Detention

The applicant's consultants have initially nominated within their Stormwater Management Plan that no stormwater detention measure were intending or justified to be provided within the development.

Subsequent to this, Council has sort compliance with standardly requested stormwater detention targets for development of this scale.

The applicant's consultants have resultantly provide further information indicating their belief that stormwater detention will be achieved within the site, and have provided industry standard DRAINS modelling summary information in support of this.

The detailing on the location and mechanisms of providing the stormwater detention elements within the site area again very conceptual in nature and will require further assessment of the detailing of these measures to ensure appropriate implementation.

***It is recommended that Conditions similar to the following be included with any approval for this development;***

- ***Detail design of Stormwater Detention measure are to be consistent with the WSP 'DRAINS' model as provided and ensure;***



- **Overall peak stormwater discharge from the site is to be limited to a maximum of 38 litres per second.**
- **Peak stormwater discharge from the Visitor carpark catchment is to be limited to 22 litres per second, with a 12,000 litre stormwater detention storage capacity.**
- **Peak stormwater discharge from the Staff carpark catchment is to be limited to 16 litres per second, with a 47,000 litre stormwater detention storage capacity.**

### 3.4 Stormwater Connection to Public Stormwater Network

The applicant's consultants have nominated a direct stormwater connection from the development site to the public existing underground public stormwater system in Sir Donald Bradman Drive.

This nature of arrangement is supported by Council in circumstances where it facilitates the improve practice of stormwater management (in particular Stormwater Quality Improvement) within the site.

The support of such comes with the necessity for appropriate measures and controls to ensure that stormwater from the public street systems cannot surcharge back into the development site.

***It is recommended that Conditions similar to the following be included with any approval for this development;***

- ***A backflow prevention device must be installed on the private stormwater system, within the private site, to prevent the potential of surcharge from the public stormwater system entering the private site stormwater system.***

## 4.0 FFL (Finished Floor Level) Consideration

### 4.1 Flood Impact

The subject site is subject to low level predicated flood inundation in association with the Keswick and Brown Hill Creek systems.

The Sir Donald Bradman Drive frontage of the main building provides for the critical location for consideration of FFL flood protection, should it be desired for the whole building to be established at one common FFL.

The nominated FFL of 13.80m in relation to the provided Alexander Symonds site survey (Drawing No. A070418 Detail) has been assessed as satisfying the minimum requirement.

It is noted that there is a small portion of separated ground floor building on the eastern boundary of the development site. The FFL of this has not specifically



been nominated and if established at the similar FFL of 13.80m would not receive the typical flood protection desired. In the specific consideration of uses of this ground floor area, the necessity for standard flood protection is not seen as critical and hence a FFL of 13.80m would be accepted.

Should there be desire to step the FFL of the proposed main building, it is noted that the northern end of the building could be reduced in FFL.

Should you require further information, please contact Andrew King on the following direct extension number 8416 6393

Regards

**Andrew King**  
**Coordinator Engineering Services**



## Arboricultural Assessment of Street Trees

Development Application No: 211/1287/2018

**REFERRAL DUE DATE:** 20 December 2018

Assessing Officer: **Jordan Leverington**  
Site Address: **80-84 Sir Donald Bradman Drive, HILTON SA 5033**  
Certificate of Title: **CT-5773/544, CT-5875/402, CT-5725/70, CT-5725/71, CT-5725/72**  
Description of Development **Construction of a one and two-storey integrated community services facility, incorporating office and industry use**

### TO THE TECHNICAL OFFICER – CITY ASSETS

**Please provide your comments in relation to:**

- ☐ **The removal of or impact upon the Street Tree**
- ☐ **Species of Tree:**
- ☐ **Your advice is also sought on other aspects of the proposal as follows:**

PLANNING OFFICER - Jordan Leverington

DATE 6 December 2018

### FROM THE TECHNICAL OFFICER

**I have examined the plans as requested and provide comments as follow.**

As with all development applications it must be proven beyond reasonable doubt that all alternatives have been explored so not to hinder the progress of any street tree(s).

Any proposed development that does not consider "AS4970 Protection of Trees on Development Sites", is likely to require revision until all plans accurately correspond with the specific tree information detailed in this standard.

Verge interaction must consider all services that cross council land including stormwater outlets (and other) which will need to be maintained a minimum of 2.0m from any existing street tree (unless existing or otherwise negotiated). All services must be indicated /documented on appropriate plans for Council assessment and approval.

**A site investigation together with the information provided has revealed that there is only a minor conflict with two large existing Platanus x acerifolia (London Plane) street trees with the location of the proposed crossover on Sir Donald Bradman Drive.**

**Currently there is a 2.0m offset from the existing street tree on the eastern side of the existing crossover and 3.0m offset from the existing street tree on the western side of the existing crossover.**

---

City Operations will support maintaining the existing 6.0m crossover location on Sir Donald Bradman Drive.

With regards to the existing street trees in Pearson Street, there is no conflict with the existing Callistemon Harkness (bottlebrush) street tree located 13.2m from the northern property boundary.

The Callistemon Harkness (bottlebrush) street tree that was located on the northern side of the proposed rear carpark entry/exit crossover was removed several months ago due to a failed codominant stem requiring full removal.

There is a large existing Platanus x acerifolia (London Plane) street tree near the corner of Sir Donald Bradman Drive on Pearson Street, this street tree may require some minor pruning to allow for construction, only councils Arboricultural team can undertake any pruning to council owned assets.

As a result of the proposed development on Sir Donald Bradman Drive, City Operations will support the proposal in its current form.

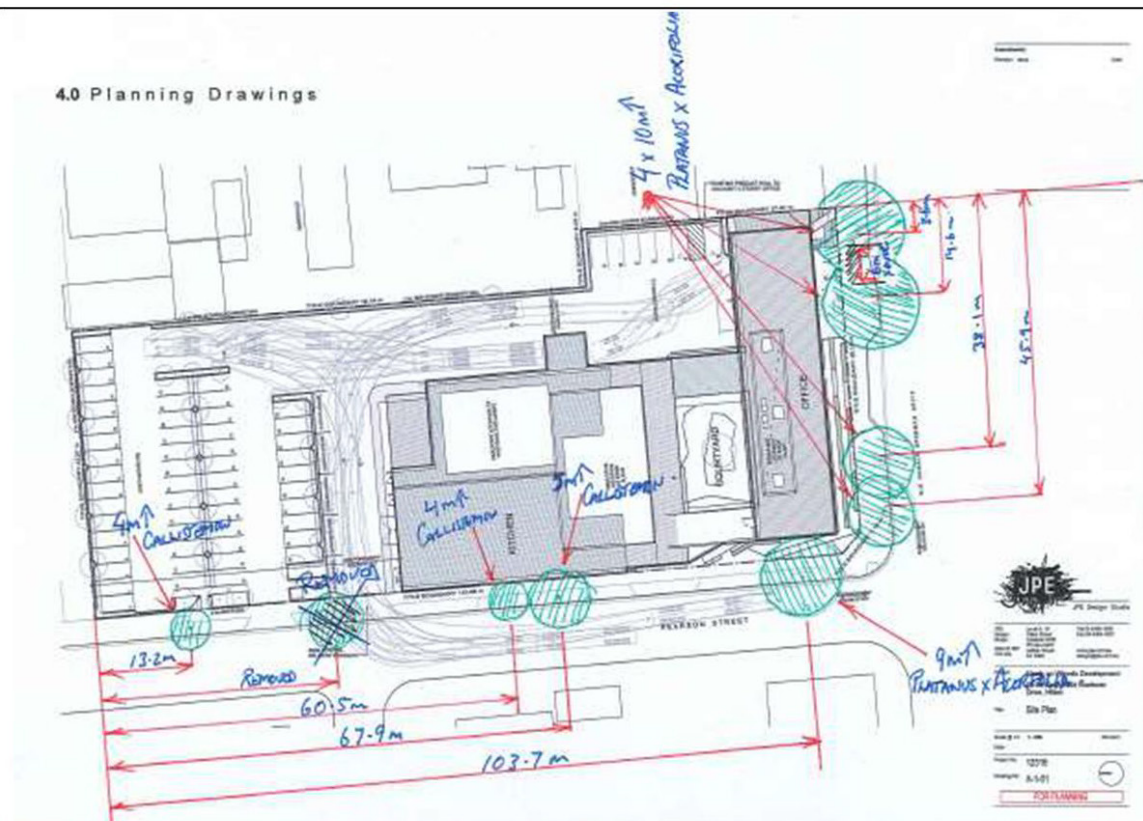
Please note, under no circumstances is any individuals other than council staff permitted to interfere with a street tree. If pruning etc. is required, council must be notified via the appropriate customer request, and council staff will perform all works associated with the community asset.

Final crossover locations will be confirmed once appropriate written correspondence has been received from the applicant and they have submitted "an application to construct a vehicle crossing place(s) across council land".

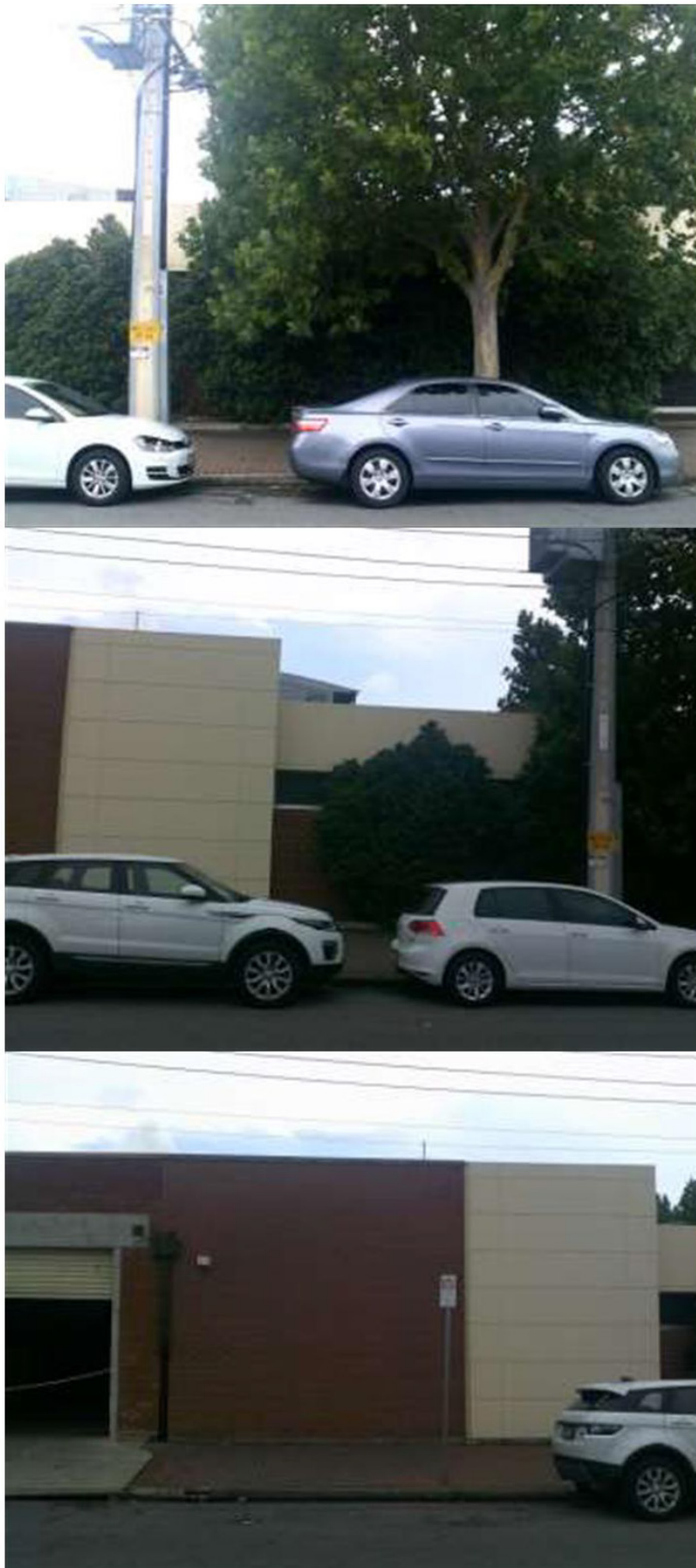
**Rick Holmes**  
Technical Support Officer Arboriculture  
165 Sir Donald Bradman Drive  
Hilton SA 5033

Telephone: 8416 6333  
Fax: 8443 5709

**DATE: 19/12/2018**



























## Environmental Health Assessment

**Development Application No: 211/1287/2018**

**Assessing Officer:** Jordan Leverington

**Site Address:** 80-84 Sir Donald Bradman Drive, HILTON SA 5033

**Certificate of Title:** CT-5773/544

**Description of Development** Construction of a one and two-storey integrated community services facility, incorporating office and industry use

### TO THE ENV HEALTH OFFICER - REGULATORY SERVICES

Please provide your comments in relation to:

- ☐ Any aspect that you feel needs further attention or detail

Reference is made to DA 211/1287/2018 referred to the Environmental Health Section for comment regarding construction of a one and two-storey integrated community services facility, incorporating office and industry use. Review of the plans submitted revealed insufficient information to provide comments.

However, the following needs to be taken into consideration during fit out and set-up of the premises to ensure compliance with the *Food Safety Standards* 3.2.2 & 3.2.3 is achieved:

#### **1. Design & construction of food businesses**

The design and construction of a food business must be appropriate and provide adequate space for the activities for which the premises is used. The design and construction of a food premises must allow for effective cleaning. A food premises must be designed and constructed to exclude dirt, dust, fumes, smoke and other contaminants, not allow pests to enter or provide pest harbourage.

*Food Safety Standard 3.2.3 3 a, b, c & d (i), (ii), (iii)*

#### **2. Floors, Walls & Ceilings.**

Floors, walls and ceilings must be constructed and designed in a way that is appropriate for the activities to be conducted. These surfaces must be able to be effectively cleansed, impervious, sealed to prevent the entry of dirt, dust and contaminants, and unable to harbour vermin. The construction materials used for the floors, walls and ceilings have not been identified on the floor plan provided.

*Food Safety Standard 3.2.3 11(1) (2) (3) a, b & c (4) a & b*

*Food Safety Standard 3.2.3 10(1) (2) a, b, c & d*



### **3. Fixtures, fittings and equipment**

Food premises must have adequate fixtures, fittings and equipment for the production of safe and suitable food. The materials to be used for work bench tops, shelving, appliances etc. have not been specified.

*Food Safety Standard 3.2.3 12(1) a & b (2) a, b, c & d (3) a, b & c*

### **4. Hand washing facilities**

Food premises must provide hand washing facilities that are located where they can be easily accessed by food handlers both within areas where food is handled or prepared and adjacent to toilets located on the premises. Hand washing facilities must be permanent fixtures, supplied with warm running potable water, suitably sized for effective hand washing and clearly designated for the sole purpose of hand washing.

Specific details have not been provided on the plan indicated hand was facilities in the food preparation area

*Food Safety Standard 3.2.3 14(1) a & b, 14(2) a, b, c & d*

### **5. Ventilation**

Food premises must have sufficient natural or mechanical ventilation to effectively remove fumes, smoke, steam and vapours from the food premises.

*Food Safety Standard 3.2.3 7*

### **6. Equipment Washing Facilities (Sinks)**

In addition to hand washing requirements a food premises must have sufficient sink facilities for food preparation, washing and sanitising. The number required will depend on the type of activities being carried out by the business. Most food businesses will require a food preparation sink for the washing of fruit and vegetables and a double bowl sink for the proper cleaning and sanitising of equipment.

*General Requirements Food Safety Standards Chapter 3*

### **8. Chemical Storage / Storage facilities**

Food Premises must have adequate storage facilities for the storage of items that are likely to be the source of contamination of food, including chemicals, clothing and personal belongings. Storage facilities must be located where there is no likelihood of stored items contaminating food or food contact surfaces.

*Food Safety Standard 3.2.3 15*

### **9. Sewage and waste water disposal**

Food Premises must have a sewage and waste water disposal system that will effectively dispose of all sewage and waste water and is constructed and located so that there is no likelihood of the waste water polluting the water supply or contaminating food.

*Food Safety Standard 3.2.3 5*

### **10. Storage of Garbage and Recyclable Matter**

Food premises must have facilities for the storage of garbage and recyclable matter that adequately contains the volume and type of waste material on the premises and is enclosed as necessary to keep pests and animals away from it. Waste storage facilities must also be designed and constructed in such a way that they be easily and effectively cleaned.

*Food Safety Standard 3.2.3 6*

### **11. Food Business Notification**

Before commencing food handling operations a food business must notify the Council of its contact details, location and the nature of business.

*Food Safety Standard 3.2.2 4(1) a, b & c*

A food business must notify Council of any proposed change to its activity or information previously supplied before the commencement of such changes. Examples include change of business name, address and introduction high risk foods for sale.

*Food Safety Standard 3.2.2 4(3)*

The applicant is strongly encouraged to contact the City of West Torrens Environmental Health Department to arrange a pre-opening / fit-out advice inspection.

Should you require further information, please contact me on 8416 6333 or email [dsethi@wtcc.sa.gov.au](mailto:dsethi@wtcc.sa.gov.au)

Deepti Sethi  
Environmental Health Officer

In reply please quote 2019/00367, Process ID: 566862  
 Enquiries to Annie Liu  
 Telephone (08) 8343 2586  
 E-mail [dpti.luc@sa.gov.au](mailto:dpti.luc@sa.gov.au)



## Government of South Australia

Department of Planning,  
 Transport and Infrastructure

POLICY, STRATEGY AND  
 PROGRAM DEVELOPMENT

Transport Assessment and  
 Policy Reform

GPO Box 1533  
 Adelaide SA 5001

ABN 92 366 288 135

9 April 2019

Mr Jordan Leverington  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON SA 5033

Dear Mr Leverington

### SCHEDULE 8 - REFERRAL RESPONSE

<b>Development No.</b>	211/1287/18
<b>Applicant</b>	MEALS ON WHEELS SA INC
<b>Location</b>	80-84 Sir Donald Bradman Drive, Hilton (Corner of Pearson Street)
<b>Proposal</b>	Construction of an integrated community services facility

I refer to the above development application forwarded to the Commissioner of Highways (CoH) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

### CONSIDERATION

The application proposes to construct a one and two-storey facility, incorporating office, training room, commercial kitchen store and 3.1-metre high acoustic barrier, advertising and ancillary carpark. The subject site abuts Sir Donald Bradman Drive, an arterial road under the care, control and management of the CoH, as well as Pearson Street, a local road under the care, control and management of the City of West Torrens. Sir Donald Bradman Drive is designated as a Major Traffic Route, Primary Freight Route, Major Cycling Route, High Activity Pedestrian Area, Standard Frequency Public Transport Corridor under the Department of Planning, Transport and Infrastructure's (DPTI) 'A Functional Hierarchy for South Australia's Land Transport Network'. At this location Sir Donald Bradman Drive carries approximately 29,800 vehicles per day (3.5% commercial vehicles) and has a posted speed limit of 60 km/h.

### Access and Road Safety

The subject site currently has an access point along the Sir Donald Bradman Drive frontage and the application seeks to retain the existing access point. It is understood that the width of the Sir Donald Bradman Drive access is restricted by a street tree located at each side of the crossover. The applicant further advised in an email, dated 19 March 2019, that the largest vehicle proposed to access the site is a Medium Rigid Vehicle (MRV). DPTI reviewed the turn paths provided and noted that ingress of a B99 vehicle is not possible while an MRV exits the site. Subsequently, DPTI recommends that appropriate flaring on the approach side of the access will allow simultaneous movements of a B99 vehicle and an MRV. The turn path from Future Urban/WSP identifies the flaring can be achieved while maintaining the clear zone from the street tree.

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It is further noted from the turn paths that a MRV straddles the dividing line on of Sir Donald Bradman Drive while entering the road. DPTI does not object to this as the proposed access arrangement is in general accordance with *Section 3.4 Access driveways, AS 2890.2:2018* for a major public road.

### **Car Parking**

Council should ensure that the proposed development provides sufficient on-site car parking designed in accordance with *AS/NZS 2890.1:2004* and *AS/NZS 2890.6:2009* and that parking facilities for commercial vehicles conform with *AS/NZS 2890.2:2018*. Furthermore, clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, should be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.

### **Metropolitan Adelaide Road Widening Plan**

The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Sir Donald Bradman Drive frontage of this site, for future upgrading of the Sir Donald Bradman Drive/South Road intersection. The consent of the CoH under the *Metropolitan Adelaide Road Widening Plan Act 1972* is required to all building works on or within 6 metres of the possible requirement. The attached consent form should be completed by the applicant and forwarded to DPTI via email ([dpti.luc@sa.gov.au](mailto:dpti.luc@sa.gov.au)), together with a copy of the approved plans.

### **North-South Corridor**

Both the South Australian and Australian Governments have indicated clear commitments to progress a non-stop North-South Corridor for Adelaide. Construction is currently underway on the Northern Connector, Torrens Road to River Torrens and Darlington sections of the corridor, with funding committed towards the delivery of the Regency Road to Pym Street section of South Road. Completion of these projects by 2022 will create a 47-kilometre continuous section of the North-South Corridor between Gawler and the River Torrens.

As part of the 2019-20 budget released on 2 April 2019, the Federal Government committed a further \$1.5 billion in addition to the previously announced \$1.211 billion contribution towards the North-South Corridor. The Australian and South Australian Governments are working together to identify the next priority section of the corridor and are also exploring opportunities to accelerate the funding and delivery of the project.

On 15 October 2018, the Minister for Transport, Infrastructure and Local Government, the Hon Stephan Knoll MP, announced that a contract had been awarded for the business case development of the River Torrens to Darlington section of the North-South Corridor and the investigation into the viability of tunnels. The preliminary investigations undertaken to date indicate that tunnelling options are viable for this section of the North-South Corridor.

At this stage, further work is still being undertaken prior to a decision on the nature and timing of the upgrade of the River Torrens to Tonsley Boulevard section of the Corridor. Once the next priority section has been identified and concept plans developed, the department will have greater certainty of impacts and will be able to provide reliable advice regarding the potential impacts, future land requirements and timing.

Further information on the North-South Corridor can be obtained at [www.infrastructure.sa.gov.au/nsc](http://www.infrastructure.sa.gov.au/nsc) or if you would like to speak to a member of the North-South Corridor team, please email [northsouthcorridor@sa.gov.au](mailto:northsouthcorridor@sa.gov.au) or call 1300 951 145.

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**ADVICE**

The Department of Planning, Transport and Infrastructure (DPTI) does not object in-principle to the proposed development. The planning authority is advised to attach the following conditions to any approval:

1. All access shall be located in general accordance with the site plan provided by JPE Design Studio, Drawing Number. A-1-01, Revision D, dated 20 March 2019.
2. The Sir Donald Bradman Drive access shall cater for two-way passenger vehicles and Medium Rigid Vehicle exit movements only.
3. The Sir Donald Bradman Drive crossover shall be suitably flared from the property boundary to the kerb line to facilitate simultaneous two-way vehicular movements while keeping a minimum of 1 metre separation from the street tree on both sides of the crossover.
4. All vehicles shall enter and exit the site in a forward direction.
5. All commercial vehicle parking facilities shall be designed in accordance with *AS/NZS 2890.2:2018*
6. Clear sightlines, as shown in Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' in *AS/NZS 2890.1:2004*, shall be provided at the property line to ensure adequate visibility between vehicles leaving the site and pedestrians on the adjacent footpath.
7. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of Sir Donald Bradman Drive. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

The following note provides important information for the benefit of the applicant and is required to be included in any approval:

- The Metropolitan Adelaide Road Widening Plan shows a possible requirement for a strip of land up to 4.5 metres in width from the Sir Donald Bradman Drive frontage of this site, for future upgrading of the Sir Donald Bradman Drive/South Road intersection. The consent of the Commissioner of Highways under the *Metropolitan Adelaide Road Widening Plan Act 1972* is required to all building works on or within 6 metres of the possible requirement. The attached consent form should be completed by the applicant and forwarded to DPTI via email (dpti.luc@sa.gov.au), together with a copy of the approved plans.

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investigation into the viability of tunnels. The preliminary investigations undertaken to date indicate that tunnelling options are viable for this section of the North-South Corridor.

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Yours sincerely



**MANAGER, TRANSPORT ASSESSMENT AND POLICY REFORM**  
for **COMMISSIONER OF HIGHWAYS**

Encl: Copy of the MARWP Consent form.

A copy of the decision notification form should be forwarded to [dpti.developmentapplications@sa.gov.au](mailto:dpti.developmentapplications@sa.gov.au)

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**6.3 576 Sir Donald Bradman Drive, LOCKLEYS**

Application No 211/251/2019 (SCAP 211/D027/19)

**DEVELOPMENT APPLICATION DETAILS**

DESCRIPTION OF DEVELOPMENT	Combined Application: Land division - Torrens Title; Create one (1) additional allotment; and construction of two (2) two storey detached dwellings and alfresco areas
APPLICANT	McDougall Family Trust
LODGEMENT DATE	1 March 2019
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 21
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> <li>• City Assets</li> <li>• City Operations</li> </ul> External <ul style="list-style-type: none"> <li>• State Commission Assessment Panel (SCAP)</li> <li>• South Australian Water Corporation (SA Water)</li> </ul>
DEVELOPMENT PLAN VERSION	12 July 2018
DELEGATION	<ul style="list-style-type: none"> <li>• The relevant application proposes a merit form of development which does not meet the minimum site area requirements in the relevant Zone or Policy Area by 7.5% or more.</li> </ul>
RECOMMENDATION	Support with conditions
AUTHOR	Sonia Gallarello

**SUBJECT LAND AND LOCALITY**

The subject land is formally described as Allotment 151 in Deposited Plan 7650 in the area named Lockleys, Hundred of Adelaide, Volume 5590 Folio 157, more commonly known as 576 Sir Donald Bradman Drive, Lockleys. The subject site is rectangular in shape with a 21.6 metre (m) wide frontage to the access road of Sir Donald Bradman Drive and a site area of 692 square metres (m<sup>2</sup>).

It is noted that there are no encumbrances or Land Management Agreements on the Certificate of Title and there are no regulated trees on the subject site or on adjoining land that would be affected by the development. The site is relatively flat.

The site currently contains a red brick single storey detached dwelling with an attached carport to the east of the dwelling and shedding to the rear.

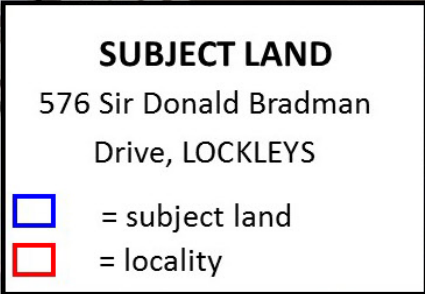
The locality consists of low density residential development to the north, east and west. The size of allotments in the locality are low density and range from 349m<sup>2</sup> (recently subdivided) to 971m<sup>2</sup>, with wide street frontages of around 20m.

The subject site is directly opposite Vimy Avenue that is used to access supermarkets and other commercial businesses on airport land. Areas for recreation near the subject land include Lockleys Oval 100m to the north; the River Torrens 400m to the north and the Kooyonga Golf Course 300m to the east. The 400m Centre Zone buffer area is 200m to the north-west of the subject site. The subject land is in close proximity to public transport along Tapleys Hill Road and Sir Donald Bradman Drive.

The amenity of the locality is moderate to high with well-maintained dwellings and streetscapes and a reasonable degree of landscaping.

The subject land and locality are shown on the aerial imagery and maps below.









-  School  
 Post Office  
 Other Health Services  
 Local Reserves  
 Waterbodies  
 Development Plan Boundary

## Location Map WeTo/6

**WEST TORRENS COUNCIL**  
Consolidated - 12 July 2018

## PROPOSAL

This is a combined application seeking Development Plan Consent and Land Division Consent for a combined development comprising the following:

- Torrens Title land division creating a total of two allotments (one additional);
- Construction of two (2) two storey detached dwellings. Each dwelling has a master bedroom, ensuite, open living, dining and kitchen, laundry and a double garage on the ground floor. The upper level contains two bedrooms, a bathroom, rumpus area and study.

The relevant plans and documents are contained in **Attachment 2**.

## PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Schedule 9, Part 1 (2)(a)(i) and Schedule 9, Part 1 (f) of the *Development Regulations 2008*.

As the proposal is Category 1, public notification was not required to be undertaken.

## INTERNAL REFERRALS

Department	Comments
City Assets	<ul style="list-style-type: none"> <li>• A street tree conflict exists with dwelling 1;</li> <li>• Stormwater connection requirements should be addressed;</li> <li>• A crossover of 3.6m wide without flaring would be acceptable;</li> </ul> <p>Amended plans were provided that addressed these concerns and an updated City Asset's referral has been provided.</p>
City Operations	<ul style="list-style-type: none"> <li>• The existing <i>Geijera parviflora</i> (Wilga) street tree is in conflict with the proposed crossover location for dwelling 1. City Operations support the removal of this street tree.</li> <li>• Another conflict exists with respect to dwelling 2 with another <i>Geijera parviflora</i> (Wilga) street tree located 1.6m outside the proposed western property boundary. City operations support the offset from this existing street tree.</li> <li>• The plans show a street tree located in direct conflict with the proposed crossover location for dwelling 2. There is no street tree at this location.</li> <li>• A fee of \$226.00 will be required for the removal of the street tree.</li> </ul>

## EXTERNAL REFERRALS

Department	Comments
SCAP	No concerns were raised by the SCAP and standard conditions have been recommended should the application be supported.
SA Water	SA Water raised no concerns with the proposal and have recommended standard conditions should the application be supported.

A copy of the relevant referral responses are contained in **Attachment 3**.



## RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the Residential Zone and, more specifically, Low Density Policy Area 21 as described in the West Torrens Council Development Plan.

The relevant Desired Character statements are as follows:

Residential Zone - Desired Character	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objectives	1, 2, 3, 4
Principles of Development Control	1, 5, 6, 7, 8, 11, 12, 13

Low Density Policy Area 21 - Desired Character	
<p><i>This policy area will have a low density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).</i></p> <p><i>There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage. In the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park, where the consistent allotment pattern is a significant positive feature of the locality, subdivision will reinforce the existing allotment pattern.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings. Buildings in the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park will be complementary to existing dwellings through the incorporation of design features such as pitched roofs, eaves and variation in the texture of building materials.</i></p>	

*Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.*

Objectives	1
Principles of Development Control	1, 2, 3

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

### QUANTITATIVE STANDARDS

The proposal is assessed for consistency with the quantitative requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
ALLOTMENT AREA <i>Low Density Policy Area 21</i> <i>PDC 3</i>	420m <sup>2</sup> (min.)	346m <sup>2</sup> (lot 1) 346m <sup>2</sup> (lot 2)  <b>Does Not Satisfy</b>
ALLOTMENT FRONTAGE <i>Low Density Policy Area 21</i> <i>PDC 3</i>	12m (min.)	10.8m (lot 1) 10.8m (lot 2)  <b>Does Not Satisfy</b>
PRIMARY STREET SETBACK <i>Residential Zone</i> <i>PDC 8</i>	8m (same as one of the adjacent buildings)	5.0m (dwelling 1) 5.0m (dwelling 2)  <b>Does Not Satisfy</b>
SIDE SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	0/1m (min. ground floor) 2m (min. upper floor)	<u>Dwelling 1</u> 1m (ground floor) 2m (upper floor)  <u>Dwelling 2</u> 1m (ground floor) 2m (upper floor)  <b>Satisfies</b>

<b>REAR SETBACKS</b> <i>Residential Zone</i> <i>PDC 11</i>	3m (min. ground floor) 8m (min. upper floor)	<u>Dwelling 1</u> 3.1m (ground floor) 12.9m (upper floor)  <u>Dwelling 2</u> 3.1m (ground floor) 12.9m (upper floor)  <b>Satisfies</b>
<b>INTERNAL FLOOR AREA</b> <i>Residential Development</i> <i>PDC 9</i>	100m <sup>2</sup> (min.)	195m <sup>2</sup> (dwellings 1&2)  <b>Satisfies</b>
<b>STORAGE</b> <i>Residential Development</i> <i>PDC 31</i>	8m <sup>3</sup> (min.)	6.3m <sup>3</sup> (dwellings 1&2)  <b>Does not Satisfy</b>
<b>PRIVATE OPEN SPACE</b> <i>Residential Development</i> <i>PDC 19</i>	60m <sup>2</sup> (min. area) 4m (min. dimension) 16m <sup>2</sup> (min. directly accessible from a habitable room).	<u>Dwellings 1 &amp; 2</u>  69m <sup>2</sup> (area) 5.5m (dimension) 69m <sup>2</sup> (accessible)  <b>Satisfies</b>
<b>CAR PARKING SPACES</b> <i>Transportation and Access</i> <i>PDC 34</i>	4 spaces (min.)	6 spaces  <b>Satisfies</b>
<b>LANDSCAPING</b> <i>Landscaping, Fencing and Walls</i> <i>PDC 4</i>	10% (min.)	17.6% (dwellings 1&2)  <b>Satisfies</b>

## ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### Land Use

Residential development, including detached dwellings, is listed as an envisaged form of development within the Policy Area. Therefore the proposed land use is considered to be appropriate and consistent with Principle of Development Control (PDC) 1 of the Low Density Policy Area 21.



### Desired Character

The desired character statement for the Zone and Policy Area seeks low density residential development. A net density of 29 dwellings per hectare is achieved. As such, the proposal achieves the desired dwelling type and density within the Policy Area.

The desired character statement also seeks buildings up to two storeys in height, a pattern of rectangular allotments and landscaping about the built form. The proposed dwellings are two storeys on rectangular shaped allotments that incorporate relatively generous landscaping about the dwellings. Accordingly, the proposed development meets the intent of the desired character, Objective 1 and PDC 2 of the Policy Area.

### Allotment area and frontage

Both the allotment areas and frontage widths fall short of the requirement specified in PDC 3 of Low Density Policy Area 21. However the proposal does meet the intent of the policy area by providing two low density allotments accommodating associated housing. The dwellings meet the majority of the quantitative requirements and therefore do not appear to be compromised despite having smaller allotment sizes and frontage widths.

Although the subject land is not within 400m of a Centre Zone, it is nonetheless within a 1.4km radius of four supermarkets / bulky good stores including Foodland, Woolworths, IKEA and ALDI. ALDI is 100m from the subject land and highly accessible. An element of practicality should be applied in this instance as the subject land has a higher degree of accessibility to facilities usually found in centre zones than a lot of land located within the 400m Centre Zone buffers.



**Figure 1: View south of ALDI in the Airfield Zone, south of Sir Donald Bradman Drive and opposite the subject land.**

In considering the accessibility of the subject land to various shops, public transport and public open space, the development meets the lesser frontage width requirement of proposals within the 400m Centre Zone buffer and is deficient by only by 4m<sup>2</sup> in relation to the size of each proposed allotment. This is considered to be a minor departure with the proposed allotment areas and frontage widths deemed acceptable.

### Built Form

The proposed dwellings are two storey with the upper levels set in from the side boundaries. A colorbond® pitched roof at 25 degrees is proposed for both dwellings with 450mm wide eaves to upper and lower floors.

A variety of materials is proposed with render, stone, facebrick, timber and colorbond® roofing. The colour palette is varied and includes colorbond® monument, colorbond® surfmist with a brick colour seed in a fairly neutral earth brown. The streetscape perspective indicates a light coloured upper level and two contrasting panel door colour selections, with dwelling 2 having timber colour and dwelling 1 having lighter coloured finishes.

Overall the built form of the two dwellings present satisfactorily to the street in terms of being a modern design with a variety of materials and colours and incorporating energy efficiency features in the form of eaves for the upper level in particular. The development accords with most of the design and appearance elements as specified in PDC 4 of the Residential Development module.

### **Garage dominance**

Both dwellings contain two garage panel lift doors. The openings of the garages constitute less than 50% of the allotment frontage therefore meeting the quantitative requirement of PDC 16 of the Residential Development module. While the panels are clearly evident from the street, measures to reduce the visual impact of the proposed garages on the streetscape include:

- columns in-between each single garage effectively 'break up' the visual continuity of the garage panels;
- use of different materials around the garage doors/entrances including render, stone and brickwork also assist in 'breaking up' the visual appearance of the garages;
- use of different coloured panels for each dwelling creating variations in light and shade;
- a slight staggering (350mm) of setback between each garage panel;
- the incorporation of porticos at ground level forward of the garage on each dwelling reduces the visual impact of the garage space;
- an upper level that is sited fairly centrally above the ground level design and provides additional built form about the dwelling to minimise the visual impact of the garages; and
- the inclusion of a landscaped hedge along the edge of each driveway to create visual interest and a softening of the built form.

Overall given the design of each dwelling and the 'breaking up' of the garage space for each dwelling, the extent of garaging is considered acceptable.

### **Setbacks**

The setback of the two dwellings from the street is considered to be acceptable given that the minimum setback is to an open portico and the dwelling façade is staggered. The main part of the ground floor is setback around 6m with the main part of the upper level being setback around 7m (although this is also staggered). It is also considered that the proposed front setbacks compare favourably with the front setbacks of newer dwellings in the locality which are generally less than those of older dwellings (4 Brecon Street, 68 and 70 Sir Donald Bradman Drive).

The side and rear setbacks meet the provisions of the Development Plan.

Overall the setbacks of the development are deemed to be reasonable with the deficiency in front setback not considered to be fatal to the application.

### **Landscaping**

The original landscaping plan provided by the applicant was of poor quality. The applicant provided an amended landscaping plan that demonstrated sufficient overall soft landscaping areas for the two lot development. Lawn is proposed to the rear and front gardens of both dwellings, with perimeter plantings at the front within the lawn and along the edge of the driveway. A manchurian pear tree is proposed for shading and visual appeal within the front yard and lawn area. The size of the area nominated for landscaping meets the requirements of PDC 4 of the Landscaping, Fences and Walls module, will assist in reducing stormwater runoff and will contribute to softening the built form.

## SUMMARY

The development provides low density housing that is compatible with the streetscape and while not within 400m of a Centre Zone has a high degree of accessibility to shops (on airport land), public open space and public transport. The design and siting of the dwellings are considered to be acceptable.

Having considered all the relevant provisions of the Development Plan, the proposal is not considered to be seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and warrants Development Plan Consent and Land Division Consent.

## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent and Land Division Consent for Application No. 211/251/2019 by McDougall Family Trust to undertake the Combined Application: Land division - Torrens Title; SCAP No. DA. 211/D027/19, Create one (1) additional allotment; and construction of two (2) two storey detached dwellings and alfresco at 576 Sir Donald Bradman Drive (CT 5590/157) subject to the following conditions of consent:

### Development Plan Consent Conditions:

1. The development shall be undertaken, completed and maintained in accordance with the following plans and information detailed in this application except where varied by any condition listed below:
  - a) Survey Plan by Pinksterboer Property, Ref 19890 - Div - V1;
  - b) Access Planning report dated 11 February 2019;
  - c) Site plan, floor plan and elevations by Bradford Homes, Sheets 1 to 5 for Dwelling 1;
  - d) Site plan, floor plan and elevations by Bradford Homes, Sheets 1 to 5 for Dwelling 2;
  - e) Site and Drainage Plan by Ginos Engineering, Drawing No. 33285, SR2-1/A (Dwelling 1);
  - f) Site and Drainage Plan by Ginos Engineering, Drawing No. 33287, SR2-1/A (Dwelling 2);
  - g) External colour selections by Bradford Homes for Dwellings 1 & 2;
  - h) Overshadowing diagrams by Bradford Homes;
  - i) Landscaping Plans by Bradford Homes for Dwellings 1 & 2.

*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*

2. Prior to the occupation or use of the development, the following overlooking treatments shall be applied:
  - upper storey windows on the rear, left and right elevations of Dwellings 1 & 2 shall be fitted with raised sill heights to a minimum height of 1.7 metres.

The glazing in all windows shall be maintained in good condition at all times to the satisfaction of Council.

*Reason: To minimise the impact on privacy to residents of adjoining dwellings.*



3. Prior to the occupation or use of the development, all driveways, parking and vehicle manoeuvring areas shall be constructed and surfaced with concrete, bitumen or paving, and shall be drained and maintained in a good condition at all times to the satisfaction of Council.  
*Reason: To provide safe and convenient parking and manoeuvring areas for users of the development.*
4. All landscaping shown on the plans forming part of this application shall be established prior to the occupation or operation of the development and shall be maintained in good health at all times to the satisfaction of Council. Any plants that become diseased or die shall be replaced with a suitable species.  
A watering system shall be installed at the time landscaping is established and operated so that all plants receive sufficient water to ensure their survival and growth.  
*Reason: To enhance the amenity of the site and locality and mitigate against heat loading.*
5. All external materials, surface finishes and colours shall be consistent with the information detailed in this application and shall be maintained in a good condition at all times to the satisfaction of Council.  
*Reason: To ensure a high standard of materials and finishes are used in the final presentation of the building.*
6. Prior to the occupation or use of the development, all stormwater design and construction shall be to the satisfaction of Council to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
  - a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create insanitary or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; or
  - e) Flow across footpaths or public ways.*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*

**Land Division Consent Conditions****Council Requirements**

Nil

**SCAP Requirements**

7. The financial requirements of the SA Water Corporation shall be met for the provision of water supply and sewerage services.  
On receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non-standard fees.  
  
On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers / owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.  
*Reason: To satisfy the requirements of the South Australian Water Corporation.*

8. Payment of \$7253 into the Planning and Development Fund (1 allotment @ \$7253 /allotment). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the State Planning Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, by cheque or credit card, at Level 5, 50 Flinders Street, Adelaide.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

9. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

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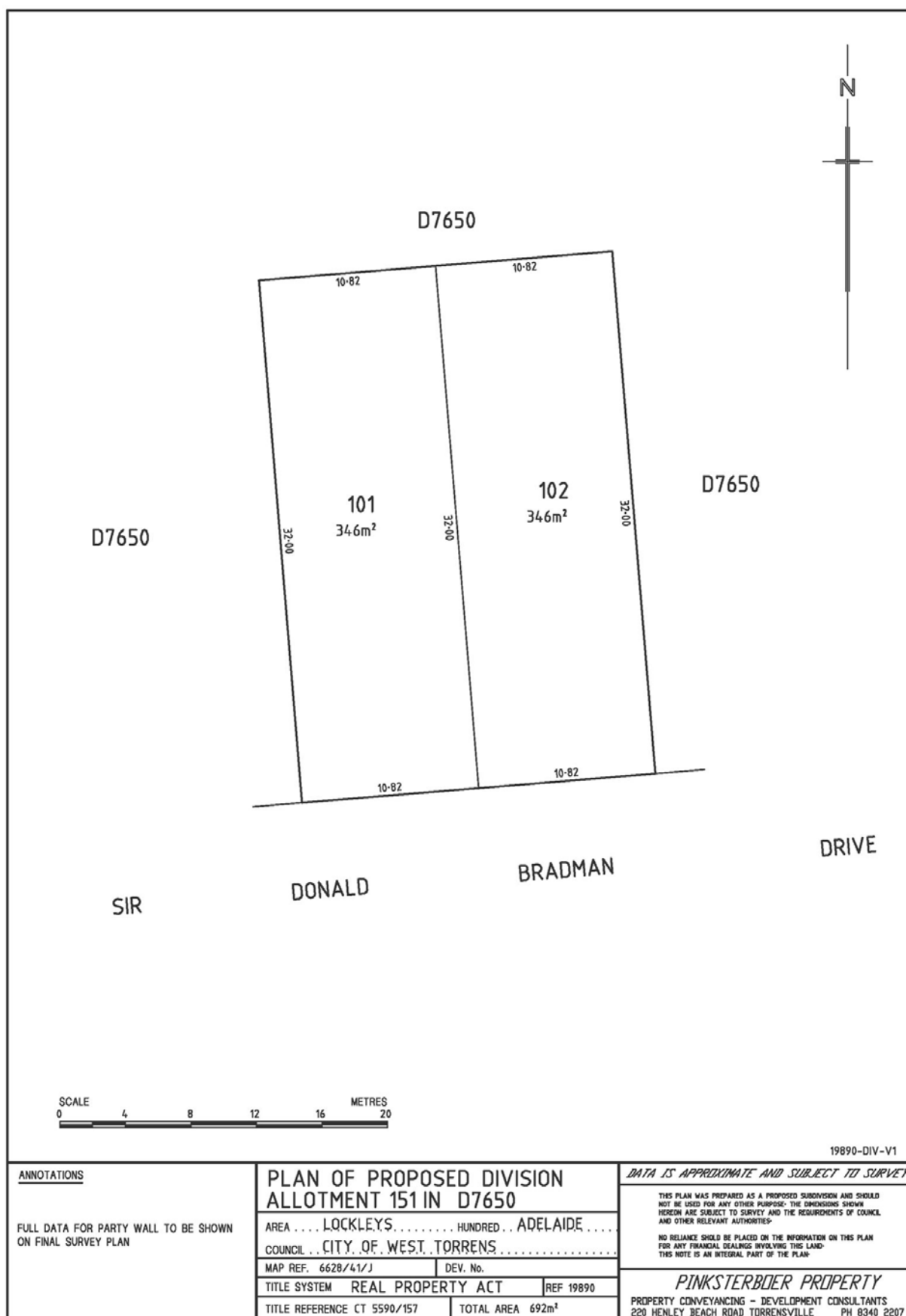
## **Attachments**

1. **Assessment table**
2. **Planning report and proposal plans**
3. **Internal and external referrals**

## Relevant Development Plan Provisions

<b>General Section</b>		
Building near Airfields	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1
Crime Prevention	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 5, 6, 7, 8, 10
Design and Appearance	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 9, 10, 13, 14, 15, 20, 21
Energy Efficiency	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2
Land Division	<i>Objectives</i>	1, 2, 3, 4
	<i>Principles of Development Control</i>	1, 2, 5, 6, 8, 12
Landscaping, Fences and Walls	<i>Objectives</i>	1, 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 6
Orderly and Sustainable Development	<i>Objectives</i>	1, 2, 3, 4, 5
	<i>Principles of Development Control</i>	1
Residential Development	<i>Objectives</i>	1, 2
	<i>Principles of Development Control</i>	1, 3, 4, 5, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 18, 19, 20, 21, 27, 31
Transportation and Access	<i>Objectives</i>	2
	<i>Principles of Development Control</i>	1, 2, 8, 24, 34, 36, 44
Waste	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2







11 February 2019

Ref: 7541planningreport

The Chief Executive Officer  
City of West Torrens  
165 Sir Donald Bradman Drive  
HILTON SA 5033

Attention: Development Assessment Team

Access Planning (SA) Pty Ltd  
ABN 57 089 702 241

235 Henley Beach Road  
Torrensview SA 5031

Telephone 08 8130 7222  
Facsimile 08 8130 7299  
admin@accessplanning.com.au  
www.accessplanning.com.au

**RE: Division of Land (2 Torrens Title Allotments from 1 existing) and Construction of two, two-storey Detached Dwellings at 576 Sir Donald Bradman Drive Road, Lockleys**

## 1.0 INTRODUCTION

I have been engaged by Bradford Homes to prepare a planning report for the above-mentioned development.

The subject land is situated within the Residential Zone as depicted on Zone Map WeTo/6, and Policy Area 21 as depicted on Policy Area Map WeTo/6 of the West Torrens Council Development Plan, as consolidated 12 July 2018

This document provides an outline on the following matters:

- the subject land and locality;
- the proposed development;
- the relevant procedural matters applicable to the application; and
- the merits of the proposed development in respect to the relevant provisions of the Council Development Plan.

## 2. THE SUBJECT LAND AND LOCALITY

The subject land is formally known as Allotment 15 in DP 7650 in the area named Lockleys, Hundred of Adelaide (as contained within Certificate of Title: Volume 5590 Folio 157). The land is more commonly known as 576 Sir Donald Bradman Drive, Lockleys.

The subject land is a rectangle shaped site of some 692 square metres (sqm) and is relatively flat. The land has a depth of 32 metres and a primary street frontage of 21.64 metres to a service road which runs parallel to the Sir Donald Bradman Drive arterial road corridor. The land is on the northern side of Sir Donald Bradman Drive which runs in an east-west direction between the City and Tapleys Hill Road.

The subject land has a colonial style, single storey detached dwelling constructed circa 1960 and is accompanied by ancillary structures including side carport, rear verandahs and outbuilding. Lawn areas are situated both at the front and rear of the land and include small and medium size bushes and trees. There are no regulated trees on the land. The property



is serviced by a single width driveway which runs along the eastern boundary of the subject land.

The locality contains a mix of very low and low-density residential properties containing single storey detached dwellings constructed both recently and approximately 50 to 60 years ago. The locality is part of a larger residential area bordered by Lockleys Oval and the River Torrens (Karra wirra-parri) to the north, Kooyonga Golf Course to the east, Sir Donald Bradman Drive to the south and Tapleys Hill Road to the west. Housing constructed when the locality was first sub-divided for residential purpose remain however, there is a notable change in housing stock as original housing is replaced by new buildings, and in many instances, the original allotment is divided into two allotments. Sir Donald Bradman Drive and Adelaide Airport are also key elements of the locality.

Sir Donald Bradman, excluding the associated service road, has a carriageway of approximately 20 metres and includes two lanes for traffic heading both towards the City (to the East) and the coast (to the West). The carriageways are separated by landscaped traffic islands. A bicycle lane, road verge with bus stops and an open stormwater drain form the southern side of the road.

Adelaide Airport is on the southern side of Sir Donald Bradman Drive and provides landscaped open space of approximately 40 metres along its northern perimeter. An industrial park on airport land is further south and includes an internal road network (which connects to Sir Donald Bradman Drive). The industrial park contains an Aldi supermarket, offices and warehouses used by transport logistic businesses and multi-national business. The industrial park development is not yet completed and additional land uses and buildings within proximity to the subject land are continuing to be constructed.

Other notable features in the broader locality include the Adelaide Airport terminal, hotel and associated building and structures, Lockley's Oval, Kooyonga Golf Course, IKEA (including free-standing sign) Harbor town shopping centre, open space areas (the River Torrens, Apex Park and cricket ovals), Tapleys Hill Road and the suburbs of Fulham and West Beach. The subject land and locality are depicted in Figure 1 as follows.





**Figure 1: Subject land and locality**

Source:([www.maps.sa.gov.au](http://www.maps.sa.gov.au))

## 2. The Proposed Development

The development application seeks consent for the division of land to create two (2) Torrens Title allotments from one (1) existing Torrens Title allotment and two (2), two-storey detached dwellings.

The proposed allotments have identical dimensions with the front and rear boundaries being 10.82 long and side boundaries being 32.00 metres to create two allotments of 346.24 sqm. The allotments will take access from the service road which runs along the northern edge of Sir Donald Bradman Drive.

The proposed dwellings have compatible floor plans, elevations, wall heights and materials and will include:

### Ground Floor:

- Front porch;
- Entry;
- Master bedroom with robe and ensuite;
- Laundry;
- Water Closet;
- Under stair storage;
- Open kitchen, meals and family area;
- Butler's pantry;
- Double garage; and
- Alfresco

### Upper floor

- Rumpus;
- Study;



- Bathroom;
- Water closet; and
- Two bedrooms.

Stairs will provide access between the ground and first floor of the dwellings.

The wall heights of the ground floor will vary between 2.4 and 2.7 metres while the upper storey walls will be 5.5 metres above the finished floor level of the ground floors. The upper storey windows to the side and rear of the dwellings will have windows with either sill heights or fixed obscured glass windows up to a height of 1.7 metres above the upper storey floor level.

The dwellings will have brick face finish, stone/tile veneer and rendered brick work to their ground floor front elevation. Brick face finish will be primarily applied to the side and rear elevations while the upper storey walls will contain coated unitex base board cladding. The dwellings will have hipped roofs of 25 degrees with colorbond cladding.

Both dwellings will have a double concrete driveway to Sir Donald Bradman Drive.

The existing dwelling on the land will be demolished to facilitate the construction of the new buildings.

### **3. Procedural matters**

#### **Characterisation**

The subject land is located within the Residential Zone as depicted on Zone Map WeTo/6 of the West Torrens Council Development Plan, Consolidated 12 July 2018. The subject land is depicted in Figure 2 below.



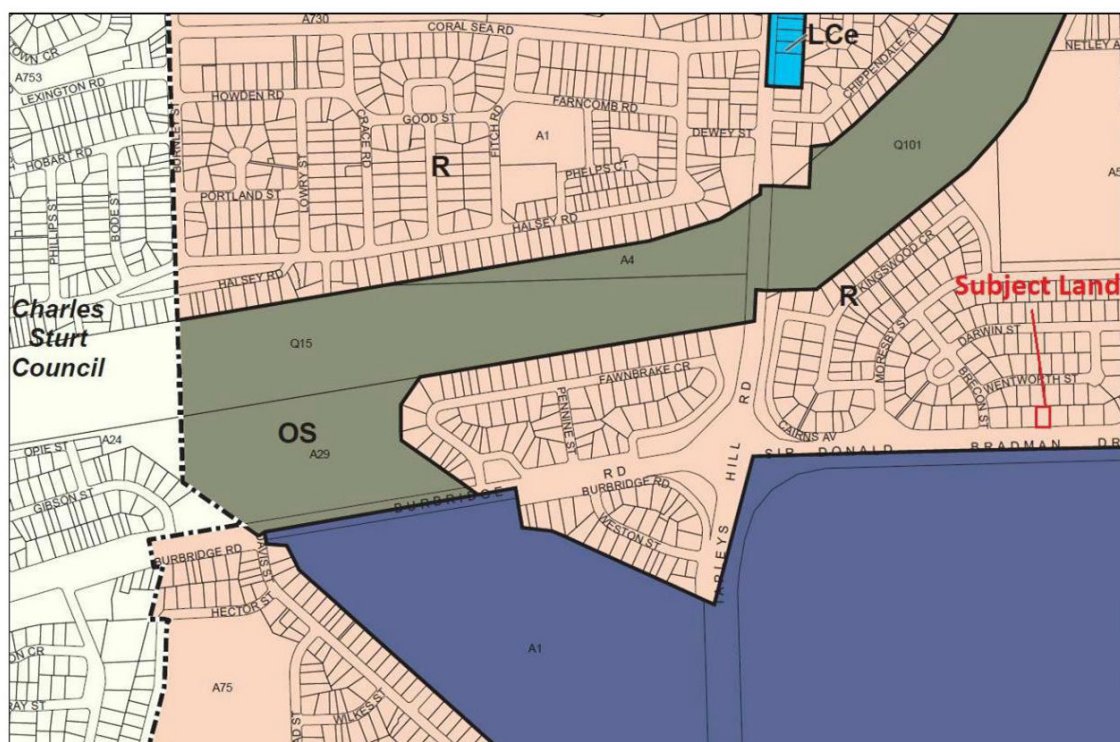


Figure 2: Zone Map

Source: Council Development Plan

The proposed development is not listed in the Procedural Matters Section of the Zone as being a non-complying development and land division is not listed as complying development. A detached dwelling is listed as complying development however in this instance the dwellings do not satisfy the conditions contained in Table WeTo/1 – Conditions for Complying Development.

The subject land is within a residential code area however the proposed allotments do not achieve the minimum site area and minimum frontage requirements specified in the relevant Development Plan.

Accordingly, the proposed development can be characterised as a “merit” assessment.

#### Public Notification

The proposed development is not listed in the Procedural Matters Section of the Zone as being either a Category 1 or Category 2 development and therefore the notification categorisation defers to Schedule 9 of the *Development Regulations 2008*.

Schedule 9 Part 1(2) of the Development Regulations states the following forms of development are Category 1 developments, except where the development is classified as non-complying under the relevant Development Plan:

*(a) the construction of any of the following (or of any combination of any of the following):*

*(i) 1 or more detached dwellings; and*

*(f) the division of land which creates not more than 4 additional allotments*





In this instance, the application is a Category 1 development pursuant to Schedule 9 Part 1(2)(a) and (f) of the Development Regulations and accordingly no public notification will be required.

#### Referral to Prescribed Bodies

Sir Donald Bradman Drive is depicted as a Secondary Arterial Road on Overlay Map WeTo/6 (Transport) however the subject land abuts a service road associated with the road corridor and not the main traffic carriageway.

A review of Schedule 8 – Referrals and concurrences (2) – Table of the *Development Regulations 2008*, notes the following forms of development are to be referred to the Commissioner of Highways (the prescribed body):

#### 2- Land Division adjacent to main Roads which:

- (1) Development that involves the division of land where the land being divided abuts a controlled access road declared pursuant to the *Highways Act 1926*
- (2) Development that involves the division of land where the land being divided abuts an arterial road and creates new road junctions on that arterial road

#### 3- Development adjacent to main roads

Development which in the opinion of the relevant authority is likely to—

- (a) alter an existing access; or
- (b) change the nature of movement through an existing access; or
- (c) create a new access; or
- (d) encroach within a road widening setback under the Metropolitan Adelaide Road Widening Plan Act 1972,

Although the subject land abuts a secondary arterial road corridor, the carriageway from which the land abuts is a service road and therefore has no direct link to the main arterial carriageway of Sir Donald Bradman Drive. In this instance it would be unnecessary to formally refer the development application to the Commissioner of Highways under the *Development Act, 1993*.

## **4. Relevant Provisions of the Development Plan**

The following provisions of the Development Plan are considered relevant in an assessment of this application.

Not all the identified Development Plan provisions have been reproduced or addressed in detail as set out below, only those considered to be most relevant to the assessment are specifically referenced.



## Policy Area 21

Objectives 1  
Desired Character  
Principles 1, 2, 3, 4 & 6

## Residential Zone

Objectives 1, 2, 3 & 4  
Desired Character  
Principles 1, 5, 6, 7, 8, 10, 11, 12 & 14

## General Section

Building Near Airfields  
Objective 1  
Principles 1, 2, 3, 4, 5 & 6

Landscaping, Fences and Walls  
Objective 1 & 2  
Principles 1, 2, 4 & 6

Crime Prevention  
Objective 1  
Principles 1, 2, 3, 6, 7, 8 & 10

Orderly and Sustainable Development  
Objectives 1, 2, 3, 4 & 5  
Principles 1, 3, 6 & 7

Design and Appearance  
Objectives 1  
Principles 1, 2, 3, 5, 9, 10, 11, 12, 13, 14, 15, 20 & 21

Residential Development  
Objective 1, 2, 3 & 4  
Principles 1, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 14, 15, 16, 18, 19, 20, 21, 27 & 31

Energy Efficiency  
Objective 1 & 2  
Principles 1, 2 & 3

Transportation and Access  
Objective 2  
Principles 2, 8, 10, 11, 23, 24, 27, 28, 34, 35 & 44

Land Division  
Objective 1, 2, 3 & 4  
Principles 1, 2, 5, 6, 8 & 12

## 5. Development Assessment

### Land Use

#### **Policy Area 21 Desired Character:**

*This policy area will have a low-density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).*



**Policy Area 21 PDC 1:** *The following forms of development are envisaged specifically in the policy area:*

- *detached dwelling*

**Residential Zone Objective 1:** *A residential zone comprising a range of dwelling types, including a minimum of 15 per cent affordable housing.*

**Residential Zone Desired Character:**

*This zone will contain predominantly residential development*

**Residential Zone PDC 1:** *The following forms of development are envisaged in the zone:*

- *dwelling*

The subject land is situated within a Residential Zone and detached dwellings are an envisaged form of development within the Zone. The construction of two, detached dwellings is not in conflict with the desired or existing land uses. The proposed continuation of the land for residential purposes with new detached dwellings are acceptable given they are envisaged forms of development within the Zone.

### **Density and character**

**Policy Area 21 Desired Character:**

*This policy area will have a low-density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).*

*There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones.*

**Policy Area 21 Objective 1:** *Development that contributes to the desired character of the policy area.*

**Policy Area 21 PDC 2:** *Development should not be undertaken unless it is consistent with the desired character for the policy area.*

**Policy Area 21 PDC 6:** *Land division should create allotments with an area of greater than 420 square metres and a minimum frontage width of 12 metres, other than where the land division is combined with an application for dwellings or follows an approval for dwellings on the site.*

**Policy Area 21 PDC 3:** *Except when located within 400 metres of a centre zone, a dwelling should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:*





Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	420 minimum	12
Semi-detached	420 minimum	12

**Policy Area 21 PDC 4:** *When a dwelling is located within 400 metres of a centre zone, it should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:*

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	350 minimum	9
Semi-detached	350 minimum	9
Row dwelling	350 average	9

**Residential Zone Objective 3:** *Increased dwelling densities in close proximity to centres, public transport routes and public open spaces.*

**Residential Zone Objective 4:** *Development that contributes to the desired character of the zone.*

**Residential Zone Desired Character:**

*Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas.*

**Residential Zone PDC 5:** *Development should not be undertaken unless it is consistent with the desired character for the zone and policy area.*

The overall locality contains predominantly very low-density housing comprising mostly single storey detached dwellings on allotments greater than 600 sqm. Much of the dwelling stock comprises the original dwellings constructed approximately 50 to 60 years ago. New development is evident across the locality with original dwellings gradually being replaced by detached dwellings and infill development comprising two new detached dwellings on allotments ranging between 350 and 410 sqm. Some of the new allotments have frontages of approximately 10 metres.

The State Government produced *30-Year Plan for Greater Adelaide (2017 update)* defines density (net residential site density\*) in the following manner:

Low density = fewer than 35 dwelling units per hectare (du/ha)

Medium density = 35-70 du/ha

High density = more than 70 du/ha.



\*(Net residential site density has been calculated using the residential site area only and excludes all other land, i.e. roads and open space, from considerations).

Further, the document *"Understanding Residential Densities: A Pictorial Handbook of Adelaide Examples"* (November 2006), also published by the State Government, describes very low-density being less than 17 dwellings per hectare (net residential site density).

The desired character statement for Policy Area 21 notes the policy area will have a low-density character with development predominantly involving the replacement of detached dwellings with the same (or buildings in the form of detached dwellings). Objective 1 and Principle of Development Control (PDC) 2 of Policy Area 21 both prescribe development being consistent with the desired character of the Policy Area, otherwise it should not be undertaken. This is also reinforced by the desired character statement, Objective 4 and PDC 5 of the Residential Zone.

In addition, PDC's 3 and 4 of Policy Area 21 aid the achievement of the desired character by prescribing minimum site areas of 350 sqm and 420 sqm and minimum frontage widths of 9 metres and 12 metres for detached dwellings, the lesser values relating to residential properties within 400 metres of a centre zone, public transport routes and public open spaces. It is apparent the change within the locality from very low to low density is being facilitated by the provisions of the Policy Area however flexibility will be required to achieve the low-density goal.

Although the subject land is not within 400 metres of a centre zone it is adjacent to an important road corridor which carries high volumes of both private and public vehicles, including frequent public bus services. An Aldi supermarket is little more than 100 metres south of the subject land and Harbor Town shopping centre approximately 1.2 kilometres. The size of the Aldi site is greater than many of the facilities found within local centre zones scattered across the western suburbs and provides a function which is compatible with the objectives of local centre zoning. The Harbor Town centre provides similar services to those found in the Brickworks and Kurrulta Park District Centres. The locality is also bound by large areas of public open space and the coast is just over 2 kilometres from the locality. The subject land clearly enjoys good access to services and natural features which can support a denser allotment pattern envisaged within parts of the Policy Area.

The proposed allotments are just under 350 sqm so they fall short of meeting the minimum areas listed in both PDC's 3 & 4 however, the net residential site density of the division will equate to 28 dwelling units per hectare, which comfortably sits within the definition of low-density development as defined in *30-Year Plan for Greater Adelaide*. The frontage width will exceed the minimum length listed by PDC 4 but will fall short of achieving the 12 metre minimum expressed in PDC 3. The scale and dimensions of the proposed allotments will not compromise the desired character envisaged for the locality because the development will create low density allotments and with dwelling types which are desired within the locality.

Further, an analysis of original allotments within the locality identified only five (5) allotments, all along Sir Donald Bradman Drive and still containing their original housing, which could achieve both the minimum frontage width and site area expressed in PDC 3 of the Policy Area. The remaining forty-eight (48) allotments, some already developed with either infill housing or new dwelling stock, are unable to achieve both the minimum frontage width and site areas unless allotments were amalgamated. Given the dominance of private ownership within the locality amalgamation is an unlikely outcome.





In addition, it is likely six (6) of the nine (9) properties with a primary frontage to Sir Donald Bradman Drive, and at the edge of the residential zone, will be eventually developed with infill development. These properties have frontages of at least 24 metres, and all but one, have site areas greater than 800 sqm. The remaining allotments have frontage widths of approximately 22 metres and site areas of approximately 700 sqm.

With further infill development likely to eventuate, there will be minimal variation in frontage widths, with development, including the subject land, creating frontage widths between approximately 11 metres and 16 metres. Although the site areas may vary between approximately 350 sqm and 460 sqm, the depths of the allotments will remain the same. Considering the circumstances, the proposed development will have a similar presentation to the street as other infill development albeit with marginally smaller frontage widths. The variation in frontage width will not be fatal to the desired character statement for the Policy Area.

The locality is likely to retain its very low-density status should the site area and frontage width outcomes expressed in PDC 3 of the Policy Area be rigidly applied. The transition from an area of very low-density to low-density, as expressed for the Policy Area, is unlikely to be achieved. Considering the context of the locality, especially its proximity to a major transport route, non-residential land uses like the Aldi supermarket, future commercial developments on nearby airport land, Harbor Town shopping centre and the abundance of public open space I believe greater weight can be afforded to PDC 4 of the Policy Area to ensure the low-density expectations for the Policy Area can be satisfied.

### **Allotment Pattern**

**Residential Development PDC 1:** *Residential allotments and sites should maximise solar orientation and have the area and dimensions to accommodate:*

- (a) the siting and construction of a dwelling and associated ancillary outbuildings*
- (b) the provision of landscaping and private open space*
- (c) convenient and safe vehicle, pedestrian and cycling access and parking*
- (d) water sensitive design systems that enable the storage, treatment and reuse of stormwater.*

**Land Division Objective 1:** *Land division that occurs in an orderly sequence allowing efficient provision of new infrastructure and facilities and making optimum use of existing under-utilised infrastructure and facilities.*

**Land Division Objective 2:** *Land division that creates allotments appropriate for the intended use.*

**Land Division Objective 3:** *Land division layout that is optimal for energy efficient building orientation.*

**Land Division Objective 4:** *Land division that is integrated with site features, including landscape and environmental features, adjacent land uses, the existing transport network and the availability of infrastructure.*

**Land Division PDC 1:** When land is divided:

- (a) stormwater should be capable of being drained safely and efficiently from each proposed allotment and disposed of from the land in an environmentally sensitive manner*
- (b) a sufficient water supply should be made available for each allotment*





(c) provision should be made for the disposal of wastewater, sewage and other effluent from each allotment without risk to health

**Land Division PDC 2:** Land should not be divided if any of the following apply:

- (a) the size, shape, location, slope or nature of the land makes any of the allotments unsuitable for the intended use
- (b) any allotment will not have a frontage to one of the following:
  - (i) an existing road
  - (ii) a proposed public road
  - (iii) access to a public road via an internal roadway in a plan of community division
- (c) the intended use of the land is likely to require excessive cut and/or fill
- (d) it is likely to lead to undue erosion of the subject land or land within the locality
- (e) the wastewater treatment plant to which subsequent development will be connected does not have sufficient capacity to handle the additional wastewater volumes and pollutant loads generated by such development
- (f) the area is unsewered and cannot accommodate an appropriate onsite wastewater disposal system within the allotment that complies with (or can comply with) the relevant public and environmental health legislation applying to the intended use(s)
- (g) any allotments will straddle more than one zone, policy area or precinct
- (h) the allotments unreasonably restrict access to publicly owned land such as recreation areas.

**Land Division PDC 5:** Land division should result in allotments of a size suitable for their intended use.

**Land Division PDC 6:** Land division should facilitate optimum solar access for energy efficiency.

**Residential Development Objective 4:** The revitalisation of residential areas to support the viability of community services and infrastructure.

**Residential Development PDC 3** Residential allotments should be of varying sizes to encourage housing diversity.

The proposed allotments will have a rectangle shape, depth and north-south orientation which is compatible with existing allotments within the locality. The proposed dwellings will have internal and external activity areas which have northern aspects to maximise solar access and the allotments provide private open space, on-site vehicle parking and landscaping opportunities for each dwelling.

The division of land will create allotments which have direct access to existing water, sewer, electricity, stormwater and telecommunications infrastructure. The land is also serviced by an all-weather road network.

The layout and size of proposed allotments are appropriate for the intended residential use and will support dwellings consistent in respect of type, scale and siting of the prevailing development of the locality. The allotments conform with good residential design principles which are reflected in the general development plan provisions relevant to land division and residential development.

## **Design**

**Policy Area 21 Desired Character:**



*Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings.*

**Residential Zone PDC 6:** *Except where otherwise specified by a policy area, dwellings and buildings containing dwellings should be designed within the following parameters:*

Parameter	Value
Maximum number of storeys	two storeys (above natural ground level)
Maximum vertical side wall height	6 metres (measured from the natural ground level)

**Residential Development PDC 4:** *Building appearance should be compatible with the desired character statement of the relevant zone, policy area or precinct, in terms of built form elements such as:*

- (a) building height*
- (b) building mass and proportion*
- (c) external materials, patterns, textures, colours and decorative elements*
- (d) ground floor height above natural ground level*
- (e) roof form and pitch*
- (f) facade articulation and detailing and window and door proportions*
- (g) verandas, eaves and parapets*
- (h) driveway crossovers, fence style and alignment.*

**Residential Development PDC 5:** *Residential development should avoid undue repetition of style and external appearance.*

**Residential Development PDC 7:** *Residential development should be designed to ensure living rooms have an external outlook.*

**Residential Development PDC 8:** *Entries to dwellings or foyer areas should be clearly visible from the street, or access ways that they face to enable visitors to easily identify individual dwellings.*

**Residential Development PDC 9:** *Residential development should provide a high quality living environment by ensuring the following minimum internal floor areas (including internal storage areas but not including balconies and car parking):*

- (a) studio (where there is no separate bedroom): 37 square metres*
- (b) 1 bedroom dwelling/apartment: 50 square metres*
- (c) 2 bedroom dwelling/apartment: 75 square metres*
- (d) 3+ bedroom dwelling/apartment: 100 square metres.*

**Design and Appearance Objective 1:** *Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form.*

**Design and Appearance PDC 1:** *Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:*

- (a) building height, mass and proportion*
- (b) external materials, patterns, colours and decorative elements*
- (c) roof form and pitch*





- (d) façade articulation and detailing*
- (e) verandas, eaves, parapets and window screens.*

**Design and Appearance PDC 2:** *The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.*

The front elevations of the dwellings incorporate a front porch and entrance and staggered double garage at ground level and a rumpus room and study area on the upper floor. Part of the garage is in line with the front façade of the upper storey however the presence of the garage is broken up by the provision of two separate openings and with one being setback behind the line of the other. The elevation comprises a variety of building lines, materials and finishes which will prevent the building appearing bulky and the garage design does not dominate the overall appearance of the dwelling. The upper storey windows are notable elements of the upper floor elevation and provide passive observation of public space from internal activity areas of the dwellings.

The side elevations of the upper floor walls will be setback more than a metre further from the side boundary than the ground floors and both the mass of the ground and upper floor walls will be broken up by windows and roofing. A large portion of the rear elevations will be broken up by windows too and the hipped roof features of both the ground floor and upper floors will help articulate the building's form.

The ground floor wall heights will be 2.4 metres, which is compatible with existing single storey housing in the locality and the upper floor walls will be 5.5 metres high with an upper roof ridge height of 7.6 metres. The two-storey form of the dwellings is not overly bulky or excessive in comparison with existing development and the designs satisfy the height, interface and articulation provisions expressed in both the Policy Area, Zone and relevant general sections of the development plan relating to design and appearance.

### **Setbacks**

**Residential Zone PDC 7:** *Dwellings should be set back from allotment or site boundaries to:*

- (a) contribute to the desired character of the relevant policy area*
- (b) provide adequate visual privacy by separating habitable rooms from pedestrian and vehicle movement.*

**Residential Zone PDC 8:** *Except where specified in Medium Density Policy Area 18 and Medium Density Policy Area 19, development (including any veranda, porch, etc) should be set back from the primary road frontage in accordance with the following table:*





Setback difference between buildings on adjacent allotments	Setback of new building
Up to 2 metres	The same setback as one of the adjacent buildings, as illustrated below:
<p>When <math>b - a \leq 2</math>, setback of new dwelling = <math>a</math> or <math>b</math></p>	
Greater than 2 metres	At least the average setback of the adjacent buildings.

**Residential Zone PDC 11:** *Except where otherwise specified by a policy area, and for party walls, dwellings and buildings containing dwellings should be set back from the side and rear boundaries in accordance with the following table:*

Parameter	Minimum value (metres)
Side boundary setback where the vertical side wall is 3 metres or less in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	1
Side boundary setback where vertical side wall measures between 3 to 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	2
Side boundary setback where the vertical side wall is greater than 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	2 metres plus an additional setback which is equal to the increase in wall height above 6 metres.
Rear boundary setback for single storey components of a building	3
Rear boundary setback for two or more storey components of a building	8

**Residential Zone PDC 12:** *Side boundary walls in residential areas should be limited in length and height to:*

- (a) *minimise their visual impact on adjoining properties*
- (b) *minimise the overshadowing of adjoining properties.*

**Residential Zone PDC 13:** *Except where otherwise specified by a policy area, side boundary walls comply with the following:*

- (a) *side boundary walls should be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining site and constructed to the same or to a lesser length and height*
- (b) *side boundary walls:*
  - (i) *should have a maximum vertical wall height of 3 metres*
  - (ii) *should have a maximum length of 8 metres*
  - (iii) *should be constructed along one side of the allotment only and no further than 14 metres from the front boundary*



The dwellings will be sited closer to the front property boundary than either dwelling on the adjoining properties however the difference will not render the dwelling's position incongruent with the setback characteristics of the streetscape. Recent infill dwellings are sited much closer to their front property boundary than original housing stock and this is expected to continue with the development of additional infill housing, particularly along Sir Donald Bradman Drive. The proposed dwellings are setback further from the street than other new dwellings and the design of the building's front elevations incorporate a variety of setbacks ranging from 5 metres (front porch) to 7 metres to provide siting compatibility with the adjoining dwellings.

The subject land also sits at the edge of the residential zone and, unlikely concentrated parts of the zone, there is no residential development on the opposite side of the street. The streetscape has a strong sense of open space courtesy of the landscaped verges and the wide carriageways of Sir Donald Bradman Drive. The abundance of open space within the streetscape will diminish the variation between the proposed setbacks of the dwellings and the setback characteristics of the original dwellings.

The side and rear setbacks of the dwellings are consistent with the setback provisions of the development plan. The side walls of the ground floors do not achieve the prescribed 1 metre setback stipulated by PDC however the shortfall is no more than 40mm and will not be obvious to the naked eye nor will it create a loss of amenity for occupants of the adjoining properties.

The dwellings will have a wall constructed on one side boundary which will have heights of 2.7 metres and lengths of 6.5 metres. Both walls will be constructed within 14 metres of the front property boundary. The boundary walls will not cause excessive visual or shadow impact on adjoining dwellings.

Overall, the setbacks of the proposed developments are satisfactory when considered against the provisions of the development plan and the context of the locality which is experiencing a change in siting characteristics as original dwellings are replaced with new housing stock.

### **Private Open Space**

**Residential Development PDC 18:** *Private open space (available for exclusive use by residents of each dwelling) should be provided for each dwelling and should be sited and designed:*

- (a) to be accessed directly from the internal living areas of the dwelling*
- (b) to be generally at ground level (other than for residential flat buildings) and to the side or rear of a dwelling and screened for privacy*
- (c) to take advantage of, but not adversely affect, natural features of the site*
- (d) to minimise overlooking from adjacent buildings*
- (e) to achieve separation from bedroom windows on adjoining sites*
- (f) to have a northerly aspect to provide for comfortable year round use*
- (g) not to be significantly shaded during winter by the associated dwelling or adjacent development*
- (h) to be partly shaded in summer*
- (i) to minimise noise or air quality impacts that may arise from traffic, industry or other business activities within the locality*
- (j) to have sufficient area and shape to be functional, taking into consideration the location of the dwelling, and the dimension and gradient of the site.*





**Residential Development PDC 19:** *Dwellings at ground level should provide private open space in accordance with the following table:*

Site area per dwelling (square metres)	Minimum area excluding any area at ground level at the front of the dwelling (square metres)	Minimum dimension (metres)	Minimum area provided at the rear or side of the dwelling, directly accessible from a habitable room (square metres)
<300	24, of which 8 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	3 (excluding balconies)	16
300-500	60, of which 10 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	4	16
>500	80, of which 10 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	4	24

**Residential Zone PDC 20:** *Private open space should not include driveways, effluent drainage areas, rubbish bin storage areas, sites for rainwater tanks and other utility areas, sites for outbuildings, and common areas such as parking areas and communal open space.*

**Residential Zone PDC 21:** *Private open space at ground level should be designed to provide a consolidated area of deep soil (an area of natural ground which excludes areas where there is a structure underneath, pools and non-permeable paved areas) to:*

- (a) *assist with ease of drainage*
- (b) *allow for effective deep planting*
- (c) *reduce urban heat loading and improve micro-climatic conditions around sites and buildings.*

Private open space is located to the rear of the dwellings, at ground level and will be directly accessible from internal activity areas of the dwelling. The private open spaces will have an area of approximately 70 sqm and no dimension under 4.7 metres, have a northerly aspect and include alfresco areas which will be shaded in summer as well as an area consisting of deep soil to assist with drainage and the planting of vegetation. The spaces have areas which exceed the minimum areas prescribed by the development plan which will not be compromised by utility functions such as bin storage, outbuildings, clothes drying or rainwater tanks.

The private open spaces of the dwellings are consistent with the relevant general residential provisions of the development plan.

### **Overshadow**

**Residential Zone PDC 14:** *Development should ensure that sunlight to solar panels of existing buildings is maintained for a minimum of 2 consecutive hours between 9.00am and 3.00pm on 22 June.*





**Residential Development PDC 10:** *The design and location of buildings should ensure that direct winter sunlight is available to adjacent dwellings, with particular consideration given to:*

- (a) windows of habitable rooms, particularly living areas*
- (b) ground-level private open space*
- (c) upper-level private balconies that provide the primary open space area for any dwelling*
- (d) access to solar energy.*

**Residential Development PDC 11:** *Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.*

**Residential Development PDC 12:** *Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:*

- (a) half of the existing ground-level open space*
- (b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).*

The proposed dwellings include a second storey, but the rear walls are setback almost 13 metres from the rear property boundary. As such, the dwellings will cast some early morning shadow partially over the side walls of adjoining dwellings to the west however this will greatly decrease by the late morning period. The same impact will be experienced late in the afternoon on the property to the east but once again the extent of the shadow is unlikely to have a detrimental impact the adjoining dwelling.

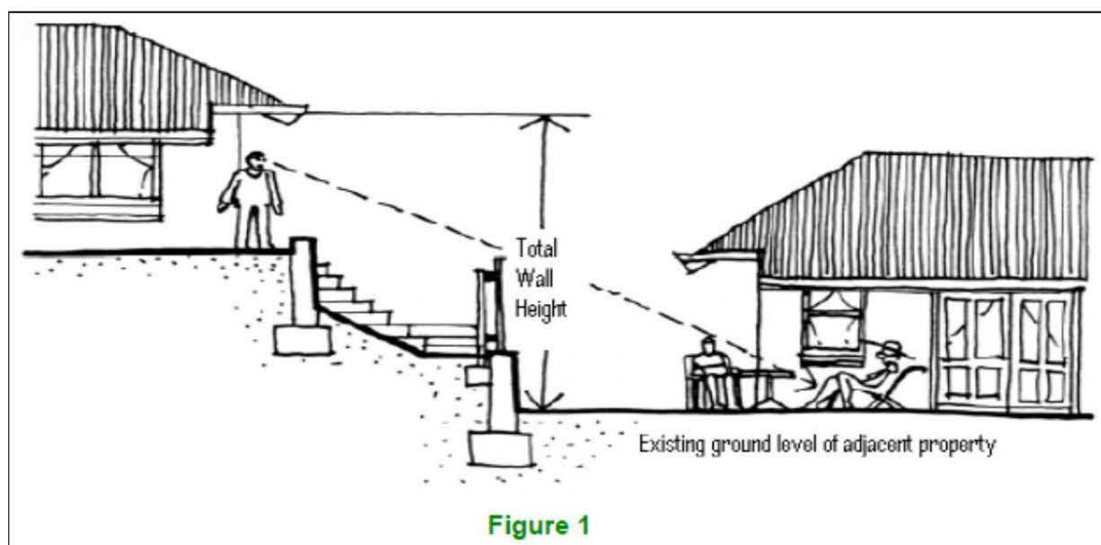
Neither north facing windows, solar panels or private spaces of adjoining dwellings will be prevented from receiving direct sunlight for extensive periods of the day and any shadow thrown over an adjoining properties will not exceed the shadow guidelines contained in the development plan.

### **Privacy**

**Residential Zone PDC 10:** *Dwelling setbacks from side and rear boundaries should be progressively increased as the height of the building, (with the total wall height of the building being measured from the existing ground level at the boundary of the adjacent property as shown by Figure 1), increases to:*

- (a) minimise the visual impact of buildings from adjoining properties*
- (b) minimise the overshadowing of adjoining properties.*

**Residential Development PDC 27:** *Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.*



The subject land is relatively flat therefore the finished floor levels of the dwellings and surrounding ground levels will be comparable with the existing dwellings on the adjoining properties. The topography of the land presents no potential loss of privacy for adjoining properties, especially from the ground floor areas in and around the proposed dwellings.

The upper storey windows on the side and rear elevation of the dwellings will comprise either fixed obscure glazing or window sill heights which extend to 1.8 metres above the finished floor level of the upper floor. These design features will ensure no direct views of private open spaces and habitable windows of adjoining dwellings will be available from the upper storey windows.

The proposed dwellings will ensure the amenity and privacy of adjoining properties is maintained as anticipated by the relevant provisions of the development plan.

### **Landscaping**

#### **Policy Area 21 Desired Character:**

*Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.*

#### **Residential Zone Desired Character:**

*Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer*

**Residential Development PDC 6:** *Dwellings and accommodation at ground floor level should contribute to the character of the locality and create active, safe streets by incorporating one or more of the following:*





*(a) front landscaping or terraces that contribute to the spatial and visual structure of the street while maintaining adequate privacy for occupants*

**Landscaping, Fences and Walls (LFW) Objective 1:** *The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.*

**LFW PDC 1:** *Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:*

- (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)*
- (b) enhance the appearance of road frontages*
- (c) screen service yards, loading areas and outdoor storage areas*
- (d) minimise maintenance and watering requirements*
- (e) enhance and define outdoor spaces, including car parking areas*
- (f) maximise shade and shelter*
- (g) assist in climate control within and around buildings*
- (h) minimise heat absorption and reflection*
- (i) maintain privacy*
- (j) maximise stormwater reuse*
- (k) complement existing vegetation, including native vegetation*
- (l) contribute to the viability of ecosystems and species*
- (m) promote water and biodiversity conservation.*

**LFW PDC 2:** *Landscaping should:*

- (a) include the planting of drought tolerant species, including locally indigenous species where appropriate*
- (b) be oriented towards the street frontage*
- (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.*

**LFW PDC 3:** *Landscaping should not:*

- (a) unreasonably restrict solar access to adjoining development*
- (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding*
- (c) introduce pest plants*
- (d) increase the risk of bushfire*
- (e) remove opportunities for passive surveillance*
- (f) increase leaf fall in watercourses*
- (g) increase the risk of weed invasion*
- (h) obscure driver sight lines*
- (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.*

**LFW PDC 4:** *A minimum of 10 per cent of a development site should be landscaped. The development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences.*

Parts of both the front yard and rear open spaces of the proposed dwellings can accommodate areas consisting of deep soil and the planting of vegetation. The areas combined equate to more than 10 percent of the total site areas and have dimensions in which the planting of scrubs, bushes and trees can be established. The opportunity to add landscaping to the areas around the dwelling will enhance both the amenity and appearance





of the subject land and allow the dwellings to have a similar presentation to the street to other existing dwellings in the locality.

### **Parking and Movement**

**Land Division PDC 12:** *On-street vehicle parking should be provided at a ratio of one car parking space for every two allotments.*

**Transport and Access Objective 2:** *Development that:*

- (a) provides safe and efficient movement for all transport modes*
- (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles*
- (c) provides off-street parking*
- (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks*
- (e) provides convenient and safe access to public transport stops.*

**Transport and Access PDC 8:** *Development should provide safe and convenient access for all anticipated modes of transport.*

**Transport and Access PDC 10:** *Driveway crossovers affecting pedestrian footpaths should maintain the level and surface colour of the footpath.*

**Transport and Access PDC 11:** *Driveway crossovers should be separated and the number minimised to optimise the provision of on-street visitor parking (where on-street parking is appropriate).*

**Transport and Access PDC 23:** *Development should have direct access from an all weather public road.*

**Transport and Access PDC 34:** *Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table WeTo/2 - Off Street Vehicle Parking Requirements.*

*Table WeTo/2 - Off Street Vehicle Parking Requirements*

Form of development	Number of Required Car Parking Spaces
Accommodation	
Dwelling <ul style="list-style-type: none"> <li>▪ detached</li> <li>▪ semi-detached</li> <li>▪ row</li> </ul>	For detached, semi detached, row dwelling and multiple dwellings to provide: 2 car parking spaces per dwelling, one of which is covered.

The dwellings are provided with double garages which can accommodate the parking of two B85 vehicles within the buildings. A further two vehicles can be parked upon the double width driveways. The provision of on-site parking exceeds the expectations of the development plan.

The separation between the crossovers which provide vehicle movement between the dwellings and Sir Donald Bradman Drive will be no less than 6 metres. This will enable one vehicle to park on-street and immediately in front of the subject land however due to the



absence of residential development on the southern side of the street an abundance of on-street parking options will still be available within the street.

Vehicles can safely reverse on to Sir Donald Bradman Drive as the service road provides no direct access to the main carriageways of the arterial road. The number of vehicle movements within the service road are relatively low as the road services mostly the immediate dwellings. All access to the major carriageway of Sir Donald Bradman Drive must be taken from either Morseby Street or Rutland Avenue intersections which provide facilities for safe left and right turn movements.

Two bus stops are located approximately 200 metres from the subject land and support a public bus service which travels to the City, Glenelg and West Beach and Harbor Town. Travel times to either Glenelg or the City take no longer than 30 minutes.

### **Airport interface**

**Building near Airfields Objective 1:** *Development that ensures the long-term operational, safety, commercial and military aviation requirements of airfields (airports, airstrips and helicopter landing sites) continue to be met.*

**Building near Airfields PDC 1:** *The height and location of buildings and structures should not adversely affect the long-term operational, safety, commercial and military aviation requirements of airfields.*

**Building near Airfields PDC 2:** *Buildings and structures that exceed the airport building heights as shown on the Overlay Maps - Development Constraints should not be developed unless a safety analysis determines that the building/structure does not pose a hazard to aircraft operations.*

**Building near Airfields PDC 3:** *Development in the vicinity of airfields should not create a risk to public safety, in particular through any of the following:*

- (a) lighting glare*
- (b) smoke, dust and exhaust emissions*
- (c) air turbulence*
- (d) storage of flammable liquids*
- (e) attraction of birds*
- (f) reflective surfaces (eg roofs of buildings, large windows)*
- (g) materials that affect aircraft navigational aids.*

**Building near Airfields PDC 4:** *Outdoor lighting within 6 kilometres of an airport should be designed so that it does not pose a hazard to aircraft operations.*

**Building near Airfields PDC 5:** *Development that is likely to increase the attraction of birds should not be located within 3 kilometres of an airport used by commercial aircraft. If located closer than 3 kilometres the facility should incorporate bird control measures to minimise the risk of bird strikes to aircraft.*

**Building near Airfields PDC 6:** *Development within areas affected by aircraft noise should be consistent with Australian Standard AS2021 - Acoustics - Aircraft Noise Intrusion - Building Siting and Construction.*





**Building near Airfields PDC 7:** *Residential development on land within areas affected by aircraft noise as shown on Overlay Map WeTo/8 - Development Constraints should incorporate noise attenuation measures.*

The subject land is in proximity of Adelaide Airport however information on Council's West Maps database confirmed the dwellings will be outside the 20 Australian Noise Exposure Forecast (ANEF) contour developed to plot estimated noise exposure caused by aircraft movements. Noise from aircraft movement, while being audible occasionally, is not expected to detrimentally affect the internal amenity enjoyed within the dwellings

The subject land is well separated from critical operational, safety, commercial and military aviation facilities supporting the operation of Adelaide Airport. The height of the dwellings is not regulated by the development plan as the subject land is not located under an airport flightpath. The materials and lighting to be used on the dwellings will not create glare or light spill which will create hazard. The proposed dwellings will have no effect on airport operations or aircraft safety.

## 6. CONCLUSION

The subject land is situated in a locality in which low-density development consisting primarily of detached dwellings is envisaged. The proposed development satisfies these expectations, albeit on allotments which do not achieve the prescribed site area or frontage widths specified for the land.

This departure is not deemed a critical parting from the objectives of the development plan as the locality enjoys access to the necessary features to support more dense allotments. These features include a large shop run by a multi-national supermarket chain, the continuing development of airport land with commercial developments, frequent public transport service and a variety of public recreational spaces. A large regional shopping centre and the coast are also conveniently accessible from the subject land. The land has similar conditions to those experienced elsewhere within the Policy Area where smaller, low-density allotments are possible.

The subject land also sits at the edge of the Residential Zone and among other properties which are likely to be developed with infill development in the coming years. The shape and size of new development will be compatible in configuration and depth. The variation in frontage widths will not cause conflict with the desired character of the area as the proposed dwellings are of a type and scale which complement both original and new residential development within the locality.

I am satisfied the proposal is an appropriate form of development for the subject land and warrants the favourable support of the Council.

Should you have any further enquiries about this report, please do not hesitate to contact me on 8130 7222, or alternatively via email on [adam@accessplanning.com.au](mailto:adam@accessplanning.com.au)

Yours faithfully,

A handwritten signature in blue ink that reads 'Adam Williams'.

Adam Williams  
**ACCESS PLANNING PTY LTD**

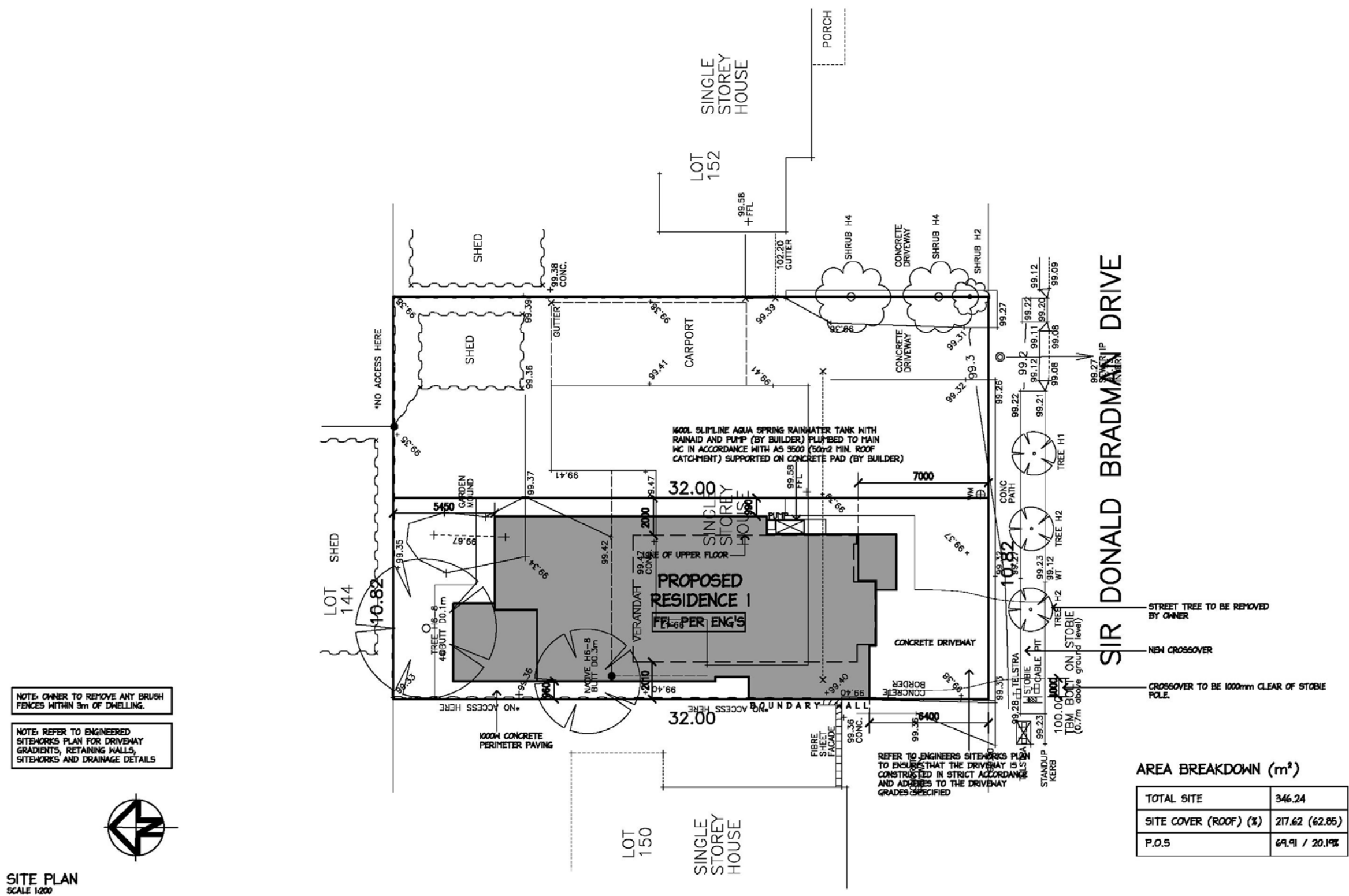


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SITE PLAN  
SCALE 1:200

SHEET 1 OF 5 PRELIMINARY WORKING DRAWINGS		PROPOSED NEW HOME FOR: McDOUGALL FAMILY TRUST  AT NO. 576 (RES 1) SIR DONALD BRADMAN DRIVE LOCKLEYS	AMENDMENT DATES			SPECIAL NOTES		AREAS m <sup>2</sup>	<div><div>B</div><div>BRADFORD HOMES</div></div> <div>© COPYRIGHT. These drawings are copyright and remain the exclusive property of Bradford Homes. Reproduction of the whole or any part of these drawings without written permission is prohibited</div> <div>A 4748 ANZAC HIGHWAY, CAMDEN PARK SA 5048 W WWW.BRADFORDHOMES.COM.AU P (08) 8350 2950</div>
Job No. 80091	SHEET SIZE: A3		REVISION	DATE	COMPLETED BY	WINDOW LEGEND			
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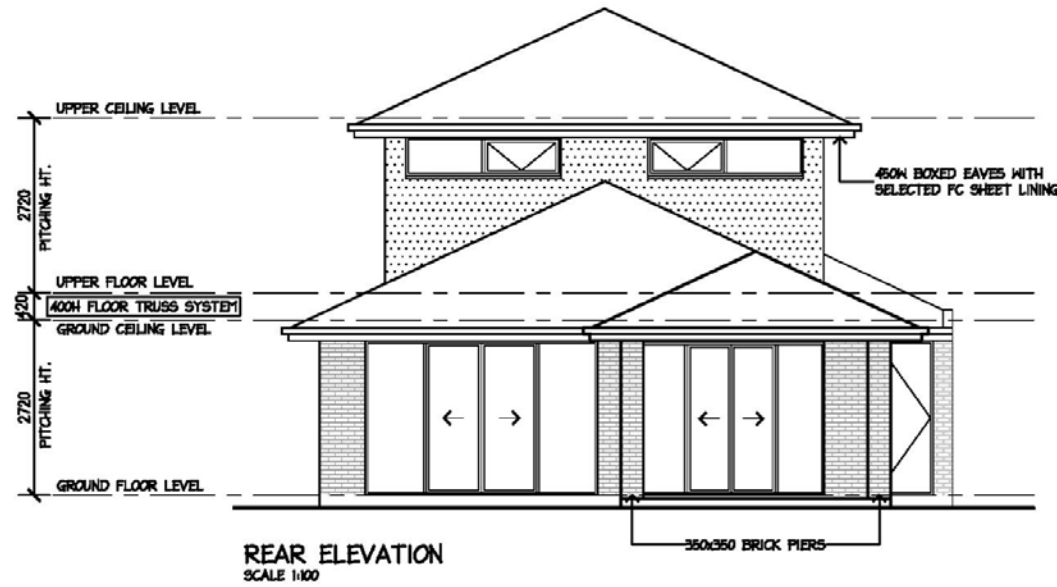
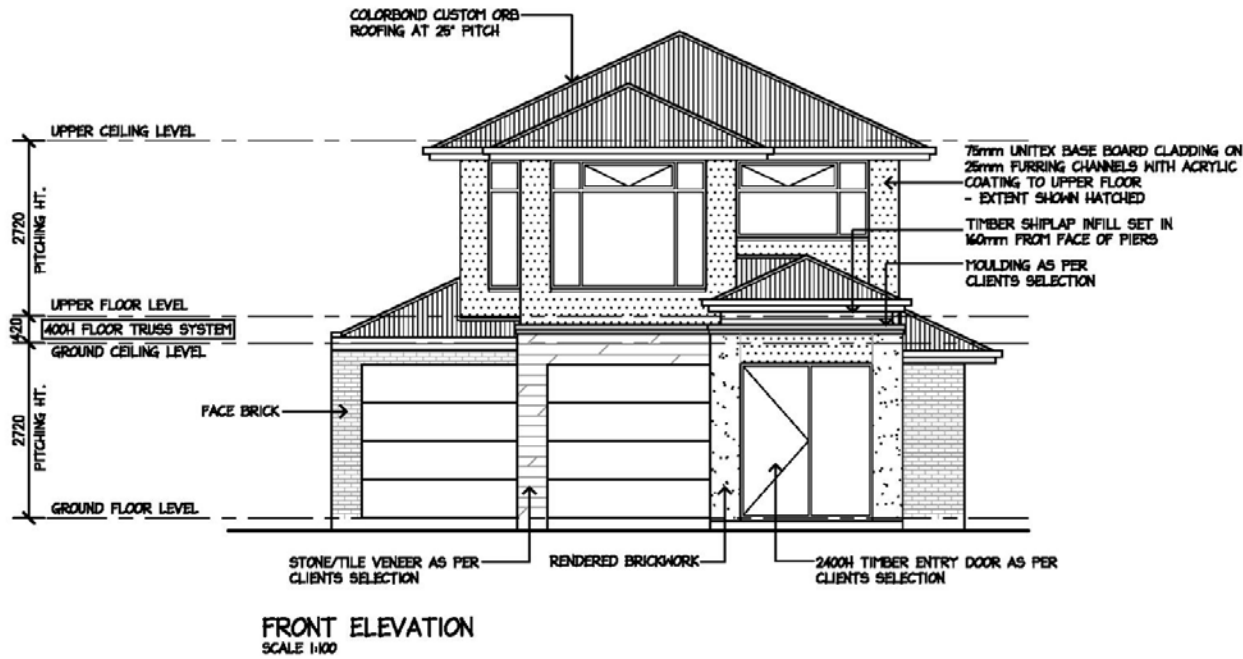
NOTES

OVERALL GARAGE DOOR OPENING CLEARANCE MAY BE REDUCED TO SUIT ROLLER DOOR DRUM

UPPER FLOOR AWNING & SLIDING WINDOWS WILL BE RESTRICTED TO 126mm OPENING WIDTH

TAPERED SILLS TO LIGHTWEIGHT WALLS

IF REQUIRED - PROVIDE ANGLE INFILL ABOVE DOORS/WINDOWS TO MATCH FRAME (CHECK ON SITE)



SHEET 4 OF 5 PRELIMINARY WORKING DRAWINGS		PROPOSED NEW HOME FOR: McDOUGALL FAMILY TRUST  AT NO. 576 (RES 1) SIR DONALD BRADMAN DRIVE LOCKLEYS	AMENDMENT DATES			SPECIAL NOTES		AREAS m <sup>2</sup>	<div><div>B</div><div>BRADFORD HOMES</div></div> <div><div>474B ANZAC HIGHWAY, CAMDEN PARK SA 5043</div><div>WWW.BRADFORDHOMES.COM.AU</div><div>(08) 8350 2950</div></div>
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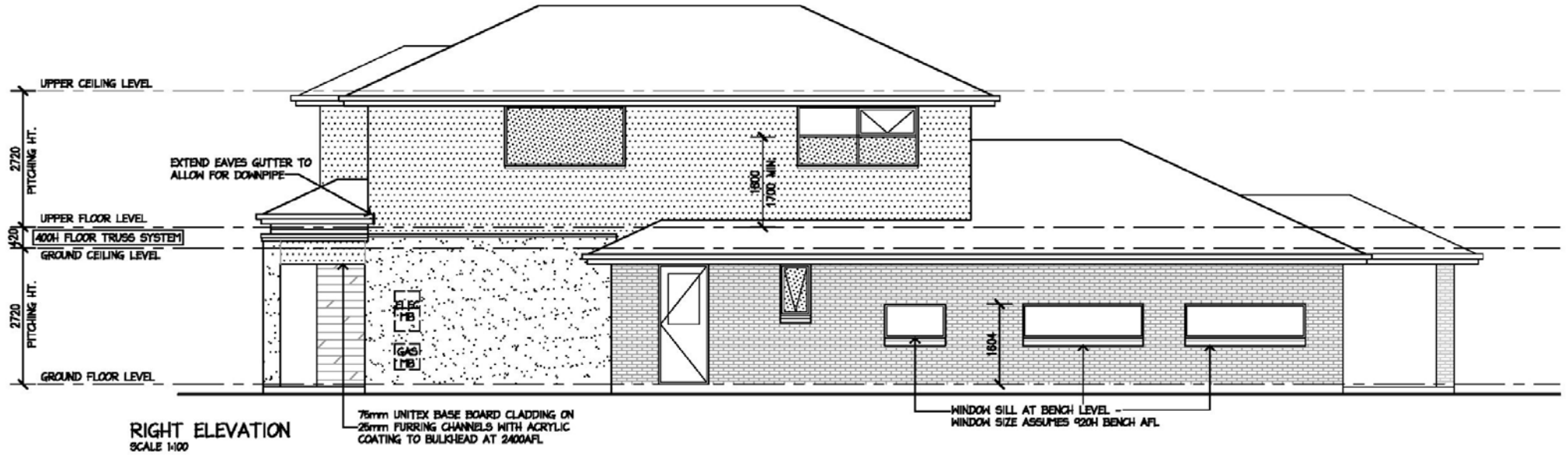
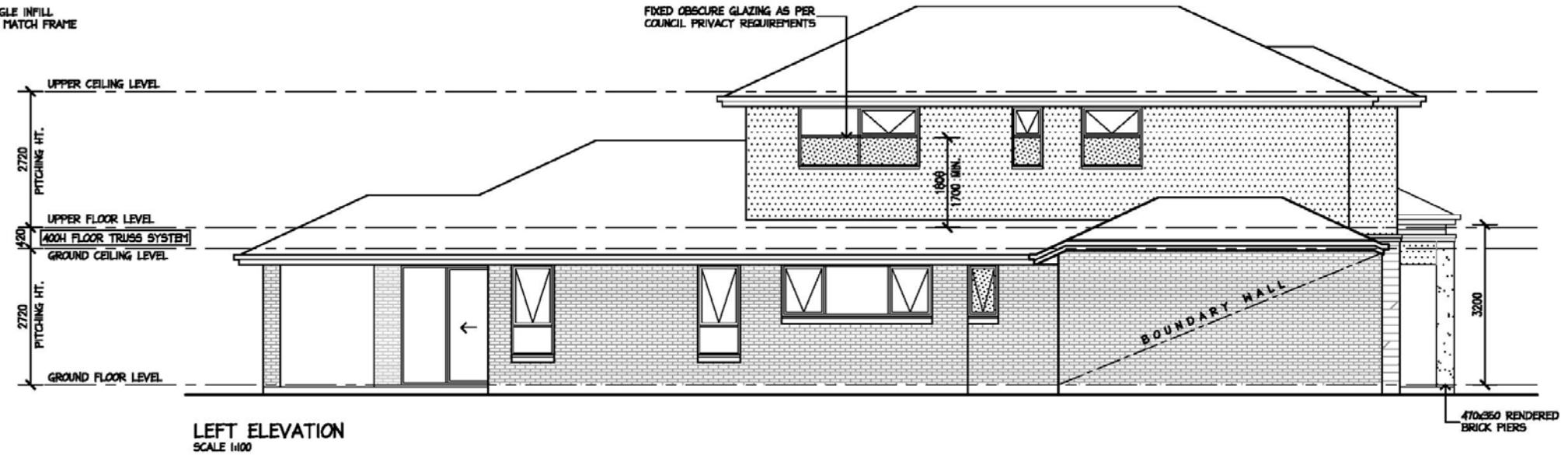
NOTES

OVERALL GARAGE DOOR OPENING  
CLEARANCE MAY BE REDUCED TO SUIT  
ROLLER DOOR DRUM

UPPER FLOOR AWNING & SLIDING  
WINDOWS WILL BE RESTRICTED  
TO 125mm OPENING WIDTH

TAPERED SILLS TO LIGHTWEIGHT WALLS

IF REQUIRED - PROVIDE ANGLE INFILL  
ABOVE DOORS/WINDOWS TO MATCH FRAME  
(CHECK ON SITE)



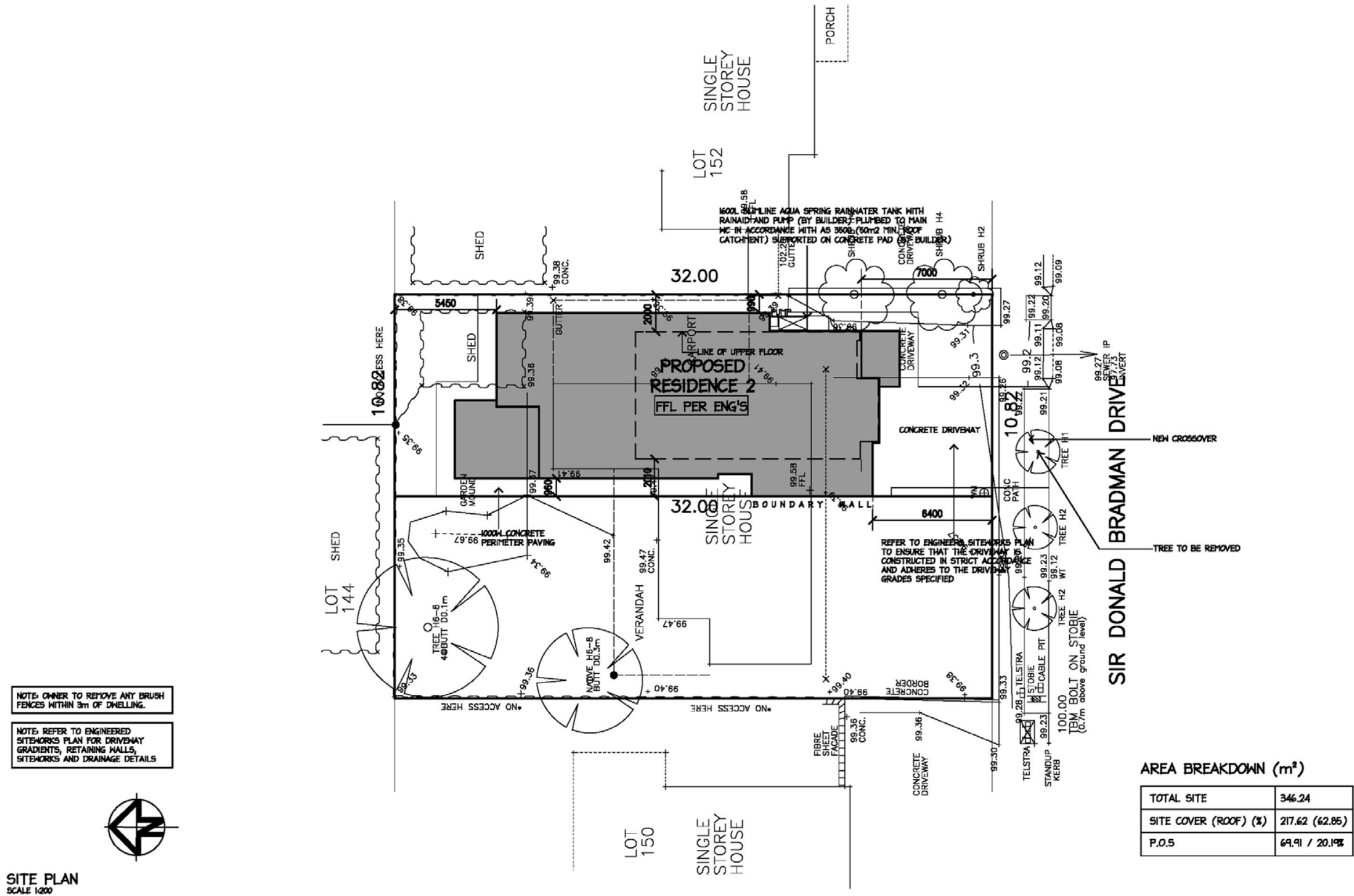
SHEET 5 OF 5 PRELIMINARY WORKING DRAWINGS		PROPOSED NEW HOME FOR: McDOUGALL FAMILY TRUST  AT NO. 576 (RES 1) SIR DONALD BRADMAN DRIVE LOCKLEYS	AMENDMENT DATES			SPECIAL NOTES		AREAS m <sup>2</sup>	<div><div>B</div><div>BRADFORD HOMES</div></div> <div>474B ANZAC HIGHWAY, CAMDEN PARK SA 5048 WWW.BRADFORDHOMES.COM.AU (08) 8350 2950</div>
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SHEET SIZE: A3						AS ALUM. HORIZONTAL SLIDING AAWN ALUM. AWNING TAWN TIMBER AWNING ASH ALUM. SINGLE HUNG TSH TIMBER SINGLE HUNG ADH ALUM. DOUBLE HUNG TDH TIMBER DOUBLE HUNG AF ALUM. FIXED SASH TF TIMBER FIXED SASH			
DATE: 27.11.18						- THIS DRAWING IS TO BE READ STRICTLY IN CONJUNCTION WITH BUILDING & SELECTION SCHEDULES			
DRAWN: LT						- FIGURED DIMENSIONS SHALL TAKE PREFERENCE OVER SCALED DIMENSIONS.			
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OWNER: \_\_\_\_\_

DATE: \_\_\_\_\_

BUILDER: \_\_\_\_\_

DATE: \_\_\_\_\_



SHEET 1 OF 5 PRELIMINARY WORKING DRAWINGS		PROPOSED NEW HOME FOR: McDOUGALL FAMILY TRUST  AT NO. 576 (RES 2) SIR DONALD BRADMAN DRIVE LOCKLEYS	AMENDMENT DATES			SPECIAL NOTES		AREAS m <sup>2</sup>	<div><div>B</div><div>BRADFORD HOMES</div></div> <div><div>A</div>4748 ANZAC HIGHWAY, CAMDEN PARK SA 5048 <div>WWW.BRADFORDHOMES.COM.AU</div><div>(08) 8350 2950</div></div>
			REVISION	DATE	COMPLETED BY	WINDOW LEGEND			
Job No. 80092		SHEET SIZE: A3	A	10.12.18	LT	AS ALUM. HORIZONTAL SLIDING ALUM. AWNING TIMBER AWNING ASH ALUM. SINGLE HUNG TSH TIMBER SINGLE HUNG ADH ALUM. DOUBLE HUNG TDH TIMBER DOUBLE HUNG AF ALUM. FIXED SASH TF TIMBER FIXED SASH		- THIS DRAWING IS TO BE READ STRICTLY IN CONJUNCTION WITH BUILDING & SELECTION SCHEDULES  - FIGURED DIMENSIONS SHALL TAKE PREFERENCE OVER SCALED DIMENSIONS.	
DATE: 27.11.18	DRAWN: LT							<div>©</div> COPYRIGHT. These drawings are copyright and remain the exclusive property of Bradford Homes. Reproduction of the whole or any part of these drawings without written permission is prohibited	







OWNER:

DATE:

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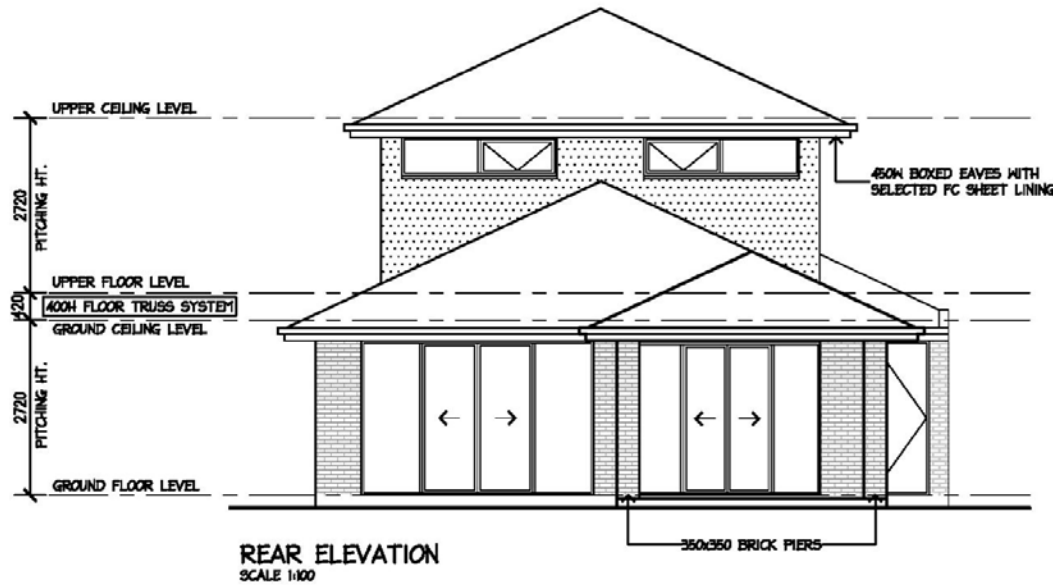
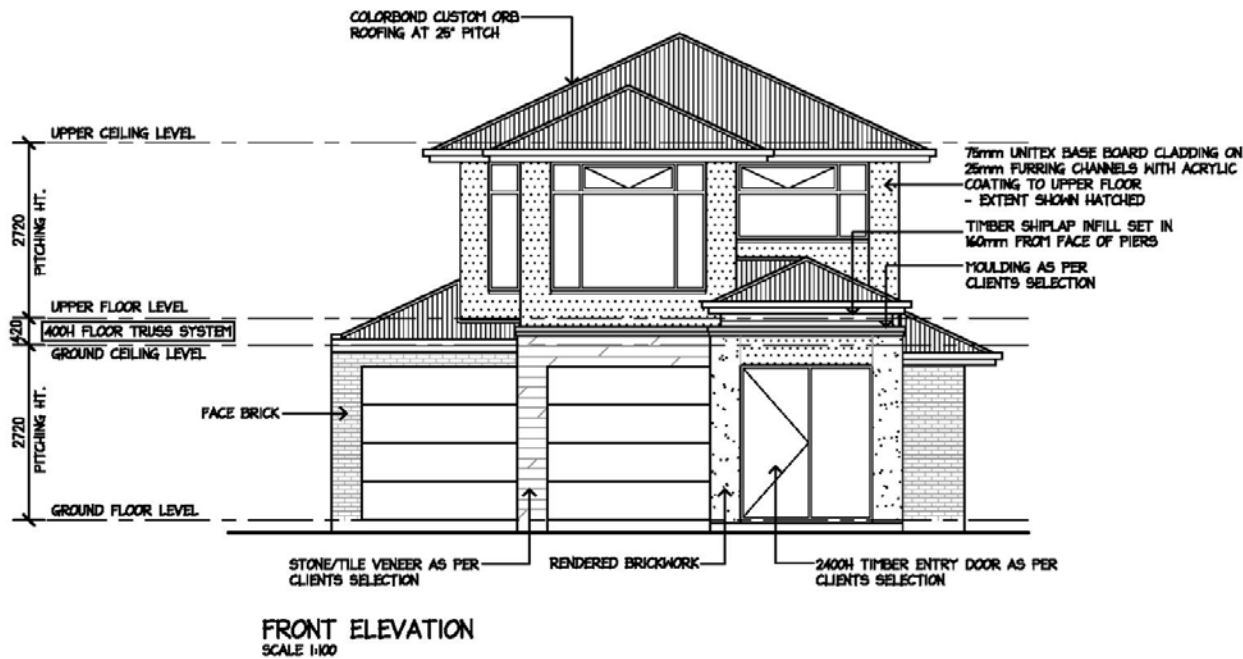
NOTES

OVERALL GARAGE DOOR OPENING CLEARANCE MAY BE REDUCED TO SUIT ROLLER DOOR DRUM

UPPER FLOOR AWNING & SLIDING WINDOWS WILL BE RESTRICTED TO 126mm OPENING WIDTH

TAPERED SILLS TO LIGHTWEIGHT WALLS

IF REQUIRED - PROVIDE ANGLE INFILL ABOVE DOORS/WINDOWS TO MATCH FRAME (CHECK ON SITE)



SHEET 4 OF 5 PRELIMINARY WORKING DRAWINGS		PROPOSED NEW HOME FOR: McDOUGALL FAMILY TRUST  AT NO. 576 (RES 2) SIR DONALD BRADMAN DRIVE LOCKLEYS	AMENDMENT DATES			SPECIAL NOTES		AREAS m <sup>2</sup>	<div><div>B</div><div>BRADFORD HOMES</div></div> <div><div>A</div><div>W</div><div>P</div></div> <div>474B ANZAC HIGHWAY, CAMDEN PARK SA 5043 WWW.BRADFORDHOMES.COM.AU (08) 8350 2950</div>
			REVISION	DATE	COMPLETED BY	WINDOW LEGEND			
Job No. 80092		SHEET SIZE: A3	A	10.12.18	LT	AS AAWN TAWN ASH TSH ADH TDH AF TF	ALUM. HORIZONTAL SLIDING ALUM. AWNING TIMBER AWNING ALUM. SINGLE HUNG TIMBER SINGLE HUNG ALUM. DOUBLE HUNG TIMBER DOUBLE HUNG ALUM. FIXED SASH TIMBER FIXED SASH	<div><div>- THIS DRAWING IS TO BE READ STRICTLY IN CONJUNCTION WITH BUILDING &amp; SELECTION SCHEDULES</div><div>- FIGURED DIMENSIONS SHALL TAKE PREFERENCE OVER SCALED DIMENSIONS.</div></div>	
DATE: 27.11.18	DRAWN: LT							<div>© COPYRIGHT. These drawings are copyright and remain the exclusive property of Bradford Homes. Reproduction of the whole or any part of these drawings without written permission is prohibited</div>	



DATE:

BUILDER:

DATE:

OWNER:

DATE:

OWNER:

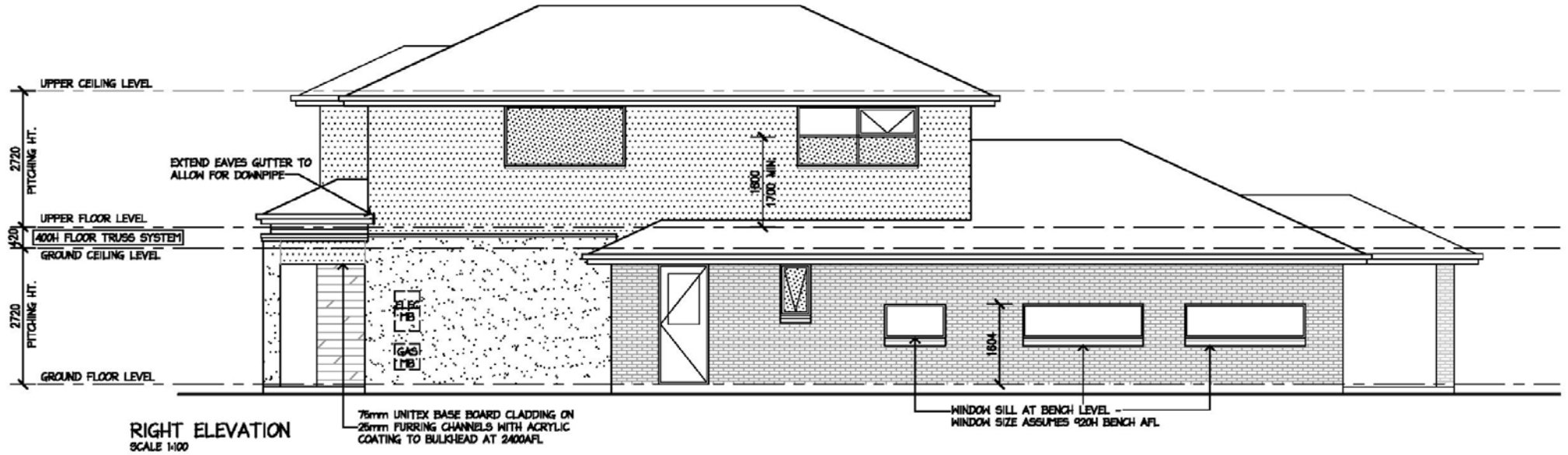
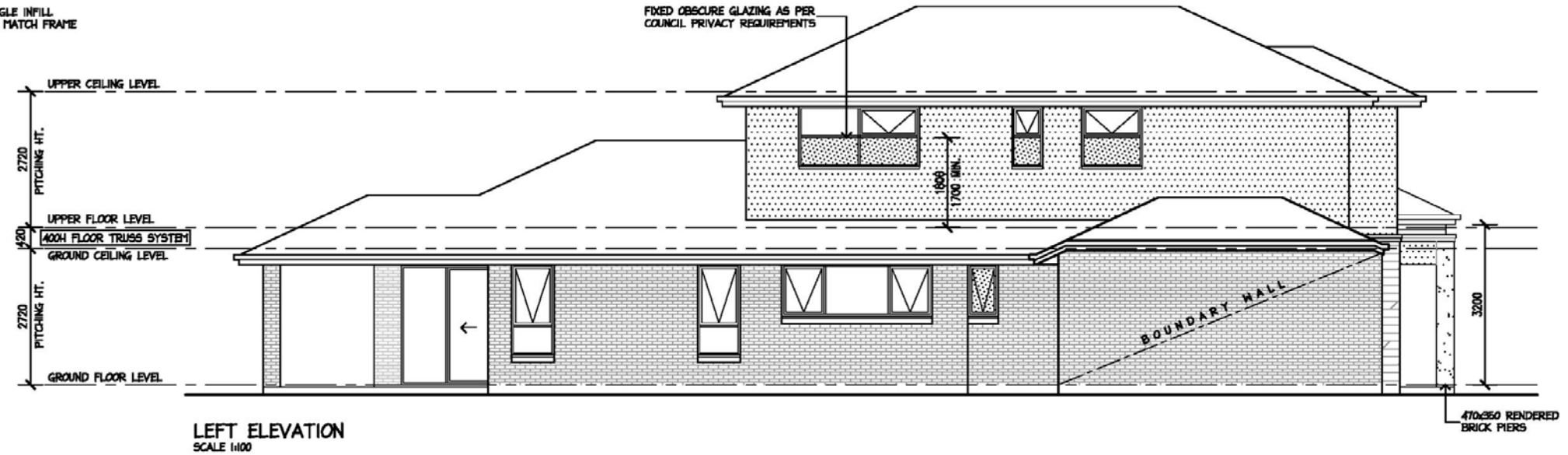
NOTES

OVERALL GARAGE DOOR OPENING CLEARANCE MAY BE REDUCED TO SUIT ROLLER DOOR DRUM

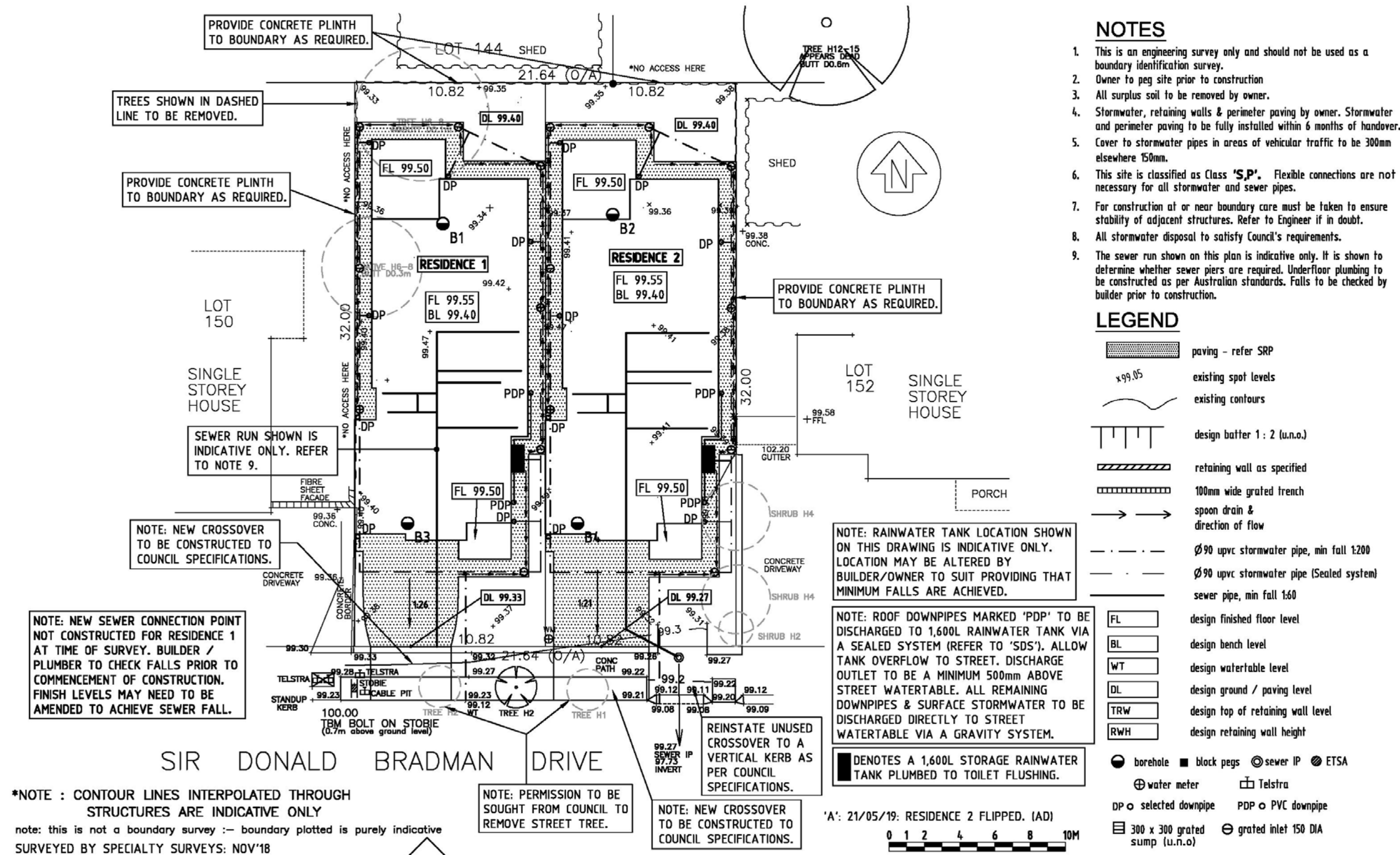
UPPER FLOOR AWNING & SLIDING WINDOWS WILL BE RESTRICTED TO 125mm OPENING WIDTH

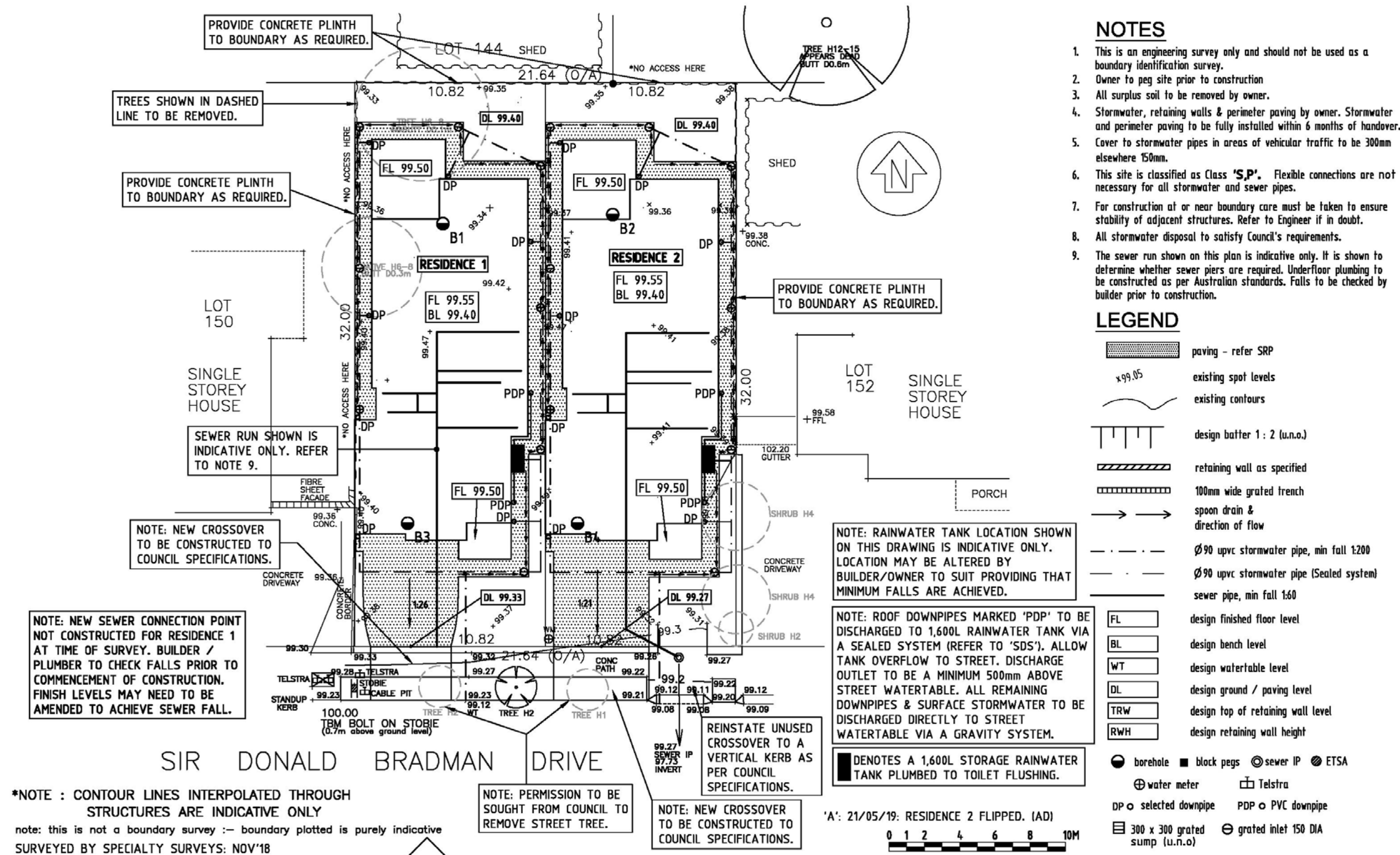
TAPERED SILLS TO LIGHTWEIGHT WALLS

IF REQUIRED - PROVIDE ANGLE INFILL ABOVE DOORS/WINDOWS TO MATCH FRAME (CHECK ON SITE)



SHEET 5 OF 5 PRELIMINARY WORKING DRAWINGS		PROPOSED NEW HOME FOR: McDOUGALL FAMILY TRUST		AMENDMENT DATES		SPECIAL NOTES		AREAS m <sup>2</sup>	<div><div>B</div><div>BRADFORD HOMES</div></div> <div><div>A</div>474B ANZAC HIGHWAY, CAMDEN PARK SA 5048</div> <div><div>W</div>WWW.BRADFORDHOMES.COM.AU</div> <div><div>P</div>(08) 8350 2950</div>
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**GINOS ENGINEERING** PTY LTD  
Consulting Engineers  
A.B.N. 22 621 716 121  
452 Pulteney Street  
Adelaide SA 5000  
Telephone: (08) 8212 4900  
Facsimile: (08) 8212 3829

**BRADFORD HOMES**  
DWELLING FOR: McDOUGALL FAMILY TRUST  
NO 576 SIR DONALD BRADMAN DRIVE - LOCKLEYS (RESIDENCE 2)  
**SITE AND DRAINAGE PLAN**

Drawn	AD	Scales	1:200
Design	AD	Drawing Number	
Approved	SK		33287
Date	JAN'19		SR2-1/A





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# External Colour Selections



<b>Consultant:</b>	Catherine	<b>Date:</b>	04.02.19
<b>Client Name:</b>	McDougall Family Trust	<b>Phone:</b>	0420 959 549
<b>Job Number:</b>	80091	<b>Email:</b>	trevjanmcd@adam.com.au
<b>Building Address:</b>	No 576 (Res 1) Sir Donald Bradman Drive Lockleys		

## BRICK / RENDER SELECTION:

Main Brick:	Seed
Joint Type:	Raked
Mortar Colour <sup>1</sup> :	Grey
Render Colour:	Colorbond Surfmist

## ROOF, GUTTERS and WINDOWS:

Roofing Material Colorbond / Tiled:	Colorbond
Colour:	Monument
Gutter Colour:	Monument
Fascia Colour:	Surfmist
Window / Sliding Doors Colour:	Surfmist

## EXTERNAL PAINT COLOURS:

Front Door:	Stained Timber*
Laundry Door:	Colorbond Surfmist
Down Pipes & Meter Box:	Colorbond Surfmist
Eaves/ Linings:	Colorbond Surfmist

## GARAGE AND CARPORT DOORS:

Front Panel Lift Colour:	Timber Look 'Jatoba'*
Panel Lift Profile:	Seville

\* Refer to your builder for upgrade queries & costing\*

<sup>1</sup> Clay bricks are a natural kiln fired product and as such are subject to variation and chipping. All endeavours are made to match the colour of the clay products on display and minimize chipping, but some variance may occur.

<sup>2</sup> Mortar is a hand mixed product and is therefore subject to variation.

Client Signature: \_\_\_\_\_

Client Name: \_\_\_\_\_

Date: \_\_\_\_\_

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Phone: 08 8443 2222

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# External Colour Selections



<b>Consultant:</b>	Catherine	<b>Date:</b>	04.02.19
<b>Client Name:</b>	McDougall Family Trust	<b>Phone:</b>	0420 959 549
<b>Job Number:</b>	80092	<b>Email:</b>	trevjanmcd@adam.com.au
<b>Building Address:</b>	No 576 (Res 2) Sir Donald Bradman Drive Lockleys		

## BRICK / RENDER SELECTION:

Main Brick:	Chiffon
Joint Type:	Raked
Mortar Colour <sup>1</sup> :	Grey
Render Colour:	Colorbond Surfmist

## ROOF, GUTTERS and WINDOWS:

Roofing Material Colorbond / Tiled:	Colorbond
Colour:	Monument
Gutter Colour:	Monument
Fascia Colour:	Surfmist
Window / Sliding Doors Colour:	Monument

## EXTERNAL PAINT COLOURS:

Front Door:	Colorbond Surfmist
Laundry Door:	Colorbond Surfmist
Down Pipes & Meter Box:	Colorbond Surfmist where located against render, Solver Chimeres where located against brickwork
Eaves/ Linings:	Colorbond Surfmist

## GARAGE AND CARPORT DOORS:

Front Panel Lift Colour:	Surfmist
Panel Lift Profile:	Nullabor Woodgrain

\* Refer to your builder for upgrade queries & costing\*

<sup>1</sup> Clay bricks are a natural kiln fired product and as such are subject to variation and chipping. All endeavours are made to match the colour of the clay products on display and minimize chipping, but some variance may occur.

<sup>2</sup> Mortar is a hand mixed product and is therefore subject to variation.

Client Signature:

Client Name:

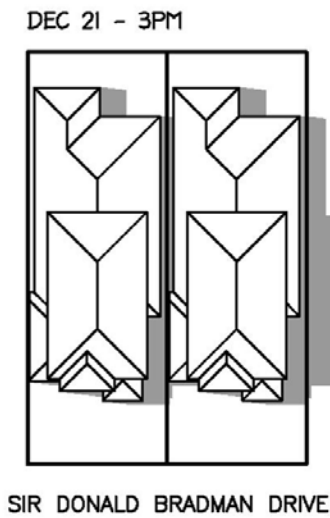
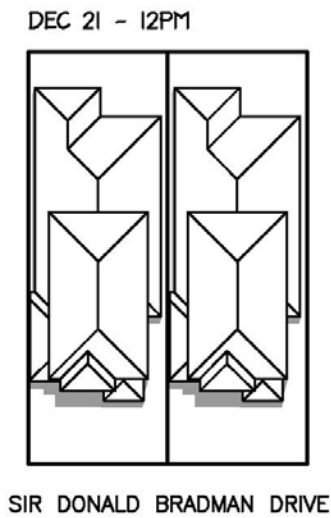
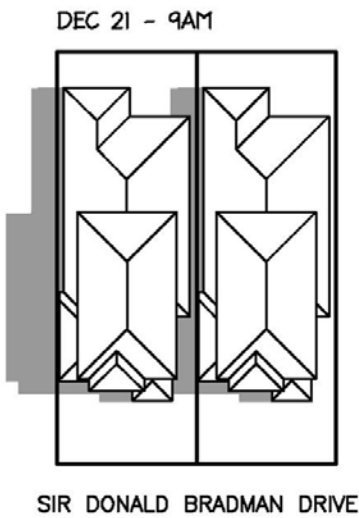
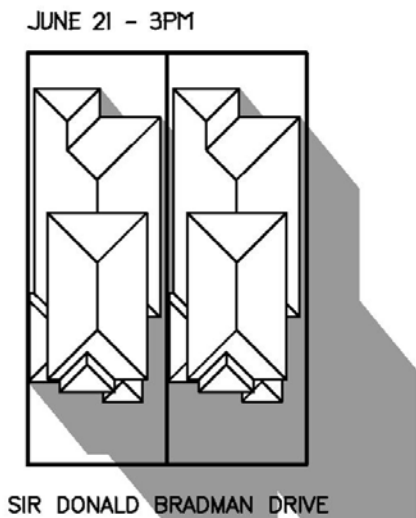
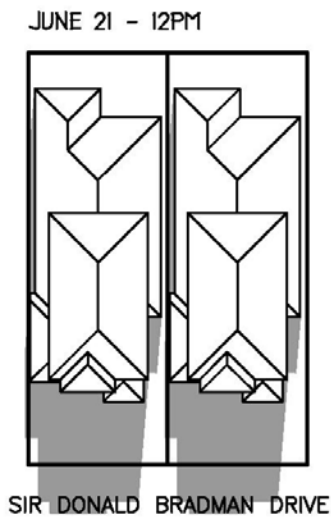
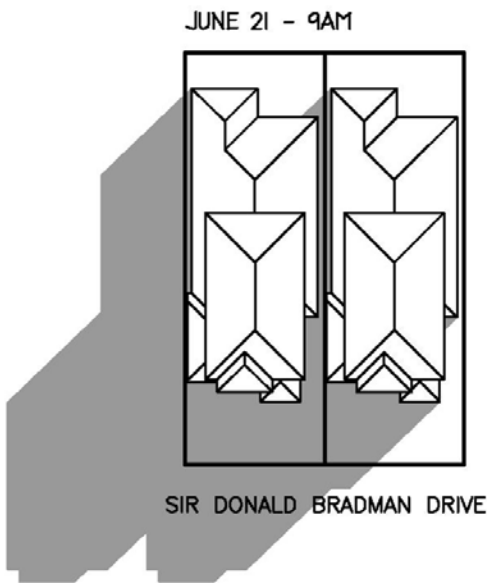
Date:

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SHADOW DIAGRAMS  
SCALE 1:500



B

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HOMES

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P (08) 8350 2950

Client:  
McDOUGALL FAMILY TRUST

Project:  
PROPOSED NEW DWELLINGS

At:  
NO 576, RESIDENCE 1+2  
SIR DONALD BRADMAN DRIVE  
LOCKLEYS

Designer:  
TD

Drawn by:  
TD

Date:  
26.04.19

Issue:

Sheet:  
1 OF 1

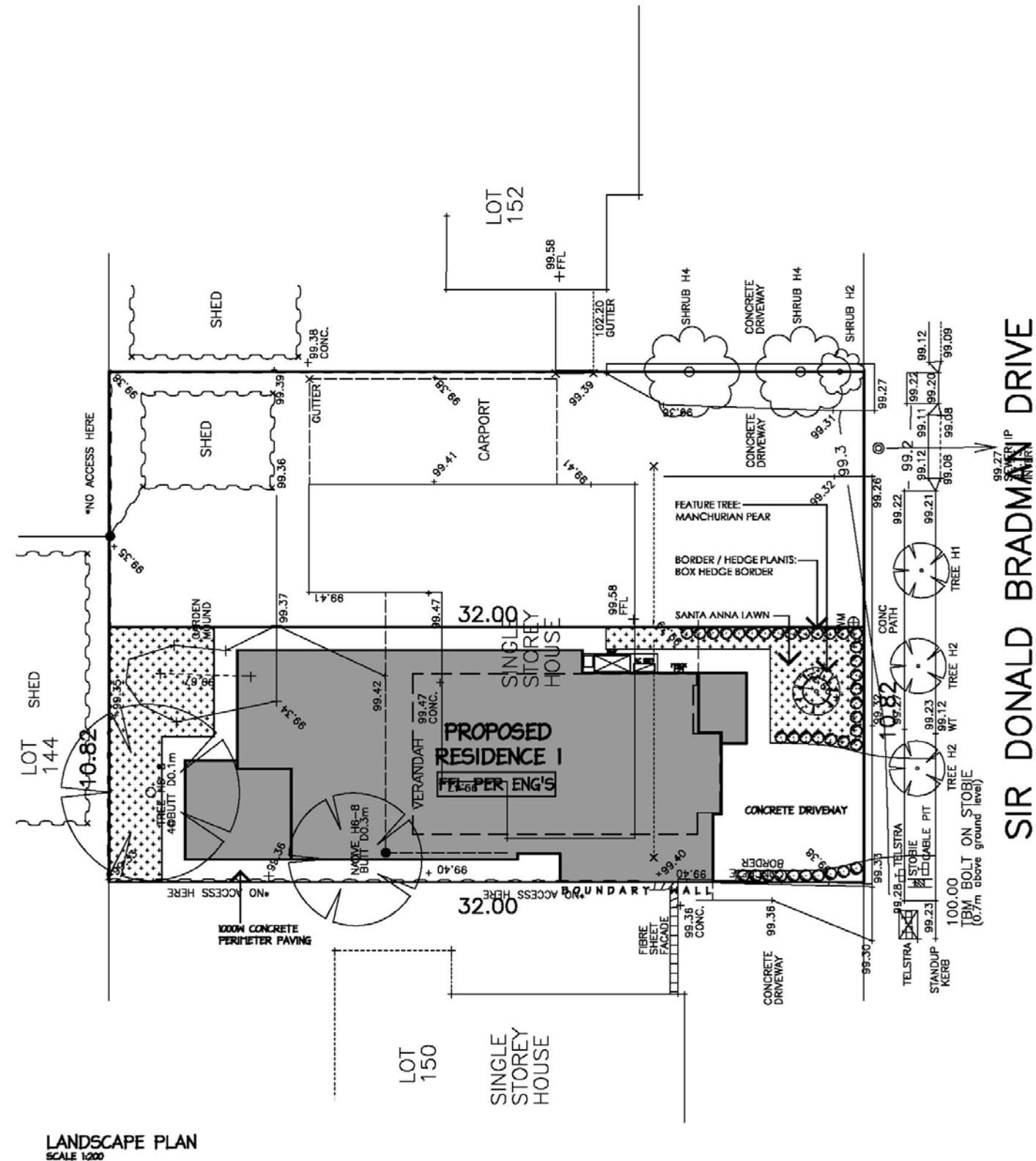
Job Number:  
80092-80093

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No Further Changes

Sign:.....

Date:.....



B

BRADFORD  
HOMES

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W WWW.BRADFORDHOMES.COM.AU  
P (08) 8350 2950

Client:  
McDOUGALL FAMILY TRUST

Project:  
PROPOSED NEW RESIDENCE

At:  
NO 576 (RES 1)  
SIR DONALD BRADMAN DRIVE  
LOCKLEYS

Designer:  
MW

Drawn by:  
MW

Date:  
27.05.19

Issue:  
B

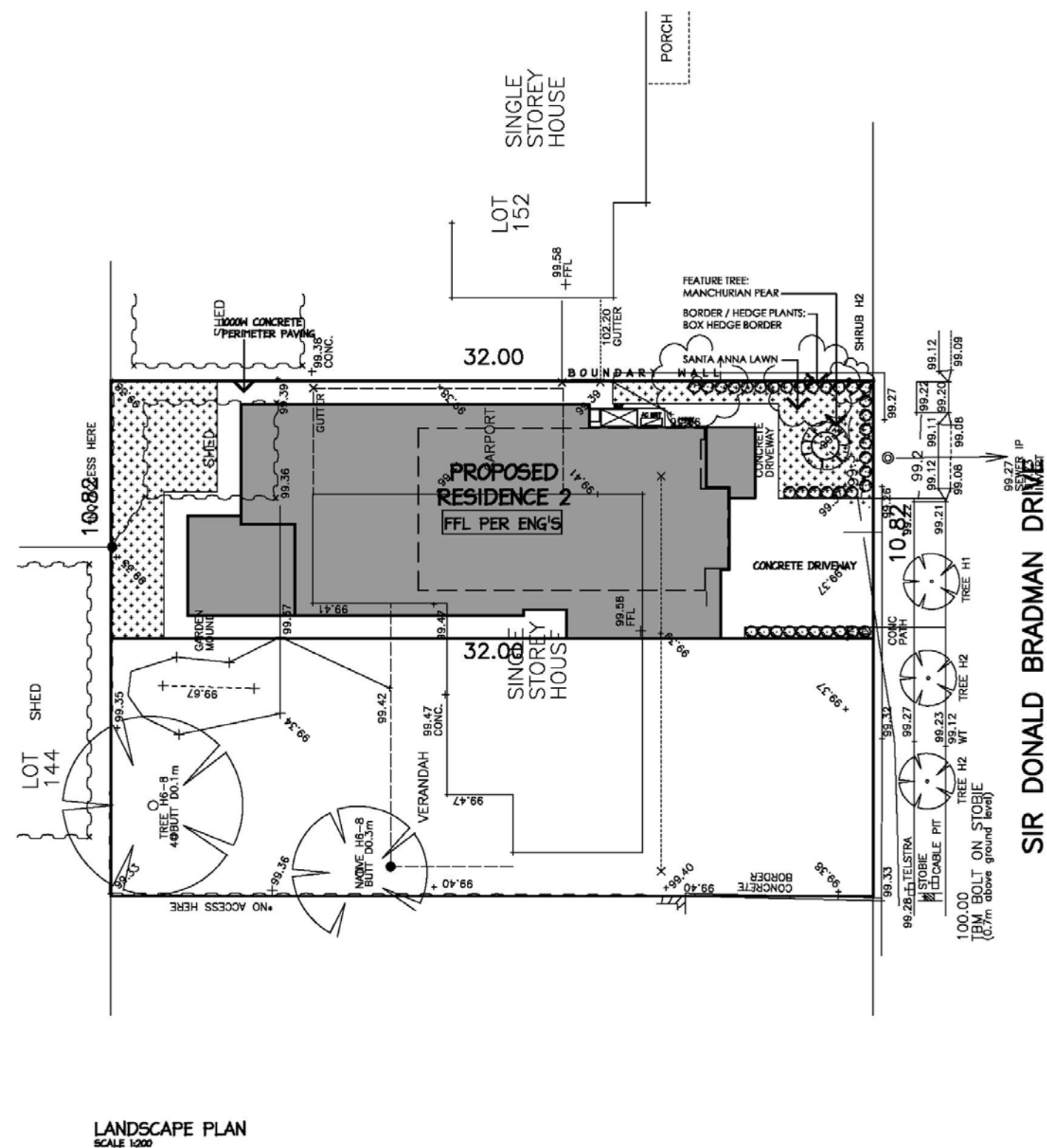
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11 June 2019

Page 204



B

BRADFORD  
HOMES

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P (08) 8350 2950

Client:  
McDOUGALL FAMILY TRUST

Project:  
PROPOSED NEW RESIDENCE

At:  
NO 576 (RES 2)  
SIR DONALD BRADMAN DRIVE  
LOCKLEYS

Designer:  
MW

Drawn by:  
MW

Date:  
27.05.19

Sheet:  
1 OF 1

Issue:  
B

Job Number:  
80092

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## **Preliminary Traffic, Flooding & Stormwater Assessment**

**Development Application No: 211/251/2019**

**Assessing Officer:** Sonia Gallarello  
**Site Address:** 576 Sir Donald Bradman Drive, LOCKLEYS SA 5032  
**Certificate of Title:** CT-5590/157  
**Description of Development:** Combined Application: Land division - Torrens Title; SCAP No. DA. 211/D027/19 , Create one (1) additional allotment; and construction of two (2) two-storey detached dwellings

### **TO THE TECHNICAL OFFICER - CITY ASSETS**

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Sonia Gallarello**

**DATE 22 March, 2019**



## Memo

To Sonia Gallarello  
From Richard Tan  
Date 22-Mar-2019  
Subject 211/251/2019, 576 Sir Donald Bradman Drive, LOCKLEYS SA 5032

Sonia Gallarello,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

**Note: It is noted that there is contradiction in civil plan and the plans provided by the builder. The crossover location and building layout for Residence 2 does not match each other. The following comments has been provided based on the Civil Plan.**

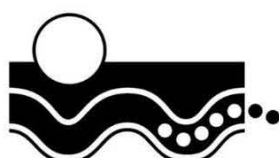
### 1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

- 1.1 In accordance with the provided 'Site and Drainage Plan' (Ginos Engineering, Ref: 33287-SR2-1, dated 01/2019), the FFLs of the proposed development (99.55 minimum) have been assessed as satisfying minimum requirements (99.50) in consideration of street and/or flood level information.

### 2.0 Verge Interaction

- 2.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically desired to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram ramps, etc. (as measured at the front property boundary). An absolute minimum offset of 0.5m from new crossovers and stormwater connections to other existing road verge elements is acceptable in cases where space is limited.



City of **West Torrens**

Between the City and the Sea

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council's Technical Officer (Arboriculture).

**2.1.1** Proposed crossover for Residence 1 clashes with existing street tree. Further assessment from Council's Arboriculture team is required.

**It is recommended that further assessment from Council's Arboriculture team is required.**

**2.1.2** Proposed crossover is less than 1m from existing stobie pole and proposed stormwater connection.

**It is recommended that proposed crossover should have minimum 1m offset distance from existing stobie pole and proposed stormwater connection.**

**2.2** It should also be nominated for the stormwater connection through the road verge area to be constructed of shape and material to satisfy Council's standard requirements

- 100 x 50 x 2mm RHS Galvanised Steel or
- 125 x 75 x 2mm RHS Galvanised Steel or
- Multiples of the above.

**It is recommended that revised plans clearly and accurately indicating satisfaction of the above criteria be provided to Council.**

### **3.0 Traffic Requirements**

**3.1** Current proposed crossover is wider than the maximum 3m single crossover (measured at boundary) which would typically be supported by City Asset. There are no evident circumstances associated with this site arrangement which would support the provision of a wider, non-standard crossing place. A crossover of 3.6m wide without flaring will be supportable.

**It is recommended that revised plans showing a crossover of 3.6m wide without flaring be provided to Council.**





Between the City and the Sea

- 3.2** With the crossover to be amended as per dot point 3.1, one on-street car parking space will be maintained per allotment.
- 3.3** The garage's internal dimension as indicated in 'Ground Floor Plan' (Bradford Homes, Ref: 80091-2/5 & 80092-2/5, dated 27/11/2018) and set back distance as measured in 'Site and Drainage Plan' (Ginos Engineering, Ref: 33287-SR2-1, dated 01/2019) have been assessed as satisfying minimum requirements.

#### **4.0 Waste Management**

- 4.1** The public kerbside space available for bin presentation has been assessed as satisfying minimum requirement.

#### **5.0 Stormwater Management**

- 5.1** Stormwater detention is not required for this development.

Regards  
Richard Tan  
Civil Engineer

## Arboricultural Assessment of Street Trees

Development Application No: **211/251/2019**

**REFERRAL DUE DATE: 5 April 2019**

Assessing Officer: **Sonia Gallarello**

Site Address: **576 Sir Donald Bradman Drive, LOCKLEYS SA 5032**

Certificate of Title: **CT-5590/157**

Description of Development **Combined Application: Land division - Torrens Title; SCAP No. DA. 211/D027/19 , Create one (1) additional allotment; and construction of two (2) two- storey detached dwellings**

### TO THE TECHNICAL OFFICER – CITY ASSETS

**Please provide your comments in relation to:**

- ☐ **The removal of or impact upon the Street Tree**
- ☐ **Species of Tree:**
- ☐ **Your advice is also sought on other aspects of the proposal as follows:**

PLANNING OFFICER - Sonia Gallarello

DATE 22 March 2019

### FROM THE TECHNICAL OFFICER

**I have examined the plans as requested and provide comments as follow.**

As with all development applications it must be proven beyond reasonable doubt that all alternatives have been explored so not to hinder the progress of any street tree(s).

Any proposed development that does not consider "AS4970 Protection of Trees on Development Sites", is likely to require revision until all plans accurately correspond with the specific tree information detailed in this standard.

Verge interaction must consider all services that cross council land including stormwater outlets (and other) which will need to be maintained a minimum of 2.0m from any existing street tree (unless existing or otherwise negotiated).

**A site investigation together with the information provided have revealed there is an existing *Geijera parviflora* (Wilga) street tree in direct conflict with the proposed crossover location for Residence 1.**

**City Operations will support the removal of this existing street tree.**

**For House 2 there is another *Geijera parviflora* (Wilga) street tree located 1.6m outside the proposed western property boundary.**

**In this instance City operations will support this offset from this existing street tree.**

**It is noted on the plan set there is a street tree located in direct conflict with the proposed crossover location for Residence 2.**

**There is no street tree at this location.**

**As a result of the proposed crossovers on Sir Donald Bradman Drive, City Operations has considered the health, structure, form, useful life expectancy, and age of the street tree and will support its removal.**

**A fee of \$226.00 will be required prior to the commencement of any work.**

Please note, under no circumstances is any individuals other than council staff permitted to interfere with a street tree. If pruning etc. is required, council must be notified via the appropriate request, and council staff will perform all works associated with the community asset.

Final crossover locations will be confirmed once appropriate consultation has been received from the applicant and they have submitted "an application to construct a vehicle crossing place(s) across council land".

---

**Rick Holmes  
Arboriculture Assistant  
165 Sir Donald Bradman Drive  
Hilton SA 5033**

**Telephone: 8416 6333  
Fax: 8443 5709**

**DATE: 28/03/19**













Contact Planning Services  
Telephone 7109 7018  
Email [didpdpdc@clearanceletters@sa.gov.au](mailto:didpdpdc@clearanceletters@sa.gov.au)



15 March 2019

The Chief Executive Officer  
City of West Torrens

Dear Sir/Madam

**Re: Proposed Application No. 211/D027/19 (ID 64422)**  
**for Land Division by McDougall Family Trust**

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 05 March 2019, I advise that the State Commission Assessment Panel (SCAP) has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the State Commission Assessment Panel has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.  
On receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non-standard fees.  
On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.
2. Payment of \$7253 into the Planning and Development Fund (1 allotment(s) @ \$7253/allotment).  
Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Department of Planning, Transport and Infrastructure and marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Level 5, 50 Flinders Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

**PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.**

Yours faithfully

A handwritten signature in blue ink, appearing to read 'Biljana B.', is written over the 'Yours faithfully' text.

Biljana Prokic  
Land Division Coordinator - Planning Services  
as delegate of  
STATE COMMISSION ASSESSMENT PANEL



15 March 2019

Our Ref: H0083088

The Chairman  
State Commission Assessment Panel  
50 Flinders St  
ADELAIDE SA 5000

Dear Sir/Madam

**PROPOSED LAND DIVISION APPLICATION NO: 211/D027/19 AT LOCKLEYS**

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

On receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non -standard fees.

On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

Yours faithfully

CAROL CARY

**for MANAGER LAND DEVELOPMENT & CONNECTIONS**

SA Water  
Level 6, 250 Victoria Square  
ADELAIDE SA 5000  
Ph (08) 7424 1119  
Inquiries CAROL CARY  
Telephone 7424 1119

## 6.4 24 Portland Street, FULHAM

Application No 211/190/2019 (SCAP 211/D020/19)

### DEVELOPMENT APPLICATION DETAILS

DESCRIPTION OF DEVELOPMENT	Combined Application: Land division - Torrens Title; Create one (1) additional allotment; and construction of two detached dwellings
APPLICANT	Peter and Todd Keough
LODGEMENT DATE	15 February 2019
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 21
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> <li>City Assets</li> </ul> External <ul style="list-style-type: none"> <li>State Commission Assessment Panel (SCAP)</li> <li>South Australian Water Corporation (SA Water)</li> </ul>
DEVELOPMENT PLAN VERSION	12 July 2018
DELEGATION	<ul style="list-style-type: none"> <li>The relevant application proposes a merit form of development which does not meet the minimum site area requirements in the relevant Zone or Policy Area by 7.5% or more.</li> </ul>
RECOMMENDATION	Support with conditions
AUTHOR	Sonia Gallarello

### SUBJECT LAND AND LOCALITY

The subject land is formally described as Allotment 434 in Deposited Plan 6148 in the area named Fulham, Hundred of Adelaide, Volume 5656 Folio 513, more commonly known as 24 Portland Street, Fulham. The subject site is irregular in shape with a 27.4 metre (m) wide frontage to Portland Street and a site area of 767 square metres (m<sup>2</sup>).

There is a sewerage easement to the South Australia Water Corporation that exists on the adjacent allotment to the rear of the subject land.

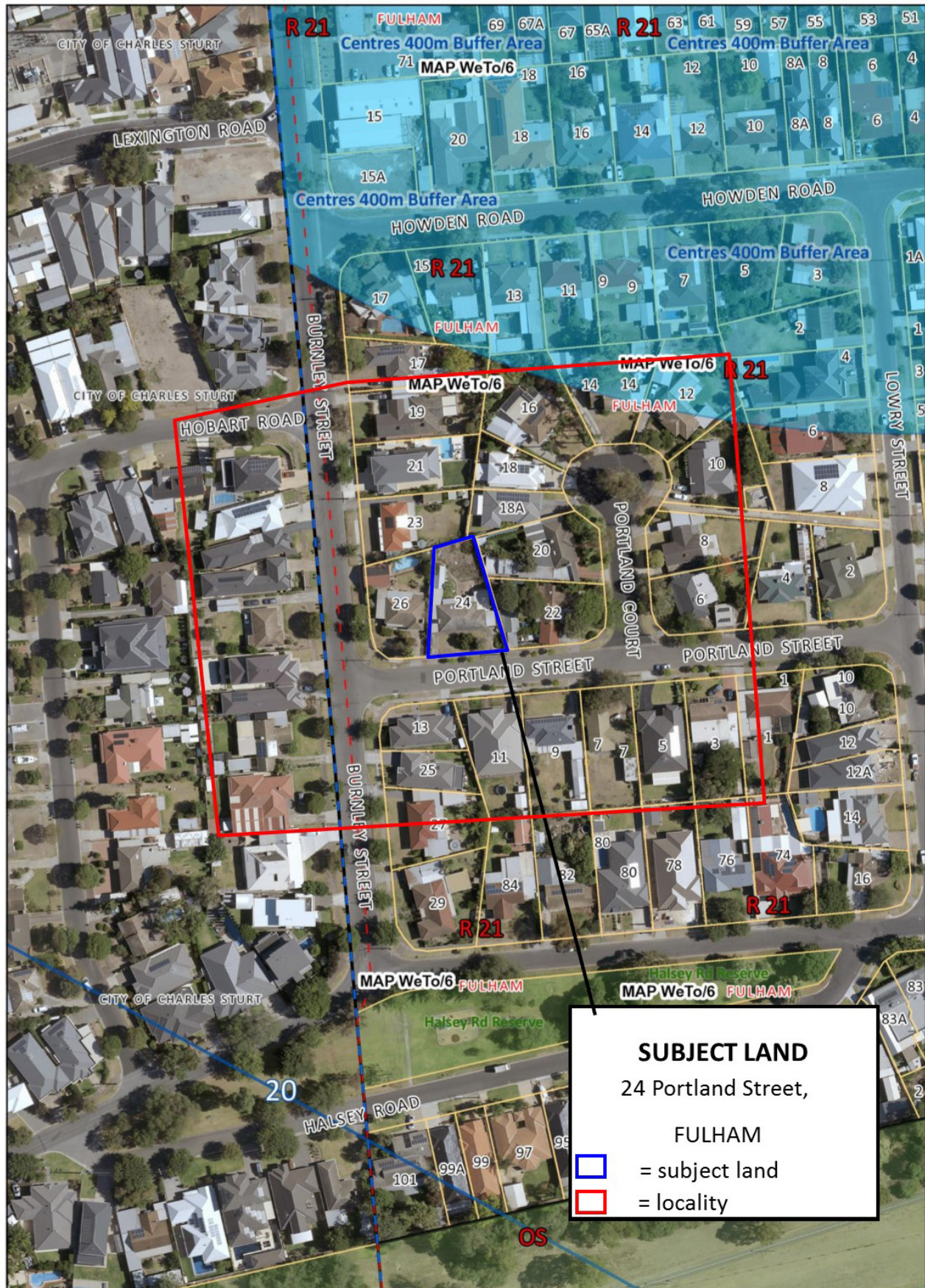
The site currently contains a single storey detached dwelling with attached carports, verandah and outbuilding. The site is relatively flat. There are no regulated trees on the subject site or on adjoining land that would be affected by the development.

The locality consists of detached dwellings at low densities and wide street frontages, although there is evidence of some smaller allotments that have recently been developed. The boundary with the City of Charles Sturt is 50m to the west of the subject land and there appears to be a more dense pattern of division in that council area. The subject land is 60m from the 400m Centre Zone buffer area, 240m north of the River Torrens, 400m south of Henley Beach Road and 800m west of Tapleys Hill Road. There is moderate accessibility to public transport in this location.

The amenity of the locality is moderate to high with well-maintained dwellings (some relatively new) and landscaping on the verge and in front yards.

The subject land and locality are shown on the aerial imagery and maps below.









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## PROPOSAL

This is a combined application seeking Development Plan Consent and Land Division Consent for a combined development comprising the following:

- Torrens Title land division creating a total of two allotments (one additional);
- Construction of two single storey detached dwellings.
- Proposed dwelling 1 has a single garage, 3 bedrooms, study, bathroom and open plan kitchen, dining room and family room.
- Proposed dwelling 2 has a single garage, 3 bedrooms, living room, bathroom and open plan kitchen, dining room and family room.

The relevant plans and documents are contained in **Attachment 2**.

## PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Schedule 9, Part 1 (2)(a)(i) and Schedule 9, Part 1 (f) of the *Development Regulations 2008*.

As the proposal is Category 1, public notification was not required to be undertaken.

## INTERNAL REFERRALS

Department	Comments
City Assets	<ul style="list-style-type: none"><li>• The only issue raised was that the applicant shall seek the easement holder's permission to encroach over an easement.</li></ul> <p>In this case the easement is held by SA Water and the placement of pavers over the easement has been deemed as acceptable.</p>

## EXTERNAL REFERRALS

Department	Comments
SCAP	No concerns were raised by the SCAP and standard conditions have been recommended should the application be supported.
SA Water	SA Water raised no concerns with the proposal and have recommended standard conditions should the application be supported.

A copy of the relevant referral responses are contained in **Attachment 3**.



## RELEVANT DEVELOPMENT PLAN PROVISIONS

The subject land is located within the Residential Zone and, more specifically, Low Density Policy Area 21 as described in the West Torrens Council Development Plan.

The relevant Desired Character statements are as follows:

Residential Zone - Desired Character	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objectives	1, 2, 3, 4
Principles of Development Control	1, 5, 6, 7, 8, 11, 12, 13

Low Density Policy Area 21 - Desired Character	
<p><i>This policy area will have a low density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).</i></p> <p><i>There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage. In the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park, where the consistent allotment pattern is a significant positive feature of the locality, subdivision will reinforce the existing allotment pattern.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings. Buildings in the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park will be complementary to existing dwellings through the incorporation of design features such as pitched roofs, eaves and variation in the texture of building materials.</i></p>	

*Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.*

Objectives	1
Principles of Development Control	1, 2, 3

Additional provisions of the Development Plan which relate to the proposed development are contained in **Attachment 1**.

### QUANTITATIVE STANDARDS

The proposal is assessed for consistency with the quantitative requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
ALLOTMENT AREA <i>Low Density Policy Area 21</i> <i>PDC 3</i>	420m <sup>2</sup> (min.)	364m <sup>2</sup> (lot 1) 404m <sup>2</sup> (lot 2)  <b>Does Not Satisfy</b>
ALLOTMENT FRONTAGE <i>Low Density Policy Area 21</i> <i>PDC 3</i>	12m (min.)	12m (lot 1) 15.4m (lot 2)  <b>Satisfies</b>
PRIMARY STREET SETBACK <i>Residential Zone</i> <i>PDC 8</i>	4m (average setback of adjacent buildings)	4m (dwellings 1 & 2)  <b>Satisfies</b>
SIDE SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	0/1m (min.)	<u>Dwelling 1</u> Eastern = 1m Western = 0.9m  <u>Dwelling 2</u> Eastern = 1m Western = 0.9m  <b>Does not Satisfy</b>
REAR SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	3m (min.)	5.8m (dwelling 1) 13.6m (dwelling 2)  <b>Satisfies</b>

PRIVATE OPEN SPACE <i>Residential Development</i> PDC 19	60m <sup>2</sup> (min. area) 4m (min. dimension) 16m <sup>2</sup> (min. area accessible from a habitable room).	<u>Dwelling 1</u> 78m <sup>2</sup> (area) 5.8m (dimension) 69m <sup>2</sup> (accessible)  <u>Dwelling 2</u> 117m <sup>2</sup> (area) 4.8m (dimension) 117m <sup>2</sup> (accessible)  <b>Satisfies</b>
STORAGE <i>Residential Development</i> PDC 31	8m <sup>3</sup> (min.)	9.2m <sup>3</sup> (dwelling 1) 8.1m <sup>3</sup> (dwelling 2)  <b>Satisfies</b>
LANDSCAPING <i>Landscaping, Fencing and Walls</i> PDC 4	10% (min.)	28%  <b>Satisfies</b>

## ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development is discussed under the following sub headings:

### Land Use

Residential development, including detached dwellings, is listed as an envisaged form of development within the Policy Area. Therefore the proposed land use is considered to be appropriate and consistent with Principle of Development Control (PDC) 1 of the Low Density Policy Area 21.

### Desired Character

The desired character statement for the Zone and Policy Area seeks low density residential development. A net density of 26 dwellings per hectare is achieved. As such, the proposal achieves the desired dwelling type and density within the Policy Area.

The desired character statement of the Policy Area seeks buildings up to two storeys in height, a pattern of rectangular allotments and landscaping about the built form. The two dwellings are single storey, the two allotments are not rectangular but provide reasonable frontage and incorporate around 28% landscaping around the dwellings.

Accordingly, the development meets the intent of the desired character, Objective 1 and PDC 2 of the Policy Area.

### Allotment area and frontage

The proposed frontage widths meet the Development Plan requirement.

The allotment areas fall short of the 420m<sup>2</sup> sought for land divisions within this policy area. However the development does meet the intent of the policy area by providing two low density allotments to accommodate associated housing. The dwellings meet the majority of the quantitative requirements, including relatively generous private open space areas and do not, as a result, appear to be compromised despite the smaller allotment sizes.



As noted, the subject land is 60m from the 400m Centre Zone buffer, and is sited 400m from Henley Beach Road and around 800m from Tapleys Hill Road. The site is in close proximity to the airport land which has a number of supermarkets and other convenience stores. The land is within 1km of accessible public transport and in close proximity to both the public open space along the River Torrens and West Beach. Accordingly, the subject land is accessible to shops, public transport and public open space.

Given these considerations, the proposed allotment areas and frontages are deemed acceptable for this development.

### **Pattern of Development**

The majority of the allotments in the locality are low density. There are a number of newer developments, including divisions at 18 Portland Court, 25 Burnley Street and 12 Lowry Street, which have resulted in some smaller allotments, although these appear more frequently on land to the west in the City of Charles Sturt. All of the examples of newer development in the locality are similar in site area and frontage to the proposed development.

As a result, it is considered that the proposed development would not be out of character with existing development in the locality and would maintain site areas and built form considered to be low density.

### **Built Form**

Both dwellings are fairly modest in size, design and proportion. Dwelling 1 contains 450mm wide eaves where not set on the boundary. The colorbond® roof is pitched at 25 degrees with a brick façade, aluminium windows, a single panel lift door and small porch surrounding bedroom 1. Dwelling 2 also has 450mm wide eaves, roof pitch of 25 degrees with a wider front elevation that contains a small portico over the entrance and a series of vertically proportioned windows facing the street.

The designs of the dwellings are different and tailored in a way to suit the differently shaped allotments. Materials and colours differ slightly to create two distinguishable dwellings that appear as modern and similar.

There is a large expanse of landscaping about the dwellings which will assist in reducing heat loads while providing some additional amenity for future residents.

### **Setbacks**

All setbacks comply with the Development Plan provisions other than minor shortfalls in western side setbacks. As dwelling 1 is staggered to the western boundary, the setback is often greater than the 0.9m minimum. Dwelling 2 similarly provides a side setback of 0.9m from the boundary fence which is considered to be a minor departure from the Development Plan provision, especially as it meets the Building Code requirement.

### **Landscaping**

There is a high degree of landscaping about these two dwellings, the majority of which is lawn. The landscaping plan is reasonably detailed with edge plantings of grasses in the rear yard and ornamental pears to the rear of the sites. The frontage includes proposed plantings of lilly pillis and one ornamental pear within the front yard of each dwelling. Accordingly, the landscaping achieves PDC 4 of the Landscaping, Fences and Walls module and will assist in softening the appearance of the built form to the street.

## SUMMARY

The development provides for two low density allotments with single storey dwellings that meet the majority of the Development Plan quantitative provisions while presenting well to the street in terms of built form, colours, materials, setbacks and landscaping.

Having considered all the relevant provisions of the Development Plan, the proposal is not considered to be seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 12 July 2018 and warrants Development Plan Consent and Land Division Consent.

## RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent and Land Division Consent for Application No. 211/190 /2019 by Peter and Todd Keough to undertake the combined application: Land division - Torrens Title; SCAP No. 211/D020/19; Create one (1) additional allotment; and construction of two detached dwellings at 24 Portland Street, Fulham (CT5656/513) subject to the following conditions of consent:

### Development Plan Consent Conditions:

1. The development shall be undertaken, completed and maintained in accordance with the following plans and information detailed in this application except where varied by any condition listed below:
  - a) Survey Plan by Zaina Stacey Development Consultants, Ref 18142, Revision C;
  - b) Location Plan, site plan, elevations by ET Design Sheets 1 to 6;
  - c) Siteworks plan by Residential Commercial Industrial Job No C26665, Sheets 1 & 2.

*Reason: To ensure the proposal is developed in accordance with the plans and documents lodged with Council.*
2. Prior to the occupation or use of the development, all driveways, parking and vehicle manoeuvring areas shall be constructed and surfaced with concrete, bitumen or paving, and shall be drained and maintained in a good condition at all times to the satisfaction of Council.

*Reason: To provide safe and convenient parking and manoeuvring areas for users of the development.*
3. All landscaping shown on the plans forming part of this application shall be established prior to the occupation or operation of the development and shall be maintained in good health at all times to the satisfaction of Council. Any plants that become diseased or die shall be replaced with a suitable species.

A watering system shall be installed at the time landscaping is established and operated so that all plants receive sufficient water to ensure their survival and growth.

*Reason: To enhance the amenity of the site and locality and mitigate against heat loading.*
4. All external materials, surface finishes and colours shall be consistent with the information detailed in this application and shall be maintained in a good condition at all times to the satisfaction of Council.

*Reason: To ensure a high standard of materials and finishes are used in the final presentation of the building.*

5. Prior to the occupation or use of the development, all stormwater design and construction shall be to the satisfaction of Council to ensure that stormwater does not adversely affect any adjoining property or public road and, for this purpose, stormwater drainage shall not at any time:
- a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create insanitary or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; or
  - e) Flow across footpaths or public ways.

*Reason: To ensure that adequate provision is made for the collection and dispersal of stormwater.*

## **Land Division Consent Conditions**

### **Council Requirements**

Nil

### **SCAP Requirements**

6. The financial requirements of the SA Water Corporation shall be met for the provision of water supply and sewerage services.

Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non-standard fees.

On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

*Reason: To satisfy the requirements of the South Australian Water Corporation.*

7. Payment of \$7253 into the Planning and Development Fund (1 allotment @ \$7253/allotment). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the State Planning Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, by cheque or credit card, at Level 5, 50 Flinders Street, Adelaide.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

8. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

*Reason: To satisfy the requirements of the State Commission Assessment Panel.*

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## **Attachments**

1. **Assessment table**
2. **Proposal plans and details**
3. **Internal and external referrals**



## Relevant Development Plan Provisions

General Section		
Crime Prevention	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 5, 6, 7, 8, 10
Design and Appearance	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 9, 10, 13, 14, 15, 20, 21
Energy Efficiency	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2
Land Division	<i>Objectives</i>	1, 2, 3, 4
	<i>Principles of Development Control</i>	1, 2, 5, 6, 8, 12
Landscaping, Fences and Walls	<i>Objectives</i>	1, 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 6
Orderly and Sustainable Development	<i>Objectives</i>	1, 2, 3, 4, 5
	<i>Principles of Development Control</i>	1
Residential Development	<i>Objectives</i>	1, 2
	<i>Principles of Development Control</i>	1, 3, 4, 5, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 18, 19, 20, 21, 27, 31
Transportation and Access	<i>Objectives</i>	2
	<i>Principles of Development Control</i>	1, 2, 8, 24, 34, 36, 44
Waste	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2

Location Restrictions for Water and Wastewater Connections

- Gas Meter

Light Poles

Stobie Poles

Stormwater Culverts

Telstra Pit

Trees
- Minimum 1.0m from a gas meter

Minimum 1.0m from a light pole

Minimum 1.5m from a stobie pole (Water)

Minimum 3.0m from a stobie pole (Sewer)

Minimum 0.5m from edge of culvert

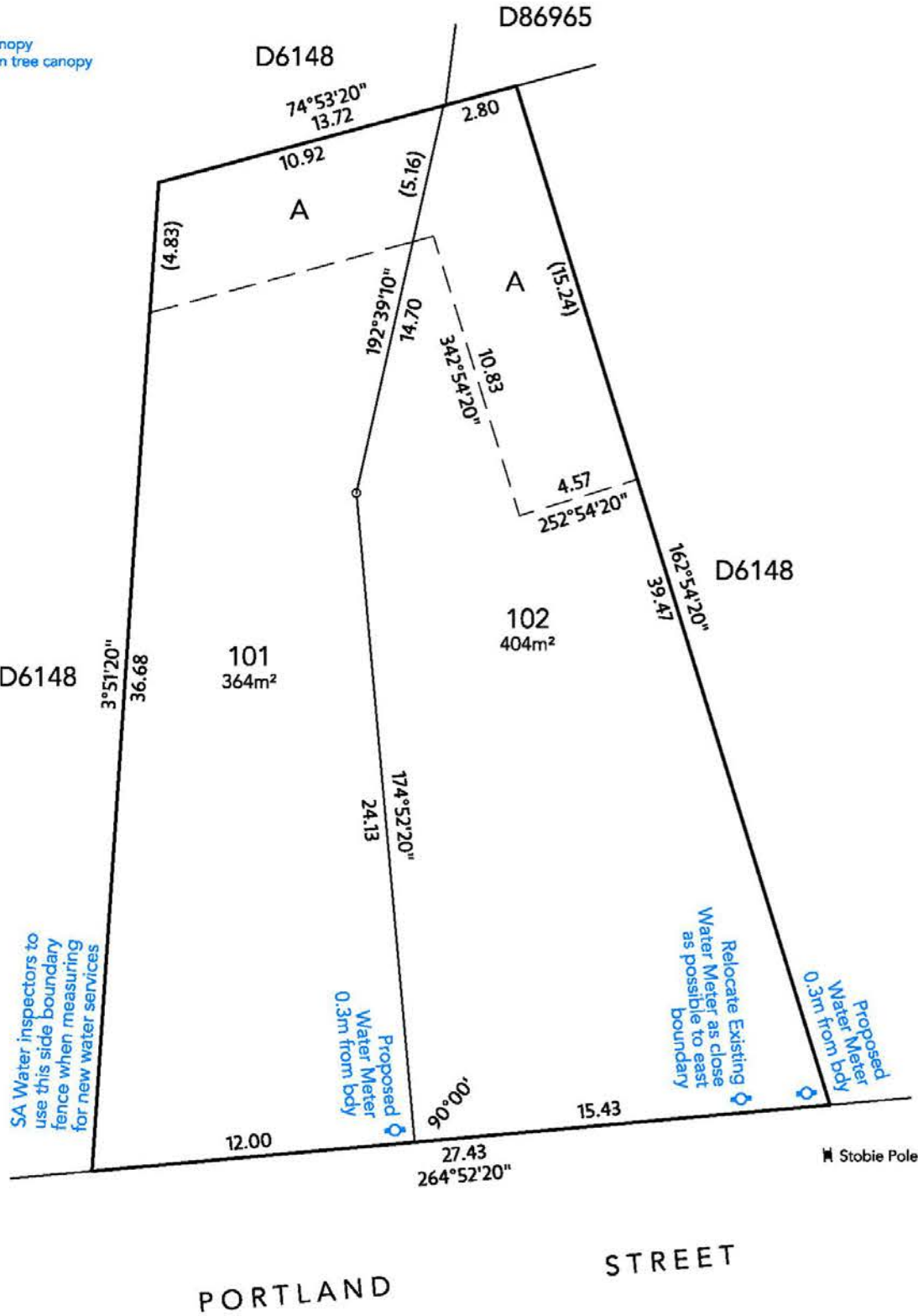
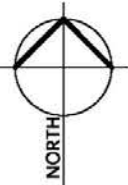
Minimum 0.5m from edge of pit

Girth > 1.0m - Minimum 3.0m from the tree and not within tree canopy

Girth < 1.0m - Possibly closer to the tree than above and not within tree canopy

Minimum 0.6m apart

SA Water Contact Details  
Amanda Mitchell  
Zaina Stacey Pty Ltd  
Phone 8379 7979



Land division application:  
**211 / D020 / 19**  
SHEET 1 OF 1 SHEETS  
City of West Torrens

Total area of site: 768m²  
Area of reserve provided: 0m²  
No. of existing allotments: 1  
No. of proposed allotments: 2  
No. of additional allotments: 1

Subject land details:  
**Allotment 434 in D6148**

Site Address: 24 Portland Street  
Suburb: Fulham  
Hundred: Adelaide  
Title(s): C.T. 5656 / 513

Annotations:  
All measurements in metres unless shown otherwise.  
Do not scale drawing. Original sheet size is A3.  
All measurements are subject to survey and final plan of division. Always check the current certificate(s) of title for any easement(s) and annotations(s) that affect the within land.  
Refer to the building plans for the proposed dwelling(s). Owner/developer or building designer to advise if the configuration of the dwellings change in any form.  
All existing structures are to be demolished. Owner or developer to apply to Council for demolition approval.  
Council Rates Department to provide street numbering on the Decision Notification Form to allow new electricity and telecommunications connections to be established as per NBN Co/Telstra Smart Communities and SA Power Networks.  
Portion of allotments 101 and 102 marked A is subject to a service easement to the South Australian Water Corporation for sewerage purposes.

0 2 4 6 8 10

SCALE 1:200m METRES

C	09/05/2019	Boundary amended
B	09/05/2019	Boundary and water meter on lot 102 amended
A	28/02/2019	Water meter for lot 102 amended
	14/02/2019	Original issue

Rev.	Date	Description
<b>ZAINA STACEY</b> DEVELOPMENT CONSULTANTS Office: 13 Avenue Road, Frewville SA Post: PO Box 1000, Torrens Park SA 5062 Phone: 08 8379 7979 Email: planning@zainastacey.com Reference: <b>18142</b>		



SITE NOTES

BUILDER TO CHECK AND CONFIRM ALL LEVELS AND DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION

ANY DISCREPANCIES IN DOCUMENTATION AND / OR ON SITE ARE TO BE REPORTED TO THE DESIGNER BEFORE ANY WORK IS COMMENCED

ALL WRITTEN DIMENSIONS TO TAKE PREFERENCE OVER SCALED DIMENSIONS.

ALL WORK TO BE IN ACCORDANCE WITH THE BUILDING CODE OF AUSTRALIA AND RELEVANT LOCAL AUTHORITY REQUIREMENTS

FOUNDATIONS, EXCAVATIONS, REINFORCEMENT PLACEMENT, ETC. TO BE INSPECTED AND APPROVED BY ENGINEER OR BUILDING SURVEYOR PRIOR TO ANY CONCRETE PLACEMENT

GROUND LEVELS AND FINISHED FLOOR LEVELS INDICATED ARE APPROXIMATE ONLY AND ARE TO BE CONFIRMED ON SITE

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REFER TO ENGINEERS DESIGN, DOCUMENTATION, CALCULATION AND SPECIFICATION FOR STRUCTURAL, ELECTRICAL, HYDRAULIC AND CIVIL DETAILS (IF APPLICABLE)

STORMWATER TO BE DISCHARGED TO STREET WATERTABLE OR RAINWATER TANK IN ACCORDANCE WITH COUNCIL REQUIREMENTS AND/OR DIRECTION

SURFACE WATER RUN-OFF FROM NOT LESS THAN 50sqm OF ROOF CATCHMENT AREA TO BE STORED IN 1000L RAIN WATER TANK AND, PLUMBED TO EITHER A TOILET, WATER HEATER OR LAUNDRY COLD WATER AND, INLET/OVERFLOW ON RAIN WATER TANK MUST BE FITTED WITH INSPECT PROOF AND NON DEGRADABLE SCREENS



LOCATION PLAN

SCALE NTS



REVISION  
PD-B  
JOB# 454

Client: PETER & TODD KEOUGH  
Address: 24 PORTLAND ST FULHAM

MICHAEL ZAINA  
M 0433 455 000  
E michael@zainastacey.com  
P 15/160 Port Street, Port Melbourne VIC 3207  
www.zainastacey.com  
ZAINA STACEY | architectural & interior design

1 OF 6  
Scale at A3  
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ALL STEEL LINTELS ARE TO BE HOT DIPPED GALVANISED, SIZES AS PER ENGINEERS DETAILS AND DESIGN

REFER TO ENGINEERS DESIGN, DOCUMENTATION, CALCULATION AND SPECIFICATION FOR STRUCTURAL, ELECTRICAL, HYDRAULIC AND CIVIL DETAILS (IF APPLICABLE)

ALL MATERIALS AND EQUIPMENT SHALL BE NEW, FREE OF BLEMISHES OR DAMAGE. ANY DEFECTIVE OR FAULTY EQUIPMENT SHALL BE REPLACED AT THE CONTRACTORS EXPENSE

ALL WORK SHALL BE CARRIED OUT IN A NEAT TRADESMAN LIKE MANNER AND TO BE CARRIED OUT BY FULLY QUALIFIED AND LICENSE TRADESPERSONS

ANY DISCREPANCIES IN DOCUMENTS AND/OR ON SITE TO BE REPORTED TO THE DESIGNER BEFORE ANY WORK IS COMMENCED

BUILDING TERMITE PROTECTION IN ACCORDANCE WITH AS 3680.1-2000

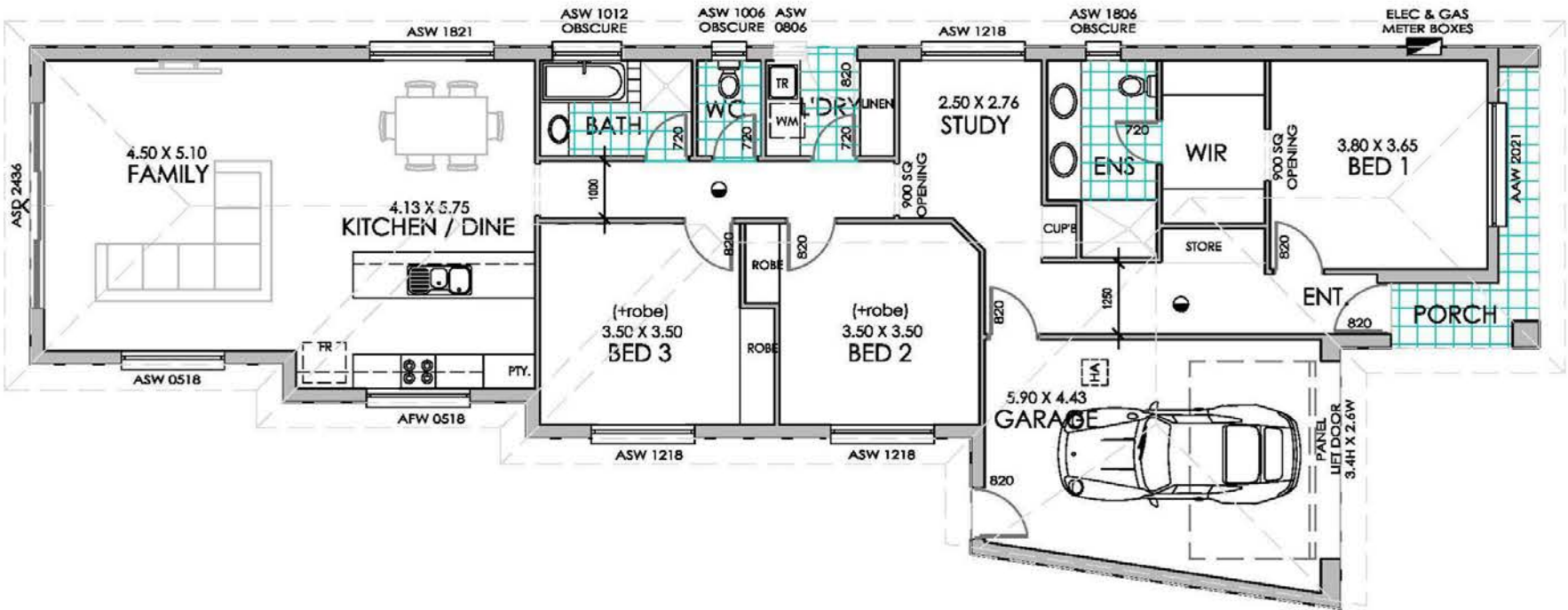
ALL GLAZING WILL BE SELECTED AND INSTALLED IN ACCORDANCE WITH AS 1288-2006 AND/OR AS 2047-2014

ALL WATERPROOFING IN WET AREAS WILL BE IN ACCORDANCE WITH AS 3740-2004 AND BCA-TABLE 3.8.1.1

SELF CONTAINED SMOKE ALARMS WILL COMPLY WITH AS 3786, CONNECTED TO CONSUMER MAINS POWER AND WILL BE INTERCONNECTED (IF MORE THAN 1)

RES 1

AREAS	m²
Living	150.10
Garage	27.10
Porch	5.90
<b>TOTAL</b>	<b>183.10</b>



FLOOR PLAN - RES 1

SCALE 1:100

3 of 6

Scale at A3

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ZAINA STACEY  
ZAINA STACEY | ARCHITECTURE

Client: PETER & TODD KEOUGH

Address: 24 PORTLAND ST FULHAM

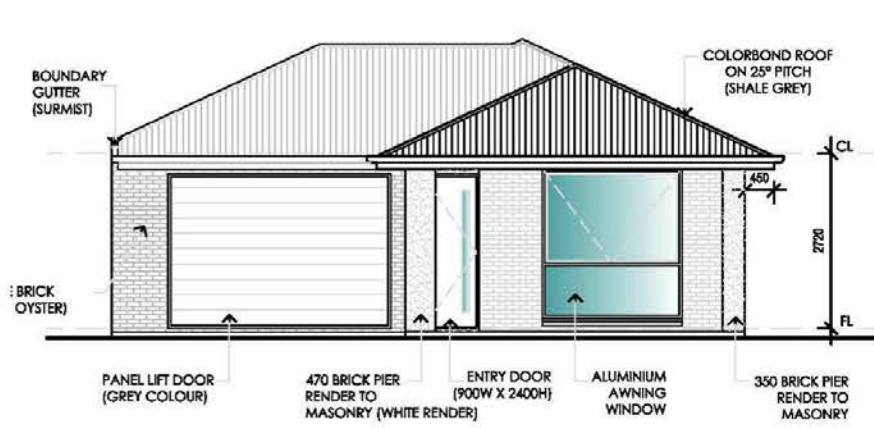
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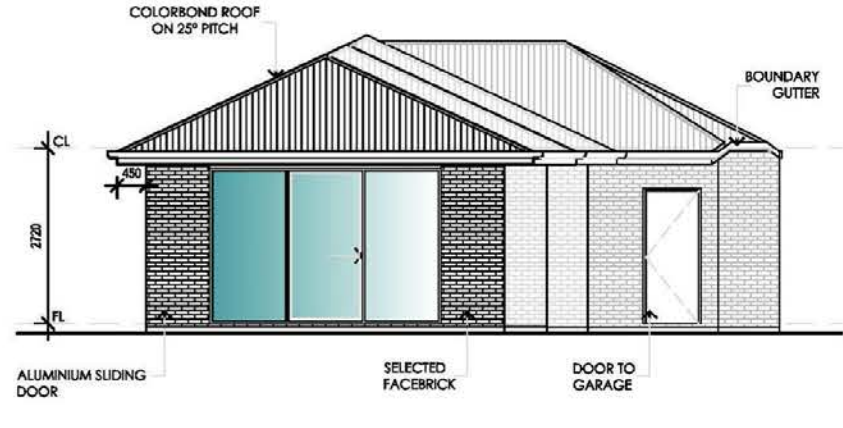
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DESIGN

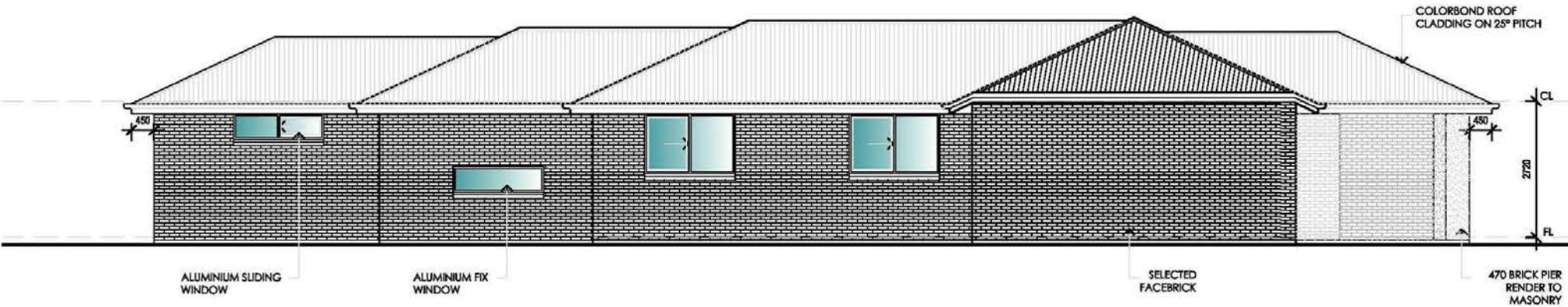




FRONT ELEVATION  
SCALE 1:100



REAR ELEVATION  
SCALE 1:100



SIDE ELEVATION  
SCALE 1:100



SIDE ELEVATION  
SCALE 1:100

4 OF 6  
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GROUND LEVELS AND FINISHED FLOOR LEVELS INDICATED ARE APPROXIMATE ONLY AND ARE TO BE CONFIRMED ON SITE

NO VARIATION MAY BE MADE TO THIS DRAWING WITHOUT PRIOR APPROVAL OF THE PROPRIETOR OR DESIGNERS

ALL STEEL LINTELS ARE TO BE HOT DIPPED GALVANISED, SIZES AS PER ENGINEERS DETAILS AND DESIGN

REFER TO ENGINEERS DESIGN, DOCUMENTATION, CALCULATION AND SPECIFICATION FOR STRUCTURAL, ELECTRICAL, HYDRAULIC AND CIVIL DETAILS (IF APPLICABLE)

ALL MATERIALS AND EQUIPMENT SHALL BE NEW, FREE OF BLEMISHES OR DAMAGE. ANY DEFECTIVE OR FAULTY EQUIPMENT SHALL BE REPLACED AT THE CONTRACTORS EXPENSE

ALL WORK SHALL BE CARRIED OUT IN A NEAT TRADESMAN LIKE MANNER AND TO BE CARRIED OUT BY FULLY QUALIFIED AND LICENSE TRADESPERSONS

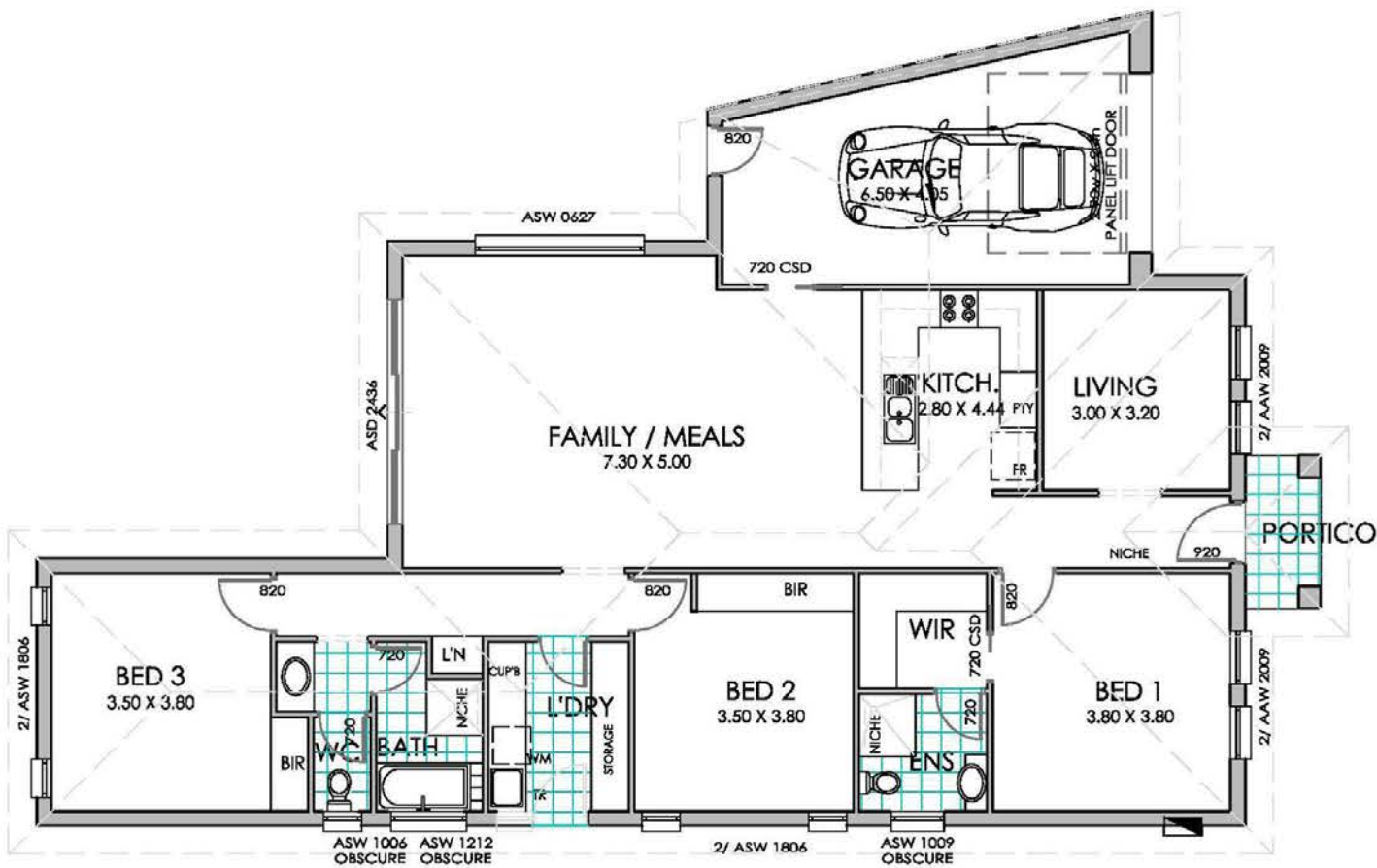
ANY DISCREPANCIES IN DOCUMENTS AND/OR ON SITE TO BE REPORTED TO THE DESIGNER BEFORE ANY WORK IS COMMENCED

BUILDING TERMITE PROTECTION IN ACCORDANCE WITH AS 3680.1-2000

ALL GLAZING WILL BE SELECTED AND INSTALLED IN ACCORDANCE WITH AS 1288-2006 AND/OR AS 2047-2014

ALL WATERPROOFING IN WET AREAS WILL BE IN ACCORDANCE WITH AS 3740-2004 AND BCA-TABLE 3.8.1.1

SELF CONTAINED SMOKE ALARMS WILL COMPLY WITH AS 3786, CONNECTED TO CONSUMER MAINS POWER AND WILL BE INTERCONNECTED (IF MORE THAN 1)



FLOOR PLAN - RES 2

SCALE 1:100

RES 2

AREAS	m <sup>2</sup>
Living	146.90
Garage	25.20
Porch	2.94
Alfresco	0.00
<b>TOTAL</b>	<b>175.04</b>

5 of 6

Scale at A3

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ZAINA STACEY | interior architects

ZAINA STACEY

Client: PETER & TODD KEOUGH

Address: 24 PORTLAND ST FULHAM

REVISION

PD-B

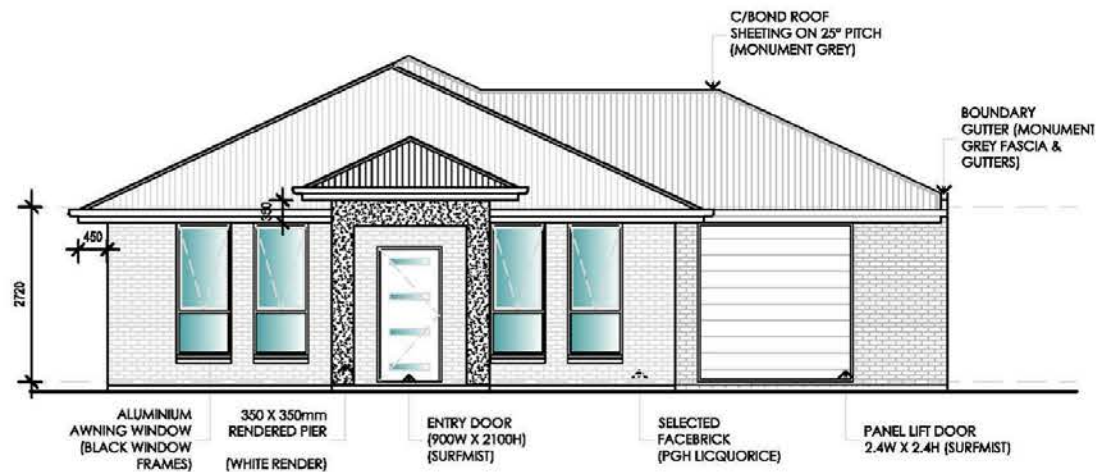
JOB# 454

www.zainastacey.com.au

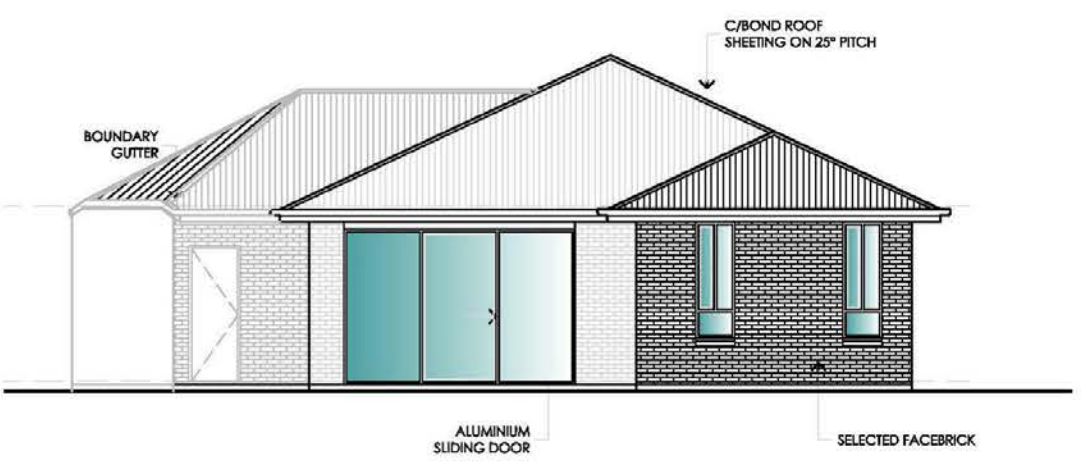
0415 568 576

4/110 Prospect Rd, PROSPECT

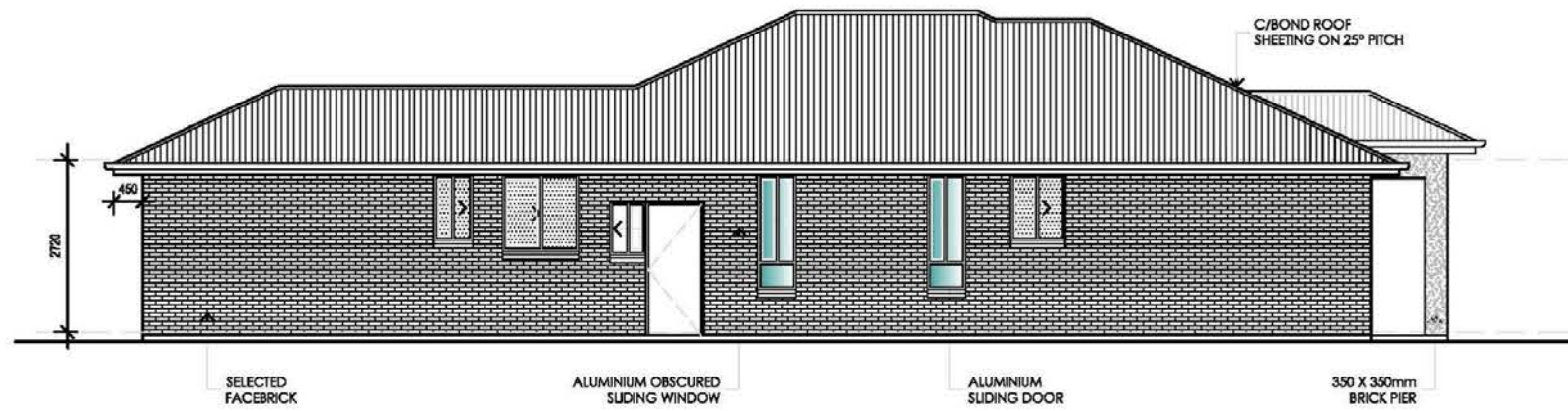
DESIGN



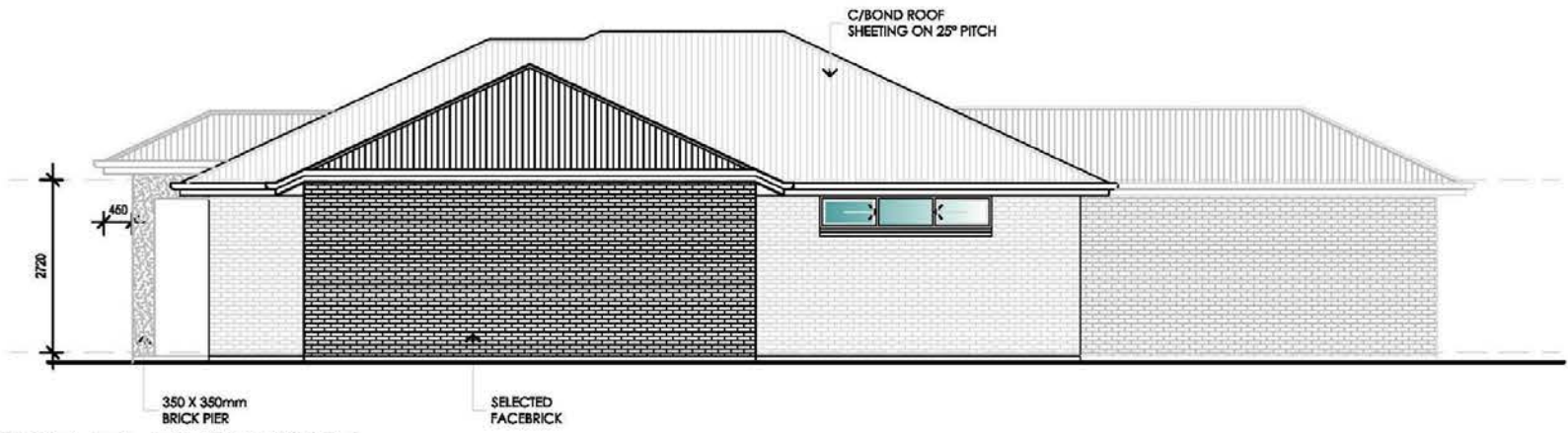
FRONT ELEVATION  
SCALE 1:100



REAR ELEVATION  
SCALE 1:100



SIDE ELEVATION  
SCALE 1:100



SIDE ELEVATION  
SCALE 1:100

6 OF 6  
Scale at A3  
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E michael@zainastacey.com  
P 1/100 Portland St Fulham  
ZAINA STACEY | architectural drawings

ZAINA STACEY

Client: PETER & TODD KEOUGH  
Address: 24 PORTLAND ST FULHAM

REVISION  
PD-B  
JOB# 454  
eamon@ztdesign.net.au | 0415 568 576 | www.ztdesign.net.au | 4/110 Prospect Rd, PROSPECT

DESIGN



A3

**PRELIMINARY**  
NOT TO BE USED FOR CONSTRUCTION**SITWORKS PLAN**(THIS PLAN IS TO BE READ IN  
CONJUNCTION WITH SITWORKS NOTES -  
SHEET 2 OF 2)**NOTE (RESIDENCE 1):**

LOCATE & CONNECT SEWER DRAIN TO NEW  
SIP. CONNECTION TO BE AS PER SA WATER  
STANDARD DETAILS. BUILDER/BUILDERS  
PLUMBER TO CHECK DEPTH OF SEWER  
CONNECTION AND ENSURE ADEQUATE SEWER  
FALL CAN BE ACHIEVED PRIOR TO COMMENCING  
EARTHWORKS. REFER ANY CHANGES TO  
PROPOSED BENCH LEVEL TO THIS OFFICE FOR  
VERIFICATION PRIOR TO COMMENCING SITE CUT.

NORTH APPROX...

(high fences—blocked visibility)

**SURVEY LEGEND**

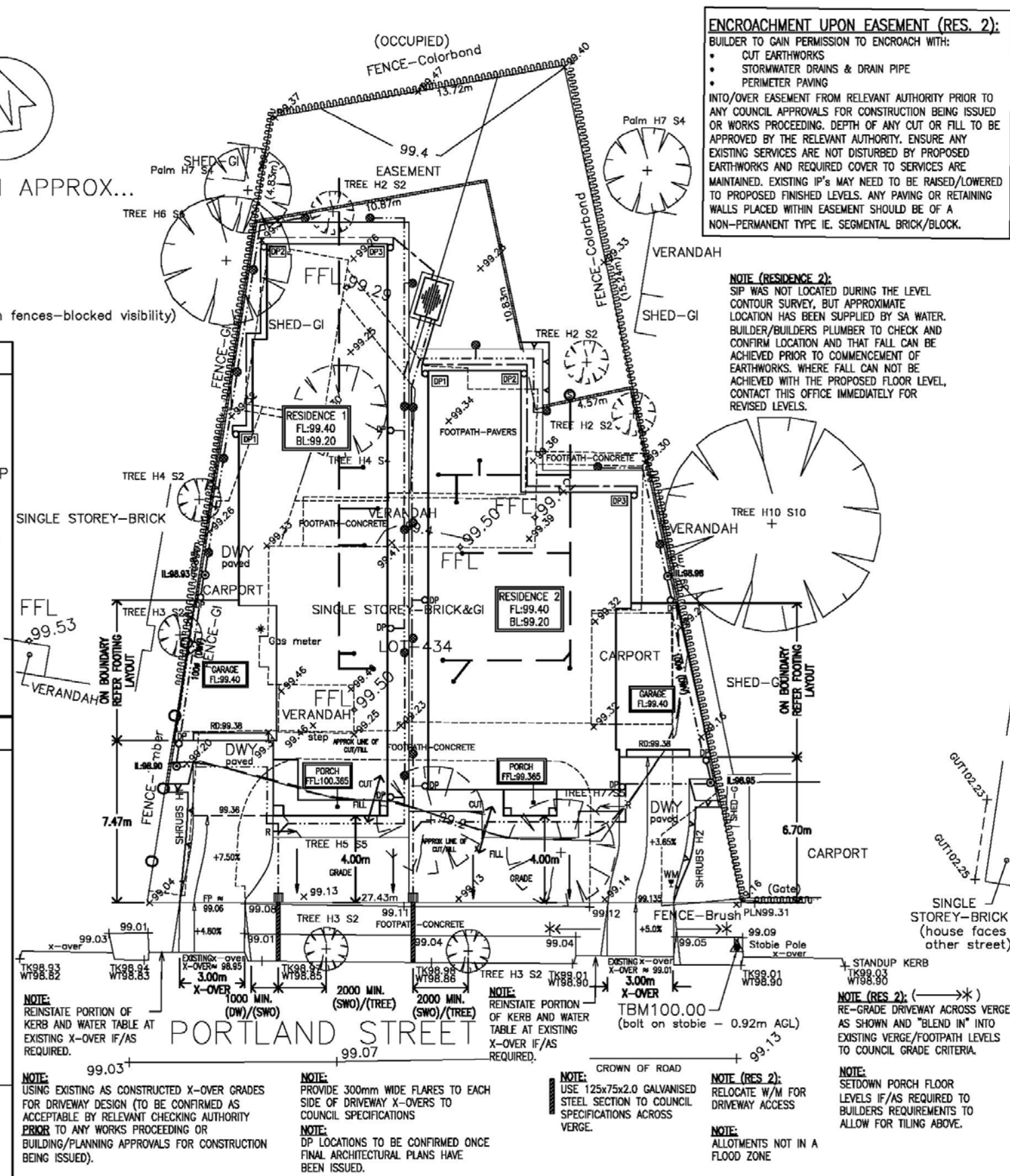
- |                         |                     |
|-------------------------|---------------------|
| □ Permanent survey mark | ⊕ Telstra pillar    |
| ▲ Temporary bench mark  | ⊞ Gas meter         |
| ■ Peg                   | ⊙ Sewer IP          |
| ⊙ Spike                 | ⊙ Water meter       |
| ● Metal pin             | ⊞ Stormwater IP     |
| ⊙ GI nail               |                     |
| ● Masonry nail          | — Brick fence       |
| ■ Picket                | — Stone fence       |
| + Spot height           | — Cyclone fence     |
| ▨ Grated inlet pit      | — Post & wire fence |
| □ Domestic outlet       | — Brush fence       |
| — Stobie pole           | — Picket fence      |
| ⊙ Light pole            | — GI fence          |
| ⊞ ETSA pillar           | — Log fence         |
| ⊞ ETSA service          | ⊕ Fence post        |
| □ Telstra JB            | ⊙ Tree              |
|                         | ⊙ Stump             |

**DESIGN LEGEND**

- : 250 SQUARE x 285 DEEP "RELIN" RAINWATER PIT (PVC), (SERIES 250) OR SIMILAR (U.N.O.)
- : GRATED SURFACE STORMWATER DRAIN 90# (U.N.O.)
- : STORMWATER DRAIN PIPE (GRAVITY FLOW) - 90# PVC (U.N.O.) AT 1 IN 250 (0.40%) MIN FALL (U.N.O.) EXCEPT ON SEALED SYSTEM
- : STORMWATER DRAIN PIPE (SEALED SYSTEM) - 90# PVC (U.N.O.)
- : 90# AGRIC DRAIN (U.N.O.)
- : INTERNAL SEWER DRAIN LOCATION (TO BE CONFIRMED BY BUILDER)
- : SEWER PIPE 100# AT 1.65% MIN (1 IN 60)
- TOP OF BATTER
- BATTERS/EARTHWORK EMBANKMENTS TO BE 50% (1 IN 2) UNLESS NOTED OTHERWISE
- EXISTING TREES AND STRUCTURES ON SITE TO BE DEMOLISHED/REMOVED BY OWNER PRIOR TO CONSTRUCTION, UNLESS OTHERWISE STATED.

**SURVEY:**

THIS IS NOT A BOUNDARY SURVEY. THEREFORE THE  
RELATIONSHIP BETWEEN OCCUPATION AND THE PLOTTED  
BOUNDARY IS INDICATIVE. LEVELS ARE BASED ON A  
TEMPORARY DATUM (UNO). THE DATUM (SHOWN ON THIS  
PLAN) IS TO BE LOCATED PRIOR TO COMMENCING SITWORKS.



TITLE:

**SITWORKS PLAN**

ADDRESS:

NO. 24 PORTLAND STREET  
FULHAM

CLIENT:

ET DESIGN

**Residential  
Commercial  
Industrial**  
Consulting Engineers  
A.B.N. 17 131 375 356

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ALBERT PARK, SA 5014  
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JOB No.

C26665

SHEET No.

1 of 2

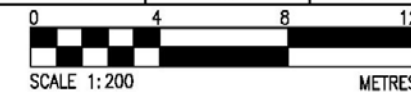
ISSUE No.

—

DRAWN : L.G.

DESIGN: D.A.

DATE: 22-Feb-19



No.	REVISION	BY	DATE

**GENERAL NOTES:**

SITWORKS AND STORMWATER DRAINAGE ARE TO BE CONSTRUCTED BY THE OWNER OR THE OWNERS REPRESENTATIVE (IE. THE BUILDER WHERE STATED WITHIN THE BUILDING CONTRACT). THIS DOCUMENT IS TO BE READ IN CONJUNCTION WITH THE FOOTING CONSTRUCTION REPORT AND ARCHITECTURAL DRAWINGS. ANY DISCREPANCIES ARE TO BE REPORTED TO THIS OFFICE IMMEDIATELY.

**SOIL CLASSIFICATION:** S/M-D (TO BE CONFIRMED)

- 10mm THICK LAGGING AROUND STORMWATER AND SEWER DRAIN PENETRATIONS THROUGH EXTERNAL FOOTINGS
- FLEXIBLE CONNECTIONS IN SEWER & STORMWATER DRAINS NOT REQUIRED

**BENCH:**

BUILDING AREA TO BE BENCHMARKED TO 200mm BELOW THE FINISHED FLOOR LEVEL.

GRADE SITE AWAY FROM HOUSE AS FOLLOWS:-

- GRADE PAVED AREAS 25mm IN 1000mm
- GRADE GRASSED AREAS 5mm IN 1000mm

**IMPORTANT NOTE:**

TO ASSIST IN AVOIDING A "DOWNHILL" MOVEMENT OF FILL ONCE IT HAS BEEN PLACED, A SERIES OF HORIZONTAL BENCHMARKED PLATFORMS SHOULD BE EXCAVATED INTO THE GROUND WHEN THE EXISTING SLOPE IS 1 IN 8 OR GREATER. THIS BENCHMARKING SHOULD BE UNDERTAKEN OVER THE ENTIRE AREA WHERE FILLING IS TO OCCUR.

**SEWER:**

THIS SEWER DESIGN IS BASED ON THE SHORTEST POSSIBLE RUN. BUILDER/PLUMBER TO CONFIRM SEWER CONNECTION INVERT LEVEL AND ASSUMED LAYOUT PROVIDED ON THIS DRAWING. CONTACT THIS OFFICE IMMEDIATELY IF ANY DISCREPANCIES EXIST AS THE FLOOR LEVEL AND/OR UNDERMINING PIER DEPTHS MAY NEED TO BE REVISED. TOP OF FLOOD GULLY AND PAVING AROUND FLOOD GULLY TO BE CONSTRUCTED 150mm BELOW THE LOWEST FIXTURE CONNECTED TO THE DRAIN.

**STORMWATER:**

GRAVITY FLOW STORMWATER SYSTEM IS TO BE LAID @ 1 IN 250 MIN GRADE WITH 100mm MIN COVER EXCEPT AS NOTED BELOW UNDERSIDE OF PAVING:-

- 50mm (SUBJECT TO PEDESTRIAN TRAFFIC)
  - 75mm (SUBJECT TO LIGHT VEHICULAR TRAFFIC)
  - 450mm (UNPAVED DRIVEWAYS)
- WHERE COVER CANNOT BE ACHIEVED ENCASE DRAIN PIPE WITHIN A CHS GALV. STEEL SLEEVE OF THICKNESS:
- 3.0mm (SUBJECT TO PEDESTRIAN TRAFFIC)
  - 5.0mm (SUBJECT TO LIGHT VEHICULAR TRAFFIC)




A4

**SITEWORKS NOTES**(THESE NOTES ARE TO BE READ IN CONJUNCTION  
WITH SITEWORKS PLAN - SHEET 1 OF 2)**NOTES:**

ANY SURPLUS SPOIL FROM SITEWORKS IS THE OWNERS RESPONSIBILITY AND SHOULD BE REMOVED OR DISPERSED AS APPROPRIATE, UNLESS STATED OTHERWISE IN THE BUILDING CONTRACT. THIS SPOIL SHOULD BE STOCKPILED SUCH THAT IT DOES NOT OBSTRUCT SITE ACCESS AND CAN BE EASILY REMOVED FROM THE SITE.

THE RETAINING WALLS SHOWN ON THIS PLAN ARE TO BE CONSTRUCTED BY THE OWNER EXCEPT WHERE THE RETAINING WALL FORMS PART OF THE BUILDING STRUCTURE OR WHERE SPECIFIED OTHERWISE WITHIN THE BUILDING CONTRACT. ANY RETAINING WALLS ADJACENT EXCAVATIONS, A SERVICE TRENCH/EASEMENT (PROPOSED OR EXISTING) OR IF FOUND IN FILL SHOULD HAVE AN UNDERMINING COMPONENT INCORPORATED IN THE DESIGN OF THEIR FOOTING/PIER SUPPORT SYSTEM.

OWNER TO RETAIN ANY MINOR CUT/FILL ON THE BOUNDARIES WITH A CONCRETE PLINTH, SLEEPER OR SIMILAR.

 : MINIMUM 1.0 KILOLITRE RAINWATER TANK AS PER THE NCC (BY OWNER/BUILDER - REFER BUILDING CONTRACT). TO BE PLUMBED TO AT LEAST A WC, WATER HEATER OR ALL LAUNDRY COLD WATER OUTLETS. WHERE A RWT IS SUPPORTED ON A STAND OR OTHER STRUCTURE, THE SUPPORTING STRUCTURE MUST COMPLY WITH AS/NZ 1170.1 AND 1170.2. ROOF GUTTER HEIGHT IN RELATION TO TANK INLET HEIGHT MAY NEED TO BE TAKEN INTO CONSIDERATION (REFER TO THE TANK SUPPLIER). THE TANK SHALL ALSO BE LOOPED INTO THE MAINS WATER SYSTEM.

--- : STORMWATER DRAIN PIPES AS A SEALED SYSTEM USED TO CONNECT [DP1] TO [DP3] TO THE RAINWATER TANK (MINIMUM 50m<sup>2</sup> OF ROOF AREA). REFER TO FOOTING CONSTRUCTION REPORT ATTACHMENT SHEET SS1.

--- : STORMWATER DRAIN PIPES UNDER GRAVITY FLOW USED TO CONNECT RAINWATER TANK OVERFLOW PIPE, SURFACE STORMWATER DRAIN PIPES & DP's NOT CONNECTED TO THE RAINWATER TANK TO THE STREET WATER TABLE.

ENSURE GRAVITY FLOW PIPES MAINTAIN SUFFICIENT GRADE TO MEET THE APPROPRIATE OUTLET AS SHOWN ON THIS PLAN. CONSTRUCT ANY SEALED SYSTEM PIPES SUCH THAT THEY DO NOT INTERFERE WITH THE GRAVITY FLOW SYSTEM.

WHERE GRATED SURFACE STORMWATER SUMPS ARE USED GRADE SOIL/PAVING IN TOWARDS SUMPS IN ACCORDANCE WITH THE "BENCH" NOTES ON THIS PAGE. ALTERNATIVELY CONSTRUCT LINED SPOON DRAINS WITH 0.3% GRADIENT AS PER THE DETAILS ON ATTACHMENT SHEET PD1.

SUMP SIZES AND QUANTITY SHOWN ARE RECOMMENDED AS A MINIMUM. LOCATIONS AND SIZES MAY BE ALTERED AT THE DRAIN AND PAVING CONTRACTORS DISCRETION DEPENDING ON AREAS THEY SERVICE AND PAVEMENT TYPE USED.

**TITLE: SITEWORKS NOTES****ADDRESS: NO. 24 PORTLAND STREET  
FULHAM****CLIENT: ET DESIGN**

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Commercial  
Industrial**  
**Consulting Engineers**  
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**JOB No. C26665****SHEET No. 2 of 2****ISSUE No. —****DRAWN : L.G.****DESIGN: D.A.****DATE: 22-Feb-19**


SCALE 1:200

METRES

No.	REVISION	BY	DATE

**PRELIMINARY**  
NOT TO BE USED FOR CONSTRUCTION

**KEY**

TK	TOP OF KERB
WT	WATER TABLE
BL	BENCH LEVEL
FL	FLOOR (POURED) LEVEL
FFL	FINISHED FLOOR LEVEL (ie. tiled/paved/decking level)
IL	INVERT LEVEL (BOTTOM OF GRAVITY FLOW STORMWATER DRAIN PIPE SYSTEM)
T.O.S	TOP OF SUMP
≈	APPROXIMATE
U.N.O.	UNLESS NOTED OTHERWISE
C.O.S	CHECK ON SITE
RD	ROLLER DOOR REBATE
FP	FOOTPATH
RWT	RAINWATER TANK
DPO	DOWNPIPE
⊙	SCREW CAP STORMWATER INSPECTION POINTS IN GRAVITY FLOW/SEALED SYSTEM SW RUN (AS A MINIMUM) TO FINISHED PAVING/GROUND LEVELS (PROVIDE PROPRIETARY CONCRETE SURROUND AND LID WHERE FOUND IN DRIVEWAY).
	CONCRETE SLEEPER RETAINING WALL
	CONCRETE PLINTH AS REQUIRED OR SIMILAR (H ≈ 250mm - MAX C.O.S)
	RAMP PAVING AT 12.5% (MAX) (LOCATION & SETUP SCHEMATIC ONLY)

## Preliminary Traffic, Flooding & Stormwater Assessment

**Development Application No: 211/190/2019**

**Assessing Officer:** Sonia Gallarello  
**Site Address:** 24 Portland Street, FULHAM SA 5024  
**Certificate of Title:** CT-5656/513  
**Description of Development:** Combined Application: Land division - Torrens Title; SCAP No. 211/D020/19; Create one (1) additional allotment; and Construction of two detached dwellings

### TO THE TECHNICAL OFFICER - CITY ASSETS

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Sonia Gallarello**

**DATE 4 April, 2019**



## Memo

To Sonia Gallarello  
From Richard Tan  
Date 04-Apr-2019  
Subject 211/190/2019, 24 Portland Street, FULHAM SA 5024

Sonia Gallarello,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

### 1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

- 1.1 In accordance with the provided 'Siteworks Plan' (RCI, Ref: C26665-1/2, dated 22/02/2019), the FFLs of the proposed development (99.40 minimum) have been assessed as satisfying minimum requirements (99.25) in consideration of street and/or flood level information.

### 2.0 Verge Interaction

- 2.1 Road verge interaction has been assessed as satisfying minimum requirements

### 3.0 Traffic Requirements

- 3.1 Proposed crossovers' width (3m) have been assessed as satisfying minimum requirements.
- 3.2 The garage's internal dimension and set back as indicated in 'Floor Plan - Res1 & 2' (ET Design, Ref: 454-PD-A-3/6 & 5/6, received 22/02/2019) and 'Siteworks Plan' (RCI, Ref: C26665-1/2, dated 22/02/2019) have been assessed as satisfying minimum requirements.

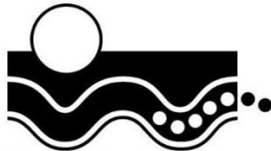
### 4.0 Waste Management

- 4.1 The public kerbside space available for bin presentation has been assessed as satisfying minimum requirement.

### 5.0 Stormwater Management

- 5.1 Stormwater detention is not required for this development.





City of **West Torrens**

Between the City and the Sea

## **6.0 Easement**

- 6.1** Proposed pavement for Residence 2 is encroaching into existing sewer easement. Approval from SA Water is required for the encroachments.

**It is recommended that applicant seek out SA Water approval for encroachment over existing sewer easement.**

Regards  
Richard Tan  
Civil Engineer

Contact Planning Services  
Telephone 7109 7016  
Email [dldptpdclearanceletters@sa.gov.au](mailto:dldptpdclearanceletters@sa.gov.au)



20 February 2019

The Chief Executive Officer  
City of West Torrens

Dear Sir/Madam

**Re: Proposed Application No. 211/D020/19 (ID 64295)  
for Land Division by Peter and Todd Keough**

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 19 February 2019, I advise that the State Commission Assessment Panel (SCAP) has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the State Commission Assessment Panel has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.  
Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.  
On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.
2. Payment of \$7253 into the Planning and Development Fund (1 allotment(s) @ \$7253/allotment).  
Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Department of Planning, Transport and Infrastructure and marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Level 5, 50 Flinders Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

**PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.**

Yours faithfully

A handwritten signature in blue ink, appearing to read 'Biljana Prokic', is written over a light blue horizontal line.

Biljana Prokic  
**Land Division Coordinator - Planning Services**  
as delegate of  
**STATE COMMISSION ASSESSMENT PANEL**



20 February 2019

Our Ref: H0082456

The Chairman  
State Commission Assessment Panel  
50 Flinders St  
ADELAIDE SA 5000

Dear Sir/Madam

**PROPOSED LAND DIVISION APPLICATION NO: 211/D020/19 AT FULHAM**

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.

On approval of the application, it is the developers/owners responsibility to ensure all internal pipework (water and wastewater) that crosses the allotment boundaries has been severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

Yours faithfully

Wendy Hebbard

**for MANAGER LAND DEVELOPMENT & CONNECTIONS**

SA Water  
Level 6, 250 Victoria Square  
ADELAIDE SA 5000  
Ph (08) 7424 1119  
Inquiries Wendy Hebbard  
Telephone 7424 1119



## **7 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER**

### **7.1 Consideration of Appeal - ERD 19-69: 19 Carlton Parade, TORRENSVILLE**

Application No. 211/1089/2018

#### **Reason for Confidentiality**

It is recommended that this Report be considered in CONFIDENCE in accordance with regulation 13(2)(a) (vii) and (viii) of the *Planning, Development and Infrastructure (General) Regulations 2017*, which permits the meeting to be closed to the public for business relating to the following:

- (vii) matters that must be considered in confidence in order to ensure that the assessment panel, or any other entity, does not breach any law, or any order or direction of a court or tribunal constituted by law, any duty of confidence, or other legal obligation or duty;
- (viii) legal advice.

as this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.

#### **RECOMMENDATION**

It is recommended to the Council Assessment Panel that:

1. On the basis that this matter is before the Environment Resources and Development Court so any disclosure would prejudice the position of Council, the Council Assessment Panel orders pursuant to regulation 13(2) of the *Planning, Development and Infrastructure (General) Regulations 2017*, that the public, with the exception of the Chief Executive Officer, members of the Executive and Management Teams, Assessment Manager, City Development staff in attendance at the meeting, and meeting secretariat staff, and other staff so determined, be excluded from attendance at so much of the meeting as is necessary to receive, discuss and consider in confidence, information contained within the confidential reports submitted by the Assessment Manager on the basis that this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.
2. At the completion of the confidential session the meeting be re-opened to the public.

## 8 SUMMARY OF COURT APPEALS

### 8.1 Summary of ERD Court matters, items determined by SCAP/Minister/Governor and deferred CAP items - June 2019

#### Brief

This report presents information in relation to:

1. any planning appeals before the Environment, Resources and Development (ERD) Court;
2. any matters being determined by the State Commission Assessment Panel (SCAP);
3. any matters determined by the Minister of Planning (Section 49);
4. any matters determined by the Governor of South Australia (Section 46); and
5. any deferred items previously considered by the Council Assessment Panel.

#### Development Application appeals before the ERD Court

DA Number	Address	Reason for Appeal	Status
211/981/2018	322 Marion Road, NETLEY	Applicant appealed CAP's refusal for removal of significant tree	Hearing, 11 June 2019
211/1089/2018	19 Carlton Parade, TORRENSVILLE	Applicant appealed CAP's refusal of two storey detached dwelling	Directions Hearing, 3 June 2019?
211/1059/2018	428 Henley Beach Road, LOCKLEYS	Applicant appealed CAP's refusal of five (5) two storey group dwellings	Preliminary Conference, 17 June 2019

#### Matters pending determination by SCAP

Reason for referral	DA number	Address	Description of development
Schedule 10	211/M030/18	192 ANZAC Highway GLANDORE	Eight-storey RF building, 40 dwellings & removal of regulated tree
Schedule 10	211/M029/18	81 Anzac Highway ASHFORD	5 storey RF building & car parking

#### Matters pending determination by the Minister of Planning

Reason for referral	DA number	Address	Description of development
Section 49	211/V007/12 V3	Lot 2 in FP 1000, West Beach Road WEST BEACH	Variation - removal of east-west internal road
Section 49	211/V010/19	Cnr James Congdon Drive & Sir Donald Bradman Drive	Construction of non-illuminated sign (3m high x 4m wide, on 2m posts)

**Matters pending determination by the Governor of South Australia**

Nil

**Deferred CAP Items**

DA number	DAP/CAP Meeting	Address	Description of development	Reason for deferral
211/796/2016	9 August 2016	22 Lindsay Street, CAMDEN PARK	Create 2 additional allotments and construct 3 two-storey dwellings within a residential flat building	Acoustic report, tree assessment report

**Conclusion**

This report is current as at 31 May 2019.

**RECOMMENDATION**

The Council Assessment Panel receive and note the information.

**Attachments**

Nil

**9 OTHER BUSINESS****10 MEETING CLOSE**