

CITY OF WEST TORRENS



# Notice of Panel Meeting

Notice is Hereby Given that a Meeting of the  
**COUNCIL ASSESSMENT PANEL**

will be held in the George Robertson Room, Civic Centre  
165 Sir Donald Bradman Drive, Hilton

on

**TUESDAY, 10 APRIL 2018  
at 5.00pm**

**Dr Donna Ferretti  
Assessment Manager**

## **City of West Torrens Disclaimer**

### **Council Assessment Panel**

Please note that the contents of this Council Assessment Panel Agenda have yet to be considered and deliberated by the Council Assessment Panel therefore the recommendations may be adjusted or changed by the Council Assessment Panel in the process of making the formal Council Assessment Panel decision.

**Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.**

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**1 MEETING OPENED****1.1 Evacuation Procedures****2 PRESENT****3 APOLOGIES**

**Leave of Absence**

**Panel Members:**

Ms Colleen Dunn

**4 CONFIRMATION OF MINUTES****RECOMMENDATION**

That the Minutes of the meeting of the Council Assessment Panel held on 13 March 2018 be confirmed as a true and correct record.

**5 DISCLOSURE STATEMENTS**

In accordance with section 7 of the *Assessment Panel Members – Code of Conduct* the following information should be considered by Council Assessment Panel members prior to a meeting:

A member of a Council Assessment Panel who has a direct or indirect personal or pecuniary interest in a matter before the Council Assessment Panel (other than an indirect interest that exists in common with a substantial class of persons) –

- a. must, as soon as he or she becomes aware of his or her interest, disclose the nature and extent of the interest to the panel; and
- b. must not take part in any hearings conducted by the panel, or in any deliberations or decision of the panel, on the matter and must be absent from the meeting when any deliberations are taking place or decision is being made.

If an interest has been declared by any member of the panel, the assessment manager will record the nature of the interest in the minutes of meeting.

## 6 REPORTS OF THE ASSESSMENT MANAGER

### 6.1 425 Anzac Highway, CAMDEN PARK

Application No 211/676/2017

Appearing before the Panel will be:

Representors: **Mr George Penglis** of 24 Gunnawarra Avenue, Camden Park wishes to appear in support of the representation.

**Ms Oggi Stojanovich** of 22 Gunnawarra Avenue, Camden Park, wishes to appear in support of the representation.

Applicant/s **Milly Nott** of Future Urban Group wishes to appear to respond to representations.

#### DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Demolition of the existing dwelling and the construction of a three storey residential flat building containing 8 dwellings, with associated car parking and landscaping
APPLICANT	Walpol Development Pty Ltd
APPLICATION NO	211/676/2017
LODGEMENT DATE	14 June 2017
ZONE	Residential
POLICY AREA	Medium Density Policy Area 18
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 3
REFERRALS	Internal <ul style="list-style-type: none"><li>▪ City Assets</li></ul> External <ul style="list-style-type: none"><li>▪ DPTI</li></ul>
DEVELOPMENT PLAN VERSION	30 May 2017
MEETING DATE	10 April 2018

#### RECOMMENDATION

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/676/2017 by Walpol Development Pty Ltd to undertake the demolition of the existing dwelling and the construction of a three storey residential flat building containing 8 dwellings with associated car parking and landscaping at 425 Anzac Highway, Camden Park (CT5111/230) subject to the following conditions:

#### Development Plan Consent Conditions

1. That the development shall be undertaken and completed in accordance with the plans and information detailed in this application except where varied by any conditions listed below.

2. That all driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) and properly drained, and shall be maintained in a reasonable condition at all times.
3. That any driveways, parking and manoeuvring areas and footpaths shall be lit in accordance with the Australian Standard 1158 during the hours of darkness that they are in use. Such lights shall be directed and screened so that overspill of light into nearby premises is avoided and minimal impact on passing motorists occurs.
4. That all landscaping shall be planted in accordance with the approved plans prior to the occupancy of the development. Any person(s) who have the benefit of this approval shall cultivate, tend and nurture the landscaping, and shall replace any landscaping which may become diseased or die.
5. The development will only be offered a 'Shared' Waste Service from Council, limited to a maximum of 4 x 140 litre General Waste Bins, 4 x 240 litre Dry Recycling Bins and 2 x 240 litre Food Organics Bins.
6. Organic Waste generated through the maintenance of landscaping within the site must be removed from the site by the landscape maintenance service for the site, and not disposed of in the Council waste collection bins.
7. The stormwater connection through the road verge area is to be constructed of shape and material to satisfy Council's standard requirements:
  - 100 x 50 x 2mm RHS Galvanised Steel or
  - 125 x 75 x 2mm RHS Galvanised Steel or
  - Multiples of the above.
8. Water-cooled and refrigerated air-conditioning units to be located and operated so as not to become an environmental nuisance and emit noise levels in excess of the Environment Protection (Machine Noise) Policy 1994. Maximum permitted noise levels shall not exceed 45db(A) during the specified times from 8pm on any night (except Saturday night) until 8am the following morning, and from 8pm on a Saturday night until 9am on the following Sunday morning. Any bleed off or filter backwash water from any water-cooled or bleed off/thaw from any refrigerated air-conditioning plant shall be disposed of via the sewerage system.

Conditions imposed at the recommendation of DPTI:

9. The site shall be served by a single access point direct to/from Anzac Highway. No additional vehicular access shall be permitted.
10. The Anzac Highway access shall be a minimum of 6.0 metres in width at the property boundary and extend at this width for a minimum of 6.0 metres into the site.
11. The existing Anzac Highway crossover shall be permanently closed and reinstated to Council standard kerb and gutter at the applicant's expense.
12. The access point shall comply with Figure 3.3 '*Minimum Sight Lines for Pedestrian Safety*' as defined in AS/NZ 2890.1:2004.
13. All on-site vehicle parking shall be consistent with AS/NZS 2890.1:2009.
14. All vehicles shall enter and exit the site in a forward direction.
15. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of Anzac Highway. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

**Notes:**

1. The existing street tree(s) has been assessed and considered acceptable to be removed by Council's Arborist.

- a) Based on Council's standard schedule of fees and charges, a fee for the removal of the street tree(s) inspected is currently valued at \$1308.00. The fee stated is a value for the current financial year and will vary depending upon the year of removal, which is due to annual price increases and changes to the tree.

I ..... as the property owner / developer / applicant - for the Development Application number 211/676/2017, acknowledge that this application will result in the necessity for the removal of ..... (one) street tree.

Prior to the commencement of any physical works it is acknowledged that a fee must be paid to Council in accordance with the City of West Torrens Street Tree Removal Policy.

I acknowledge that the street tree shall only be removed by a Council staff member or contractor who is acting on behalf of City of West Torrens Council.

Signature.....

- b) If the street tree is removed outside of this financial year, a further inspection and re-valuation of the street tree will be undertaken and the tree removal fee will be amended and a new invoice posted to the applicant for payment. There is a further requirement for confirmation that this revised fee will override any previous quote, prior to any works being undertaken for the street tree removal.
  - c) The applicant / property owner / developer acknowledges that the removal of street tree(s) must only be undertaken by Council's arboricultural officer or an approved contractor instructed to act on behalf of the City of West Torrens.
  - d) At no stage should an applicant / property owner / developer undertake to remove or prune the street tree(s) mentioned as a Penalty up to \$5,000 applies under the Local Government Act.
2. The applicant is reminded of its general environmental duty, as required by Section 25 of the Environment Protection Act, to take all reasonable and practical measures to ensure that the activities on the whole site, including during construction, do not pollute the environment in a way which causes or may cause environmental harm.

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**BACKGROUND**

The development proposal is presented to the Council Assessment Panel (CAP) for the following reason:

- All Category 2 or 3 applications where a representor has requested to be heard shall be assessed and determined by the CAP.

**PREVIOUS RELEVANT APPLICATION/S**

Nil

## SITE AND LOCALITY

The subject site is located on the northern side of Anzac Highway between Queens Road and Clifton Street. It is a rectangular shaped allotment with a frontage of 18.29m, a depth of 41.15m that results in an area of 752.63m<sup>2</sup>.

The site currently contains a single storey detached dwelling, verandah and a detached outbuilding. Access to the allotment is gained via a single width crossover in the south eastern corner of the allotment. A driveway runs along the length of the eastern boundary to a detached garage.

There are no easements, significant trees or other known constraints that affect the allotment.

The locality is mixed use in nature comprised of residential, recreational, industrial and open space land uses. Residential land uses range from low to medium density and include single storey detached dwellings, group dwellings (within a retirement village) and a three storey residential flat building.

Non-residential land uses in the locality include:

- The SA Jockey Club bistro and function centre to the south west of the subject site;
- An old service station, now used for the storage of construction vehicles, east of the subject site; and
- The Westside bike way and linear park north of the subject site.

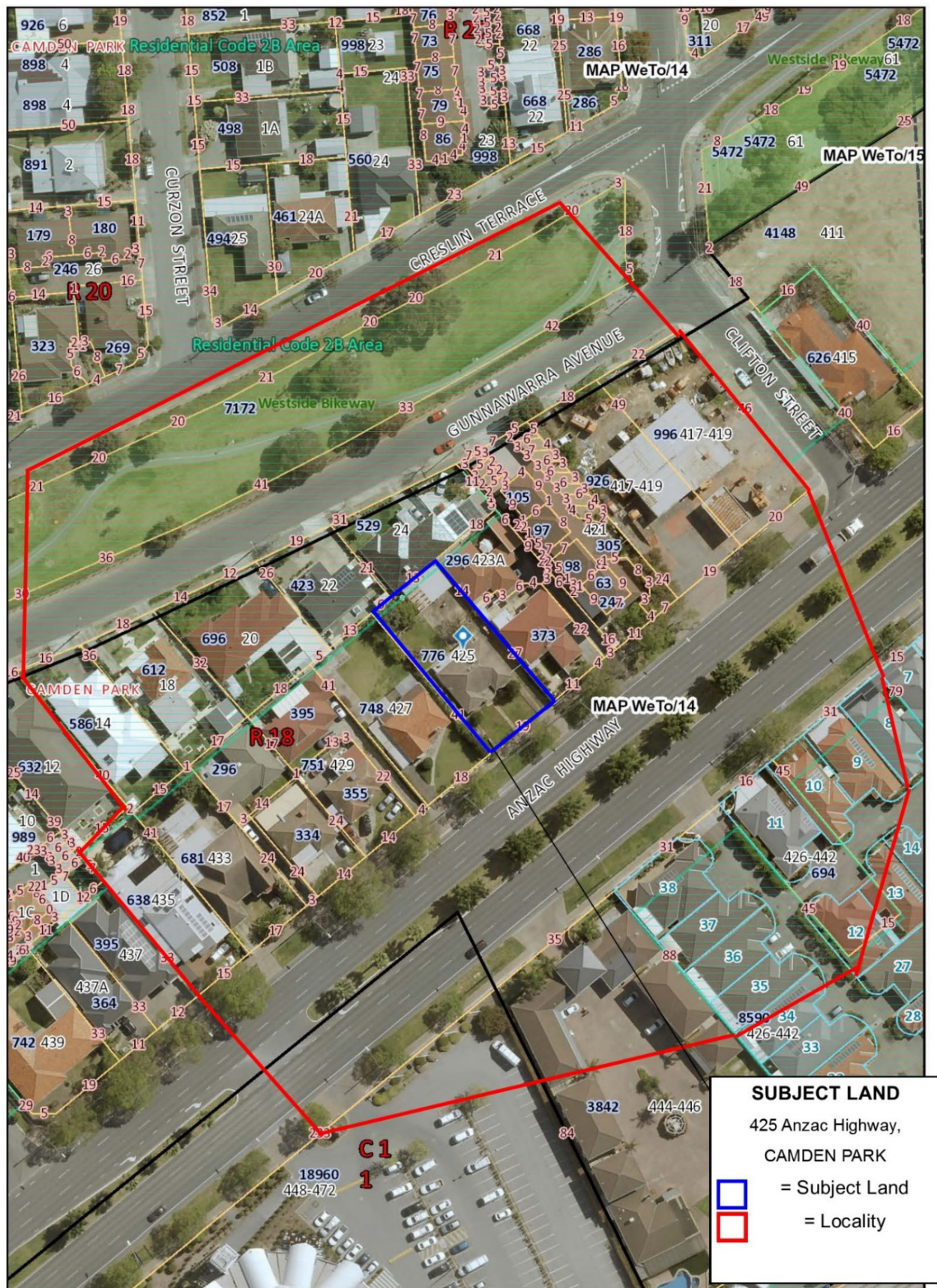
With the exception of the SA Jockey Club, all of the above land uses are located within the Residential Zone.

A key feature of the locality is that Anzac Highway, as a Primary Arterial Road, accommodates an average annual daily traffic count of approximately 33,900 vehicles.

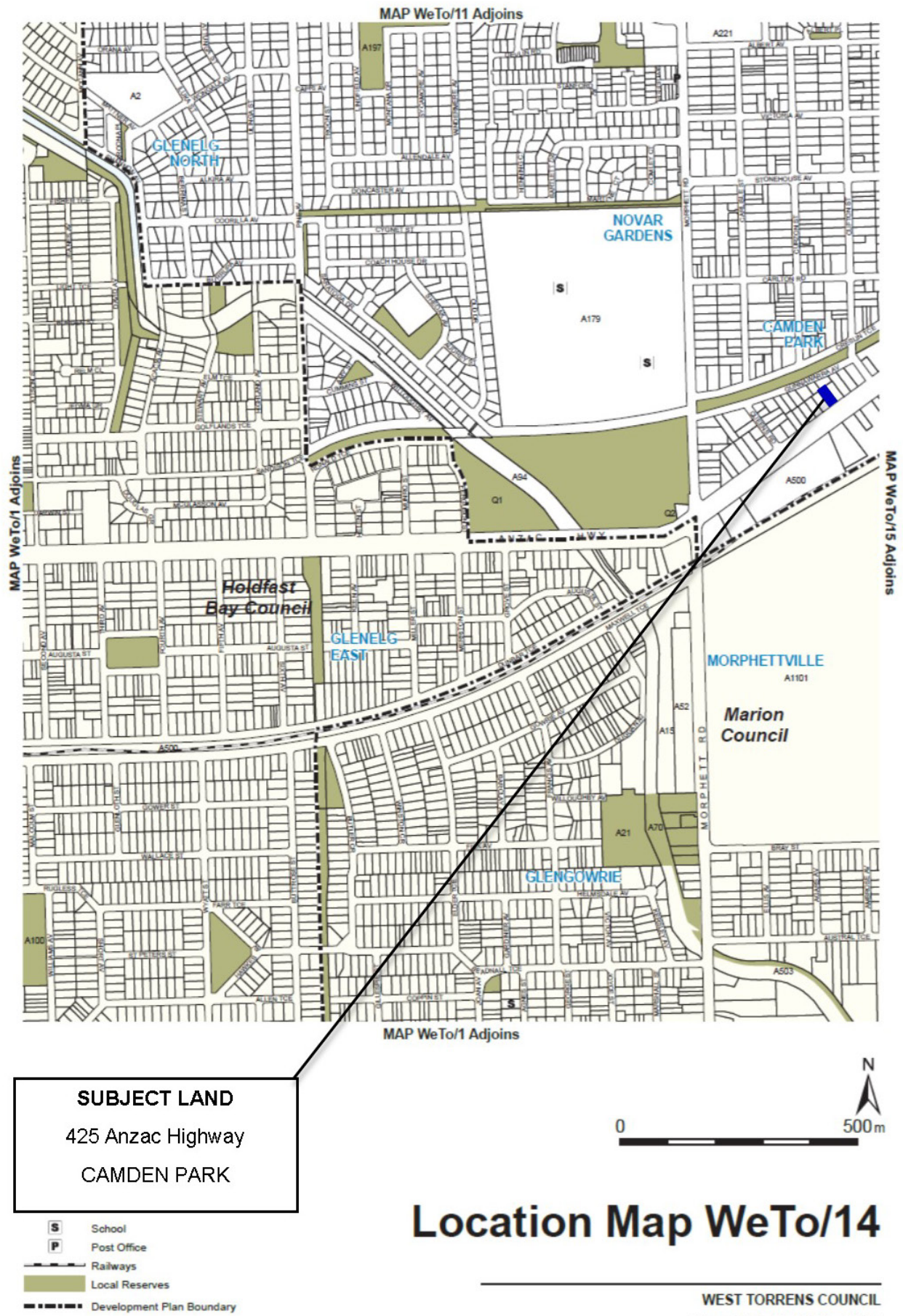
Overall it is considered that the prevailing character of the locality provides a low to medium level of amenity for its residents.

The subject site and locality can be viewed in the following maps.









## PROPOSAL

The proposal is for the demolition of the existing dwelling and shed from the subject site and the construction of a three storey residential flat building containing:

- At ground level:
  - Entry foyer;
  - 19 car parking spaces; and
  - Two bike racks accommodating a total of eleven bicycles.
- Each car parking space is to be provided with an over bonnet storage unit with a 1.8m<sup>3</sup> storage capacity. Each dwelling is attributed two car parking spaces, totalling 3.6m<sup>3</sup> of external storage.
- A bin enclosure and wash area is located between the visitor and residents' parking area.
- Landscaping elements comprised of:
  - 500mm strip along each side boundary; and
  - Three deep soil areas allowing for tree planting, with a total area of 67.2m<sup>2</sup>.
- 1.8m high Colorbond panel type fencing along the side and rear boundaries.
- Two levels of dwelling comprising 4, two bedroom dwellings on each level each with their own balconies.
- Each dwelling contains:
  - Two bedrooms;
  - Open plan kitchen, living and dining area;
  - Bathroom and ensuite; and
  - Balcony.
- Environmentally sensitive design features include:
  - Roof mounted solar panels;
  - LED lighting;
  - Water saving fixtures and fittings;
  - High performance ceiling insulation; and
  - Drought tolerant vegetation.

The proposal plans are contained in **Attachment 1**. An accompanying planning report is contained in **Attachment 2**.



## PUBLIC NOTIFICATION

The application is a Category 3 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations and Residential Zone, Procedural Matters in the West Torrens Council Development Plan.

<b>Properties notified:</b>	50 properties were notified during the public notification process.
<b>Representations:</b>	Two representations were received.
<b>Persons wishing to be heard:</b>	Two representors identified that they wish to address the Panel. <ul style="list-style-type: none"> <li>• Mr George Penglis of 24 Gunnawarra Avenue, Camden Park.</li> <li>• Ms Oggi Stojanovich, of 22 Gunnawarra Avenue, Camden Park.</li> </ul>
<b>Summary of Representations:</b>	<p>Concerns were raised regarding the following matters:</p> <ul style="list-style-type: none"> <li>• The use of materials, such as painted concrete, that are likely to discolour and deteriorate;</li> <li>• Location of rubbish bins and bin wash area being too close to outdoor living areas;</li> <li>• The possibility of unsightly clothes drying on balconies;</li> <li>• Loss of privacy from inadequate screening of balconies;</li> <li>• Lack of provision of a dedicated area for the storage and collection of recyclables on the site;</li> <li>• Noise generated from air-conditioning units and putting waste into bins;</li> <li>• Bulk and scale of the building;</li> <li>• Lack of landscaping to soften appearance of building to adjoining properties;</li> <li>• No provision for on-site commercial collection of waste;</li> <li>• The bins may be used to climb over fences for unauthorised access into neighbour's premises; and</li> <li>• The bin storage area should be of solid construction to minimise noise and sited as far away as possible from habitable rooms and private open space.</li> </ul>

The Applicant has provided a response to the representations, as summarised below:

- The bin storage and wash down area has been relocated towards Anzac Highway and is now enclosed, which will reduce noise and odours;
- Trees at the rear of the property have been altered to medium sized Chinese Pistachios;
- Each dwelling will be provided with internal clothes dryers to discourage outdoor clothes drying on balconies;
- The proposed trees will soften the appearance of the building;
- The balconies include 1.7m high screens, however as the building is three storeys high, no screening is required; and
- The Zone and Policy Area actively encourages buildings up to four storeys in height.

A copy of the representors' concerns and the applicant's response is contained in **Attachments 3** and **4** respectively.

## REFERRALS

### Internal

- City Assets

Comments were provided regarding the following matters:

- Traffic movements are acceptable;
- The height clearance in the ground level car park would only support domestic sized vehicles as per AS/NZ 2890.1:2004;
- A waste management plan establishing likely total household waste volumes, storage requirements and demonstration of vehicle access and movements is required;
- Based on the car park clearance height, lifting of bins into a refuse truck could only occur from the right hand side of a refuse collection vehicle; an arrangement for which they are not usually designed;
- Proposed stormwater management is acceptable;
- Removal of the street tree in the driveway is necessary for access into the development; and
- City Assets is supportive of the application subject to the above mentioned matters being resolved.

Amended plans were provided and have resolved the concerns around waste management.

- Amenity Officer
- The proposed driveway will impact on an existing *Fraxinus angustifolia* (Claret Ash) street tree; and
- It is considered acceptable to remove the street tree subject to Council's standard fees and charges.

A full copy of the relevant reports are contained in **Attachments 5, 6** and **7**.

### External

Pursuant to Schedule 8 of the Development Regulations, the application was referred to:

- Department of Planning Transport and Infrastructure (DPTI)

Comments were provided regarding the following matters:

- Anzac Highway is a Major Traffic Route, Primary Freight Route and Major Cycling Route with an AADT of 33,700 vehicles per day. No objection is raised, subject to Council allowing the street tree to be removed;
- A six metre wide entry and the security gate setback distance is acceptable;
- The visitor park at the front of the site does not allow for convenient and adequate vehicle turning paths as there will be conflict with the landscaped area at the front of the site;

- It should be noted that parking adjacent the site is restricted and Council should ensure that sufficient on-site parking is provided in accordance with AS/NZ 2890.1: 2004; and
- Seven items of advice are provided with recommended conditions relevant to vehicle movements and the function of the arterial road, should the application be approved.

A full copy of the relevant report is contained in **Attachment 8**.

## ASSESSMENT

The subject land is located within the Residential Zone, Medium Density Policy Area 18 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Crime Prevention	Objectives	1
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8 & 10
Design and Appearance	Objectives	2
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19 & 20
Energy Efficiency	Objectives	1 & 2
	Principles of Development Control	1, 2, & 3
Hazards	Objectives	1 & 2
	Principles of Development Control	1
Infrastructure	Objectives	1 & 3
	Principles of Development Control	1, 5, 6 & 8
Interface between Land Uses	Objectives	1 & 2
	Principles of Development Control	1, 2, 4, & 8
Landscaping, Fences and Walls	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4, 5 & 6
Medium and High Rise Development	Objectives	1, 2, 3, 4, 5 & 7
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 13, 15, 20, 21, 23, 24, 25, 26, 28 & 29
Natural Resources	Objectives	1, 2, 5, & 6
	Principles of Development Control	1, 2, 5, 7, 8, 9, 10, 13, 14 & 16
Orderly and Sustainable Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 2, 3 & 5
Residential Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 22, 23, 24, 27, 28, 29, 30 & 31
Transportation and Access	Objectives	2, 4 & 5
	Principles of Development Control	1, 2, 3, 8, 10, 11, 12, 14, 17, 18, 19, 20, 21, 22, 23, 24, 26, 27, 28, 31, 32, 34, 35, 37, 38, 39, 40, 41, 42, 43, 44 & 45
Waste	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4, 5, 6, 10, & 12

<b>Zone: Residential</b>	
<b>Desired Character Statement:</b>	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a <b>Historic Conservation Area</b>.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
<b>Objectives</b>	1, 2, 3 & 4
<b>Principles of Development Control</b>	1, 5, 7, 9, 10, 11, 12, 13 & 14.

<b>Policy Area: Medium Density Policy Area 18</b>	
<b>Desired Character Statement:</b>	
<p><i>Allotments in this policy area will be at medium density, accommodating a range of dwelling types including residential flat buildings, row dwellings, group dwellings, semi-detached dwellings and some detached dwellings on small allotments. Allotment amalgamation to create larger development sites will occur to maximise the density of development while also achieving integrated design outcomes, particularly within a comfortable walking distance of centre zones. Vehicle access will occur from side streets and new rear public and private laneways wherever possible, also supporting the retention of existing street trees.</i></p> <p><i>New buildings will contribute to a highly varied streetscape. Buildings will be up to 3 storeys and provide a strong presence to streets, other than in the part of the policy area in Underdale, Ashford (other than allotments adjacent to <b>Residential Character Ashford Policy Area 22</b>) and allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park where buildings will be up to 4 storeys. Parking areas and garages will be located behind the front facade of buildings.</i></p> <p><i>Buildings on the edge of the policy area which adjoin residential policy areas at lower densities will pay particular attention to managing the interface with adjoining dwellings, especially in terms of the appearance of building height and bulk, and overshadowing.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
<b>Objectives</b>	1
<b>Principles of Development Control</b>	1, 4, 5, 6, 7, & 8

**QUANTITATIVE ASSESSMENT**

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

<b>DEVELOPMENT PLAN PROVISIONS</b>	<b>STANDARD</b>	<b>ASSESSMENT</b>
SITE AREA <i>Residential Zone, Medium Density Policy Area 18</i> PDC 6	150m <sup>2</sup> minimum	Average 94m <sup>2</sup>  <b>Does Not Satisfy</b>
SITE FRONTAGE <i>Residential Zone, Medium Density Policy Area 18</i> PDC 6	15m minimum	18.29m  <b>Satisfies</b>
SITE COVERAGE <i>Residential Zone, Medium Density Policy Area 18</i> PDC 5	70% maximum	67%  <b>Satisfies</b>
STREET SETBACK <i>Residential Zone, Medium Density Policy Area 18</i> PDC 5.	3m minimum	4.98m - 5.93m  <b>Satisfies</b>
SIDE/REAR SETBACKS <i>Residential Zone</i> PDC 11	Side 2m minimum up to 6m high wall  2m + wall height above 6m = 6.3m minimum  Rear 4m minimum	Western side setback 1.5m - 2.8m  Eastern side setback 2.4m  <b>Does Not Satisfy</b>  4m  <b>Satisfies</b>
BUILDING HEIGHT <i>Residential Zone, Medium Density Policy Area 18</i> PDC 5	3 storeys or 12.5m maximum	10.45m  <b>Satisfies</b>

PRIVATE OPEN SPACE <i>General Section, Residential Development</i> PDC 2	11m <sup>2</sup> minimum (dwellings above ground level)	Dwellings 1,4 & 8 = 10.9m <sup>2</sup>  <b>Do Not Satisfy</b>  Dwellings 2, 3, 6 & 7 = 11.2m <sup>2</sup>  <b>Satisfies</b>
LANDSCAPING <i>Landscaping, Fences &amp; Walls</i> PDC: 4	10% minimum	15.8%  <b>Satisfies</b>
CARPARKING SPACES <i>General Section, Transportation and Access</i> PDC 34 & Table WeTo/2	Two spaces per two bedroom dwelling + 0.25 visitor spaces per dwelling = 18 parking spaces required	18 spaces provided (16 + 2 visitor parks)  <b>Satisfies</b>
BICYCLE PARKING <i>General Section, Residential Development</i> PDC 30	Bicycle parking to be provided where more than six dwellings	Two bicycle racks with a total of 11 spaces is provided  <b>Satisfies</b>
STORAGE <i>General Section, Residential Development</i> PDC 31	8m <sup>3</sup> per dwelling	Each dwelling has 3.6m <sup>3</sup> (2 x 1.8m <sup>3</sup> over vehicle parking bay storage)  Apartments 1, 2, 3, 5, 6 & 7 - 6.8m <sup>3</sup> internal storage - total (3.6 + 6.8) = 10.4m <sup>3</sup>  Apartments 4 & 8 - 9.4m <sup>3</sup> internal storage - total (3.6 + 9.4) = 13m <sup>3</sup>  <b>Satisfies</b>

## QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the most relevant considerations are discussed under the following sub headings:

### Land Division

There is no land division associated with the proposal. This is considered satisfactory because whether or not there is a land division, the nature and function of the development would be the same. Therefore, it is considered acceptable to consider the built form first and then a subsequent land division.

## Land Use and Zoning

As previously described, the subject site is located within the Residential Zone, Medium Density Policy Area 18. As the proposal is for a residential flat building, the residential land use is envisaged and supported within the Zone and Policy Area and is therefore appropriate in land use terms.

## Desired Character

The Desired Character statement of the policy area states:

*"Allotments in this policy area will be at medium density, accommodating a range of dwelling types including residential flat buildings, row dwellings, group dwellings, semi-detached dwellings and some detached dwellings on small allotments."*

The density proposed is considered to be high density in accordance with the definitions listed in *"Understanding Residential Densities: A Pictorial Handbook of Adelaide Examples"*. This guide is a useful tool in establishing density and lists net medium density as being between 34-67 dwellings per hectare (or lots in the range of 149m<sup>2</sup> - 294m<sup>2</sup>). The proposed development will have a net density of 106 dwellings per hectare with site areas less than 149m<sup>2</sup>, meaning that it is a high density development that does not strictly accord with the desired character for the area.

Dwelling density calculations do not differentiate between dwelling types i.e. the subject site could contain five studio apartments or five 3 bedroom apartments and still have the same density.

Notwithstanding this departure from the desired character, it is important to consider the circumstances around the policy intent for the area summarised as follows:

1. The subject site is located within a residential zone and policy area where residential (as opposed to mixed use or non-residential) developments of up to four storeys in height are specifically encouraged.
2. Using the residential density guidelines cited above, only five dwellings could be built on the subject site in order for it to be developed at medium density.

This situation highlights a potential policy contradiction between the Desired Character Statement of the relevant policy area and the density guidelines cited in the pictorial handbook (as well as the most recent density guidelines published in *The 30-Year Plan for Greater Adelaide*). As these density guidelines do not take account of the development height (or number of storeys) they are effectively out of step with current planning policy contained in the West Torrens Council Development Plan and Development Plans in most other inner city Councils within the Adelaide Metropolitan Area.

In order to achieve *medium density* residential development as defined by both the 30-Year Plan and the pictorial handbook on this site, either each apartment would need to have a very large floor area or, alternatively, only a portion of the site could be developed leaving a considerable portion of the site unutilised. Both of these options are considered to be undesirable for the subject site as discussed below.

PDC 9 of the General Section, Residential Development states the following:

*"PDC 9 Residential development should provide a high quality living environment by ensuring the following minimum internal floor areas (including internal storage areas but not including balconies and car parking):*

- (a) studio (where there is no separate bedroom): 37 square metres*
- (b) 1 bedroom dwelling/apartment: 50 square metres*
- (c) 2 bedroom dwelling/apartment: 75 square metres*
- (d) 3+ bedroom dwelling/apartment: 100 square metres."*

The policy recognises that even the largest dwelling/apartment unit only requires a minimum internal floor area of 100m<sup>2</sup>. The proposed development envisages eight (8) 2 bedroom apartments - 4 on level 1 and 4 on level 2 - with floor areas ranging between 84.5 - 92.3m<sup>2</sup>. This clearly complies with the policy requirement.

There are a number of ways of developing the site, however it is considered that the two most feasible alternatives that would meet the requirement of 'medium density' would be to have either:

- i. One of more apartments above each other. On the assumption that the ground floor would be car parking (as proposed) it would be possible to have two apartments of 150m<sup>2</sup> on the first and second floors and a third apartment of 300m<sup>2</sup> on the third floor. Even with this generous floor area, only approximately one-third of the site could be developed; or
- ii. Each apartment being four storeys. With the ground floor as car parking (as proposed) and each apartment with floor levels of approximately 35 - 40m<sup>2</sup>, the resultant development would yield a similar outcome to the above example with only around one-third of the site utilised.

Neither of these alternatives are considered to be in line with the policy intent of both the Zone and Policy Area, especially due to the site's proximity to public transport and local facilities.

Development undertaken in such a form would also be uneconomic and likely to result in a poor design outcome.

Whilst it could be argued that each of the apartments could have larger floor areas, it is considered that the market is unlikely to respond favourably to apartments with floor areas greater than 300m<sup>2</sup> in this location. Potential purchasers looking for this much space would in all likelihood be seeking detached dwellings.

The Desired Character Statement seeks to encourage the amalgamation of sites in order to "*maximise the density*". In practice, site amalgamation is often difficult and sometimes impossible to achieve. It relies on developers being able to 'land bank' whilst negotiating with neighbouring property owners who often seek unreasonable prices for their land as they are aware that the developer needs it to fulfil their development. This often results in projects being abandoned.

The proposed development is effectively seeking a compromise between the desired density and built form by proposing eight dwellings within a three storey residential flat building.

Notwithstanding the policy tension between the desired character and the definition of 'medium density', it is considered, on balance, that the proposal is generally in keeping with the Desired Character of the Zone and Policy Area.

### Site Area

As the proposal is not seeking a land division at this stage, it is important to consider the *site* area rather than the *allotment* area. Medium Density Policy Area 18 states that the minimum average site area per dwelling within a residential flat building should be 150m<sup>2</sup>. The proposed average site area per dwelling is 94m<sup>2</sup>. This deficiency is not considered fatal to the application for the reasons provided in the Land Use, Zoning and Desired Character section of this report.

The Applicant has drawn attention to two other residential flat buildings within the local area that have average site areas in the order of 80m<sup>2</sup> and 119m<sup>2</sup>. While it is acknowledged that these were approved and constructed under different legislation, they nonetheless comprise part of the character of the area. It is also worth noting that this higher density form of development has been deemed appropriate in this location since at least the 1970's.

The Applicant has also drawn parallels between this development and what can be achieved as affordable housing. Whilst it is acknowledged that affordable housing could be located in this area and have reduced site areas, the proposed development is not part of the affordable housing scheme and therefore cannot utilise the benefits (lesser site areas) attributed to them.



## Appearance, Bulk and Scale

The proposed three storey built form is supported by the Desired Character of the Medium Density Policy 18 that seeks buildings up to four storeys in height along the section of Anzac Highway between Morphett Road and Cromer Street.

The proposed building is of a relatively simple cubic form. Through the use of strong horizontal and vertical lines, changes in colours and textures and a degree of articulation this assists in limiting the apparent bulk and scale of the proposal.

There is an existing example of three storey built form, two properties north east of the subject site, at 421 Anzac Highway. This three storey residential flat building contains 12 dwellings, four more than what is proposed at the subject site. This building exhibits design characteristics commonly found in buildings from the 1970's e.g. unarticulated cream brick walls, no balconies, external staircases and repetitive window placement. This building is considerably larger than surrounding development and, as such, is not in keeping with the existing character of the area in terms of bulk and scale.

Whilst the proposed building will have a similar overall height as 421 Anzac Highway, the articulation of the building, use of colours and materials, as well as the use of strong vertical and horizontal elements will aid in reducing its visual bulk. The proposed building references human scale with the inclusion of a parapet/verandah element protruding from the main face.

Whilst the locality does not currently exhibit many large buildings, the Desired Character of the Policy Area is actively encouraging buildings of this scale as a means of changing the character of the area.

PDC 6 of the Medium and High Rise Development (3 or More storeys) states:

*"Materials and finishes should be selected to be durable and age well to minimise ongoing maintenance requirements. This may be achieved through the use of materials such as masonry, natural stone and prefinished materials that minimise staining, discolouring or deterioration."*

Durability of materials was raised as an issue in the representations. The majority of the building is finished with face brick, although the eastern façade is comprised of painted tilt up concrete panels. When in good condition, this design element will be a positive attribute to the overall visual amenity of the building. If left to deteriorate, this same element is likely to degrade the visual amenity of the building. In the absence of a land division, it will be in the owner's best interests to maintain the building to a high standard in order to rent out the apartments. When a land division is submitted, building maintenance will be covered by the Scheme Description and managed by the body corporate. There is sufficient room between the building and the property boundaries to undertake maintenance of the property.

## Setbacks

There are tensions in the relevant provisions of the Development Plan that relate to side setbacks and the four story built form encouraged by the Desired Character of the Policy Area. Side setback requirements are not specified in the Medium Density Policy Area 18 provisions meaning that the side setback provisions of the Residential Zone apply. PDC 11 states:

*PDC 11 Except where otherwise specified by a policy area, and for party walls, dwellings and buildings containing dwellings should be set back from the side and rear boundaries in accordance with the following table:*

<i>Parameter</i>	<i>Minimum value (metres)</i>
<i>Side boundary setback where the vertical side wall is 3 metres or less in height (measured from the existing ground level at the boundary of the adjacent property as per Figure 1)</i>	1
<i>Side boundary setback where vertical side wall measures between 3 to 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per Figure 1)</i>	2
<b><i>Side boundary setback where the vertical side wall is greater than 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per Figure 1)</i></b>	<b><i>2 metres plus an additional setback which is equal to the increase in wall height above 6 metres.</i></b>
<i>Rear boundary setback for single storey components of a building</i>	3
<i>Rear boundary setback for two or more storey components of a building</i>	8

When calculating the side boundary setback for the proposed development, the height of the wall above six metres is to be added to the two metres stated in the table above. The proposed development has a wall height of 10.45m, meaning that the side setback of a portion of the second storey and the third storey should be 6.45m from each side. This equates to a total of 12.9m being used for side setbacks on an allotment that is 18.29m wide. This would only allow a width of 5.39m to accommodate a third level and would not allow for a fourth level of any dimension in an area for which this scale of development is specifically envisaged.

This is an area in which the majority of sites are similar to the subject land in terms of size. Even if two allotments were developed together, this setback requirement would be a significant constraint to achieving three and four storey development. It seems reasonable to accept some variation from the setback provisions in this part of the Policy Area if the Desired Character is to be achieved. If lesser setbacks are proposed it should be demonstrated that there will not be detrimental impacts arising from the deficiency. One of the biggest impacts that can occur is overshadowing (discussed below).

The building has a minimum setback from the western boundary of 1.5m and a minimum setback from the eastern boundary of 2.4m. The 1.5m setback is taken from the front portion of the building which is the staircase. The staircase has two windows (obscured to 1.7m above floor level) that provide light internally. The remainder of the building is setback 2.8m from the western boundary. The eastern side of the of the building is setback 2.4m from the boundary, however levels two and three include an articulated built form that changes between solid walls and screened balconies, incorporating a variety of materials and adding visual interest to the design.

The three storey residential flat building at 421 Anzac Highway has side setbacks of approximately 2.5m from the western side boundary and 3m from the eastern side boundary. These setbacks are also similar to those of the two and three storey residential flat buildings at 445 and 447- 449 Anzac Highway.

Considering the articulated built form, obscured glazing and balconies of the proposed development, the desired built form and other examples in the area, the reduced setbacks are considered suitable. The resultant siting and built form provide adequate separation between adjoining properties and will not impose any unreasonable visual impacts beyond the site.

## Amenity

### Overlooking

The issue of overlooking has been addressed by the amended plans which respond to concerns expressed by neighbours of the subject site. Direct overlooking has been mitigated by the inclusion of perforated screens fitted to all balconies to a height of 1.7m above the relevant floor levels. Side and rear windows will also be fitted with fixed obscure glazing to a height of 1.7m above floor levels. It is considered that the potential for direct overlooking into the adjacent premises has been adequately controlled and will therefore satisfy the General Section, Residential Development PDC 27.

Similarly, it is considered that the fixed obscured glazing and perforated screening to the balconies provides adequate protection to the private open space and habitable room windows of adjoining properties.

The proposed perforated screens on the balconies will also mitigate the representors' concerns about the potential visual impact arising from washing being hung out on the balconies. The Applicant has also indicated that clothes dryers will be installed in each apartment.

### Overshadowing

The Development Plan uses a number of techniques to ensure that adequate daylight and sunlight remains available to adjoining dwellings and private open space. Principles of Development Control provide some guidance for daylight and sunlight access in General Section - Residential Development PDCs 10, 11, 12 & 13.

- |                |  |
|----------------|--|
| <i>"PDC 10</i> | <i>The design and location of buildings should ensure that direct winter sunlight is available to adjacent dwellings, with particular consideration given to:</i>  |
|                | <i>(a) windows of habitable rooms, particularly living areas</i>   |
|                | <i>(b) ground-level private open space</i>   |
|                | <i>(c) upper-level private balconies that provide the primary open space area for any dwelling</i>   |
|                | <i>(d) access to solar energy.</i>   |
| <i>PDC 11</i>  | <i>Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.</i> |
| <i>PDC 12</i>  | <i>Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:</i>   |
|                | <i>(a) half of the existing ground-level open space</i>  |
|                | <i>(b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).</i>   |
| <i>PDC 13</i>  | <i>Development should not increase the overshadowed area by more than 20 per cent in cases where overshadowing already exceeds these requirements."</i>  |

Shadow diagrams have been provided by the Applicant demonstrating that on 21 June the adjacent dwelling at 427 Anzac Highway will be partially overshadowed. However, this overshadowing will only occur between 9am and 12pm. The north facing windows of this dwelling will receive at least three hours of direct winter sunlight between 1pm and 5pm, which is consistent with PDC 11. The private open space of this dwelling will receive direct sunlight between at least 1pm and 5pm, which exceeds the minimum requirements set out by General Section - Residential Development PDC 12.

### Noise

Air conditioning units are often a source of noise and disturbance to neighbouring properties and this was raised as an issue by the representors. In the response to the representations, it was stated that the air-conditioning units will be fitted to the roof in order to minimise noise impacts. As most of the adjoining dwellings are single storey in height, this will mean that the noise sensitive locations (bedrooms, POS etc) will be considerably setback from the noise source.

### Waste Storage and Collection

In the original proposal, the bin enclosure was to be an open area located at the rear of the site. The location of this enclosure was reconsidered having regard to the concerns raised by the representors. The amended proposal incorporates a fully enclosed bin storage and wash down area located immediately behind the visitor car park and screened by landscaping. This reasonably addresses issues of litter, odour, flies and vermin.

The Applicant has provided expert advice that the development will only require eight mobile garbage bins and that this is within Council's range for its kerbside waste collection service. Therefore, issues relating to the availability of small commercial waste collection vehicles, on-site manoeuvring constraints and noise from collection vehicles are avoided. This arrangement is considered acceptable by Council's City Assets Department.

### Landscape Assessment

PDC 4 of the General Section - Landscaping, Fences & Walls Module states:

*"PDC 4 A minimum of 10 per cent of a development site should be landscaped. The development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities."*

The proposed development incorporates 15.8% of the site area as landscaping, thereby satisfying the above provision. The proposal also includes three areas of deep soil landscaping providing opportunity for trees to be planted. The planting of trees adds a green vertical element that will help soften the appearance of the built form and offset the extent of hard paved surfaces. These deep soil areas are located at the front and rear of the subject site and total 72m<sup>2</sup> which exceeds the 4m x 4m area described in PDC 8 of the General Section, Landscaping, Fences and Walls and PDC 23 of the Medium and High Rise Development (3 or More storeys). Minimal landscaping is proposed along the side boundaries.

The extent of landscaping at the front and rear of the development is considered sufficient to soften and enhance the appearance of the car parking and waste collection areas and is considered acceptable as a result.

### Heritage

The Claret Ash street trees along Anzac Highway, including the tree in front of the subject site, are listed as a Local Heritage Place in Table WeTo/4 and ordinarily would not be suitable for removal. However, the tree is in poor health and for this reason the Amenity Officer has agreed to its removal subject to Council's normal fees.

As the tree is located in the road reserve, the Applicant does not have the ability to replant it in another location. The replacement of this tree will be considered by Council's arboriculture staff and will likely include a replacement tree of the same species located in a more suitable location. There is no consistent distance between the street trees and therefore the replacement in a slightly different position will not look out of place.

### **Stormwater**

The Applicant has provided a stormwater management plan by mlie Consulting. The details of this report have been considered by Council's Assets department and they are supportive of it as it satisfies Council's standard minimum requirements.

### **SUMMARY**

The proposed development is of a type that is generally envisaged by the Zone and Policy Area. Whilst the density, average site area and side setbacks do not meet the relevant quantitative provisions, these aspects are not considered fatal to the application. The subject site is located in an area that specifically seeks higher density residential development in conjunction with built form up to four storeys in height. There are examples of similar development within the area, the closest of which is located two properties north of the subject site.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 30 May 2017 and warrants Development Plan Consent.

### **Attachments**

1. **Application documents**
2. **Applicant's planning report**
3. **Public Representations**
4. **Applicant's response to representations**
5. **City Assets' response**
6. **City Assets' response re waste management**
7. **Amenity Officer's response**
8. **DPTI response**







AREAS- SECOND LEVEL	
APARTMENT 5	89.13 sqm
APARTMENT 6	84.46 sqm
APARTMENT 7	84.46 sqm
APARTMENT 8	100.78 sqm
COMMON/ STAIRS/ LIFT	54.38 sqm
<b>TOTAL FLOOR AREA</b>	<b>413.21 sqm</b>

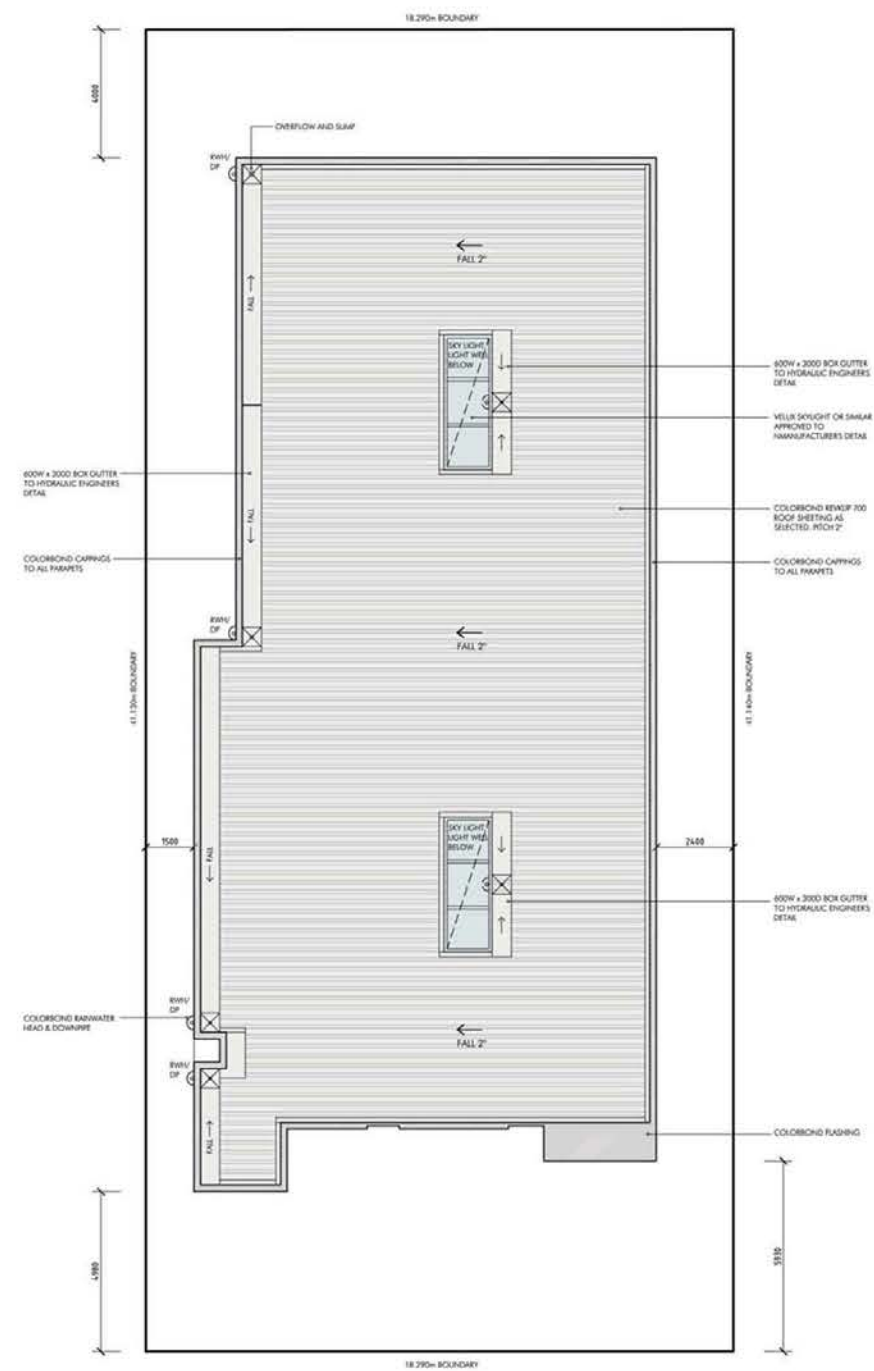
PLANNING RFI AMENDMENTS [P5]

PITEODESIGN  
ARCHITECTS









 **ROOF PLAN**  
1:100 @ A1, 1:200 @ A3

AREAS- FIRST LEVEL	
APARTMENT 1	85.63 sqm
APARTMENT 2	84.46 sqm
APARTMENT 3	84.46 sqm
APARTMENT 4	100.78 sqm
COMMON/ STAIRS/ LIFT	47.36 sqm
TOTAL FLOOR AREA	402.69 sqm

AREAS- SECOND LEVEL	
APARTMENT 5	89.13 sqm
APARTMENT 6	84.46 sqm
APARTMENT 7	84.46 sqm
APARTMENT 8	100.78 sqm
COMMON/ STAIRS/ LIFT	54.38 sqm
TOTAL FLOOR AREA	413.21 sqm

PLANNING RFI AMENDMENTS [P5]

project no. 1703	drawing no. PA-03	amendments

PROPOSED 3-STORY APARTMENT DEVELOPMENT  
ADDRESS: 425 ANZAC HIGHWAY, CAMDEN PARK SA 5038  
FOR: ANGELO & MARIA POLYMENEAS

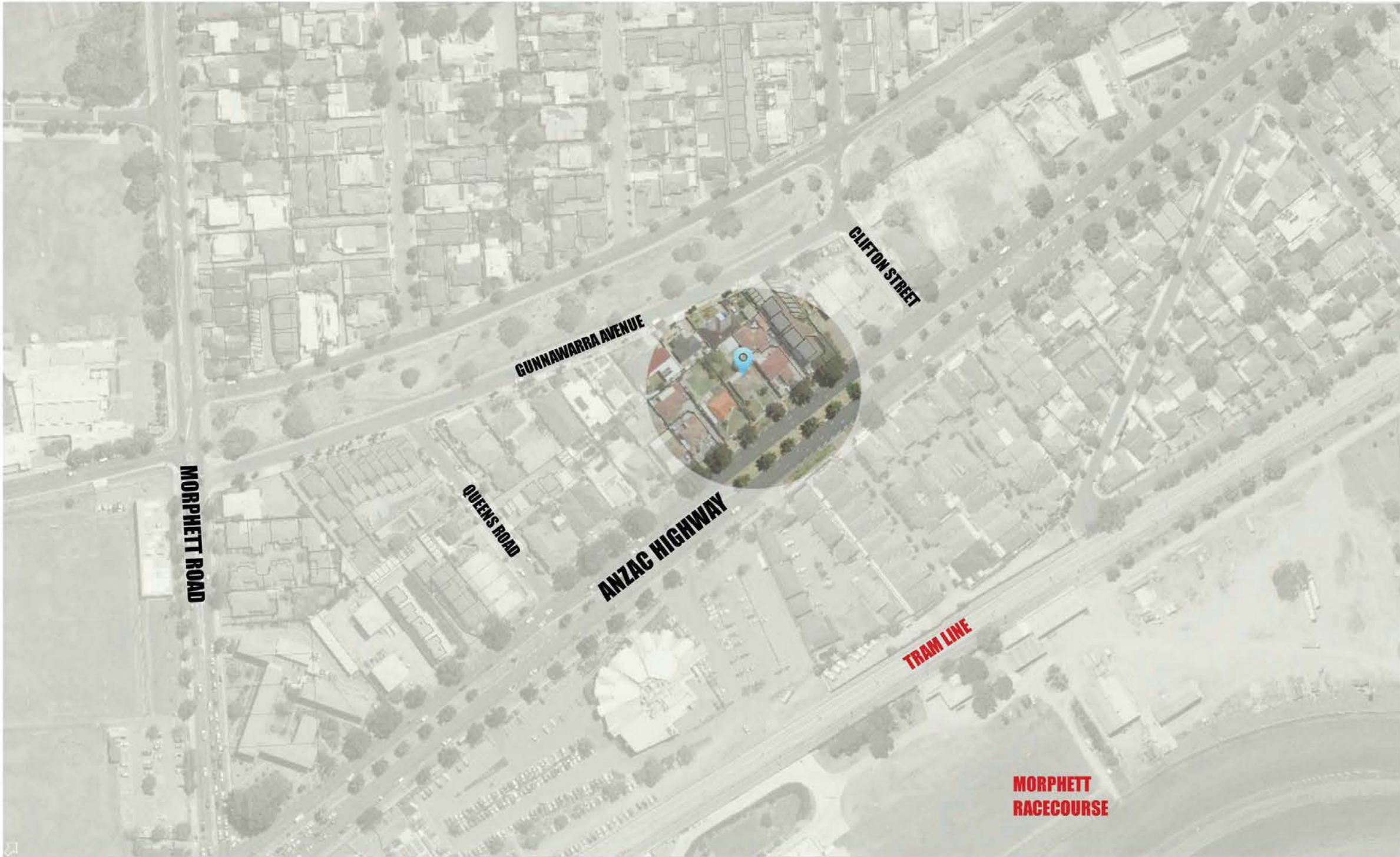
drawn  
checked  
scale  
issue date  
revision  
AC  
PP  
AS NOTED  
29/09/2017

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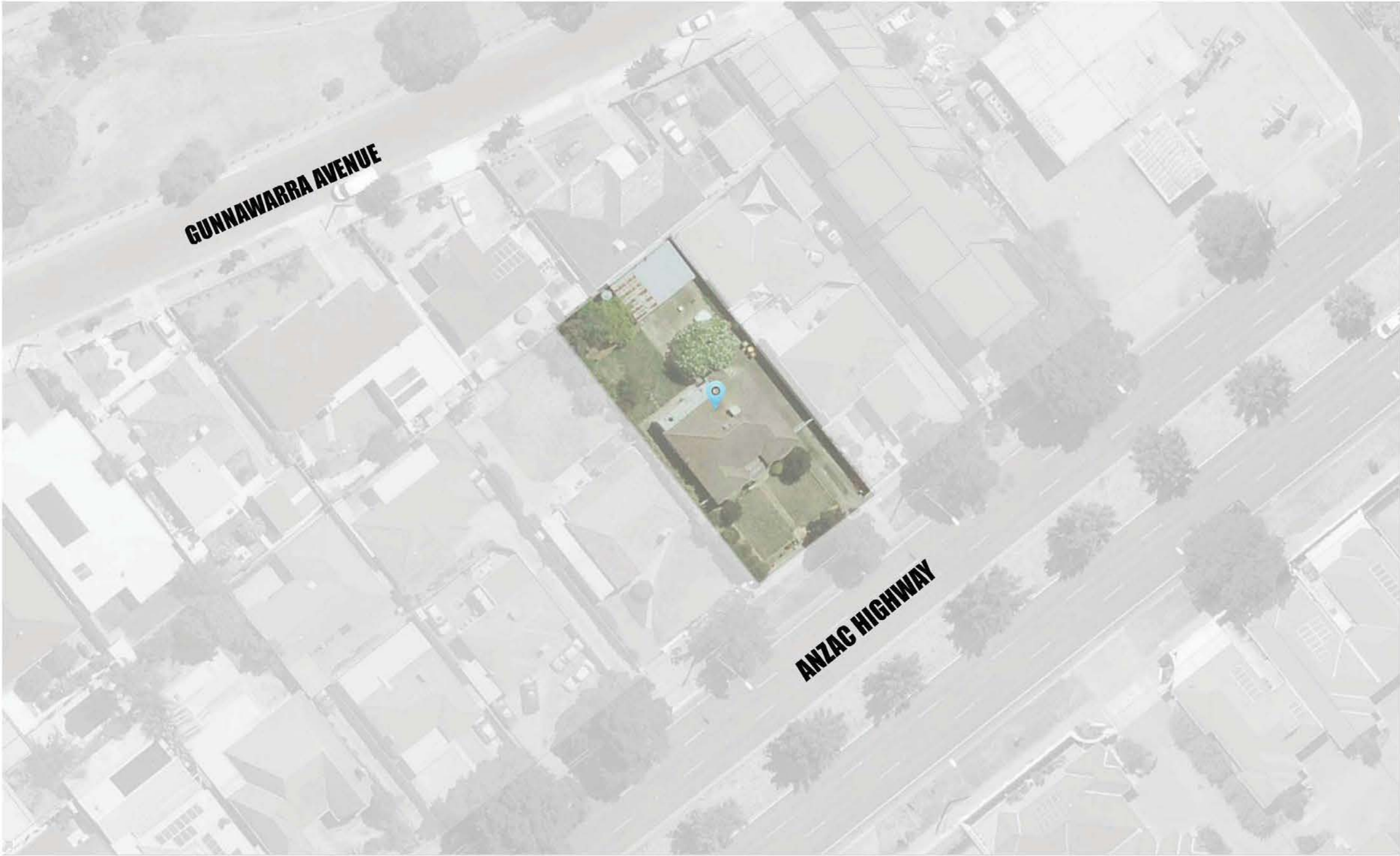


01 Location

SITE ANALYSIS

project no.	PROPOSED 3 STOREY APARTMENT DEVELOPMENT	drawn AC	<div><div></div><div>Member Australian Institute of Architects</div></div>	<div><div>p +61 8 8231 7550</div><div>w piteodesign.com.au</div><div>a 196 Gilbert Street, Adelaide, 5000</div></div>	<div><div>PITEODESIGN</div><div>ARCHITECTS</div></div>
drawing no.	ADDRESS: 425 ANZAC HIGHWAY, CAMDEN PARK SA 5038	checked PP			
	FOR: ANGELO & MARIA POLYMENEAS	scale AS SHOWN			
		issue date 06/02/2017			
		Revision	<div><div>documents are not for construction purposes unless stamped FOR CONSTRUCTION builder to verify all dimensions/ levels on site before commencing copyright © 2017</div></div>		



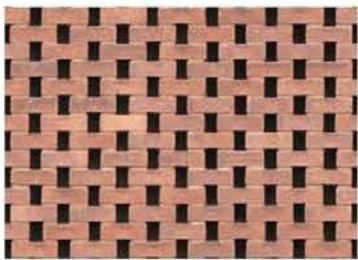


02 Existing Site

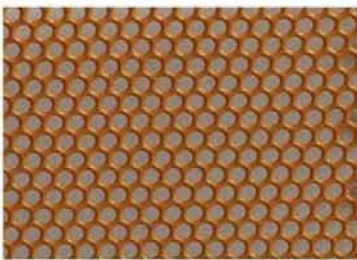
SITE ANALYSIS

project no.	PROPOSED 3 STOREY APARTMENT DEVELOPMENT	drawn AC	<small>documents are not for construction purposes unless stamped FOR CONSTRUCTION. builder to verify all dimensions/ levels on site before commencing. copyright © 2017</small>	 Member Australian Institute of Architects	p +61 8 8231 7550 w piteodesign.com.au a 196 Gilbert Street, Adelaide, 5000	PITEODESIGN ARCHITECTS
drawing no.	ADDRESS: 425 ANZAC HIGHWAY, CAMDEN PARK SA 5038 FOR: ANGELO & MARIA POLYMENEAS	checked PP				
		scale AS SHOWN				
		issue date 06/02/2017				
		Revision				





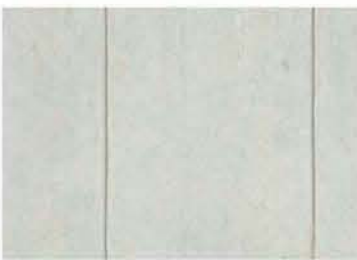
brick



perforated metal



clear glass



precast concrete

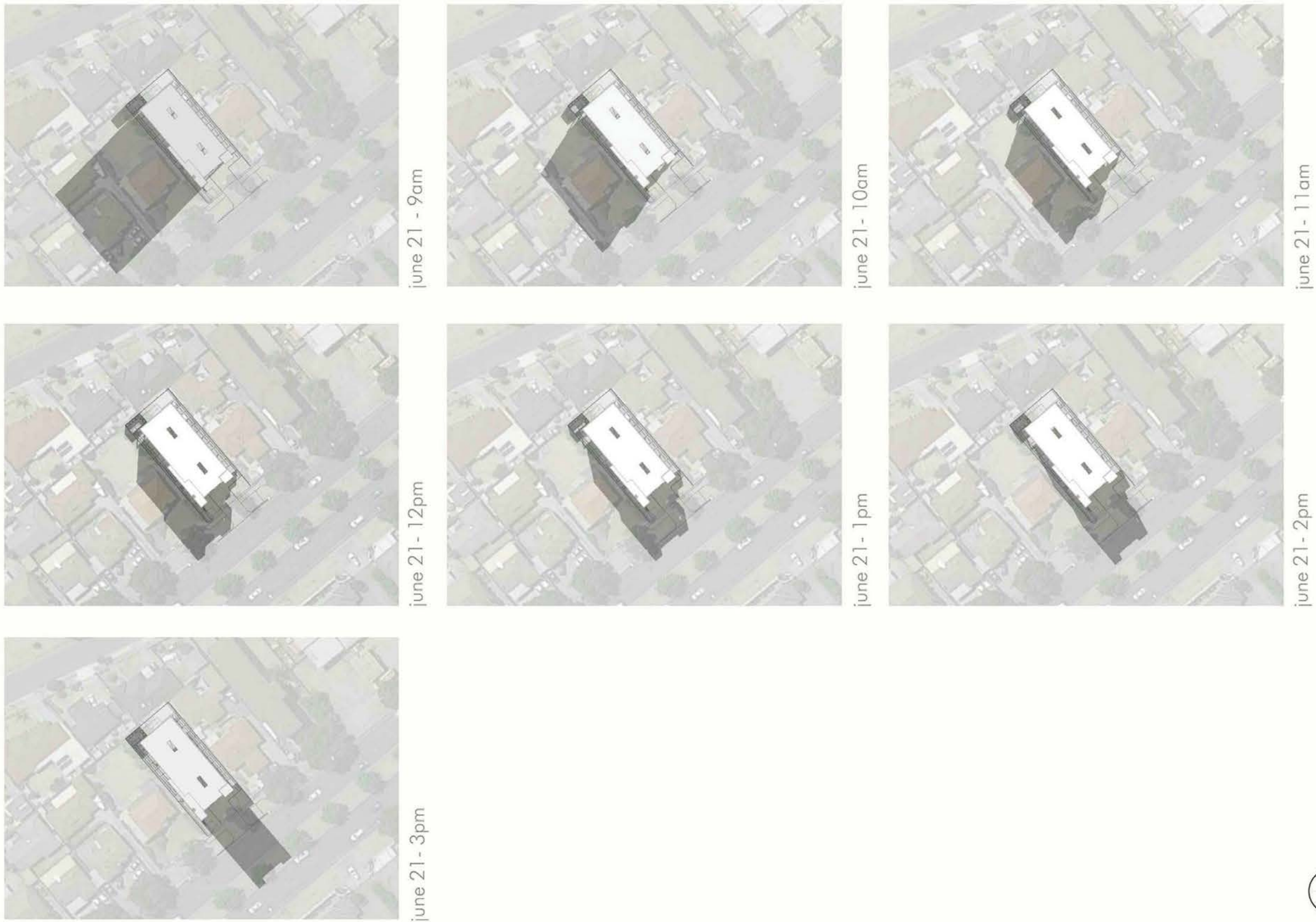


vertical batten screening

### 03 Materiality & Texture

project no.	PROPOSED 3 STOREY APARTMENT DEVELOPMENT		drawn	AC	<div>documents are not for construction purposes unless stamped FOR CONSTRUCTION. builder to verify all dimensions/ levels on site before commencing. copyright © 2017</div> <div>Member Australian Institute of Architects</div>	p +61 8 8231 7550 w piteodesign.com.au a 196 Gilbert Street, Adelaide. 5000	PITEODESIGN ARCHITECTS
drawing no.	ADDRESS: 425 ANZAC HIGHWAY, CAMDEN PARK SA 5038	checked	PP				
	FOR: ANGELO & MARIA POLYMENEAS	scale	AS SHOWN				
		issue date	06/02/2017				
		Revision					





SHADOW DIAGRAMS- JUNE 21 [P1]

project no.	drawing no.	amendments	PROPOSED 3-STORY APARTMENT DEVELOPMENT ADDRESS: 425 ANZAC HIGHWAY, CAMDEN PARK SA 5038 FOR: ANGELO & MARIA POLYMENEAS	drawn: checked: scale: issue date: revision:	AC: PP: AS NOTED 10/04/2017	documents are not for construction purposes unless stamped FOR CONSTRUCTION. builder to verify all dimensions/ levels on site before commencing. copyright © 2017	 Member Australian Institute of Architects	p +61 8 8231 7550 w piteodesign.com.au a 196 Gilbert Street, Adelaide, 5000	<b>PITEODESIGN</b> ARCHITECTS
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CONSULTING  
ENGINEERS

14<sup>th</sup> of September 2017

Reference: 2017-5796CP

Mr A & Mrs M Polymeneas  
C/- Mr Paul Piteo – Piteo Design Architects  
196 Gilbert Street,  
ADELAIDE SA 5000  
Email: [paul@pitedesign.com.au](mailto:paul@pitedesign.com.au)

Dear Mr A & Mrs M Polymeneas,

**DETAILED STORMWATER DRAINAGE ADVICE FOR THE PROPOSED DEVELOPMENT AT 425 ANZAC  
HIGHWAY, CAMDEN PARK**

MLEI have been engaged to assess the implications of a potential development at 425 Anzac Highway, Camden Park and provide a detailed stormwater drainage assessment based on our findings.

**Site Description**

The subject site has an approximate area of 753 square metres. It is bounded by neighbouring residential properties on both sides including the rear, and fronts to Anzac Highway. The site has a high point of 100.21m AHD (Australian Height Datum) located at the street boundary on the existing driveway, and gradually slopes towards the rear of the property.

The existing site is a single residential property and consists of a combination of surfaces, including concrete, landscaped and roof areas. Details of the existing stormwater drainage system are unknown, and there does not appear to be an existing street kerb outlet at the front of the property on Anzac Highway.

**Proposed Development**

The potential development comprises a multi-storey apartment building, including an 18 car ground level carpark (including 2 visitor parks and disability parking). The apartment building will house a total of 8 apartments over 2 residential floors.

After discussions with a West Torrens City Council representative, it has been agreed that the proposal will consider the following;

- A pre-development runoff coefficient of 0.25 will be adopted for the 20 year average recurrence interval (ARI) storm event.
- The proposed stormwater system will be sized to serve the 20 ARI storm event.
- Water treatment to be in accordance with the State Government's Water Sensitive Urban Design Policy.

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### **Pre-development Hydrology**

As indicated by the site survey, currently stormwater runoff flows from the concreted areas onto the landscaped areas. It is assumed that these areas act as soakage for stormwater. As mentioned above, there is no street kerb outlet and thus, it can only be speculated that stormwater captured on the roof of the dwelling is conveyed through the eaves gutters and discharged either directly into the existing underground stormwater infrastructure within Anzac Highway or onto the soakage areas within the site. Given the sloping of the site, in large rainfall events stormwater will begin pooling in the rear of the property.

Based on the 20 year ARI storm event and an adopted runoff coefficient of 0.25, the pre-development site generates a peak stormwater flow of 6.33L/s.

### **Catchment Analysis**

The proposed development was analysed as 4 separate catchment areas; paved, carpark, roof and soft landscaped. As shown on the architectural plans, portions of the carpark are not covered and hence, grated inlet pits (GIP) have been included in the undercover carpark to eliminate any flooding or pooling of water.

As per the requirement of Council, stormwater runoff from the carpark and roof area will pass through an 'Ecosol Storm Pit 10L (Class 2)' filtration system, where stormwater will be treated to the water quality standard specified in the State Government Water Sensitive Urban Design Policy.

Given the Council requirement of a pre-development runoff coefficient of 0.25, the 1 in 20 ARI storm detention volume required for the development is calculated to be 2,674L. The catchment will utilise a 4,000L capacity 'DRAINWELL subsurface water management system' to achieve the required detention volume, which includes an additional 1,326L of retention capacity. The retention/detention system will be located under the carpark entrance driveway. At the outlet of the underground tank system a 70.5mm diameter orifice plate will restrict flows to the calculated allowable discharge of 5.09L/s, allowing the remaining 1.24L/s to discharge unrestricted.

Water exiting the detention tank will be pumped to a proposed GIP located at the boundary of the property and finally conveyed to a street kerb outlet on Anzac Highway by means of gravity.

We trust that the detailed stormwater drainage advice has demonstrated a strategy to ensure the receiving stormwater drainage system is not adversely affected by the potential development at 425 Anzac Highway, Camden Park.

If you have any queries regarding this letter, please contact the undersigned on 8231 2832 or by email [agiannini@mlei.com.au](mailto:agiannini@mlei.com.au)

Kind Regards,

**MLEI Consulting Engineers**

**Anthony Giannini** B.E Hons. (Civil & Structural)  
**Graduate Civil Engineer**

*Enc:*

- *Civil stormwater calculations*
- *Music modelling report*

---

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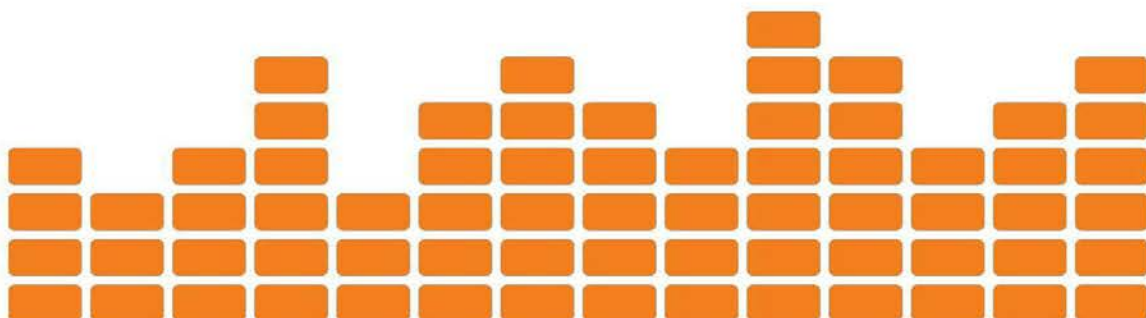
## **CIVIL STORMWATER CALCULATIONS**

**425 Anzac Highway, Camden Park**

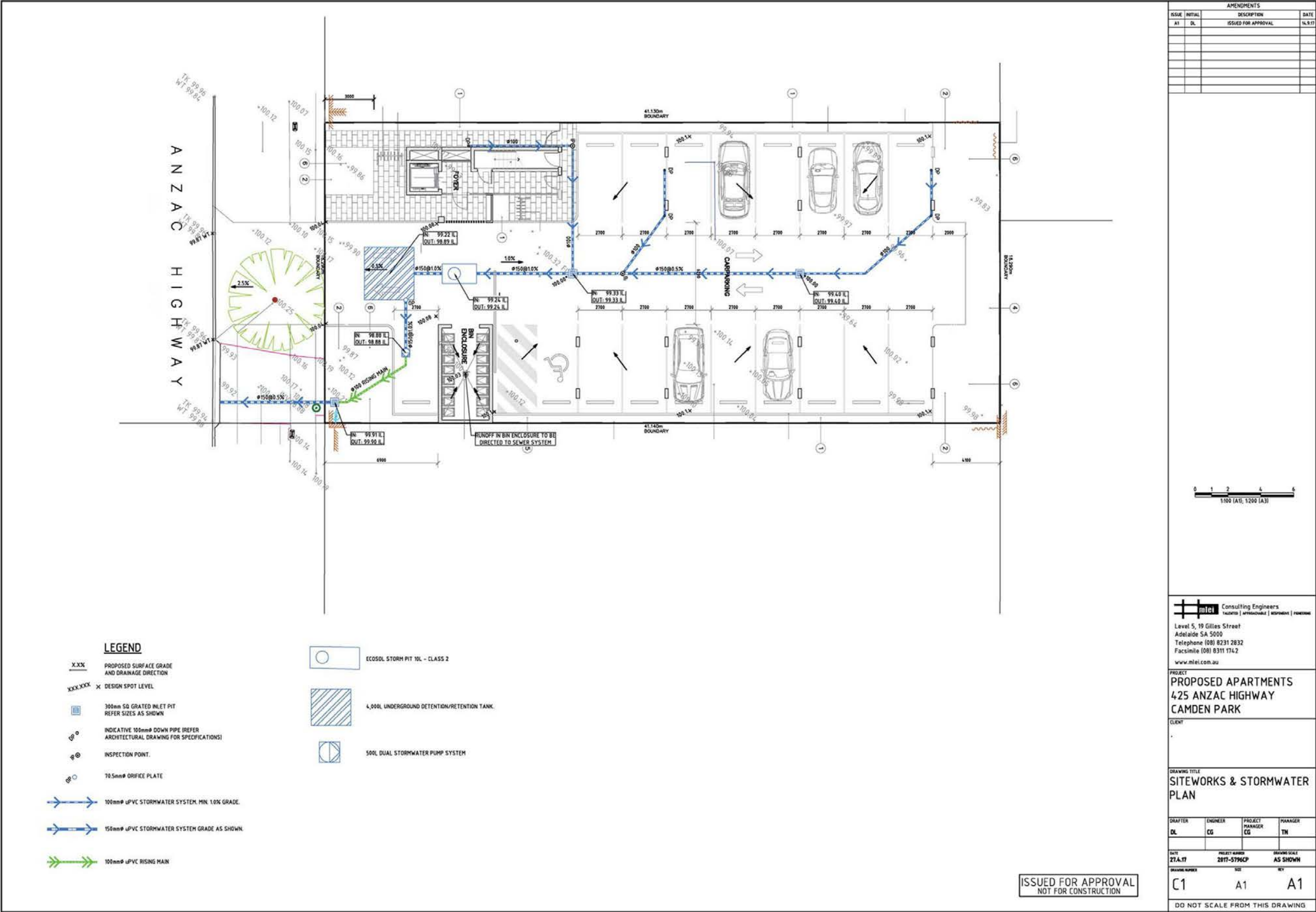
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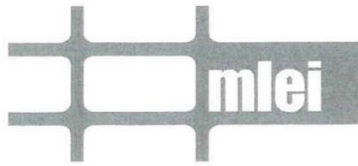
**ISSUE B**

**DATE: 14/09/2017**



ADELAIDE: LEVEL 5, 19 GILLES STREET ADELAIDE SA / [08] 8231 2832  
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CONSULTING  
ENGINEERSReference 2017-5796CPProject 425 Anzac Highway, Camden Park

Checked by \_\_\_\_\_

Designer Y.CDate 24/04/2017Index 1**Council Requirement:**

- Pipes to be sized for 20 years ARI
- 20 years ARI of pre-development = 20 years ARI of post development, where coefficient of runoff  $C = 0.25$  for pre-development.
- Assume time of concentration  $t_c = 5 \text{ min.}$

**Pre-development Flow:**

Refer 'Pre-development Plan'

Using BOM-IFD,  ${}_{10}^{1\text{hr}} I = 25.9 \text{ mm/hr}$ ,  ${}_{20}^{5\text{min}} I = 12 \text{ mm/hr}$ 

$$\begin{aligned}
 C_{10} &= 0.1 + 0.0133 ({}_{10}^{1\text{hr}} I - 25) \\
 &= 0.1 + 0.0133 (25.9 - 25) \\
 &= 0.1 + 0.0133 (0.9) \\
 &= 0.11197 \quad \text{--- ①}
 \end{aligned}$$

Eq. 14.12 ARR

 $C_{20} = 0.25$  (for previous development plan only)

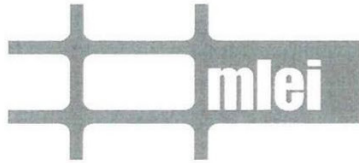
$$Q_{\text{pre}} = 0.278 \times C_{20} \times {}_{20}^{t_c} I \times A_{\text{site}},$$

Eq. 5.1 ARR

assume time of concentration  $t_c = 5 \text{ mins}$ 

$$\begin{aligned}
 &= 0.278 \times 0.25 \times 12 \times 752.58 \times 10^{-6} \\
 &= 6.3288 \times 10^{-3} \text{ m}^3/\text{s} \\
 &= 6.33 \text{ L/s}
 \end{aligned}$$



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ENGINEERSReference 2017-5796 CPProject 425 Anzac Highway, Camden Park

Checked by \_\_\_\_\_

Designer K.CDate 24/04/2017Index 2Post-development flow:

Refer 'Post-development Plan'

\* Unrestricted flow (allowing impervious surface area to drain unrestricted)

$$f = 1.0 \text{ (impervious surface)}$$

$$\text{using } \textcircled{1}, C'_{10} = 0.11197$$

$$C_{10} = 0.9(1.0) + 0.11197(1-1)$$

$$= 0.9$$

Eq. 14.11 ARR

$$\therefore C_{20} = F_{20} \times C_{10} \quad \text{where } F_{20} = 1.05 \text{ from table 14.6 ARR}$$

$$= 1.05 \times 0.9$$

$$= 0.945$$

Eq. 14.13 ARR

Assume time of concentration,  $t_c = 5 \text{ min}$

using BOM-IFD,  $\frac{5 \text{ min}}{20} = 121 \text{ mm/hr}$

$$Q_{\text{unrestricted}} = 0.278 \times C_{20} \times \frac{5 \text{ min}}{20} \times A_{\text{imp}}$$

$$Q_{\text{unrestricted}} = 0.278 \times C_{20} \times \frac{5 \text{ min}}{20} \times A_{\text{imp}}$$

$$= 0.278 \times 0.945 \times 121 \times 38.95 \times 10^{-6}$$

$$= 1.238 \times 10^{-3} \text{ m}^3/\text{s}$$

$$= 1.238 \text{ L/s}$$

$$\therefore \text{Allowable outflow, } Q_{\text{allow}} = Q_{\text{pre}} - Q_{\text{unrestricted}}$$

$$= 6.33 - 1.238 (\text{L/s})$$

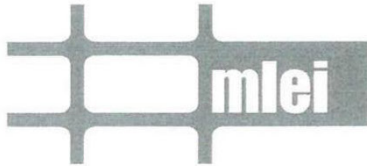
$$= 5.092 (\text{L/s})$$

Detained roof area:

$$f = \frac{A_{\text{roof}}}{A_{\text{site}} - A_{\text{imp}}} = \frac{424.89}{752.58 - 38.95} = 0.60$$

$$\text{using } \textcircled{1}, C'_{10} = 0.11197$$

ADELAIDE: LEVEL 5, 19 GILLES STREET ADELAIDE SA / (08) 8231 2832  
 PORT LINCOLN: OFFICE 5, 12 LEWIS STREET PORT LINCOLN SA 5606 / (08) 8683 3884  
 WWW.MLEI.COM.AU / MLEI@MLEI.COM.AU / ABN: 29 125 240 459

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Designer X.C.Date 24/04/2017Index 3

$$C_{10} = 0.9(0.6) + 0.1197(1-0.6)$$

$$= 0.581$$

$$C_{20} = F_{20} \times C_{10}$$

$$= 1.05 \times 0.581$$

$$= 0.61$$

Detained soft landscape area:

Assume the runoff coefficient of grass is 0.1 for a 20-year event.

$$\text{Area of soft landscape : } A_{\text{landscape}} = 107.83 \text{ m}^2$$

$$C_{20(\text{total})} = C_{20(\text{roof})} + C_{20(\text{soft landscape})}$$

$$= 0.61 + 0.1$$

$$= 0.71$$

$$A_{(\text{total})} = A_{(\text{roof})} + A_{\text{soft landscape}}$$

$$= 424.89 + 107.83$$

$$= 532.72 \text{ m}^2$$

Detention Calculation:

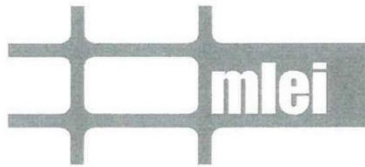
Refer 'Detention Calculations' Excel

Required detention volume of 2674 L for an allowable outflow

Round off to 3000, and add another extra 1000 L for retained water.

∴ Adopt a 4000 L RWT

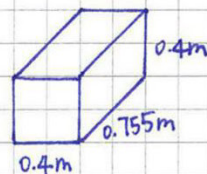


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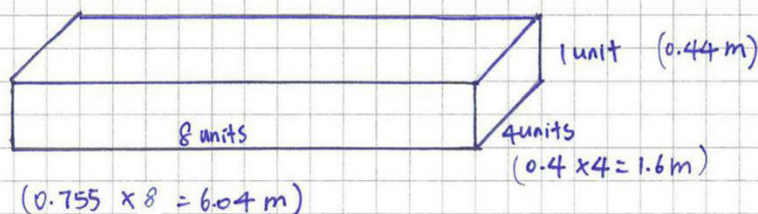
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Designer Y.C. Date 24/04/2017 Index 4Detention tank sizing:

Refer "Drainwell Modular detention cell brochure"

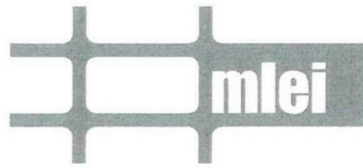
Individual unit:Detention volume:  $0.132 \text{ m}^3$ 

$$\begin{aligned} \text{Total units required} &= \text{Required detention volume} \div \text{detention per unit} \\ &= 4 \div (0.132 \times 0.95) \\ &\approx 32 \text{ units} \end{aligned}$$

Required unit arrangement:Stormwater quality:

Refer 'post-development plan'

Extra carpark area of  $166.98 \text{ m}^2$  to be treated.Assume  $t_c = 5 \text{ mins}$ . $\therefore$  Using BOM-IFD,  $\frac{5 \text{ min}}{20} = 121 \text{ mm/hr}$ Using ①,  $C'_{10} = 0.11197$

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Designer Y.C.Date 24/04/2017Index 5

$$C_{10} = (0.9 \times 0.9) + 0.1197(1 - 0.9)$$

$$= 0.8212$$

$$C_{20} = F_{20} C_{10}$$

$$= 1.05 \times 0.8212$$

$$= 0.8623$$

$$Q_{20} = 0.278 \times C_{20} \times \left[ \frac{5 \text{ min}}{20} \right] \times A_{\text{carpark}}$$

$$= 0.278 \times 0.8623 \times 121 \times 166.98 \times 10^{-6}$$

$$= 4.84 \text{ LIS}$$

$\therefore$  Peak flow off car park area is 4.84 LIS

Refer Figure 5.4.11.2(a), assuming a grade of 1%, pipe required is 150 mm  $\phi$

Refer 'Ecosol - table 10 high Flow Ecosol GPT table'

$\therefore$  Required GPT size/model is GPT 4200.



## Site Detention Volume - 20 YEAR

Cro = Runoff Coefficient

ARI 20		Area (m2)	tc (mins)	Cro			
		532.72	5	0.71			
Duration	mm/h	In flow L/s	Out flow allowed		Qin - Qout	Detentions size for that duration	
5	121		12.71277089	5.092	7.620770889	2286.231267	
6	112		11.76719289	5.092	6.675192889	2403.06944	
7	106.3		11.16832682	5.092	6.076326822	2552.057265	
8	100.7		10.57996718	5.092	5.487967178	2634.224245	
9	95		9.981101111	5.092	4.889101111	2640.1146	
10	89.3		9.382235044	5.092	4.290235044	2574.141027	
11	86.6		9.098561644	5.092	4.006561644	2644.330685	
12	83.8		8.804381822	5.092	3.712381822	2672.914912	
13	81.1		8.520708422	5.092	3.428708422	2674.392569	
14	78.4		8.237035022	5.092	3.145035022	2641.829419	
15	75.7		7.953361622	5.092	2.861361622	2575.22546	
16	72.9		7.6591818	5.092	2.5671818	2464.494528	
17	70.2		7.3755084	5.092	2.2835084	2329.178568	
18	67.5		7.091835	5.092	1.999835	2159.8218	
19	64.7		6.797655178	5.092	1.705655178	1944.446903	
20	62		6.513981778	5.092	1.421981778	1706.378133	
21	60.7		6.377398289	5.092	1.285398289	1619.601844	
22	59.4		6.2408148	5.092	1.1488148	1516.435536	
23	58		6.093724889	5.092	1.001724889	1382.380347	
24	56.7		5.9571414	5.092	0.8651414	1245.803616	
25	55.4		5.820557911	5.092	0.728557911	1092.836867	
26	54.1		5.683974422	5.092	0.591974422	923.4800987	
27	52.8		5.547390933	5.092	0.455390933	737.733312	
28	51.4		5.400301022	5.092	0.308301022	517.9457173	
29	50.1		5.263717533	5.092	0.171717533	298.788508	
30	48.8		5.127134044	5.092	0.035134044	63.24128	
31	48.2		5.064095511	5.092	-0.02790449	-51.90234933	
32	47.6		5.001056978	5.092	-0.09094302	-174.6106027	
33	47.1		4.948524867	5.092	-0.14347513	-284.080764	
34	46.5		4.885486333	5.092	-0.20651367	-421.28788	
35	45.9		4.8224478	5.092	-0.2695522	-566.05962	
36	45.3		4.759409267	5.092	-0.33259073	-718.395984	
37	44.8		4.706877156	5.092	-0.38512284	-854.9727147	
38	44.2		4.643838622	5.092	-0.44816138	-1021.807941	
39	43.6		4.580800089	5.092	-0.51119991	-1196.207792	
40	43		4.517761556	5.092	-0.57423844	-1378.172267	
41	42.5		4.465229444	5.092	-0.62677056	-1541.855567	
42	41.9		4.402190911	5.092	-0.68980909	-1738.318904	
43	41.3		4.339152378	5.092	-0.75284762	-1942.346865	
44	40.7		4.276113844	5.092	-0.81588616	-2153.939451	
45	40.2		4.223581733	5.092	-0.86841827	-2344.72932	
46	39.6		4.1605432	5.092	-0.9314568	-2570.820768	
47	39		4.097504667	5.092	-0.99449533	-2804.47684	
48	38.4		4.034466133	5.092	-1.05753387	-3045.697536	
49	37.8		3.9714276	5.092	-1.1205724	-3294.482856	
50	37.3		3.918895489	5.092	-1.17310451	-3519.313533	
51	36.7		3.855856956	5.092	-1.23614304	-3782.597716	
52	36.1		3.792818422	5.092	-1.29918158	-4053.446523	
53	35.5		3.729779889	5.092	-1.36222011	-4331.859953	
54	35		3.677247778	5.092	-1.41475222	-4583.7972	
55	34.4		3.614209244	5.092	-1.47779076	-4876.709493	
56	33.8		3.551170711	5.092	-1.54082929	-5177.186411	
57	33.2		3.488132178	5.092	-1.60386782	-5485.227952	
58	32.7		3.435600067	5.092	-1.65639993	-5764.271768	
59	32.1		3.372561533	5.092	-1.71943847	-6086.812172	
60	31.5		3.309523	5.092	-1.782477	-6416.9172	
						2674	



# ORIFICE DIAMETER CALCULATION

Using **DRAINS Equation 5.7**

OSD storages are usually controlled by circular orifices with the discharge equation being :

$$Q = C_c \cdot \frac{\pi}{4} \cdot d^2 \cdot (2gh)^{0.5}$$

... (Equation 5.7)

where  $C_c$  is a contraction coefficient, taken as a constant of 0.6 in *DRAINS*,  
 $d$  is the orifice diameter (m),  
 $g$  is the acceleration due to gravity ( $m/s^2$ ), and  
 $h$  is the height from the water surface to the centre of the orifice (m).

**Rainwater Detention System**

*Drainwell Module DW1592*  
*8x8 module arrangement*

Width (m)	Length (m)	Height (m)
3.2	3.02	0.44

*Orifice Calculation*

Gravity, g (m/s2)	Contraction coefficient, Cc	Orifice diameter, d (m)	Detention Volume, V (m3)	Height of water surface to orifice centre, h (m)	Discharge rate, Q (m3/s)
9.81	0.6	0.07050	2.674	0.241	0.005097765

Orifice diameter (mm) =

70.5

## Intensity-Frequency-Duration Table

Location: 34.975S 138.550E NEAR.. 425 Anzac highway Issued: 20/4/2017

Rainfall intensity in mm/h for various durations and Average Recurrence Interval

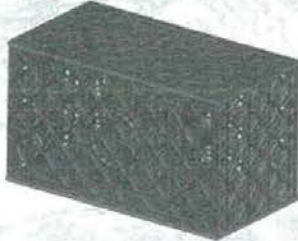
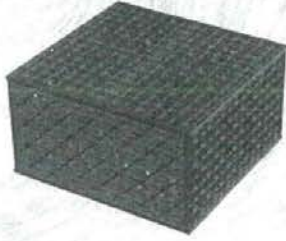
## Average Recurrence Interval

Duration	1 YEAR	2 YEARS	5 YEARS	10 YEARS	20 YEARS	50 YEARS	100 YEARS
5Mins	43.3	58.4	81.3	98.2	121	156	186
6Mins	40.3	54.4	75.5	91.1	112	144	173
10Mins	32.5	43.8	60.4	72.6	89.3	114	136
20Mins	23.1	31.0	42.4	50.7	62.0	79.1	93.8
30Mins	18.4	24.7	33.6	40.0	48.8	62.0	73.4
1Hr	12.2	16.2	21.9	25.9	31.4	39.7	46.8
2Hrs	7.90	10.5	14.0	16.5	19.9	24.9	29.3
3Hrs	6.11	8.09	10.7	12.6	15.2	19.0	22.2
6Hrs	3.93	5.19	6.82	7.96	9.55	11.9	13.8
12Hrs	2.50	3.29	4.28	4.97	5.93	7.31	8.49
24Hrs	1.55	2.03	2.60	3.00	3.55	4.35	5.01
48Hrs	.917	1.19	1.51	1.72	2.01	2.44	2.80
72Hrs	.661	.851	1.07	1.21	1.41	1.70	1.94

aw data: 16.9, 3.43, 0.88, 34.99, 6.58, 1.54, skew=0.59, F2=4.47, F50=14.98)

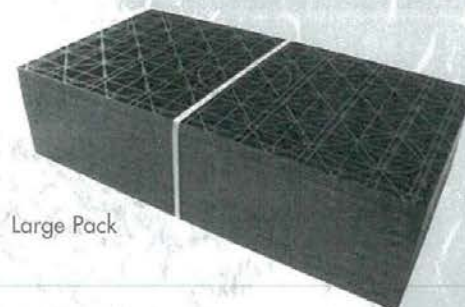
© Australian Government, Bureau of Meteorology

# specifications

			
Drainwell™ Model		DW1592	DW1992
Dimensions (mm)	Length	755	755
	Width	400	800
	Height	440	440
Volume (m3)		.132	.264
Tanks per m3		7.5	3.7
Weight (Kg)	3 Panels	6.65	12.85
	4 Panels	7.3	14
	5 Panels	7.9	15.15
Maximum Load (tons/m²)	3 Panels (1 Internal)	23.66 t/m²	9.45 t/m²
	4 Panels (2 Internal)	29.76 t/m²	9.61 t/m²
	5 Panels (3 Internal)	32.42 t/m²	16.20 t/m²
Internal Open Area		<95%	<95%
Polymer Type		Polypropylene UV Stabilised	
Service Temp		-20°C to 120°C	
Chemically inert and not affected by Moulds and Algae			

## PALLET PACKAGING

Drainwell modules are delivered to site flat-pack on pallets. A forklift is required for unloading of pallets. A full pallet of Drainwell panels weighs approximately 600kg.





## 9.2 THE ECOSOL™ STORM PIT (CLASS 2) HYDRAULIC SPECIFICATION

This Ecosol™ Storm Pit (Class 2) is designed to treat stormwater runoff for typical urban catchments of 100 – 750m<sup>2</sup> based on the product selection and on a 100% impervious catchment area. The system provides both primary and secondary treatment within the one compact underground device thereby reducing the space and cost of downstream tertiary treatment measures

ECOSOL™ STORM PIT - CLASS 2 (PRIMARY TREATMENT SYSTEM)											
Ecosol Product Code	Outlet Pipe Gradient (%)	Maximum Treatable Flow Rate (L/s) Outlet Pipe Diameters					Maximum By-Pass Flow Rates (L/s)				
		100mm	150mm	225mm	300mm	375mm	100mm	150mm	225mm	300mm	375mm
Ecosol™ Storm Pit 10L (Class 2)	0.5	4	11	11	11		4	11	32	68	
	1.0	5	11				5	15	45		
	2.0	7					7	22	63		
	5.0	11					12	34			
Ecosol™ Storm Pit 20L (Class 2)	0.5	4	11	22	22	22	4	11	32	68	124
	1.0	5	15	22			5	15	45	97	
	2.0	7	22				7	22	63		
	5.0	11					12	34	100		

Table 5 – Ecosol™ Storm Pit (Class 2) Hydraulic Specification



**Ecosol™**  
WASTEWATER FILTRATION SYSTEMS

## 10.2 THE ECOSOL™ STORM PIT (CLASS 2) POLLUTANT REMOVAL EFFICIENCIES

ECOSOL™ STORM PIT - CLASS 2 (SECONDARY TREATMENT SYSTEM)	
Contaminant/Pollutant	Estimated Mean Pollutant Removal Efficiency (%)
<b>PRIMARY TREATMENT CHAMBER<sup>1</sup></b>	
Gross Pollutants (>600µm)	100
Total Suspended Solids (TSS) (20 - 600µm)	41
<b>SECONDARY TREATMENT CHAMBER<sup>2,3</sup></b>	
TSS	80
TN	63
Organic-N	37
PON	80
NOX	90
NH4-N	49
TP	90
Copper	90
Lead	90
Zinc	90
Cadmium	78
Total Petroleum/Hydrocarbon's	98

<sup>1</sup>Typical pollutant removal efficiencies based on results derived from the independently tested Ecosol™ Litter Basket and Ecosol™ Storm Pit (Class 1) published performance data.

<sup>2</sup>Typical nitrogen and phosphorous removal efficiencies derived from international work undertaken on the performance of porous polypropylene, Clinoptilolite, and laterite as per references described in section 3.0 of the product technical specification.

<sup>3</sup>Typical removal efficiencies derived from the Port of Townsville Pilot Program testing (Athanasiadis et al., 2014). The removal efficiency depends heavily on the arsenic speciation (arsenite or arsenate)

Table 8 – Ecosol™ Storm Pit (Class 2) Pollutant Removal Efficiencies.



**Ecosol™**  
WASTEWATER FILTRATION SYSTEMS

## Water Sensitive Urban Design - MUSIC Model Report

The City of West Torrens Council has requested the proposed development at 425 Anzac Highway to consider stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants into Council's receiving stormwater system. MLEI are providing MUSIC data with target requirements identified by the City of West Torrens Council. MLEI have used industry best practices in the assessment.

It is industry practice that the 3-month ARI rainfall event is being used for stormwater quality modelling; this is widely accepted as the catchment is considered sufficiently clean after a rain event of this nature.

The following target reduction requirements are in accordance with the State Government's Water Sensitive Urban Design Policy (2013):

- Total Suspended solids                      80% reduction
- Total phosphorus                              60% reduction
- Total Nitrogen                                 45% reduction
- Gross Pollutants                              90% reduction

The industry recognised software package 'MUSIC' by eWater has been used to assess the reduction in pollutants for this development. Unless noted below as an input, the default data from eWater has been used in the assessment. Individual properties of proprietary water treatment facilities have been manually customised in the treatment train. Including within the model, the use of an Ecosol Storm Pit (Class 2) and the model has used published information from ECOSOL. The reduction capacities of the ECOSOL product are displayed in Figure 1.

### 10.2 THE ECOSOL™ STORM PIT (CLASS 2) POLLUTANT REMOVAL EFFICIENCIES

ECOSOL™ STORM PIT - CLASS 2 (SECONDARY TREATMENT SYSTEM)	
Contaminant/Pollutant	Estimated Mean Pollutant Removal Efficiency (%)
<b>PRIMARY TREATMENT CHAMBER<sup>1</sup></b>	
Gross Pollutants (>600µm)	100
Total Suspended Solids (TSS) (20 - 600µm)	41
<b>SECONDARY TREATMENT CHAMBER<sup>2,3</sup></b>	
TSS	80
TN	63
Organic-N	37
PON	80
NOX	90
NH4-N	49
TP	90
Copper	90
Lead	90
Zinc	90
Cadmium	78
Total Petroleum/Hydrocarbon's	98

<sup>1</sup>Typical pollutant removal efficiencies based on results derived from the independently tested Ecosol™ Litter Basket and Ecosol™ Storm Pit (Class 1) published performance data.

<sup>2</sup>Typical nitrogen and phosphorous removal efficiencies derived from international work undertaken on the performance of porous polypropylene, Clinoptilolite, and laterite as per references described in section 3.0 of the product technical specification.

<sup>3</sup>Typical removal efficiencies derived from the Port of Townsville Pilot Program testing (Athanasiadis et al., 2014). The removal efficiency depends heavily on the arsenic speciation (arsenite or arsenate)

Figure 1 - Ecosol Storm Pit (Class 2) Pollutant Removal Efficiencies



The following reduction in pollutants shown in Figure 2 was achieved with the inclusion of the Ecosol Storm Pit (Class 2):

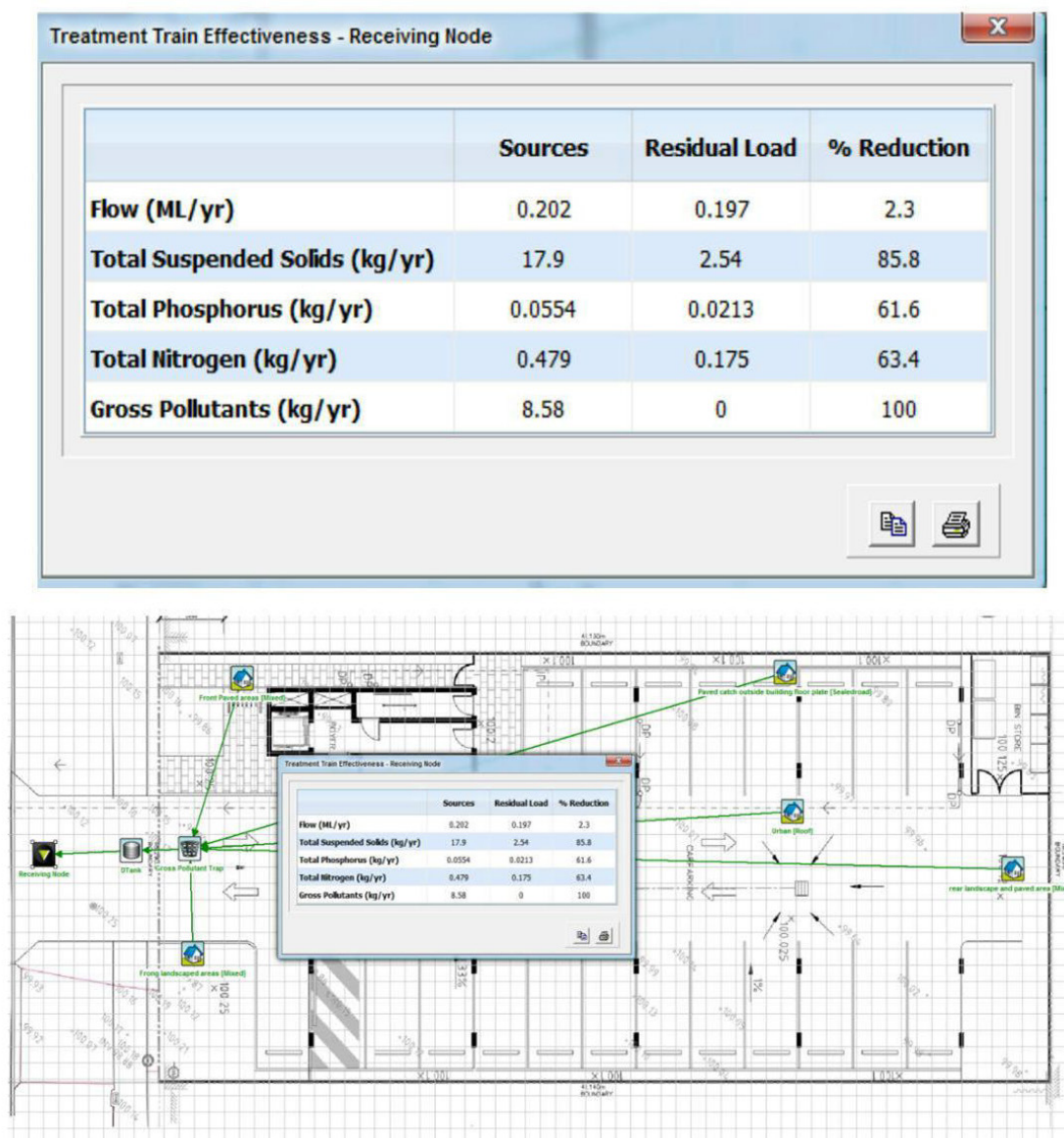


Figure 2 - Treatment Train Effectiveness at Receiving Node

Pollutant	CWTC Requirement	Treatment Train Effectiveness
Gross pollutants	90%	100%
Suspended solids	80%	85.8%
Total phosphorus	60%	61.6%
Total nitrogen	45%	63.4%

The achieved reductions are a betterment than the requirements outlined by the City of West Torrens Council and in the opinion of MLEI is an acceptable solution for this development.





## PLANNING REPORT

Prepared for:  
Walpol Development PTY LTD

Date:  
9 June 2017



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REF 0093 | 9 June 2017





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## 1. INTRODUCTION

This planning statement has been prepared in support of a proposal to demolish all existing buildings on the subject site and construct a three storey residential flat building (total of 8 apartments) with ground level car parking (20 car parking spaces) and associated landscaping.

This development application also includes the following:

- Appendix 1 – Development Application form;
- Appendix 2 – Electricity Act Declaration form;
- Appendix 3 – Certificate of title;
- Appendix 4 – Stormwater and Drainage Plan prepared by MLEI and dated 24 April 2017; and
- Appendix 5 – Proposed plans and drawings prepared by Piteo Design Architects dated 10 April 2017, 15 May 2017, 25 May 2017, and 1 June 2017.

Upon our inspection of the site and locality and our assessment of the proposal against the relevant provisions of the City of West Torrens Development Plan (consolidated 30 May 2017), we have formed the opinion that the proposal displays significant planning merit and therefore warrants Development Plan Consent.



## 2. PROPOSAL DESCRIPTION

The proposed development seeks to demolish all existing buildings on the subject site, and construction of a three storey residential flat building (total of 8 apartments) with ground level vehicle parking (20 car parking spaces) and associated landscaping.

The proposal will require the removal of one street tree.

The proposed development will be discussed in greater detail below.

### 2.1 Ground Level

The ground level of the proposed development will include the vehicle parking area, bin store, bicycle parking spaces, lift/stair access, foyer, mail boxes and landscaping.

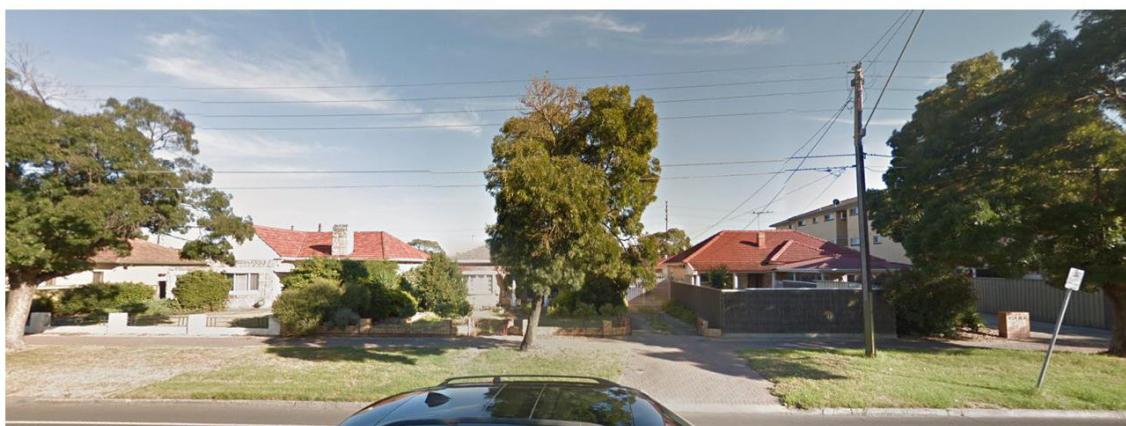
The car parking area will be completely open sided underneath the floor area of Level 1, containing a total of 20 car parking spaces of which 16 spaces will be allocated to residents (two for each apartment) and four spaces for visitors. One of the visitor parking spaces is located outside of the car parking area between Anzac Highway and the auto-sliding gate, and will be uncovered. Entrance to the car park will be controlled with a key card or remote control. Visitor access to the undercover vehicle parking spaces will be controlled by residents via an intercom system.

Above each resident car parking space will be an over-bonnet storage unit. These units will be freestanding to an overall height of 2.02 metres over the landscaped boundary edges of the vehicle parking area. Two of these units will be afforded to each apartment providing a total external storage area of 3.6 cubic metres.

There will be a bicycle parking rack in front of the foyer area able to accommodate up to 6 bicycles. Another secure and undercover bicycle parking rack will be provided in the space adjacent to the auto-sliding gate, providing a further 6 spaces.

The existing crossover is proposed to be relocated to the centre of the site which will involve the removal of a street tree (pictured below). The applicant proposes, at their cost, two additional street trees (species at Council's discretion) either side of the new crossover.

**Figure 2.1** Council street tree proposed to be removed.







Landscaping will be provided surrounding the subject site in accordance with the plans prepared by Piteo Architects and included at Appendix 5. Species proposed include:

- Daniella Caerulea “Little Jess”;
- Westringia Fruiticosa “Mundi”;
- Pinnacle Syzygium Australe;
- Westringia “Aussie Box”; and
- Chinese Pistachio trees.

The proposed ground level will also include a waste room containing both recycling and general waste bins for all apartments sufficient in area for the proposed number of apartments.

With the exception of the bin storage area, the building will be setback 4.1 metres from the rear boundary. There will be a setback of between 1.5 metres and 2.5 metres from the southwestern boundary, and a 2.4 metre setback from the north eastern balcony (excluding the storage cages). The building will be setback between 4.8 metres and 6.9 metres from Anzac Highway.

## 2.2 Level 1 and Level 2

Levels 1 and 2 will contain four apartments each. All apartments are proposed to have two bedrooms and two bathrooms with floor areas ranging between 84.46 square metres and 92.25 square metres. Apartments will also have north eastern facing balconies with areas ranging between 10.88 square metres and 13.64 square metres.

Each apartment will have 9.8 cubic metres of storage space, comprising of 6.2 cubic metres internally, and 3.6 cubic metres in the external over-bonnet storage cages conveniently located at ground level.

At these levels, the building will be setback 4 metres from the rear, between 4.98 metres and 6.93 metres from Anzac Highway, 2.4 metres from the north eastern side boundary, and between 1.5 metres and 2.8 metres from the south western side boundary.

## 2.3 Building Design

The proposed building is contemporary in design and appearance featuring the following materials and colours:

- precast concrete panels;
- face brick walls;
- hit and miss face brick ‘breeze wall’;
- painted render finishing to blockwork;
- timber composite batten screening;
- obscure and clear glass, and
- woven wire perforated metal screening and balustrades; and
- powder-coated aluminium privacy screens.



The three storey built form of the proposal is highly articulated through varying building materials, window openings and projecting elements, providing a play of shadow and light to create visual interest across the facades. The concrete panels and brick work create a simple, contemporary and clean appearance to the building, which is complemented by the landscaping along the street frontage and along all other boundaries.

## 2.4 Environmental Considerations

### 2.4.1 Ecological Sustainable Development

The intent of the proposed ESD initiatives is to reduce the demands on active systems by enhancing the passive performance of the building. This includes optimising the network of installed systems with the development's built form.

The design also features shading, insulation, natural light, ventilation and lifespan considerations:

- solar panels on roof;
- energy efficient LED lighting;
- water saving fixtures and fittings
- high performance ceiling insulation; and
- low environmental impact by use of low embodied energy materials and finishes and low water usage features and plantings.

### 2.4.2 Waste

The waste storage area is located to the rear of the subject site, and will be accessible via the vehicle parking area. This area will be sited on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water. Further, a bin wash down area has also been included, allowing property management to regularly wash bins and control odour.

Residents will be required to walk their waste down to the communal bin storage area. Property management will then be responsible for transporting these bins to/from the street for collection by Council.

### 2.4.3 Noise

Given the nature of the proposed land uses, specific measures have been recommended to ensure that the apartments are able to comply with the requirements of Building Code of Australia for sound insulation and the *Minister Specification SA 78B Construction requirements for the control external sound*.

### 2.4.4 Contamination

Given the existing and historical use of the site as a residential property, it is considered that there would be a very low risk of contamination being present which would pose unacceptable health or environmental risks to future residents, visitors and pedestrians and other users accessing the site.



## 2.5 Category of Development

The proposed development is not listed in the City of West Torrens Development Plan or the *Development Regulations, 2008* as either a category 1 or category 2 type of development. As such, the proposal defaults to category 3.

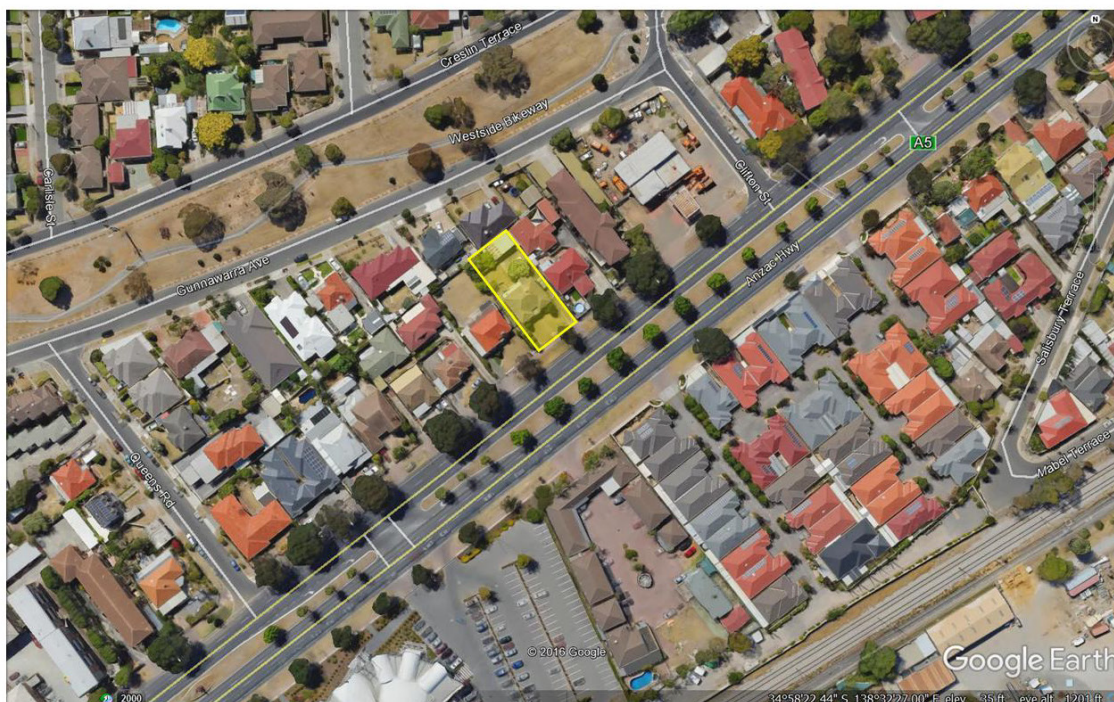




### 3. SITE AND LOCALITY

The proposed development is located on the northern side of Anzac Highway, between Clifton Street to the east and Queen Road to the west. The subject site comprises of a single allotment legally described as Allotment 124 in Certificate of Title Volume 5111 Folio 230, otherwise known as 425 Anzac Highway, Camden Park.

Figure 3.1 Study Area



The subject site has a frontage to Anzac Highway of 18.29 metres, a depth of 41.13 metres and an overall site area of 752 square metres. The site is located within a designated 4 storey height area of Medium Density Policy Area 18 of the Residential Zone.

A single storey, detached dwelling and associated outbuildings exist on the subject site. Access to the site is gained from Anzac Highway via an existing, single width crossover adjacent to the north east boundary.

The dwellings immediately surrounding the subject site are in the form of single storey, detached and group dwellings. A three storey residential flat building exists to the north east of the subject site and other similar buildings exist to the southwest.

The broader locality is similar in character containing a number of battle axe allotments, semi-detached dwellings and a variety of residential flat buildings. Overall, this side of Anzac Highway represents a diverse medium density character.

Directly south of the subject site on the opposite side of Anzac Highway land uses vary and include a large retirement village, Morphetville Motor Inn and The Junction Hotel. Further south, is the Morphetville Racecourse.





Approximately 300 metres to the west of the subject site is quality open space in the form of the West Torrens Birkalla Soccer Club ovals. The Westside Bikeway is also located less than 50 metres to the north.

The subject site is highly accessible by public transport, with the tram network located approximately 300 metres to the south, and bus stops just over 100 metres away along both sides of Anzac Highway.

Anzac Highway is a two-way arterial road which supports three vehicle lanes in both directions, separated by a raised median strip. The road reserve is approximately 40 metres wide with on-street car parking permitted outside of clearway times. Anzac Highway is restricted to 60 kilometres per hour and carries approximately 39,000 vehicles per day.

The median strip, as well as the verges on either side of the roadway are vegetated with lawns and large mature trees.

The site does not adjoin another Zone.



## 4. DEVELOPMENT ASSESSMENT

### 4.1 Development Plan

The proposed development is located within Medium Density Policy Area 18 of the Residential Zone as specified in the City of West Torrens Development Plan (consolidated version 30 May 2017).

Our planning assessment focuses on the matters which we consider most relevant in determining the merit, or otherwise, of the proposal, namely:

- Desired character;
- Density;
- Building appearance and design;
- Setbacks;
- Apartment design;
- Parking, access and traffic;
- Crime prevention;
- Environmental Considerations; and
- Affordable Housing.

Our following assessment should be read in conjunction with the proposal plans and details accompanying the development application and included as appendices to this planning statement.

### 4.2 Desired Character

Medium Density Policy Area 18 seeks a range of development types such as residential flat buildings, which will contribute to increasing housing to medium densities and creating a highly varied streetscape. In order to maximise the density of development while also achieving integrated design outcomes, allotments should be amalgamated to create larger development sites, particularly within close distance to centre zones. Allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park will be up to four storeys, and parking areas and garages will be located behind the front facade of buildings.

The desired dwelling types anticipated in each policy area will be supported by the allotment sizes and further, the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each Policy Area, and reinforce the distinction between policy areas.

Landscaping will also be included in new developments to enhance the appearance of buildings from the street as viewed by pedestrians and to assist with transition between the public and private realm.

We acknowledge that the proposal does not amalgamate sites to encourage a four storey outcome. In our opinion, the form of the proposed three storey residential flat building (lower than that envisaged), located in an area where similar developments exist on comparable sized allotments and with minimal impacts upon surrounding properties is appropriate in this particular instance. The proposal still achieves the intent of the desired character statement to contribute to increasing housing to medium densities and creating a highly varied streetscape through high quality design.



The proposal will contribute to a highly varied streetscape, and includes landscaping to the Anzac Highway frontage, sides and rear of the development. Further, the ground level car parking is sufficiently screened in keeping with the intention of the desired character to deliver high quality pedestrian amenity.

Although not strictly consistent with the envisaged average minimum site areas (as discussed further in the following section), the proposal is lower than the maximum height limit and presents no detrimental impacts given the character and amenity of the locality. Further, there are other similar developments with comparable densities in the locality and the site is accessible to nearby areas of open space, bicycle paths, public transport and the Commercial Zone.

The Council street tree proposed for removal in order to allow for a central crossover is to be replaced with two trees either side of the crossover. The tree proposed for removal is comparatively smaller than others in the locality, and as such does not contribute as greatly to the streetscape. Further, the planting positions for trees lining Anzac Highway are irregular as a result of previous development along Anzac Highway, therefore the replacement trees will not significantly interrupt the planting pattern. In recognition of the intent to deliver medium density housing with landscaped frontages as sought by the desired character, the proposed two replacement trees will contribute positively to the desired character.

Respecting the above, we have formed the opinion that the proposal substantially conforms to the desired character.

#### 4.3 Density

The following provisions are what we consider to be the most relevant in assessing the density of the proposed development.

##### *Medium Density Policy Area 18*

- PDC 4** *Medium density development that achieves gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare, where net density can be calculated by dividing 10000 by the site area and multiplying that number by the number of proposed dwellings for the site) should be in the form of 2 to 4 storey buildings.*
- PDC 6** *A dwelling should have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown in the following table:*
- Residential Flat Building: Site area of 150sqm and 15m frontage*
- PDC 7** *Dwellings constituting affordable housing (as defined by the South Australian Housing Trust Regulations as amended) should be designed within the following parameters and have a minimum site area (and in the case of residential flat buildings, an average site area per dwelling) not less than that shown in the following table:*





Parameter	Detached Dwelling	Semi-detached Dwelling	Group Dwelling	Residential flat building	Row Dwelling
Site area (square metres)	150 minimum	150 minimum	100 minimum	100 average	100 minimum
Minimum area of private open space for ground level dwellings (square metres)	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres	24 with a minimum dimension of 3 metres
Minimum area of private open space in the form of a balcony for dwellings above ground level (square metres)	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres	8 with a minimum dimension of 2 metres
Minimum number of on-site car parking spaces (one of which should be covered)	1	1	1	1	1

#### *Orderly and Sustainable Development*

**Objective 3** *Development that does not jeopardise the continuance of adjoining authorised land uses.*

**Objective 5** *Urban development located only in zones designated for such development.*

**PDC 1** *Development should not prejudice the development of a zone for its intended purpose.*

**PDC 5** *Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.*

Medium Density Policy Area 18 seeks net densities of between 40 and 67 dwellings per hectare within buildings up to four storeys in height. These higher densities should be encouraged on sites within close proximity to public transport, centres and areas of public amenity.

The proposal exceeds the prescribed average site area requirement, however we are of the opinion that the proposed density is acceptable in this particular instance for the following reasons:

- the subject site is located within 300 metres of high quality public open space, public transport (buses and trams), a public bikeway, Immanuel College and Primary School, and Morphetville Medical Centre;
- the development is less than the maximum 4 storeys envisaged for this part of Anzac Highway;
- there are other examples of similar densities in the locality all located within the 4 storey designated area of Policy Area 18, including:
  - » a 3 storey residential building at 421 Anzac Highway which has an average site area of approximately 80 square metres;





- » a 2 storey residential flat building at 443 Anzac Highway which has an average site area of approximately 119 square metres; and
- » the 3 storey residential flat building at 445 Anzac Highway which has an average site area of 99 square metres.
- the proposal has a site coverage of only 55 per cent well below the prescribed maximum of 70%;
- the proposed form of the building does not present any detrimental impacts upon surrounding properties in relation to overshadowing, visual amenity or overlooking;
- the proposed development provides sufficient carparking, landscaped open space and storage to function efficiently without detrimental impact upon surrounding properties and the adjacent road network;
- the proposal represents an overall built form which complements similar development along this part of Anzac Highway and will therefore sit comfortably in the streetscape; and
- the envisaged average site area for affordable housing in Medium Density Policy Area 18 is only 100 square metres which is comparable to the proposed average site area of 94 square metres. Given that this is an acceptable density in the Policy Area, we are of the opinion that if affordable housing were adopted it would result in no material change to the likely built form outcome as affordable housing is still required to satisfy all other applicable guidelines of the Development Plan.

With respect to the Orderly and Sustainable Development provisions of the Development Plan we note that the proposal:

- will not jeopardise the continued use or future development of the neighbouring dwellings to achieve the intended purpose of the Residential Zone or Policy Area (Objective 3 and PDC 1);
- is located in a zone designated for residential development up to four storeys in height, in the form of residential flat buildings which is an envisaged dwelling type (Objective 5); and
- allows greater accessibility to public services and infrastructure located within close proximity to the subject site (PDC 5).

With the above in mind, the proposal in our view is not an overdevelopment of the site.

#### 4.4 Building Appearance and Design

The following provisions are what we consider to be the most relevant in assessing the appearance and design of the proposed development.

##### *Medium Density Policy Area 18*

**PDC 1**                      *The following forms of development are envisaged in the policy area:*

- *residential flat building*

**PDC 5**                      *Dwellings should be designed within the following parameters:*



Parameter	Value
Minimum setback from primary road frontage	3 metres
Minimum setback from secondary road frontage	1 metres
Minimum setback from back boundary	4 metres
Maximum site coverage (the area of a site covered by the ground floor level of a building, including the dwelling, garage, carport and outbuilding, but excluding unroofed balconies, verandas and pergolas)	70 per cent
Maximum building height (from natural ground level)	<p>Allotments bounded by Holbrooks Road, Garden Terrace and the River Torrens in Underdale: four storeys or 16.5 metres.</p> <p>Allotments bounded by Anzac Highway, Morphet Road and Cromer Street in Camden Park: four storeys or 16.5 metres</p> <p>Allotments in Ashford (other than those adjacent to Residential Character Ashford Policy Area 22): four storeys or 16.5 metres</p> <p>All other allotments: three storeys or 12.5 metres.</p>

#### *Building Near Airfields*

- PDC 1**      *The height and location of buildings and structures should not adversely affect the long-term operational, safety, commercial and military aviation requirements of airfields.*

#### *Design and Appearance*

- Objective 1**      *Development of a high design standard and appearance that responds to and reinforces positive aspects of the local environment and built form.*

- PDC 1**      *Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:*

- (a) building height, mass and proportion*
- (b) external materials, patterns, colours and decorative elements*
- (c) roof form and pitch*
- (d) façade articulation and detailing*
- (e) verandas, eaves, parapets and window screens.*

- PDC 2**      *The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.*

- PDC 5**      *Balconies should:*

- (a) be integrated with the overall form and detail of the building*



*(b) include balustrade detailing that enables line of sight to the street*

*(c) be recessed where wind would otherwise make the space unusable*

*(d) be self-draining and plumbed to minimise runoff.*

**PDC 14** *Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.*

**PDC 15** *Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.*

**PDC 18** *Multi level vehicle parking areas within buildings should be designed, sited and screened from public view by an appropriate combination of built form, landscaping and/or visual art while still allowing for natural ventilation within these structures.*

**PDC 19** *Outdoor storage, loading and service areas should be:*

*(a) screened from public view by a combination of built form, solid fencing and/or landscaping*

*(b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles*

*(c) sited away from sensitive land uses.*

*Medium and High Rise Development (3 or more stories)*

**PDC 1** *Buildings should be designed to respond to key features of the prevailing local context within the same zone as the development. This may be achieved through design features such as vertical rhythm, proportions, composition, material use, parapet or balcony height, and use of solid and glass.*

**PDC 4** *Buildings should:*

*(a) achieve a comfortable human scale at ground level through the use of elements such as variation in materials and form, building projections and elements that provide shelter (for example awnings, verandas, and tree canopies)*

*(b) be designed to reduce visual mass by breaking up the building façade into distinct elements*

*(c) ensure walls on the boundary that are visible from public land include visually interesting treatments to break up large blank facades.*

**PDC 6** *Materials and finishes should be selected to be durable and age well to minimise ongoing maintenance requirements. This may be achieved through the use of materials such as masonry, natural stone, prefinished materials that minimise staining, discolouring or deterioration, and avoiding painted surfaces particularly above ground level.*



**PDC 10**

*Entrances to multi-storey buildings should:*

- (a) be oriented towards the street*
- (b) be visible and clearly identifiable from the street, and in instances where there are no active or occupied ground floor uses, be designed as a prominent, accentuated and welcoming feature*
- (c) provide shelter, a sense of personal address and transitional space around the entry*
- (d) provide separate access for residential and non-residential land uses*
- (e) be located as close as practicable to the lift and/or lobby access*
- (f) avoid the creation of potential areas of entrapment.*

**PDC 23**

*Deep soil zones should be provided to retain existing vegetation or provide areas that can accommodate new deep root vegetation, including tall trees with large canopies.*

*One way of achieving this is in accordance with the following table:*

Site area	Minimum deep soil area	Minimum dimension	Tree/ deep soil zones
<300m <sup>2</sup>	10m <sup>2</sup>	1.5 metres	1 small tree / 10m <sup>2</sup> deep soil
300-1500m <sup>2</sup>	7% site area	3 metres	1 medium tree / 30m <sup>2</sup> deep soil
>1500m <sup>2</sup>	7% site area	6 metres	1 large or medium tree / 60m <sup>2</sup> deep soil
Tree size and site area definitions			
Small tree	< 6 metres mature height and < less than 4 metres canopy spread		
Medium tree	6-12 metres mature height and 4-8 metres canopy spread		
Large tree	12 metres mature height and > 8 metres canopy spread		
Site area	The total area for development site, not average area per dwelling		

*Landscaping, Fences and Walls***PDC 1**

*Development should incorporate open space and landscaping and minimise hard paved surfaces in order to:*

- (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)*
- (b) enhance the appearance of road frontages*
- (c) screen service yards, loading areas and outdoor storage areas*
- (d) minimise maintenance and watering requirements*
- (e) enhance and define outdoor spaces, including car parking areas*





- (f) maximise shade and shelter*
- (g) assist in climate control within and around buildings*
- (h) minimise heat absorption and reflection*
- (i) maintain privacy*
- (j) maximise stormwater reuse*
- (k) complement existing vegetation, including native vegetation*
- (l) contribute to the viability of ecosystems and species*
- (m) promote water and biodiversity conservation.*

**PDC 2**

*Landscaping should:*

- (a) include the planting of drought tolerant species, including locally indigenous species where appropriate*
- (b) be oriented towards the street frontage*
- (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.*

**PDC 4**

*A minimum of 10 per cent of a development site should be landscaped. The development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities.*

**PDC 6**

*Fences and walls, including retaining walls, should:*

- (a) not result in damage to neighbouring trees*
- (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality*
- (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance*
- (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street*
- (e) assist in highlighting building entrances*
- (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites*



*(g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land*

*(h) be constructed of non-flammable materials.*

Buildings along this section of Medium Density Policy Area 18 are envisaged to be up to four storeys in height, and should seek to achieve a human scale at ground level in order to protect and enhance streetscape amenity.

The proposal satisfies key provisions which relate to building appearance and design, and can be summarised as follows:

- The scale of the proposed building is similar to other buildings in the locality (specifically other 3 storey residential flat buildings along this part of Anzac Highway. Further, the design incorporates external materials, colours, articulation, parapets and screening to create a visually interesting building which will sit comfortably in the streetscape satisfying Design and Appearance Objective 1 and PDC 1, and Medium and High Rise Development PDC 1;
- The external materials and finishes proposed for the development are highly durable and will not require ongoing maintenance (Medium and High Rise Development PDC 6);
- Three medium-sized trees (Chinese Pistachios) are proposed to be located between the building façade and Anzac Highway, with another single tree located to the rear of the site (Medium and High Rise Development PDC 23);
- A human scale is achieved by the proposal as the varying front setbacks allow for the inclusion of meaningful landscaping, whilst the building form itself includes visually interesting treatments and building projections which create interest at pedestrian level (Medium and High Rise Development PDC 4);
- The building entrance has been emphasised through the use of external materials, a canopy and paving, and will be accessible directly from Anzac Highway and the vehicle parking areas (Design and Appearance PDC 15 and Medium and High Rise Development PDC 10);
- Extensive areas of uninterrupted walling have been avoided through articulation, materials and windows (Design and Appearance PDC 14);
- No side boundary development is proposed (Zone PDC 12);
- Although not multi-storey, the vehicle parking area will be sufficiently screened from public view through the use of landscaping and semi-permeable building materials. The open sided ground floor area will also not require mechanical ventilation (Design and Appearance PDC 19);
- The bin storage area has been located to the rear of the property, away from public view and sensitive areas. Further, the area will be directly accessible through the vehicle parking area to permit convenient access for residents and property management on collection days (Design and Appearance PDC 20);
- The development will not impact the long term operational safety of the airport (Building Near Airfields PDC 1);



- Landscaping has been included in the development in accordance with Landscaping, Fencing and Walls PDC 2 and PDC 4, as 10 percent of the subject site will be landscaped and the majority of landscaping will be orientated towards the street frontage. This will complement and enhance the appearance of the proposed development, screen the vehicle parking area, assist in climate control and support environmental efficiency (Landscaping, Fences and Walls PDC 1); and
- Colorbond “Good Neighbour” fencing has been selected to be included at a height of 1.8 metres surrounding the proposed development and bin storage area. This is expected to be a sufficient height in order to maintain amenity, privacy and security for both the subject and neighbouring sites (Landscaping, Fences, and Walls PDC 6).

Respecting the above, we do not believe the proposed density results in a built form outcome where it could be considered to be seriously at variance with the desired character and the Development Plan as a whole.

#### 4.5 Setbacks

Below we have extracted what we consider to be the most relevant qualitative and quantitative provisions for assessing the setbacks of the proposed development.

##### *Medium Density Policy Area 18*

##### **PDC 5** *Dwellings should be designed within the following parameters:*

Parameter	Value
Minimum setback from primary road frontage	3 metres
Minimum setback from secondary road frontage	1 metres
Minimum setback from back boundary	4 metres
Maximum site coverage (the area of a site covered by the ground floor level of a building, including the dwelling, garage, carport and outbuilding, but excluding unroofed balconies, verandas and pergolas)	70 per cent
Maximum building height (from natural ground level)	<p>Allotments bounded by Holbrooks Road, Garden Terrace and the River Torrens in Underdale: four storeys or 16.5 metres.</p> <p>Allotments bounded by Anzac Highway, Morphet Road and Cromer Street in Camden Park: four storeys or 16.5 metres</p> <p>Allotments in Ashford (other than those adjacent to Residential Character Ashford Policy Area 22): four storeys or 16.5 metres</p> <p>All other allotments: three storeys or 12.5 metres.</p>





### Residential Zone

**PDC 7** Dwellings should be set back from allotment or site boundaries to:

- (a) contribute to the desired character of the relevant policy area
- (b) provide adequate visual privacy by separating habitable rooms from pedestrian and vehicle movement.

**PDC 10** Dwelling setbacks from side and rear boundaries should be progressively increased as the height of the building, (with the total wall height of the building being measured from the existing ground level at the boundary of the adjacent property as shown by Figure 1), increases to:

- (a) minimise the visual impact of buildings from adjoining properties
- (b) minimise the overshadowing of adjoining properties.

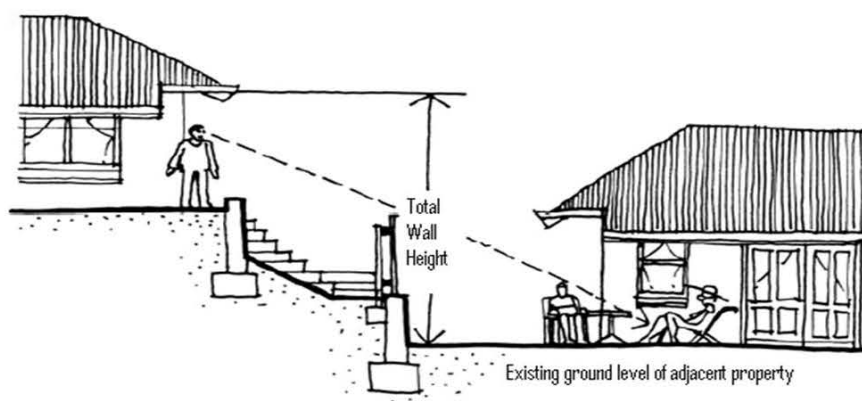


Figure 1

**PDC 11** Except where otherwise specified by a policy area, and for party walls, dwellings and buildings containing dwellings should be set back from the side and rear boundaries in accordance with the following table:

Parameter	Minimum value (metres)
Side boundary setback where the vertical side wall is 3 metres or less in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	1
Side boundary setback where vertical side wall measures between 3 to 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	2
Side boundary setback where the vertical side wall is greater than 6 metres in height (measured from the existing ground level at the boundary of the adjacent property as per <a href="#">Figure 1</a> )	2 metres plus an additional setback which is equal to the increase in wall height above 6 metres.





Upon our review of the Development Plan there are a range of quantitative and qualitative provisions that guide building setbacks for the proposed development. Policy Area PDC 5 and Zone PDC 11 list the quantitative requirements for front, side and rear setbacks as follows:

- Setback from Anzac Highway – 3 metres.
- Rear setback – 4 metres.
- Side setbacks:
  - » Where the vertical side wall is 3 metres or less – 1 metre; and
  - » Where the vertical side wall is greater than 6 metres – 2 metres plus an additional setback equal to the increase in wall height above 6 metres (in this proposal the requirement is 6.3 metres).
- Site coverage of 70 percent.

The proposed development includes the following setbacks:

- Setback from Anzac Highway – between 4.98 metres and 6.93 metres.
- Rear setback – 4.1 metres.
- Side setbacks:
  - » North eastern boundary – 2.4 metres; and
  - » South western boundary – between 1.5 metres and 2.5 metres.
- Site coverage of approximately 55 percent.

In relation to the front setback, adequate separation is achieved between the building and Anzac Highway to allow for the provision of meaningful and high quality landscaping. The building setback and design of the ground level interface will contribute positively to the human scale along Anzac Highway. The proposed landscaping will complement the design of the building and will provide an additional form of screening to what we consider a well-designed ground level vehicle parking area.

The rear setback requirement of 4 metres is achieved, therefore we are comfortable that adequate separation is provided between the proposed development and the sites to the north west.

While the proposed side setbacks do not strictly satisfy Zone PDC 11, the intent of Zone PDC 10 is achieved in that no detrimental overshadowing or visual impacts will be presented upon adjacent properties. The overshadowing diagrams demonstrate that adjacent properties will receive adequate natural light in accordance with the Development Plan requirements. Further, the proposed height is less than the 4 storeys which is anticipated and in an expected built form outcome along this part of Anzac Highway.

Overall, we have formed the opinion that the proposed setbacks are appropriate.

#### 4.6 Apartment Design

The following provisions are what we consider to be the most relevant in assessing the apartment design of the proposed development.



*Medium and High Rise Development (3 or more storeys)*

- PDC 14** Residential buildings (or the residential floors of mixed use buildings) should have habitable rooms, windows and balconies designed and positioned with adequate separation and screening from one another to provide visual and acoustic privacy and allow for natural ventilation and the infiltration of daylight into interior and outdoor spaces.
- PDC 15** Living rooms should have a satisfactory short range visual outlook to public or private open space.
- PDC 19** Multi-storey buildings should include a variety of internal designs that will facilitate adaptive reuse, including the conversion of ground floor residential to future commercial use (i.e. by including floor to ceiling heights suitable for commercial use).
- PDC 25** Dwellings should provide a covered storage area of not less than 8 cubic metres in one or more of the following areas:
- (a) in the dwelling (but not including a habitable room)
  - (b) in a garage, carport, outbuilding or an on-site communal facility and be conveniently located and screened from view from streets and neighbouring properties.

*Design and Appearance*

- PDC 5** Balconies should:
- (a) be integrated with the overall form and detail of the building
  - (b) include balustrade detailing that enables line of sight to the street
  - (c) be recessed where wind would otherwise make the space unusable
  - (d) be self-draining and plumbed to minimise runoff.

*Residential Development*

- PDC 22** Dwellings located above ground level should provide private open space in accordance with the following table:

Dwelling type	Minimum area of private open space
Studio (where there is no separate bedroom)	No minimum requirement
1 bedroom dwelling	8 square metres
2 bedroom dwelling	11 square metres
3+ bedroom dwelling	15 square metres



Residential Apartments within medium to high residential developments should provide a variety of dwelling types, be adaptive, and of high amenity (Medium and High Rise Development PDC 19). The level of amenity within these apartments is influenced by natural light and ventilation, storage and private open space (in addition to floor area), car parking and bicycle parking, visual and acoustic privacy and amenity.

By way of design, the orientation and configuration of apartment layouts achieve all relevant passive design principles with natural daylight and ventilation to all habitable rooms, high quality outlooks, and high quality external spaces.

Internally, all bedrooms will be provided with built in wardrobes and will receive natural light via light wells running centrally through the building, or through glass sliding doors to the balcony areas. The bedroom and bathroom windows adjacent these light wells will be obscured to maintain privacy. Internal (excluding habitable rooms) and external storage areas for each apartment exceed the minimum prescribed areas for all apartment types, and are not inclusive of storage areas in habitable rooms. A linen closet is provided to each apartment, along with two bathrooms for the convenience of the future occupants.

All apartments will include specific measures to ensure that the dividing walls and floors between the apartments comply with the requirements of Building Code of Australia for sound insulation.

Balconies range between 10.88 square metres and 13.64 square metres. Only three apartments fall short of the minimum private open space area stipulated by Residential Development PDC 22, by a mere 12 square centimetres which is not fatal given that these balconies are appropriately dimensioned to be functional and have excellent orientation (north east).

Each apartment has also been provided with two secure car parking spaces and conveniently located bicycle parking.

In consideration of all the above, we have formed the opinion that the proposed development offers a high level of amenity for occupants.

#### 4.7 Parking, Access and Traffic

The following provisions are what we consider to be the most relevant in assessing the parking, access and traffic arrangements for the proposed development.

##### *Transportation and Access*

**PDC 8** *Development should provide safe and convenient access for all anticipated modes of transport.*

**PDC 20** *Development should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:*

- (a) showers, changing facilities and secure lockers*
- (b) signage indicating the location of bicycle facilities*
- (c) bicycle parking facilities provided at the rate set out in Table WeTo/7 - Off-street Bicycle Parking Requirements for Urban Corridor Zone.*



- PDC 21** *On-site secure bicycle parking facilities should be:*
- (a) located in a prominent place*
  - (b) located at ground floor level*
  - (c) located undercover*
  - (d) located where surveillance is possible*
  - (e) well lit and well signed*
  - (f) close to well used entrances*
  - (g) accessible by cycling along a safe, well lit route.*
- PDC 34** *Development should provide off-street vehicle parking and specifically marked disabled car parking places to meet anticipated demand in accordance with Table WeTo/2 - Off Street Vehicle Parking Requirements.*
- PDC 35** *Development should be consistent with Australian Standard AS 2890 Parking facilities.*
- PDC 36** *Vehicle parking areas should be sited and designed to:*
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development*
  - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network*
  - (c) facilitate safe and convenient traffic circulation*
  - (d) result in minimal conflict between customer and service vehicles*
  - (e) avoid the necessity to use public roads when moving from one part of a parking area to another*
  - (f) minimise the number of vehicle access points onto public roads*
  - (g) avoid the need for vehicles to reverse onto public roads*
  - (h) where practical, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for multiple access points*
  - (i) not dominate the character and appearance of a site when viewed from public roads and spaces*
  - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas*





*(k) include infrastructure such as underground cabling and connections to power infrastructure that will enable the recharging of electric vehicles.*

**PDC 39** *Vehicle parking areas that are likely to be used during non-daylight hours should provide floodlit entry and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the parking area.*

**PDC 43** *On-site visitor parking spaces should be sited and designed to:*

- (a) not dominate internal site layout*
- (b) be clearly defined as visitor spaces not specifically associated with any particular dwelling*
- (c) be accessible to visitors at all times.*

**PDC 45** *Vehicle parking areas servicing more than one dwelling should be of a size and location to:*

- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely*
- (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area*
- (c) reinforce or contribute to attractive streetscapes.*

The Development Plan envisages parking areas that are safe, functional, and convenient, and that do not unreasonably impact on existing transport networks. Further, these areas should be appropriately lit for non-daylight hours, provided with landscaping, and should not dominate the character and appearance of a site when viewed from public roads and spaces.

With reference to *Table WeTo/2 – Off Street Vehicle Parking Requirements*, residential flat buildings are required to have 2 car parking spaces per dwelling (one of which is covered) and 0.25 car parking spaces per apartment for visitor parking.

The proposal offers each of the eight proposed apartments with two car parking spaces, and a total of four visitor spaces. The proposal provides a total of 20 car spaces which exceed the minimum requirements. Although there is not a specific requirement for bicycle parking in this Zone/Policy Area, the development provides 12 bicycle parking spaces (6 covered, and 6 uncovered for visitors) to encourage this alternate mode of transport, especially given the nearby public bikeway to the north east (Transport and Access PDC 20).

In order to maximise the functionality of the vehicle parking area, the proposal seeks to relocate the existing single crossover to the centre of the site and extend it to be double width. This proposed change in access will not substantially impact the functionality of Anzac Highway and will permit safe, simultaneous egress and ingress of vehicles. Further, this access arrangement allows adequate internal space within the parking area for the safe and efficient movement of vehicles (cars and bicycles).



This parking area has been integrated within the overall building design behind the main façade, and will not dominate the streetscape. Further, low level lighting and landscaping to the front, sides and rear of the building maintain a high amenity and pedestrian scale, whilst enhancing safety for residents and visitors.

In light of the above, the proposed traffic, car parking and access arrangements are considered to be more than acceptable and demonstrate that the proposal is not an overdevelopment of the site.

#### 4.8 Crime Prevention

We consider the following provisions most relevant in assessing the ability of the proposed development to prevent and deter crime.

##### *Crime Prevention*

**Objective 1** *A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.*

**PDC 1** *Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.*

**PDC 2** *Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.*

**PDC 3** *Development should provide a robust environment that is resistant to vandalism and graffiti.*

**PDC 4** *Development should provide lighting in frequently used public spaces including those:*

- (a) along dedicated cyclist and pedestrian pathways, laneways and access routes*
- (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.*

**PDC 5** *Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.*

**PDC 6** *Landscaping should be used to assist in discouraging crime by:*

- (a) screen planting areas susceptible to vandalism*
- (b) planting trees or ground covers, rather than shrubs, alongside footpaths*
- (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.*

**PDC 7** *Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.*

**PDC 10** *Development should avoid pedestrian entrapment spots and movement predictors (e.g. routes or paths that are predictable or unchangeable and offer no choice to pedestrians).*



The Development Plan seeks to maximise safety by encouraging clear lines of sight, appropriate lighting and the use of visibly permeable barriers (Crime Prevention PDC 1). Crime Prevention PDC 2 also states that buildings should overlook public and communal streets, and PDC 6 describes how landscaping could be utilised to discourage crime.

The proposal has been designed with a focus on creating a high quality experience at a pedestrian level and as such, fundamental considerations have been made in relation to ensuring pedestrian safety. The landscaping, building entrances, lighting, use of high quality materials, and casual surveillance promoted through the proposed design is anticipated to create an environment which deters crime.

In our opinion the proposed development achieves the intent of the Development Plan provisions through the following:

- promoting natural surveillance of the public realm (Anzac Highway) from upper level balconies;
- ensuring clear lines of sight are provided at locations at ground level with direct sightlines capable of being achieved between Anzac Highway and the building entrance;
- avoiding any opportunities for concealment;
- entrance to the car park will be controlled with a key card or remote control, with visitor access to the undercover vehicle parking spaces being controlled by residents via an intercom system. This promotes territoriality or sense of ownership through the clear delineation;
- the use of robust and durable design features to discourage vandalism; and
- inclusion of appropriately located trees and ground covers to reduce concealment opportunities.

As such, we have formed the opinion that the proposal satisfies the relevant provisions relating to crime prevention.

## 4.9 Environmental Considerations

Following are the most relevant environmental provisions relating to the proposed development.

### 4.9.1 Overshadowing

#### *Residential Development*

**PDC 11** *Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.*

**PDC 12** *Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:*

- (a) half of the existing ground-level open space*
- (b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).*





The shadow diagrams included at Appendix 5 as prepared by Piteo Design Architects show that at 2:00pm and 3:00pm on the 21 June, the neighbouring dwelling to the south west of the subject site will be exposed to full sun. Although not shown, it is fair to conclude that the dwelling will continue to receive full sun for the remainder of the afternoon due to the sun's path to the west satisfying PDC 11. With specific reference to the private open space for this dwelling, over half of this area will receive full sunlight from 11:00am on the 21 June, satisfying PDC 12.

The shadow analysis further demonstrates that the proposed development is not an over-development of the site.

#### 4.9.2 Overlooking

##### *Design and Appearance*

**PDC 10** *Development should minimise direct overlooking of the main internal living areas and private open spaces of dwellings through measures such as:*

- (a) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct*
- (b) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms*
- (c) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity.*

**PDC 11** *Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes.*

Design techniques such as offsetting balconies and windows, and use of screening devices are identified in Design and Appearance PDC 10 as suitable ways of minimising overlooking.

The proposal minimises opportunities for overlooking through the use of screening devices, obscured glazing to a height of 1.7 metres, and built form elements.

In consideration of these elements and the existing built form in the locality, we are comfortable that opportunities for overlooking will be adequately addressed by the proposed development.

#### 4.9.3 Energy Efficiency

##### *Energy Efficiency*

**PDC 1** *Development should provide for efficient solar access to buildings and open space all year around.*

**PDC 2** *Buildings should be sited and designed:*

- (a) to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings*





(b) so that open spaces associated with the main activity areas face north for exposure to winter sun.

**PDC 3** Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:

(a) taking into account overshadowing from neighbouring buildings

(b) designing roof orientation and pitches to maximise exposure to direct sunlight.

*Medium and High Rise Development (or more storeys)*

**PDC 20** Multi-storey buildings should:

(a) minimise detrimental micro-climatic and solar access impacts on adjacent land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, glare and shadow

(b) incorporate roof designs that enable the provision of rain water tanks (where they are not provided elsewhere), photovoltaic cells and other features that enhance sustainability.

Development is sought to be designed and sited to allow year-round solar access to buildings and open space both on the subject site, and neighbouring allotments. Further, multi-storey buildings should seek to reduce their detrimental micro-climatic impacts through building design, and also include sustainable initiatives such as solar hot water and photovoltaic cells.

We have previously commented on the impact on adjacent properties. With respect to the impacts on-site, all apartments will receive natural light and ventilation, and all are designed to reduce reliance on active systems through the provision of the following ESD initiatives:

- light wells;
- solar panels on roof;
- energy efficient LED lighting;
- water saving fixtures and fittings
- high performance ceiling insulation; and
- low environmental impact by use of low embodied energy materials and finishes and low water usage features and plantings.

We have formed the opinion that the above initiatives satisfy the Development Plan requirements. As such, we believe the proposal exceeds the standard in relation to energy efficiency and sustainable design.



#### 4.9.4 Waste

##### *Medium and High Rise Development (3 or more storeys)*

**PDC 26** *Development should provide a dedicated area for the on-site collection and sorting of recyclable materials and refuse, green organic waste and wash-bay facilities for the ongoing maintenance of bins. This area should be screened from view from public areas so as to not detract from the visual appearance of the ground floor.*

##### *Waste*

**PDC 1** *Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:*

- (a) avoiding the production of waste*
- (b) minimising waste production*
- (c) reusing waste*
- (d) recycling waste*
- (e) recovering part of the waste for re-use*
- (f) treating waste to reduce the potentially degrading impacts*
- (g) disposing of waste in an environmentally sound manner.*

**PDC 2** *The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.*

**PDC 5** *Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.*

**PDC 6** *Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:*

- (a) screened and separated from adjoining areas*
- (b) located to avoid impacting on adjoining sensitive environments or land uses*
- (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system*
- (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water*
- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours*



- (f) *stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.*

The Development Plan encourages medium to high rise developments to utilise a dedicated waste storage area, which should be screened and separated from sensitive areas to avoid negative impacts, sealed to contain waste materials and odours, and conveniently located for collection.

The proposed development provides a communal waste storage area on the ground level, away from the sensitive living areas. Residents will be able to conveniently access this area to dispose of their waste into either the general or recycling bins provided. Further, property management will easily be able to move the bins through the vehicle parking area, and out onto the street on Council collection days.

The bin storage area will be sited on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water. The bins themselves will be cleaned regularly by property management within the wash down area to manage any unpleasant odours.

Respecting the above, we have formed the opinion that the proposed waste arrangements are appropriate for the proposed development.

#### 4.9.5 Stormwater

##### *Natural Resources*

- PDC 5**                      *Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.*
- PDC 9**                      *Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.*
- PDC 10**                    *Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.*

Stormwater should be managed in developments to maximise conservation, minimise consumption and encourage the re-use of water resources. There should be adequate provision to control over-flow runoff, minimise pollutant transfer, mitigate peak flows and manage the rate and duration of stormwater discharges.

The proposal will include a 4000L underground detention tank to control and manage the discharge of the peak flows. This will ensure that the pre-development flow rates are achieved. Further, the landscaping included in the proposal design will create a more permeable surface and reduce uncontrolled runoff.

Respecting the above, the proposal is considered to adequately satisfy the relevant stormwater management provisions.



#### 4.10 Affordable Housing

The following provisions are what we consider to be the most relevant in assessing the inclusion of affordable housing in the proposed development.

##### *Residential Zone*

**PDC 15**      *Development should include a minimum 15 per cent of residential dwellings for affordable housing.*

The Affordable Housing Overlay applies to the proposal. The Overlay is not mandatory, however the applicant investigated the opportunity to provide affordable housing within the development. In this instance, the difference between the construction cost versus the likely selling price point was too low representing a risk to the overall feasibility of the project. The applicant still intends to deliver high quality owner occupier apartments at a price point just beyond the affordable housing price threshold, therefore affordable housing will not be provided in this particular instance.





## 5. CONCLUSION

In our opinion, the proposal satisfies the majority of the Development Plan provisions and represents a form of development that is envisaged within Medium Density Policy Area 18. Of particular note, the proposal:

- provides an overall building appearance and design that represents architectural merit in its entire presentation including at pedestrian level;
- proposes a built form which is consistent with existing development in the locality;
- is sited such that the building height, bulk, scale and mass will not present any unreasonable impacts upon surrounding properties;
- provides high quality apartments which are each afforded with functional balconies and an attractive view or outlook;
- provides apartment layouts which achieve all relevant passive design principles with natural daylight and ventilation to all habitable rooms, including shading;
- provides more than adequate car and bicycle parking;
- allows safe, convenient and effective movement for all vehicle types and pedestrians;
- will replace one Council street tree with two street trees either side of the new crossover;
- provides for a safe and effective waste management strategy;
- creates a safe, secure and crime resistant environment;
- incorporates a number of energy efficiency features;
- provides an acceptable stormwater management plan;
- presents no unreasonable overshadowing, overlooking, or visual impacts upon the surrounding area; and
- contributes to a range of housing types just above the affordable housing threshold within close proximity to public transport links.

Accordingly, we believe Council should grant Development Plan Consent.



## APPENDIX 1. DEVELOPMENT APPLICATION FORM



## APPENDIX 2. ELECTRICITY ACT DECLARATION FORM



### APPENDIX 3. CERTIFICATES OF TITLE





**APPENDIX 4. STORMWATER AND DRAINAGE PLAN**  
PREPARED BY MLEI

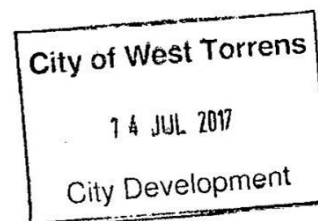


**APPENDIX 5. PROPOSED PLANS AND DRAWINGS**  
PREPARED BY PITEO DESIGN ARCHITECTS

Ms Oggi Stojanovich  
22 Gunnawarra Avenue  
CAMDEN PARK SA 5038

14 July 2017

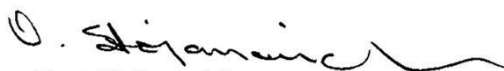
The Chief Executive Officer  
City of West Torrens  
165 Sir Donald Bradman Drive  
HILTON SA 5033

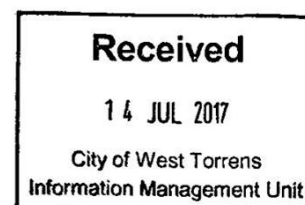


Dear Sir / Madam

Please find attached my representations regarding the Notice of Application for Category 3 Development Pursuant to Section 38 (5) of the Development Act, 1993 in relation to 425 Anzac Highway, Camden Park, SA 5038.

Yours sincerely

  
Oggi Stojanovich



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act, 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

DEVELOPMENT No. 211/676/2017  
 PROPERTY ADDRESS: 425 Anzac Highway, CAMDEN PARK SA 5038

YOUR FULL NAME	Ms Oggi STOJANOVICH
YOUR ADDRESS	22 GUNNAWARRA AVENUE CAMDEN PARK SA 5038
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	owner of land adjoining part of the back fence. (eg. Adjoining resident, owner of land in the vicinity etc.)
REASON/S FOR REPRESENTATION See attachments including photo.	
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought) See attachments.	

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

- ☒ I DO NOT WISH TO BE HEARD  
☐ I DESIRE TO BE HEARD PERSONALLY  
☐ I DESIRE TO BE REPRESENTED BY \_\_\_\_\_

(PLEASE SPECIFY)

SIGNED

*O. Stojanovich*

☒ It is my preference not to have to be heard in the hope  
☒ my issues can be resolved  
☐ however, I have a desire to be heard personally if they cannot be resolved.

DATE 14.7.17

Responsible Officer: Phil Harnett  
 Ends: Friday 21 July 2017

\* N.B. Please note: I will be overseas from 1-8 August 2017 inclusive and will not have access to the internet or mobile phone during this time. *O. Stojanovich*

If space insufficient, please attach sheets



ATTACHMENT 1**REASONS FOR REPRESENTATION****1. Location of the rubbish bin / bin-wash area**

I object to the location of the rubbish bin area which is being proposed to be located in the very corner at the back of the property which backs directly onto part of my back fence. I have a very small back yard (see attached photo) and my objections to the proposed bin location / bin-wash area are for the following reasons:

- a) My outdoor dining table is 2½ metres from the back fence directly on the other side of the proposed bin / bin-wash site.
- b) There is a bedroom with window very close to the outdoor dining table.
- c) The bathroom window is very close to the outdoor dining table.
- d) The back door is very close to the back fence.

Given it is proposed that 8 flats will be housed in this development, the number of bins and the bin-wash area will emit smells, particularly in the hotter weather. As we entertain and spend a great deal of time dining out the back, I find it unacceptable that I and my family and friends would have to be subjected to rubbish bins smells.

I also want to be able to open the bedroom / bathroom windows and the back door to allow fresh air into the house. It is unacceptable that I would have to keep my windows and door shut to keep out rubbish bin / bin wash smells out of my house.

**MY REPRESENTATIONS WOULD BE OVERCOME BY:**

The proposed rubbish bin / bin-wash area be shifted to the front and along one side of the building in an enclosed area so that it blends in with the proposed building and does not detract the location's appearance from Anzac Highway. I have seen many homes where an area of the front yard has been cordoned off with fence-like materials which blend in with the surrounds of the residence to conceal rubbish bins.

Looking at the current plans, it appears that occupants of the flats will have quite a way to walk from their flats to their bins and also when wheeling them out from the back to the front each week and wheeling them back in.

Trees (preferably tall deciduous trees) could be planted against the perimeter of the whole back fence and the back corner areas in place of the proposed rubbish bin / bin-wash area. This would help provide privacy and also absorb car emissions emanating from the ground level carpark.

**2. The 2 balconies at the back of the proposed development**

My concern here relates to the possibility of occupants hanging out washing or draping other things over the balcony which could be unsightly.

**MY REPRESENTATIONS WOULD BE OVERCOME BY:**

I seek clarification on whether there are, or there will be, any regulations to ensure this will not occur. If this matter has not been considered and there are no regulations governing this at present, I request that this be made a regulation to ensure the building maintains a high standard.

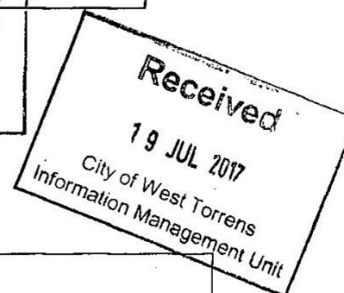
Ref: WTCC Objections to building

ATTACHMENT 2Backyard of 22 GUNNAWARRA AVE, CAMDEN PARK.

**STATEMENT OF REPRESENTATION**  
Pursuant to Section 38 of the Development Act, 1993

TO Chief Executive Officer  
City of West Torrens  
165 Sir Donald Bradman Drive  
HILTON 5033

19 JUL 2017  
City Development



DEVELOPMENT No. 211/676/2017  
PROPERTY ADDRESS: 425 Anzac Highway, CAMDEN PARK SA 5038

YOUR FULL NAME	GEORGE PENGLIS
YOUR ADDRESS	24 GUNYAWARRA AVENUE CAMDEN PARK S.A. 5038
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	ADJOINING RESIDENT <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION REASONS ARE OUTLINED IN SEPERATE 5 PAGE ATTACHMENT.	
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought) REFER TO ATTACHED 5 PAGE ATTACHMENT.	



Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO NOT WISH TO BE HEARD ☐

I DESIRE TO BE HEARD PERSONALLY ☒

I DESIRE TO BE REPRESENTED BY \_\_\_\_\_ ☐

(PLEASE SPECIFY)

SIGNED

*George Penglis*

DATE 14/7/17

Responsible Officer: Phil Harnett  
Ends: Friday 21 July 2017

If space insufficient, please attach sheets



To the Chief Executive Officer

City of West Torrens

165 Sir Donald Bradman Drive

HILTON SA 5033

Re: Development No. 211/676/2017

Property Address: 425 Anzac Highway, Camden Park SA 5038

Statement of Representation

Pursuant to Section 38 of the Development Act, 1993.

Dear Sir/ Madam

I make the following representations in respect of the above development application.

My representations seek to identify where the proposal does not comply with the West Torrens Development Plan or is otherwise unsuitable to proceed in its current form.

I have referred to the relevant development principles as I believe they should be applied in assessing the proposal.

I am not qualified in planning but I believe the development plan provides general principles for the development of land for the benefit of the whole community not just those who seek to profit from the development of a property.

Construction of a new residential building carries with it the potential for numerous problems to arise in the future long after sales contracts are fulfilled.

The Development Plan is a mechanism for reducing the potential problems at the development application stage.

In reviewing the application at Council chambers on Friday 14 July 2017 I was perplexed and disappointed that the siting of the bin storage area and the privacy issues created by the rear corner balconies was not resolved earlier.

I was also disappointed to observe no diagrams showing the scale of the proposed development as viewed from the rear in Gunnawarra Avenue.

There also did not appear to be any comments, statements or information provided to Council on the likely impact of this proposal on the adjoining property owners.

The following are excerpts from the relevant parts of the Development Plan and the wording in red italics below each item form my representation as they relate to the specific development plan principles.

I submit these representations for your full and earnest consideration.

George Penglis ,Property Owner

24 Gunnawarra Avenue

Camden Park SA 5038



Page 1.

## Medium and High Rise Development (3 or More Storeys)

### Design and Appearance

6. Materials and finishes should be selected to be durable and age well to minimise ongoing maintenance requirements. This may be achieved through the use of materials such as masonry, natural stone, prefinished materials that minimise staining, discolouring or deterioration, and avoiding painted surfaces particularly above ground level.

*The proposed building has substantial areas of its outside structure intended to be of painted concrete which is contrary to this development principle.*

*Due to the visual impact of the proposed building the materials used in its construction should be high quality, attractive and of good aesthetic appeal.*

7. Balconies should be integrated into the overall architectural form and detail of the development and should:

(c) allow views and casual surveillance of the street while providing for safety and visual privacy of nearby living spaces and private outdoor areas

*The proposal does not adequately provide for the visual privacy of nearby living spaces and private outdoor areas.*

*The location of the screening proposed for the rear corner balconies does not restrict views to "casual surveillance of the street" but allows unrestricted views to all areas of my property.*

*My property has bathroom, kitchen and toilet windows within 2 metres of the rear boundary.*

### Site Facilities and Storage

26. Development should provide a dedicated area for the on-site collection and sorting of recyclable materials and refuse, green organic waste and wash-bay facilities for the ongoing maintenance of bins. This area should be screened from view from public areas so as to not detract from the visual appearance of the ground floor.

27. Where the number of bins to be collected kerbside is 10 or more at any one time, provision should be made for on-site commercial collection.

*The dedicated area for recyclable materials and refuse can be located anywhere on the site as there are no ground floor units.*

*When there is joint recycling and waste collection there could be up to 16 bins to be collected kerbside and there is no provision for on-site commercial collection.*

**Page 2****Zone Interface**

29. Unless separated by a public road or reserve, development site(s) adjacent to any zone that has a primary purpose of accommodating low rise (1-2 storey) residential activity should incorporate deep soil zones along the common boundary to enable medium to large trees to be retained or established to assist in screening new buildings of 3 or more storeys in height.

One way of achieving this is for development comprising building elements of three or more storeys in height to be setback at least 6 metres from a zone boundary, and incorporate a deep soil zone area capable of accommodating medium to large trees with a canopy spread of not more than 8 metres when fully mature.

*Consideration should be given to this development principle as the proposed building is effectively in a newly zoned medium density area which abuts residential housing formerly zoned as a lower density land use area.*

"...development site(s) adjacent to any zone that has a primary purpose of accommodating low rise (1-2 storey) residential activity...."

*It is wholly consistent with this principle to require that this proposal have a setback of 6 metres from each adjoining boundary.*

*The primary purpose of the adjacent properties is low rise (1-2 storey) residential activity.*

*The principle should not require consideration only where a proposed development abuts an area that is actually zoned for lower density developments.*

*The proposed building should be set back 6 metres from the rear boundary and appropriate plantings of medium to large trees should occur.*

**Visual Privacy**

27. Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.

*The proposal incorporates frosted glass screening of 1.7 metres in height on both sides of the rear (corner) balcony areas. This is contrary to this development principle.*

*Note that the correct development principle is outlined in "Medium and High Rise Development (3 or More Storeys) Design and Appearance. Part 7 (c).*



**Page 3.****Noise**

28. Noise generated by fixed noise sources such as air conditioning units and pool pumps should be located, designed and attenuated to avoid nuisance to adjoining landowners and occupiers.

*The proposed building is flat roofed and is likely to require building occupants to have their own individual air conditioning units to be installed.*

*As the proposal does not indicate or specify the type or location of any air conditioning units it is not possible to estimate, evaluate or conclude whether noise will be a nuisance or not at this time.*

**Site Facilities and Storage**

30. Site facilities for group dwellings, multiple dwellings and residential flat buildings should include:

(c) household waste and recyclable material storage areas away from dwellings.

*The proposed siting of the bin storage area adjacent to the back boundary is contrary to this principle.*

*The proposed location is completely unsuitable as it adjoins the rear of my neighbours' property and is likely to severely impact on the amenity and enjoyment of the outside areas of the neighbours' property.*

*In addition the bin storage area and cleaning area would be adjoining my rear boundary and my home is sited less than 2 metres from that boundary.*

*I do not therefore consider that the bin storage area is sufficiently away from my dwelling to satisfy this principle.*

*I have lived before for many years in medium density accommodation in predominantly owner occupied units but there is no end to the types and volume of waste that will be left in a bin storage area.*

*The reality of bin storage areas in residential multiple dwellings is such that the following problems will arise:*

- 1. Operation of the area will occur at all times of the day and night*
- 2. Noise from disposal of waste and other recyclables into the bins is likely to be high and of considerable nuisance given the bins can be used 24 hours a day.*
- 3. Sorting of containers with deposits is also likely to generate considerable noise*
- 4. Cleaning of the bins will generate further noise and smells and flies will be attracted to the waste that is washed out of the bins onto the ground.*
- 5. There will likely be accumulation of unwanted household items in this area such as appliances furniture, household effects, soiled mattresses and so on. These will in turn create risk of vermin, fire, smells and mosquito breeding.*

**Page 4.**

6. *Bins left open or not will attract flies and generate unpleasant odours within metres of my bathroom, toilet, kitchen, bedrooms and outside areas*
7. *There is risk that third parties or others may use the bins to climb over the adjoining fence to steal from our properties or to cause injury to the occupants.*
8. *The location of the bins will create unnecessary noise as bins are moved to the kerb and back.*
9. *The proposal to use the adjoining fence as one side of the storage area is likely to cause considerable noise as bins lids are opened against the fence and as bins are placed back into the area against the fence.*
10. *The storage area should be of solid construction to minimize noise and sited so as to be as far away as possible from as many habited areas as possible.*
11. *Use of a boundary fence for the purpose of enclosing storage area for a multiple unit dwelling is completely unacceptable and not in keeping with the development plan.*
12. *Use of a boundary fence for the purpose of enclosing a bin storage area for a multiple unit dwelling does not meet any test of reasonableness or common sense.*
13. *Location of the bin storage area against the back fence of 2 existing property owners displays a contempt for community spirit and I am disappointed that this was not pointed out to the applicant during the early stages of processing of their application.*

*The only solution to the siting of the bin storage and cleaning areas is to relocate the area to the front of the site behind the proposed screened area to the right front of the property adjacent the sliding gates.*

*This area would make more sense for collection of the bins, recycling activities and would move the area well away from any dwelling.*

*Siting at the front is the usual location in multiple unit residences*

**Transportation and Access****Undercroft and Below Ground Garaging and Parking of Vehicles**

46. Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct and ensure:

(a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties

(g) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development)

(h) the height of the car park ceiling does not exceed 1 metre above the finished ground level

*This development has an overall car park height of 3 metres so does not meet this development plan principle.*

*The resultant overall height of the building is similar to nearby residential flat building that does not have undercroft parking but has 3 levels of dwelling units.*



**Page 5.**

*Undercroft car parking was not envisaged for this zone and is not a feature of any other recent residential developments in this area and is a feature associated with commercial buildings.*

*The resultant overall height of the building therefore adversely affects the streetscape character of the locality with regard to bulk and visual impact.*

*Use of undercroft parking at this site appears to be the only way that the proposed development can meet car parking and communal living space requirements.*

*The undercroft parking must be made to meet the 1 metre height restriction in accordance with this principle.*

REF: 0093-1- 425 Anzac Highway

6 November 2017

Mr Josh Banks  
Team Leader City Development – Urban Services  
West Torrens Civic Centre  
165 Sir Donald Bradman Drive  
HILTON SA 5033



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E: [info@futureurbangroup.com](mailto:info@futureurbangroup.com)  
ABN: 34 452 110 398

Dear Josh,

#### RESPONSE TO REPRESENTATIONS FOR DA 211/676/2017 AT 425 ANZAC HIGHWAY, CAMDEN PARK

We write in response to representations received during category 2 public notification of the above-mentioned development application and to the letter received from the City Assets Department of the City of West Torrens (dated 8 August 2017).

We have been provided with a total of two representations submitted by:

- Ms Oggi Stojanovich (22 Gunnawarra Avenue, Camden Park); and
- Mr George Penglis (24 Gunnawarra Avenue, Camden Park).

The locations of the representors in relation of the subject site are identified in Figure 1 below.

**Figure 1** Subject site and location of representors.



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The proposal has been amended in response to the representations received and the comments made by Council's City Assets Department. These amendments are reflected in the plans submitted with this response, and summarised below:

- reduction in car parking spaces allocated to visitors (two spaces now proposed instead of four);
- the width of car spaces located adjacent columns has been increased to 2.7 metres;
- relocation of the waste storage area (and wash down area) to the space adjacent to the automatic sliding gate (closer to the Anzac Highway frontage);
- directing waste water from the bin wash down area to the sewer system;
- provision of additional landscaping, including one additional medium sized tree adjacent to the north western boundary of the subject site.

In addition to the above, MLEI Consulting Engineers have updated the Stormwater Management Plan (response enclosed), and the Waste Management Plan has been amended by GTA Consultants to confirm private and onsite waste collection (also enclosed).

Following, we respond to the issues raised by the representors and then we turn our attention to the comments raised by Council's City Assets Department.

## RESPONSE TO REPRESENTORS

The representors outline the following concerns in relation to the proposed development:

- Location of the waste bin area and the consequential impacts on their properties by way of general amenity, noise, odour, and security;
- Rear setback of the proposal and the inclusion of medium to large trees;
- 'Improvement' of sill heights and permanent screens to prevent overlooking into private areas;
- Ceiling height of the proposed car park, and the consequential height, bulk and scale of the building; and
- View of the proposed development from Gunnawarra Avenue.

Further to the above, the representors also sought further information in relation to the following:

- removal of organic waste (waste from landscaping);
- the location of clothes drying facilities; and
- the location of air conditioning units.

These concerns and queries will be addressed under their respective headings below.

## WASTE STORAGE AREA

The applicant acknowledges the points raised by Mr Penglis and Ms Stojanovich in relation to the location of the bin storage area and the potential impacts this location will have on their properties (adjoining to the north of the subject site).





Respecting these concerns, the waste storage room has been relocated to an enclosed space adjacent to the Anzac Highway frontage. Two medium-sized, deciduous “Chinese Pistachio” trees and other landscaping is proposed along the north-western boundary of the subject site (adjoining the boundaries of the representor properties).

The enclosed bin area will be separated by the auto-sliding gate, and a considerable distance from the properties to the rear. We believe this amendment will alleviate the following representor concerns:

- the impact of noise associated with waste disposal (movement of bins, slamming of lids, sorting of recyclables, etc.) will be considerably mitigated as the bin storage area will be enclosed;
- the bin area will be accessible only to residents and the waste collection contractors to prevent inappropriate use of the waste area by third parties and attraction of vermin;
- odours associated with waste will be confined to within the bin enclosure, but will also be managed through regular cleaning of the bins; and
- the wash down area is now located within the bin enclosure, restricting odours and noise to within the enclosure only.

The representors also expressed concern in relation to the accumulation of hard rubbish such as mattresses and appliances on site. It is proposed that property management will coordinate the communal collection of hard rubbish from the subject site by either Council or separate contractors on an “as needs” basis. This will prevent hard rubbish from accumulating on site, and addresses the relevant representor’s concerns.

We are comfortable that the amended location of the bin enclosure resolves representor concerns related to waste collection.

#### REAR SETBACK

Ms Stojanovich and Mr Penglis state in their representations that Medium and High Rise Development PDC 29 has not been satisfied by the proposed development. They form the view that the proposed development should adhere to a 6 metre rear setback with medium to large trees provided within the setback distance.

We note that PDC 29 only applies to “*development site(s) adjacent to any zone that has a primary purpose of accommodating low rise (1-2 storey) residential activity...*”. The subject land and all adjoining sites (including those properties owned by the representors) are located within Medium Density Policy Area 18 which contemplates development up to four storeys. Therefore, PDC 29 does not apply in this instance.

The proposed rear setback of 4 metres satisfies Policy Area PDC 5 which is the applicable rear setback provision.

Notwithstanding the fact that Medium and High Rise Development PDC 29 does not apply to the subject site, two medium sized trees are proposed along the north-western boundary. These trees will grow to a height of approximately 8 metres and will soften any impact the building may have upon the representors properties.

As such, we have formed the opinion that the development will not unreasonably impact upon the amenity of the representors properties.





## OVERLOOKING

Ms Stojanovich and Mr Penglis express concern in relation to the consistency of the application with Residential Development PDC 27, as it includes frosted glass to a height of 1.7 metres. Residential Development PDC 27 states:

*“Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.”*

[underline our emphasis]

It is important to note that the proposed development is a three storey building and therefore this provision does not technically apply. Notwithstanding, to mitigate overlooking the proposal provides 1.7 metre sill heights/screens and obscured glass to reduce the opportunity for overlooking from the proposed development into the neighbouring private open space.

In our opinion, the adoption of these sill heights/screens/obscured glass along with the trees proposed to the rear of the site should alleviate the representors concerns.

## CAR PARK CEILING HEIGHT, BULK AND SCALE

Both representors refer to Transport and Access PDC 46, and note that undercroft and below ground garaging is not envisaged in the Zone (or Policy Area) and that the proposed ceiling height is greater than the maximum of 1 metre above finished ground level prescribed. This provision specifically relates to “Undercroft and Below Ground garaging”. The car parking area is at-grade and does not constitute an undercroft or below ground car park. An undercroft by definition should have a ceiling height that does not exceed 1 metre above finished ground level. Nonetheless, we the representors are concerned that the car parking element results in an overall building height which *“adversely affects the streetscape character of the locality in regard to bulk and visual impact”*.

First, we note that the relevant Zone provisions envisage development up to four storeys on the subject land. On this basis, we have formed the opinion that the proposed three storey building represents an overall height, bulk and scale that is anticipated on the site and within the zone. The provision of at-grade car parking that is sufficiently screened is an efficient use of the land, and provides an adequate car parking supply for the proposed development.

Further, the scale of the proposed development is not dissimilar to other developments along this part of Anzac Highway. With reference to the Building Perspectives prepared by Piteo Design Architects (enclosed), the building design incorporates external materials, colours, parapets, screening and landscaping to create a visually interesting and articulated building which will achieve a human scale and sit comfortably in the streetscape.

Respecting the above, we believe that the representor concerns in relation to the undercroft car parking, and the overall bulk and scale of the proposed development have been addressed.

## PROVISION OF CLOTHES LINE FACILITIES

Mr Stojanovich sought clarification in relation to the location of clothes drying facilities, and expressed concern that residents would drape clothes and other items over the balcony to dry.

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We can confirm that each apartment will be provided with internal dryers to discourage occupants using balconies for clothes drying. Should occupants use balconies for drying clothes, the balcony screens will visually screen clothes from neighbouring properties.

Should occupants seek to dry clothes on a clothesline, an opportunity exists to provide a communal drying area to the rear of the building.

#### LOCATION OF AIR CONDITIONING UNITS

We can confirm that air conditioning units will be located on the roof, so as to not unreasonably impact upon occupants of the proposed building, or neighbouring land.

#### REMOVAL OF ORGANIC WASTE

One of the representors was concerned that no arrangements had been made in relation to the removal of organic waste from the proposed development. We confirm that landscaping waste is proposed to be collected and removed from the subject land by contract landscape gardeners, who will be coordinated by property management.

Given the size of the proposed development, accommodation of a separate stream for organic waste for the apartments has not been considered necessary at this stage.

#### RESPONSE TO CITY ASSETS

Council's City Assets Department raise matters relating to:

- Traffic;
- Waste;
- Stormwater; and
- Green wall/ green roof.

We respond to each matter in turn.

#### TRAFFIC

Council's City Assets Department recommended that the car parking spaces adjacent columns or walls be widened to 2.7 metres and that a parking space for use by persons with a disability be provided.

The car parking arrangement has been revised to incorporate spaces of the additional width requested.

*Table WeTo/2 – Off Street Vehicle Parking Requirements* outlines that one parking space should be provided for persons with a disability in a development which has between 10 and 25 car parking spaces. GTA have considered it appropriate to convert the visitor car park adjacent to the turning bay to a space for persons with a disability. The turning bay area will act as the required shared space through the inclusion of a retractable bollard. This bollard can be lowered by the waste contractor during collection times only, ensuring that it is a shared space at all other times. This approach is considered appropriate as waste collection will occur over a short period of time and outside of peak visitor periods.



We are comfortable that the amendments to the car park are satisfactory, and that based on GTA's advice, a parking space for use by persons with a disability is not necessary.

## WASTE

The City Assets department recommended that a detailed Waste Management Plan be developed to demonstrate the appropriate scale and frequency of waste servicing, and explore design requirements for on-site manoeuvrability.

GTA Consultants have developed an arrangement for onsite collection of waste, using a smaller refuse vehicle. This arrangement allows adequate space for the lifting and collection of waste bins, forward entry and exit of the waste truck, and allows a light vehicle to pass when the waste truck is parked in the turning bay.

With the continued provision of two waste bins (one for general and one for recycling) to each apartment, the proposed development provides adequate waste storage for private collection. Further, the on-site collection strategy is consistent with the Development Plan and resolves City Asset's concerns in relation to the functionality of the previously proposed Council on-street collection.

## STORMWATER

Council's City Assets Department sought clarification and further information relating to the following aspects of the Stormwater Management Plan:

- confirmation of the proposed mechanism to control the maximum stormwater discharge rate;
- details in relation to the connection of the site stormwater drainage systems to the street kerb and mechanisms to prevent backflow of stormwater from the public system into the private land;
- revised consideration of the stormwater quality improvement mechanisms; and
- redirection of water from the bin wash down area to the sewer, not the street.

In response, MLEI have provided an amended stormwater drainage assessment which can be summarised as follows:

- the subject site will utilise a 4,000L capacity 'DRAINWELL subsurface water management system' to achieve the required detention volume, and includes an additional 1,326L of retention capacity;
- a 70.5mm diameter orifice plate will be located at the outlet of the underground tank system to restrict flows to the calculated allowable discharge of 5.09L/s, allowing the remaining 1.24L/s to discharge unrestricted;
- water exiting the detention tank will be pumped to a proposed GIP located at the boundary of the property, then conveyed to a street kerb outlet on Anzac Highway by gravity; and
- An 'Ecosol Storm Pit 10L (Class 2)' filtration system is now proposed for inclusion in the development in order to effectively treat the water in order to achieve a quality standard specified in the State Government Water Sensitive Urban Design Policy.

Waste water runoff from the bin wash down area has also now been redirected to the sewer system.





We trust that the additional information provided by MLEI is sufficient to respond to the issues raised by Council's City Assets Department.

### GREEN WALL / GREEN ROOF

The City Assets Department suggests that the proposed development warrants the inclusion of a green roof or wall.

We note that Medium and High Rise Development PDC 21 encourages green roofs which can be used as a substitute for private or communal open space (where these spaces are accessible for building occupants), however to our knowledge, the Development Plan makes no reference to the inclusion of green walls. Notwithstanding, the proposal provides private open space to each apartment in excess of the minimum guidelines. As such, we do not consider it necessary for the proposal to provide a communal open space to compensate for a shortfall in private open space.

Council's City Assets department also notes that *"the site arrangement leaves almost no remaining pervious area or areas of landscaping"*. We disagree with this comment, as the proposed development provides 128 square metres of soft landscaping (excluding paving) constituting approximately 17 percent of the site area which exceeds the minimum 10 percent envisaged by Landscaping, Fences and Walls PDC 4. Of this area, there are three deep soil zones provided for medium sized trees which satisfies Medium and High Rise Development PDC 23.

### CONCLUSION

In reviewing comments and queries received by representors and Council's City Assets department, we believe that the proposed development has been amended to address all relevant concerns. Where amendments have not been made, we believe our response satisfactorily addresses the concerns raised in the context of the relevant provisions of the Development Plan.

Respecting the above, we have formed the opinion that the proposed development warrants Development Plan Consent.

Should the representors concerns remain unresolved we confirm our attendance at the upcoming Council Assessment Panel meeting.

Should you wish to discuss any of the above matters, please do not hesitate to contact the undersigned on (08) 8221 5511 or 0450 965 858.

Yours Sincerely

A handwritten signature in black ink, appearing to read 'Milly Nott'.

**Milly Nott**  
Urban Planner

Encl.      GTA Response  
             Amended Stormwater Management Plan  
             Amended Plans

REF 0093 | 6 November 2017





## Preliminary Traffic, Flooding & Stormwater Assessment

**Development Application No:** 211/676/2017

**Assessing Officer:** Jordan Leverington  
**Site Address:** 425 Anzac Highway, CAMDEN PARK SA 5038  
**Certificate of Title:** CT-5111/230  
**Description of Development:** Construction of a three storey residential flat building

### TO THE TECHNICAL OFFICER - CITY ASSETS

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Jordan Leverington**      **DATE**    22 January, 2018



## Memo

To Jordan Leverington  
From Andrew King  
Date 22/01/2018  
Subject 211/676/2017, 425 Anzac Highway, CAMDEN PARK SA 5038

Jordan Leverington,

The following City Assets Department comments are provided in regards to the e-mail dated Monday, 8 January 2018 5:10 PM from Milly Not assessment regarding waste commentary for the above application.

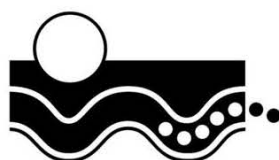
### 1.0 Waste Comments:

The applicant has persisted in the belief that the provision of a detailed waste management plan for the development is unnecessary, despite the recommendation of Council on several location of the benefits this would provide in improving assurances of the practical functionality of the site. Waste management commentary has been limited to selective comments from the applicant's planning consultant and traffic consultant.

The application as currently presented has actively chosen to not accommodate the separation of domestic/household green waste from the general waste stream. It is believed that this separation should now considered to be as standard practice, however I leave determination of the appropriateness or desirability of this to the planning officer. It is noted that if this separation of disposal and collection was to be accommodated, it would only further complicate the below commentary.

It is considered that in locations like Anzac Highway, the consequential impact of the servicing of the site not being well considered and not providing a practically achievable solution are quite high. If waste vehicles ultimately end up reversing into or out of the site because of vehicle size or necessary manoeuvrability is blocked or too tight brings with it high risk, if private waste collection is pushed to the kerbside because of the practical difficulties of accessing the site, then clearway and the safe operation of Anzac Highway can be compromised.

The applicant argue that as a 'waste' and a 'recycling' wheelie bin are provided for each dwelling, like a conventional Council service, this should be considered satisfactory as a waste approach for the development. However, best assumption as can be made from the provided information would suggest that the proposed provision is not equivalent to a standard service. Council standard service utilised a combination of 240 litre and 140 litre bins, although there is not evident information or notation on plans, it can only be interpolated from the dimensions of the waste storage room that the proposed bins for this development would likely only be



City of **West Torrens**

Between the City and the Sea

somewhere between 100 and 140 litre bins. This is not saying this scale of provision is unacceptable, as it has not been demonstrated one way or another, the reason this becomes important related more to the scale and frequency of servicing requirement and subsequently the degree of impact on the site.

As currently presented, the development provides for the desired two visitor parking spaces, however to accommodate private waste servicing, parking in one of these spaces will require banning for certain times to accommodate the waste servicing. Again, along Anzac Highway the provision and preservation of visitor parking should be considered to be of high importance. Council has previously considered that the banning of parking in this visitor space was to occur for a few hours on one or two days of the week, the degree of impact of this is likely to be reasonable. If the limitations of the scale of waste storage being provided in the development ultimately required the servicing of waste on a nearly daily basis, then the impact on parking would likely be considered unreasonable.

In the absence of demonstration of supply and demand and viable servicing practices for the site, the practical assurance and assessment of this is extremely difficult.

Discussion of 18 January 2018 with Milly Nott of Future Urban (with Andrew King, Coordinator Engineering Services) sort to eliminate at least one of these outstanding variables in relation to the practicality of waste management. It was requested if the applicant could provide confirmation from at least a single example waste service provider that they have the necessary vehicles within their fleet which would match (or outperform) the vehicle nominated in the GTA manoeuvrability report, that these vehicles can service from either the driver or passenger side of the vehicle (ie not front or rear) - hence suiting the site restrictions and that this fleet can service both 'general waste' and 'recycling'. This nomination of a compliant contractor would not be for the purposes of forcing or conditioning the applicant to utilise this contractor, but purely to demonstrate a verifiable real world solution to the site challenges.

***In summary, it is recommended by the City Assets Department that although the applicant does not see the merit in it, the provision of a Waste Management Plan for this development would still greatly assist in demonstrating the practicality of numerous elements associated with the functional operation of the site. As part of this, the provision of information indicating that there is currently a 'real world' waste service organisation which utilises the nominated service vehicle (for which the whole site design is critically based on) is needed and has been verbally requested from the applicant's Planning Consultant.***

Based on the demonstration of all elements of the waste servicing of the site being considered reasonable, it is suggested that the following associated conditions be included in any approval of the application;

- The site must be serviced by private waste collection arrangements - no convention Council kerbside collection service will be offered to this development.
- All servicing of the development must be undertaken from within the development site.





- All vehicles servicing this development must enter and exit the site in a forward direction.
- All parking spaces within the development must remain clear of obstructions (eg bins) at all times and free for utilisation of parking, servicing and manoeuvrability.
- All vehicles servicing the development must be limited in size to no larger than 7.4m in length, with a minimum turn radius (to outside wheel) of 7.4m and suitable to clear a physical height restriction of 3.0m.
- The visitor car parking space closest to Anzac Highway and the adjacent 'turning bay' area are to be clearly signposted as 'No Parking - Except for Waste Collection'. This parking control is to also be clearly indicated to be restricted to between either '9am-10.00am' or '1.00pm-3.00 pm' on a maximum of two nominated days of the week.
- The nominated turn bay shall be line marked and have bollard installation in accordance the requirements of a 'shared area' as outlined in 'AS/NZS 2890.6:2009 Part 6: Off-street parking for people with disabilities'.
- The 'turning bay' bollard is only permitted to be removed during the parking exemption period and must be reinstated during all other times.

Should you require further information, please contact Andrew King on the following extension number 8416 6333.

Regards

**Andrew King**  
**Coordinator- City Assets**



**From:** [Andrew King](#)  
**To:** [Jordan Leverington](#); [Dennis Batge](#)  
**Cc:** [Jane Teng](#)  
**Subject:** Waste Management - 425 Anzac Highway Development - 211-0676-2017  
**Date:** Wednesday, 21 February 2018 1:24:45 PM

---

Gentleman

I make reference to the above proposed development in relation to waste management information recently provided.

I Waste Management Proposal letter from Colby Industries, dated 2 February 2018, was recently received and has been reviewed.

The report outlining that with the development being considered High Density (greater than 70 du/ha) in nature, and with each of the 8 dwellings be limited to only 2 bedrooms each, then it can be justifiably demonstrated that the site could be serviced from a conventional Council waste arrangement, resulting in less than 10 bins presented to the street at any point in time. This arrangement requiring the resident of the development to share a standard 140 litre General Waste, 240 litre Recycling Waste and 240 litre Household Organic Waste bins. The report nominates for the necessity of 4 shared General Waste and 4 Shared Recycling Waste bins being the maximum arrangement presented to the street at any time. The subject allotment is in the order of 18m wide, with a common driveway of approximately 6m in width, widening to a max of around 8 to 10m at the kerbing. Using a 1m per bin width allowance, this would provide for sufficient remaining frontage to the property to accommodate the nominated number of bins.

I have reviewed the analysis behind this and can confirm that I believe this as being a reasonable approach for the specifics of this development and location.

DPTI recently typically provided commentary on Development Application along Anzac Highway requesting waste collection to be achieved on-site, with appropriate movements accommodated. It would be my understanding that this commentary would be preface on circumstances where the waste management is being proposed to be undertaken by private contractors requiring long stop servicing of the site and the necessity for vehicle operator having to exit the vehicle. It would be considered that these DPTI would not be pertinent to the provision of a Council standard waste collection to the site.

It would be recommended that in association with any approval for this development Conditions similar to the following should be included;

- The development will only be offered a 'Shared' Waste Service from Council, limited to a maximum of 4x 140 litre General Waste Bins, 4x 240 litre Dry Recycling Bins and 2x 240 litre Food Organics Bins.
- Organic Waste generated through the maintenance of landscaping within the site must be removed from the site by the landscape maintenance service for the site, and not disposed of in the Council waste collection bins.

It is noted that these City Assets comments exclusively relate to Waste Management and the status of any other previous commentary from the City Assets Department would remain in accordance with that previous commentary.

Many Thanks,  
**Andrew King**  
**Coordinator Engineering Services**  
**City Assets**  
City of West Torrens  
165 Sir Donald Bradman Drive  
Hilton SA 5033

## Arboricultural Assessment of Street Trees

Development Application No: **211/676/2017**

**REFERRAL DUE DATE:** 30 June 2017

Assessing Officer: **Phil Harnett**  
Site Address: **425 Anzac Highway, CAMDEN PARK SA 5038**  
Certificate of Title: **CT-5111/230**  
Description of Development: **Construction of a three storey residential flat building**

### TO THE TECHNICAL OFFICER – CITY ASSETS

Please provide your comments in relation to:

- ☐ The removal of or impact upon the Street Tree
- ☐ Species of Tree:
- ☐ Your advice is also sought on other aspects of the proposal as follows:

**Claret Ash Trees along ANZAC Highway are locally heritage listed.**

PLANNING OFFICER - Phil Harnett

DATE 16 June 2017

### FROM THE TECHNICAL OFFICER

**I have examined the plans as requested and provide comments as follow.**

As with all development applications it must be proven beyond reasonable doubt that all alternatives have been explored so not to hinder the progress of any street tree(s).

Any proposed development that does not consider "AS4970 Protection of Trees on Development Sites", is likely to require revision until all plans accurately correspond with the specific tree information detailed in this standard.

Verge interaction must consider all services that cross council land including stormwater outlets (and other) which will need to be maintained a minimum of 2.0m from any existing street tree (unless existing or otherwise negotiated). All services must be indicated /documented on appropriate plans for Council assessment and approval.

A site investigation together with the information provided has revealed that the location of the proposed crossover for the dwellings from Anzac Highway will impact on an existing Fraxinus angustifolia 'raywood' (Claret Ash) street tree.

With reference to the City of West Torrens, Fees and Charges Document 2016-2017 "Tree removal for driveway construction", once Council has assessed all circumstances and considered it acceptable that a street tree can be removed, a fee is calculated based on Council's standard schedule of fees and charges.

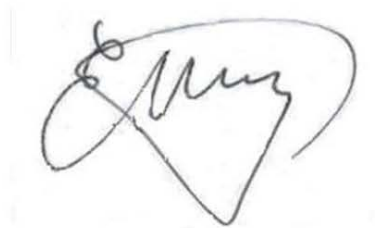
The fee is used to offset the loss of the asset (street tree) to the community, with funds received invested in Council's annual Greening Program.

As a result of the proposed crossover on Anzac Highway, City Works has considered the health, structure, form, useful life expectancy and age of the street tree and will support the removal, in this particular situation.

**A fee of \$1308.00 will be required prior to the commencement of any work.**

Please note, under no circumstances is any individual other than council staff permitted to interfere with a street tree. If pruning etc. is required, council must be notified via the appropriate customer request, and council staff will perform all works associated with the community asset.

Final crossover locations will be confirmed once appropriate documentation has been received from the applicant and they have submitted "an application to construct a vehicle crossing place(s) across council land".

A handwritten signature in black ink, appearing to read 'Enio Trombetta', enclosed within a hand-drawn oval shape.

Enio Trombetta  
Technical Officer Arboriculture

DATE: 26/06/2017

In reply please quote 2017/01931/01, Process ID: 465265  
 Enquiries to Vittorio Varricchio  
 Telephone (08) 8226 8393  
 Facsimile (08) 8226 8330  
 E-mail dpti.luc@sa.gov.au



**Government of South Australia**  
 Department of Planning,  
 Transport and Infrastructure

24/07/2017



**SAFETY AND SERVICE –  
 Traffic Operations**

GPO Box 1533  
 Adelaide SA 5001

Telephone: 61 8 8226 8222  
 Facsimile: 61 8 8226 8330

ABN 92 366 288 135

Mr Phil Harnett  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON SA 5033

Dear Mr Harnett,

### **SCHEDULE 8 - REFERRAL RESPONSE**

<b>Development No.</b>	211/676/17
<b>Applicant</b>	Walpol Developments Pty Ltd
<b>Location</b>	425 Anzac Highway, Camden Park
<b>Proposal</b>	3 Storey Residential Flat Building

I refer to the above development application forwarded to the Safety and Service Division of the Department of Planning, Transport and Infrastructure (DPTI) in accordance with Section 37 of the *Development Act 1993*. The proposed development involves development adjacent a main road as described above.

The following response is provided in accordance with Section 37(4)(b) of the *Development Act 1993* and Schedule 8 of the *Development Regulations 2008*.

### **THE PROPOSAL**

The application proposes to demolish an existing building and construct a three storey residential flat building comprising of eight apartments.

### **CONSIDERATION**

The subject site abuts Anzac Highway, an arterial road under the care, control and management of DPTI. Anzac Highway is identified as a Major Traffic Route, a Primary Freight Route, a Major Cycling Route and a Standard Frequency Public Transport Corridor under DPTI's 'A Functional Hierarchy for South Australia's Land Transport Network'. This section of Anzac Highway has an AADT of 33,700 vehicles per day (2.5% commercial vehicles).

The subject site has an existing access adjacent the north-eastern property boundary of the site. The application proposes a single shared two-way access point located centrally along the Anzac Highway frontage to serve the site. As this is in line with DPTI policy to minimise access points on the arterial road network in the interests of road safety, DPTI does not object in-principle to the proposed shared access. The existing Anzac Highway access point should be permanently closed and reinstated to Council standard kerb and gutter at the applicant's expense. It should also be noted

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that, subject to Council approval, the proposed access would require the removal of an existing street tree.

Plans provided to DPTI show that the access is 6.0 metres wide at the property boundary to allow for convenient two-way simultaneous movements and incorporates a clear area of 6.0 metres by 6.0 metres inbound from the property boundary to allow vehicles to store off-street while waiting for another vehicle to enter or exit the property. Additionally, DPTI is satisfied that the proposed gate is sufficiently setback from the Anzac Highway property boundary to allow a vehicle to store off-street while waiting for the gate to open or close.

Furthermore, it should be also ensured that pedestrian sight lines at the access comply with AS/NZS 2890.1:2004 Figure 3.3.

It is also DPTI policy that vehicles should enter and exit arterial roads in a forward direction in the interest of road safety. Although turn paths show that sufficient area is available for visitor vehicles to achieve this, it is noted that a visitor park is provided at the front of the site. Turn paths produced by DPTI show a vehicle parked at the front of the site would not be able to conveniently manoeuvre to exit this parking space due to the proposed vegetation adjacent the parking space. A vehicle would be required to undertake several manoeuvres within the functional area of the access in order to exit the parking space. Turn paths also show that the car park would not function if the electric gate is shut. Consequently, this has the potential to create vehicular conflict and lead to a reduction of safety adjacent the access point. Therefore, it is recommended that the front car park be removed from the application.

### **Refuse Collection**

DPTI has been informed that, at present, waste collection has not been specified by the applicant. It is unclear how such a large number of bins will be able to be safely located on the verge. Furthermore, it would appear that a refuse vehicle undertaking waste collection would potentially interfere with the free flow of the adjacent intersection. Consequently, it is recommended that on-site pick-up should be considered. In the event that waste removal is undertaken on site, turn paths should be provided showing that the largest vehicle expected on-site can achieve forward entry and exit to/from Anzac Highway.

### **Car Parking**

The application proposes a total of 19 on-site car parks (excluding the front car park) to serve the site. Council should ensure that the proposed development provides sufficient on-site car parking designed in accordance with AS/NZS 2890.1:2004. It should also be noted that parking adjacent the site is restricted.

### **CONCLUSION**

Whilst DPTI does not object in-principle to the proposed development, it subject to the following conditions.

### **ADVICE**

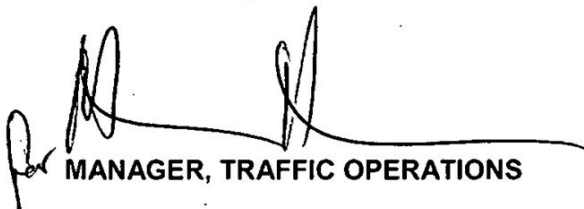
The planning authority is advised to attach the following conditions to any approval:

1. The site shall be served by a single access point direct to/from Anzac Highway. No additional vehicular access shall be permitted.

3

2. The Anzac Highway access shall be a minimum of 6.0 metres in width at the property boundary and extend at this width for a minimum of 6.0 metres into the site.
3. The existing Anzac Highway crossover shall be permanently closed and reinstated to Council standard kerb and gutter at the applicant's expense.
4. The access point shall comply with Figure 3.3 'Minimum Sight Lines for Pedestrian Safety' as defined in AS/NZ 2890.1:2004.
5. All on-site vehicle parking shall be consistent with AS/NZS 2890.1:2009.
6. All vehicles shall enter and exit the site in a forward direction.
7. Stormwater run-off shall be collected on-site and discharged without jeopardising the integrity and safety of Anzac Highway. Any alterations to the road drainage infrastructure required to facilitate this shall be at the applicant's cost.

Yours sincerely,



**MANAGER, TRAFFIC OPERATIONS**

For **COMMISSIONER OF HIGHWAYS**

A copy of the decision notification form should be forwarded to [dpti.developmentapplications@sa.gov.au](mailto:dpti.developmentapplications@sa.gov.au)

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## 6.2 16 Warwick Avenue, KURRALTA PARK

Application No 211/905/2017 & 211/731/2017

Appearing before the Panel will be:

Representors: **Rita Olbrycht** and **Anton Olbrycht** of 14 Warwick Avenue, Kurralta Park wish to appear in support of the representation.

**Voula Haliabalias** and **Steve Haliabalias** of 21 Wood Street, Kurralta Park wish to appear in support of the representation.

Applicant **Matthew King** of URPS on behalf of **A & R Satani** wishes to appear to respond to the representations.

### DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Land Division - Community Title; Create four (4) additional allotments and common property	Demolition of existing dwelling and ancillary structures and construction of a two-storey residential flat building comprising five (5) dwellings
APPLICANT	A & R Satani	ZYBECK Consulting
APPLICATION NO	211/905/2017	211/731/2017
LODGEMENT DATE	27 July 2017	21 June 2017
ZONE	Residential	Residential
POLICY AREA	Medium Density Policy Area 18	Medium Density Policy Area 18
APPLICATION TYPE	Merit	Merit
PUBLIC NOTIFICATION	Category 1	Category 2
REFERRALS	Internal <ul style="list-style-type: none"> <li>▪ Nil</li> </ul> External <ul style="list-style-type: none"> <li>▪ DAC</li> <li>▪ SA Water</li> </ul>	Internal <ul style="list-style-type: none"> <li>▪ City Assets</li> </ul> External <ul style="list-style-type: none"> <li>▪ Nil</li> </ul>
DEVELOPMENT PLAN VERSION	30 May 2017	30 May 2017
MEETING DATE	10 April 2018	10 April 2018

### RECOMMENDATION 1

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent, Land Division Consent and Development Approval for Application No. 211/905/2017 by A & R Satani to undertake Land Division - Community Title; Create four (4) additional allotments and common property at 16 Warwick Avenue, Kurralta Park (CT5550/941) subject to the following conditions of consent:

#### Development Plan Consent Conditions

1. That the development shall be undertaken and completed in accordance with the plans and information detailed in this application except where varied by any condition(s) listed below.

## Land Division Consent Conditions

### Council Requirements

Nil

### Development Assessment Commission Conditions

2. The financial requirements of the SA Water Corporation shall be met for the provision of water and sewerage services (SA Water H0061890).
3. Payment of \$27,320 into the Planning and Development fund (4 lots @ \$6,830/lot). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the State Planning Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person by cheque or card, at Level 5, 50 Flinders Street, Adelaide.
4. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

## RECOMMENDATION 2

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/731/2017 by ZYBECK Consulting to undertake demolition of existing dwelling and ancillary structures and construction of a two-storey residential flat building comprising five (5) dwellings at 16 Warwick Avenue, Kurralta Park (CT5550/941) subject to the following conditions of consent:

### Development Plan Consent Conditions

1. The development shall be undertaken and completed in accordance with the plans and information detailed in this application except where varied by any condition(s) listed below.
2. All stormwater design and construction will be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and for this purpose stormwater drainage will not at any time:
  - a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create unhealthy or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; and not flow across footpaths or public ways.
3. The nominated stormwater water quality device (SPEL Stormceptor Model 100/15) shall be maintained and cleaned in accordance with the manufacturer's best practice recommendations.
4. A minimum 5KL trafficable underground detention storage tank is to be provided for the site as annotated on the proposed the Civil & Drainage Plan (KP Squared Engineering, Project No: 180305, Drawing No.C2, Issue A).
5. The stormwater discharge from the detention tank to be limited to 6L/s at all times.



6. All driveways, parking and manoeuvring areas will be formed, surfaced with concrete, bitumen or paving, and be properly drained prior to occupation, and shall be maintained in reasonable condition at all times to the satisfaction of Council.
  7. All landscaping will be planted in accordance with the approved plan within three (3) months of the occupancy of the development. Any person(s) who have the benefit of this approval will cultivate, tend and nurture the landscaping and shall replace any plants which may become diseased or die.
  8. The upper level bedroom windows to the northern elevation of all dwellings shall include fixed obscure glazing to a minimum height of 1.7 metres from the upper floor level, and shall be maintained to the reasonable satisfaction of Council.
  9. The balustrades of all balconies shall comprise obscure glazing to a height of 1.7 metres from the upper floor level and shall be installed prior to occupation of the dwelling.
  10. All wall cladding, roofing materials and external building finishes and colours used on the dwellings shall be natural and non-reflective, and shall be maintained to the reasonable satisfaction of Council.
- 

## **BACKGROUND**

The development proposal is presented to the Council Assessment Panel (CAP) for the following reasons:

- With regard to residential development and land division applications, where all proposed allotments and or sites fail to meet, nor are within 5% of, the minimum frontage widths and site areas designated in respective zones and policy areas within the West Torrens Council Development Plan.
- All Category 2 or 3 applications where a representor has requested to be heard shall be assessed and determined by the CAP.

## **PREVIOUS AND RELATED APPLICATIONS**

### **DA 211/881/2015**

Combined and division and land use application community title - DAC No -211/C104/15 - create one (1) additional allotment and construction of a residential flat building containing two (2) dwellings and the construction of an attached carport associated with the existing dwelling.

- Refused by DAP 13 September 2016.

### **DA 211/368/2015**

Land division - Torrens Title DAC No.211/C104/15 - create one (1) additional allotment.

- Development Plan Consent granted 9 September 2016.

## SITE AND LOCALITY

The subject land is Allotment 14 Deposited Plan 1983 in the area named Kurralta Park, Hundred of Adelaide as contained in Certificate of Title Volume 5550, Folio 941. The land is more commonly known as 16 Warwick Avenue, Kurralta Park.

The subject land is located on the western side of Warwick Avenue and is a regularly-shaped allotment totalling 903 square metres. The site has a primary frontage of 18.28 metres and a depth of 49.3 metres. Topographically the site is relatively flat.

A single storey dwelling circa 1920s is currently on the land with an attached carport and later additions to the rear of the dwelling. A free-standing outbuilding is situated in the north-eastern corner of the allotment. The subject land has a good coverage of vegetation but it does not contain any Regulated or Significant trees, nor are there Regulated or Significant trees in proximity to the subject land. Vehicle access and egress is via a single-width driveway crossover to Warwick Avenue on the northern side boundary.

The subject land is located within the Residential Zone, and more particularly the Medium Density Policy Area 18. The land is located approximately 160 metres north of an Urban Corridor Zone, and approximately 260 metres from Anzac Highway and 470 metres from South Road.

Warwick Street currently comprises primarily single storey detached dwellings, with some instances of two storey dwellings within the locality. A number of newer buildings comprising detached dwellings and group dwellings are evident from more recent infill development. Overall the locality contains a diverse range of dwelling types, including residential flat buildings, with architectural styles ranging from conventional 1950s dwellings to recently constructed contemporary dwellings such as the residential flat building immediately west of the subject land.

The allotment pattern within the locality is considered mixed, as it contains some allotments that have wide frontages in the order of 18 to 19 metres, residential flat buildings with smaller allotments, battle-axe development and land that has been more recently subdivided resulting in narrower street frontages of approximately 10 to 11 metres.

The site and locality are shown on the following maps:







## Location Map WeTo/13

WEST TORRENS COUNCIL  
Consolidated - 30 May 2017



## PROPOSAL

The proposal consists of two development applications, being the land division and the land use (built form).

### Land Division

The land division proposes the creation of four additional allotments and common property in the form of a community title.

Allotment	Site Area	Frontage
1	150m <sup>2</sup>	11.4m
2	103m <sup>2</sup>	N/A
3	104m <sup>2</sup>	N/A
4	104m <sup>2</sup>	N/A
5	104m <sup>2</sup>	N/A
Common Property	338m <sup>2</sup>	10.29m

This results in an average site area per dwelling (less the common area) of 113m<sup>2</sup>.

### Land Use

The land use proposes demolition of the existing dwelling and associated structures, and the construction of a two storey residential flat building comprising five (5) dwellings. Each dwelling will contain:

#### Ground Floor:

- double garage
- bedroom with walk-in-robe and ensuite
- study corner
- laundry under stairs
- private open space.

#### First floor:

- two bedrooms
- bathroom and toilet
- open-plan kitchen
- dining and living area
- balcony with 1.7m high screening.

The proposal presents a single, uninterrupted building mass located on the northern side of the subject land. The design of the building includes rendered walls, aluminium windows and upper walls being a mix of render and linear cladding. Roofing will be flat Colorbond® Trimdeck sheeting. Coloured elevations are shown, but specific colours have not been specified.

The common property provides driveway access that extends along the majority of the southern side of the land. Limited areas of landscaping are proposed against boundaries, with larger planter beds located towards the front of the subject land between the proposed building and the road and at the far end of the common driveway area.

The dwellings are internally oriented on the subject land. A shared area is proposed at the rear of the subject land designated for domestic storage sheds for each dwelling, although it is noted that this is not marked as sub-lots on the proposed land division plan.

A copy of the proposed plan of division and the proposed dwelling development is contained in **Attachment 1**.

## PUBLIC NOTIFICATION

The land use application 211/731/2017 is a Category 2 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations, and more particularly the Procedural Matters of the Residential Zone in the West Torrens Council Development Plan.

<b>Properties notified:</b>	25 properties were notified during the public notification process.
<b>Representations:</b>	3 representations were received.
<b>Persons wishing to be heard:</b>	Three representors identified that they wish to address the Panel. <ul style="list-style-type: none"> <li>• R. R. Olbrycht &amp; A. P. Olbrycht</li> <li>• Y. Shan &amp; F. Gao</li> <li>• V. Haliabalias &amp; S. Haliabalias</li> </ul>
<b>Summary of Representations:</b>	Concerns were raised regarding the following matters: <ul style="list-style-type: none"> <li>• land use and density</li> <li>• landscaping</li> <li>• desired character</li> <li>• design and appearance</li> <li>• setbacks</li> <li>• private open space</li> <li>• privacy</li> <li>• overshadowing</li> <li>• site coverage</li> <li>• car parking</li> <li>• noise and air pollution</li> <li>• sewerage</li> <li>• stormwater management</li> <li>• storage</li> <li>• decreased land value</li> </ul>

The Applicant provided a comprehensive response to the representations. A summary of the response is as follows:

- Acknowledging a locality shift from low to medium density, the proposal of 55 dwellings per hectare achieves the desired medium density of PDC 4 of this Policy Area;
- The existing non-Regulated vegetation demonstrates no special environmental qualities, and just over 10% of the subject land will be landscaped;
- The proposal addresses the Desired Character with a two-storey height and with a strong presence to streets creating a highly varied streetscape;
- The building will have a contemporary appearance, be limited to two storeys, and use articulation resulting in a scale that is appropriate to the locality;
- The setbacks are mostly met, other than the balconies, but are acceptable in the context of the development envisaged in the policy area;
- Each dwelling is provided with an amount of private open space that exceeds the minimum 24 square metres;
- All upper level side and rear windows and balconies are screened to 1.7 metres above the upper floor level, other than a portion of the balcony of Dwelling 1 that faces the street and is front of the adjoining dwelling at 18 Warwick Avenue. The applicant is willing to accept a condition to change this if required;

- The setback from 14 Warwick Avenue (to the south) is such that there will be some overshadowing at 3pm on the winter solstice, and for the majority of the year as the sun will be higher in the sky;
- The total site coverage is 46.8%. The Development Plan allows up to 70%;
- The proposal has been amended by removing an independent driveway to Dwelling 1, maximising on-street parking in the absence of an on-site visitor parking space;
- The proposal is residential and anticipated within a Residential Zone and will not present unreasonable emissions or noise levels;
- The land division referral response from SA Water was not unsupportive of the proposal;
- Stormwater management has been engineered and detailed;
- Each dwelling is to be provided with a designated 8m<sup>3</sup> domestic storage area; and
- Land values are not contemplated by Development Plans, and are not a valid planning matter.

A copy of the representations and the Applicant's response is contained in **Attachment 2**.

## REFERRALS

### Internal

- City Assets

The land use application was referred to Council's City Assets Department for advice on finished floor levels, driveway access/verge interaction, vehicular manoeuvrability, waste management and stormwater management. Initial concerns raised have all been resolved by way of amendments to the design and the provision of amended plans, including a civil site and drainage plan with associated calculations. City Assets are now satisfied with the proposal subject to the inclusion of conditions outlined in the recommendation.

### External

Pursuant to Section 38 and Schedule 8 of the Development Act and Regulations, the application was referred to:

- State Commission Assessment Panel (SCAP)

SCAP raised no concerns with the proposal. Standard conditions of consent have been included in the recommendation.

- SA Water

SA Water raised no concerns with the proposal. The developer will be required to meet the requirements of SA Water for the provision of water and sewerage services. Standard conditions of consent have been included in the recommendation.

A full copy of the relevant advice and reports are contained in **Attachment 3**.

## ASSESSMENT

The subject land is located within the Residential Zone and more particularly Medium Density Policy Area 18 as described in the West Torrens Council Development Plan (Consolidated 30 May 2017). The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
<i>Crime Prevention</i>	<i>Objectives</i>	1
	<i>Principles of Development Control</i>	1, 2, 3, 6, 7 & 8
<i>Design and Appearance</i>	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 9, 10, 12, 13, 14, 15, 16, 21, 22 & 23
<i>Energy Efficiency</i>	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1, 2 & 3
<i>Land Division</i>	<i>Objectives</i>	1, 2, 3 & 4
	<i>Principles of Development Control</i>	1, 2, 4, 5, 6 & 8
<i>Landscaping, Fences and Walls</i>	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 6
<i>Medium and High Rise Development (3 or More Storeys)</i>	<i>Objectives</i>	1, 2 & 4
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 7, 12 & 13
<i>Orderly and Sustainable Development</i>	<i>Objectives</i>	1, 2, 3, 4 & 5
	<i>Principles of Development Control</i>	1 & 3
<i>Residential Development</i>	<i>Objectives</i>	1, 2, 3, 4 & 5
	<i>Principles of Development Control</i>	1, 4, 5, 7, 8, 9, 10, 11, 12, 13, 14, 16, 18, 19, 20, 21, 22, 28, 30, 31, 32 & 33
<i>Transportation and Access</i>	<i>Objectives</i>	2
	<i>Principles of Development Control</i>	1, 2, 8, 9, 11, 23, 24, 30, 34, 35, 36, 37 & 44



<b>Zone: Residential Zone</b>	
<b>Desired Character Statement:</b>	
<p><i>“This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.”</i></p>	
<b>Objectives</b>	1, 2, 3 & 4
<b>Principles of Development Control</b>	1, 5, 6, 7, 10, 11, 12, 13 & 14

<b>Policy Area: Medium Density Policy Area 18</b>	
<b>Desired Character Statement:</b>	
<p><i>“Allotments in this policy area will be at medium density, accommodating a range of dwelling types including residential flat buildings, row dwellings, group dwellings, semi-detached dwellings and some detached dwellings on small allotments. Allotment amalgamation to create larger development sites will occur to maximise the density of development while also achieving integrated design outcomes, particularly within a comfortable walking distance of centre zones. Vehicle access will occur from side streets and new rear public and private laneways wherever possible, also supporting the retention of existing street trees.</i></p> <p><i>New buildings will contribute to a highly varied streetscape. Buildings will be up to 3 storeys and provide a strong presence to streets, other than in the part of the policy area in Underdale, Ashford (other than allotments adjacent to Residential Character Ashford Policy Area 22) and allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park where buildings will be up to 4 storeys. Parking areas and garages will be located behind the front facade of buildings.</i></p> <p><i>Buildings on the edge of the policy area which adjoin residential policy areas at lower densities will pay particular attention to managing the interface with adjoining dwellings, especially in terms of the appearance of building height and bulk, and overshadowing.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.”</i></p>	
<b>Objectives</b>	1
<b>Principles of Development Control</b>	1, 4, 5, 6 & 8

**QUANTITATIVE ASSESSMENT**

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

<b>DEVELOPMENT PLAN PROVISIONS</b>	<b>STANDARD</b>	<b>ASSESSMENT</b>
SITE AREA <i>Medium Density Policy Area 18</i> <i>PDC 8</i>	250m <sup>2</sup> (min.)	113m <sup>2</sup> (average, not including common areas)  <b>Does not satisfy</b>
SITE FRONTAGE <i>Medium Density Policy Area 18</i> <i>PDC 6</i>	9m (min.)	18.29m  <b>Satisfies</b>
SITE COVERAGE <i>Medium Density Policy Area 18</i> <i>PDC 5</i>	70% (max.)	46.8%  <b>Satisfies</b>
PRIMARY STREET SETBACK <i>Medium Density Policy Area 18</i> <i>PDC 5</i>	3m (min.)	3m  <b>Satisfies</b>
SIDE SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	Vertical wall 3m or less 1m (min.) Vertical wall 3-6m 2m (min.)	Dwellings 1-5 - 1m Dwellings 1-5 - 1m  <b>Does not satisfy</b>
REAR SETBACK <i>Medium Density Policy Area 18</i> <i>PDC 5</i>	4m (min.)	4m setback  <b>Satisfies</b>
BUILDING HEIGHT <i>Medium Density Policy Area 18</i> <i>PDC 5</i>	3 storeys or 12.5m (max.)	2 storeys (6.8m)  <b>Satisfies</b>
PRIVATE OPEN SPACE <i>Medium Density Policy Area 18</i> <i>PDC 7</i>	24m <sup>2</sup> with a minimum dimension of 3m	Dwelling 1 - 35.5m <sup>2</sup> Dwelling 2 - 28.4m <sup>2</sup> Dwelling 3 - 28.5m <sup>2</sup> Dwelling 4 - 28.5 m <sup>2</sup> Dwelling 5 - 28.6m <sup>2</sup>  <b>Satisfies</b>

<b>LANDSCAPING</b> <i>Module: Landscaping, Fences &amp; Walls</i> <b>PDC 4</b>	A minimum of 10 per cent of a development site	10.2%  <b>Satisfies</b>
<b>CARPARKING SPACES</b> <i>Module: Transportation and Access</i> <b>PDC 34</b> <i>Table WeTo/2</i>	2 car parking spaces per dwelling, one of which is covered plus an additional 0.25 car parking spaces per dwelling.  11.25 required	10 spaces provided (No visitor parking)  <b>Does not satisfy</b>

## QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the most relevant considerations are discussed under the following sub headings:

### Land Division

The proposed community title land division creates five community lots consistent with the layout of the five proposed dwellings together with a common property driveway along the southern side of the subject land with a section at the rear for landscaping and domestic storage purposes.

Principle of Development Control (PDC) 8 of Medium Density Policy Area 18 of the Residential Zone prescribes that land division should create allotments greater than 250m<sup>2</sup>, unless where combined with an application for dwellings. On this basis and when considered in isolation, the land division demonstrates a significant shortfall in site areas. The frontages are compliant however.

In contrast, PDC 6 of the same policy area seeks a minimum 'average' site area of 150m<sup>2</sup> for dwellings within a residential flat building. The density of the proposed development achieves an average site area per dwelling of 113m<sup>2</sup> - an average shortfall per dwelling of 37m<sup>2</sup>.

The intent of minimum allotment sizes is to achieve a residential density that is consistent with the desired character for the area. The Desired Character for Medium Density Policy Area 18 is for allotments to be at medium densities. On the basis of the land having a total area of 903.16m<sup>2</sup>, the 'net' residential density of the development has been calculated at 55 dwellings per hectare. As defined by the *Planning Strategy for Metropolitan Adelaide (30 Year Plan for Greater Adelaide - 2017 Update)*, the proposed development is of 'medium' density.

Notwithstanding the allotment size shortfall it has been demonstrated that the allotments are large enough for the proposed dwellings to reasonably satisfy the relevant quantitative requirements relating to building height/scale, private open space, site coverage and vehicle access and manoeuvrability.

### Dwelling density and local character

The Desired Character Statement for the Residential Zone envisages residential flat buildings being common near centres and in policy areas where the desired density is higher. As previously discussed the proposed development has an average dwelling site area less than the desired minimum average site area provision. This average has been calculated by removing all common areas. It is noted the average site area including the common areas is 180.6m<sup>2</sup>.

There are a number of similar sized allotments that accommodate a similar form of medium density development to that proposed. The following examples are all within 150 metres of the subject site:

<b>Address</b>	<b>Average site area (m<sup>2</sup>)</b>	<b>Net density (dwellings/hectare)</b>
15 Warwick Avenue	58	89
19 Warwick Avenue	147	47
25 Warwick Avenue	154	38
27 Warwick Avenue	125	48
23 Wood Street	115	58
27 Mortimer Street	112	48
42 Mortimer Street	35	101
44 Mortimer Street	53	100
10 Gordon Street	67	94

The subject land is within 300 metres of a shopping centre that fronts Anzac Highway which is also adjacent to the Urban Corridor Zone and high frequency public transport (bus and tram) routes. This places it within a locality identified by the Development Plan as being appropriate for medium density development.

The shortfall in average site area is not considered detrimental to the proposed building's relationship to adjoining properties, on-site parking and vehicle manoeuvrability or enjoyment of the land.

From a streetscape perspective, the dwellings do not appear cramped or visually overbearing due to a high level of articulation coupled with a modest building height. Whilst the allotments for the dwellings are less than the desired 150m<sup>2</sup>, this shortfall would not easily be perceivable as the allotments are positioned and oriented internally. Similarly, there would be no material consequences for the development pattern as significant infill development has already occurred throughout the locality (as previously noted), with several recent instances of two storey group dwellings and residential flat buildings on relatively small battle-axe allotments.

The overall dwelling density and allotment layout of the proposal is considered to be compatible with the existing and desired built form characteristics of the locality.

## **Setbacks**

### ***Front and rear setbacks***

The front setback for dwellings in the Medium Density Policy Area 18 should be a minimum of 3 metres. The proposal contains a staggered setback that ranges from 2.9 metres to 3.19 metres. This is considered satisfactory as the building has an average setback of over 3 metres, is well-articulated and features a landscaped area in front of the building.

The rear setback for dwellings in the Medium Density Policy Area 19 should be at least 4 metres. The proposed development meets this requirement.



**Side setbacks**

The design of the proposed development has ground level portions of the building and the first floor balconies located 1 metre from the northern boundary of the subject land. This does not satisfy the Development Plan requirement for a 2 metre offset for walls 3 to 6 metres in height, although the majority of the rear wall is set back 3 metres. Notwithstanding this, the setback in this instance is considered reasonable for the following reasons:

- The adjoining properties to the north have a carport on the boundary and a rear yard, meaning no shadow would be cast over these properties;
- The locality contains dwellings and outbuildings located in close proximity to side and rear boundaries;
- The ground level of the proposed building has a modest wall height of only 2.72 metres and a roof that is not pitched; and
- There are adequate private open space areas within the rear yards for landscaping.

**Design and Appearance**

The proposed development will sit amongst a variety of dwelling types, sizes and styles, including single storey detached dwellings, group dwellings and residential flat buildings within the locality.

The façade that fronts Warwick Avenue features a variety of materials and includes varying dimensions to avoid extensive areas of uninterrupted walling. The design also incorporates a considerable amount of articulation.

The façade includes windows to both the ground and upper floors that facilitate passive surveillance of the adjacent public space. The dwelling entry points are readily identifiable and accessible from public street frontages and vehicle parking areas.

The southern and northern elevations of the building have some element of repetition, however this will not be apparent from the street. The walls are also recessed at certain points to break up the building's mass and proportions. The design also uses a number of different materials, shapes and colour variation that add visual interest without looking haphazard.

The proposed building incorporates a contemporary design that is compatible with the locality in respect to:

- building height;
- built form bulk and scale;
- front and side setbacks;
- façade articulation and detailing; and
- presentation of windows and a front door to Warwick Avenue.

In terms of dwelling design, the internal living rooms have external outlooks to private space, ample access to sunlight (given the northern orientation) and internal floor areas that provide quality living environments. The staggered and varied building lines are considered to minimise impacts to adjoining allotments, particularly in terms of bulk and scale.

The desired character and PDC 5 of the Medium Density Policy Area 18 envisages building heights up to three storeys, or 12.5 metres. The proposal is for a two storey building with a maximum height of 6.8 metres. It is noted that the locality contains many single storey buildings, however it is also important to acknowledge that this policy area is seeking a transition to medium density built forms of up to three storeys. The proposed building height is therefore considered appropriate.

Overall, the design and appearance of the proposed development is considered to adequately address the relevant provisions of the Design and Appearance module, including Objective 1, PDCs 1, 2 and 5.

## Car Parking

The proposal includes a double garage for each dwelling, totalling ten car parking spaces. PDC 34 of the Transportation and Access module, and *Table WeTo/2* of the Development Plan seek 0.25 of a parking space per dwelling for visitors. This would result in 1 visitor space being made available on the site. The proposal does not accommodate any visitor parking. At Council's suggestion the applicant amended the proposal by deleting independent driveway access to Dwelling 1, resulting in 11 metres of upright kerbing in front of the site. This area can accommodate one or possibly two on-street parking spaces, which is considered to be a reasonable compromise.

The lack of an on-site visitor parking space is not considered to impact on traffic flows or safe and efficient movements to and from the site. Furthermore the subject land is within 300 metres of high-frequency public transport routes on Anzac Highway.

## Landscaping

The proposal meets the minimum landscaping requirement of 10% of the site with a variety of planting to the front and rear of the subject land, and with limited planting along the side of the common area and to each of the private open space areas. PDC 4 of the Landscaping, Fences and Walls module is therefore satisfied.

Landscaping along the southern side of the driveway is a nominal strip of 300mm which allows appropriate access and on-site manoeuvrability to be achieved. Planting within this strip is proposed to provide clumping, strappy plantings that are low-maintenance and will provide effective softening of the fence line and adjoining driveway area.

The landscaping beds proposed at the front and rear of the site will be large enough to facilitate deeper-rooting plants and trees as specified in the proposed landscaping schedule, and will assist in providing areas for drainage. The functionality of landscaping to maximise shade and shelter, reduce visual impact and the like as sought in PDC 1 of this module is considered reasonably met given the constraints of the site.

## Private Open Space

General Section - Residential Development PDC 19 outlines minimum private open space provisions, being 24m<sup>2</sup> for site areas under 300m<sup>2</sup>. As detailed above, Dwelling 1 will have approximately 35m<sup>2</sup> and Dwellings 2 to 5 will have approximately 28m<sup>2</sup> of private open space. The ground level courtyards and upper level balconies of all dwellings achieve the minimum dimension guidelines and are therefore considered acceptable.

## Overlooking

Visual privacy is dealt with in PDC 27 of the Residential Development module, and PDC 10 of the Design and Appearance module of the Development Plan. The development has been designed having regard to this provision with the majority of upper storey windows having window sill heights of 1.7 metres and/or fixed obscure glazing to this height. The upper level windows of Dwelling 1 do not include these treatments due to the proposed orientation, which is considered reasonable in this instance as it will facilitate passive surveillance of the street but avoid overlooking into habitable room windows or private open space of adjoining land.

## Overshadowing

The proposed residential flat building is positioned to the north of the subject land, and is offset 6 metres from the southern boundary. There will naturally be some overshadowing onto adjoining land in winter, however the majority will be over the common driveway area. The extent of overshadowing is considered reasonable given the design of the building being limited in height to two storeys, and the setback to the southern boundary of 6 metres. Adjoining properties will still receive adequate access to more than 3 hours of sunlight, thereby satisfying PDCs 11 and 12 of the Residential Development module.

**Stormwater management**

The civil plan has been considered by Council's engineers as being suitable, with appropriate finished floor levels and drainage systems that can adequately cater for and manage stormwater runoff for the development. PDCs 9, 10 and 11 of the Natural Resources module are therefore considered satisfied.

**Waste collection**

The proposed development can adequately cater for on-street waste collection with sufficient verge space available, and therefore a private waste contractor is not required. This meets Council's requirements and complies with Objective 2 - Waste module of the General Section of the Development Plan.

**SUMMARY**

When balanced against the existing site and locality characteristics and the Desired Character for Medium Density Policy Area 18, the proposed division of land and associated residential development is considered to be a desirable, orderly and appropriate form of development.

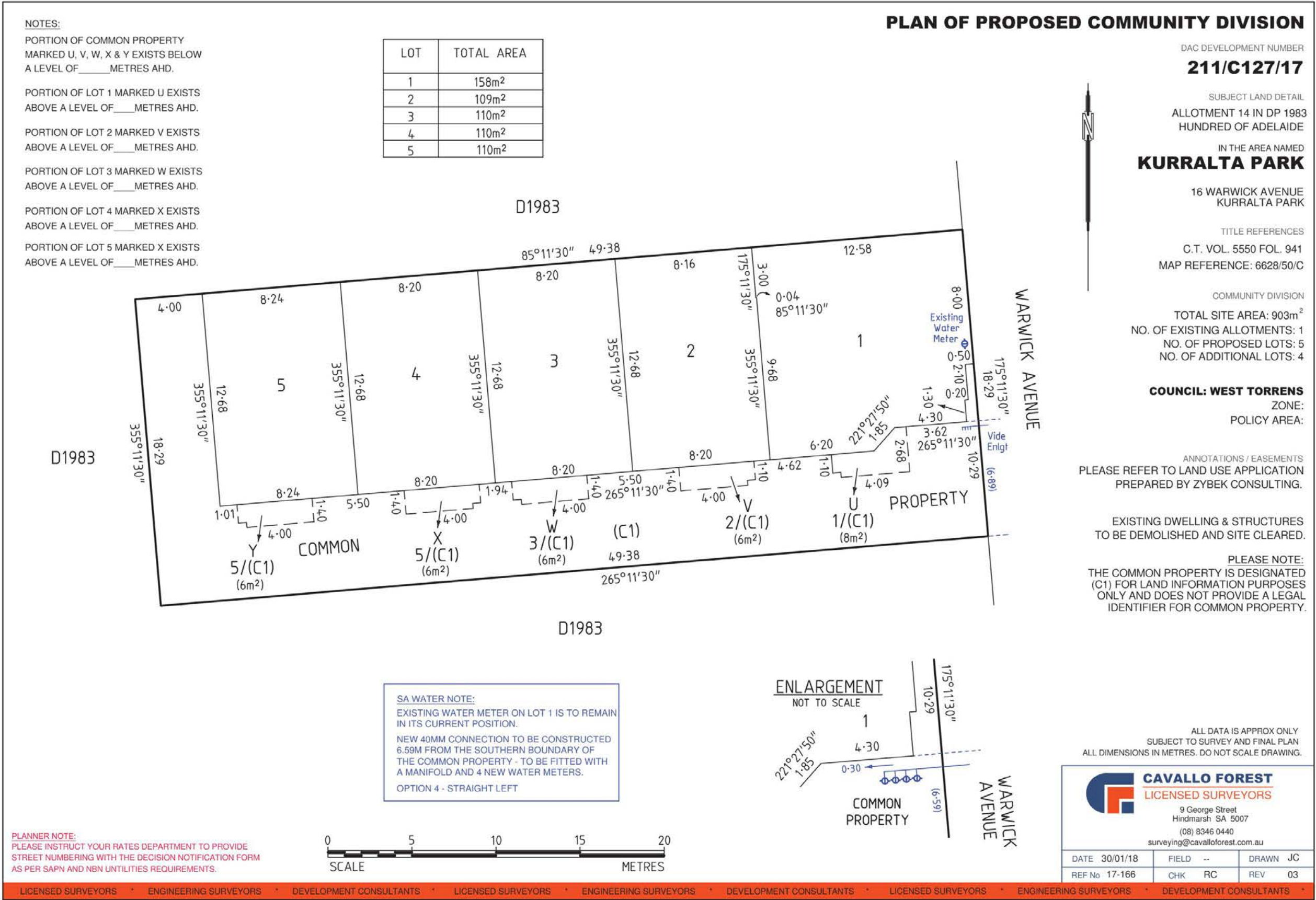
The dwelling density and allotment layout of the proposal sufficiently accords with the Desired Character and is compatible with the established pattern and built form characteristics of the locality. With the exception of quantitative shortfalls in site areas, side setbacks and a single visitor car parking space, the proposal satisfies the relevant quantitative provisions of the Development Plan.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 30 May 2017 and warrants Development Plan Consent.

**Attachments**

1. **Proposed plan set and supporting statement**
2. **Representations and Response**
3. **Referral responses**



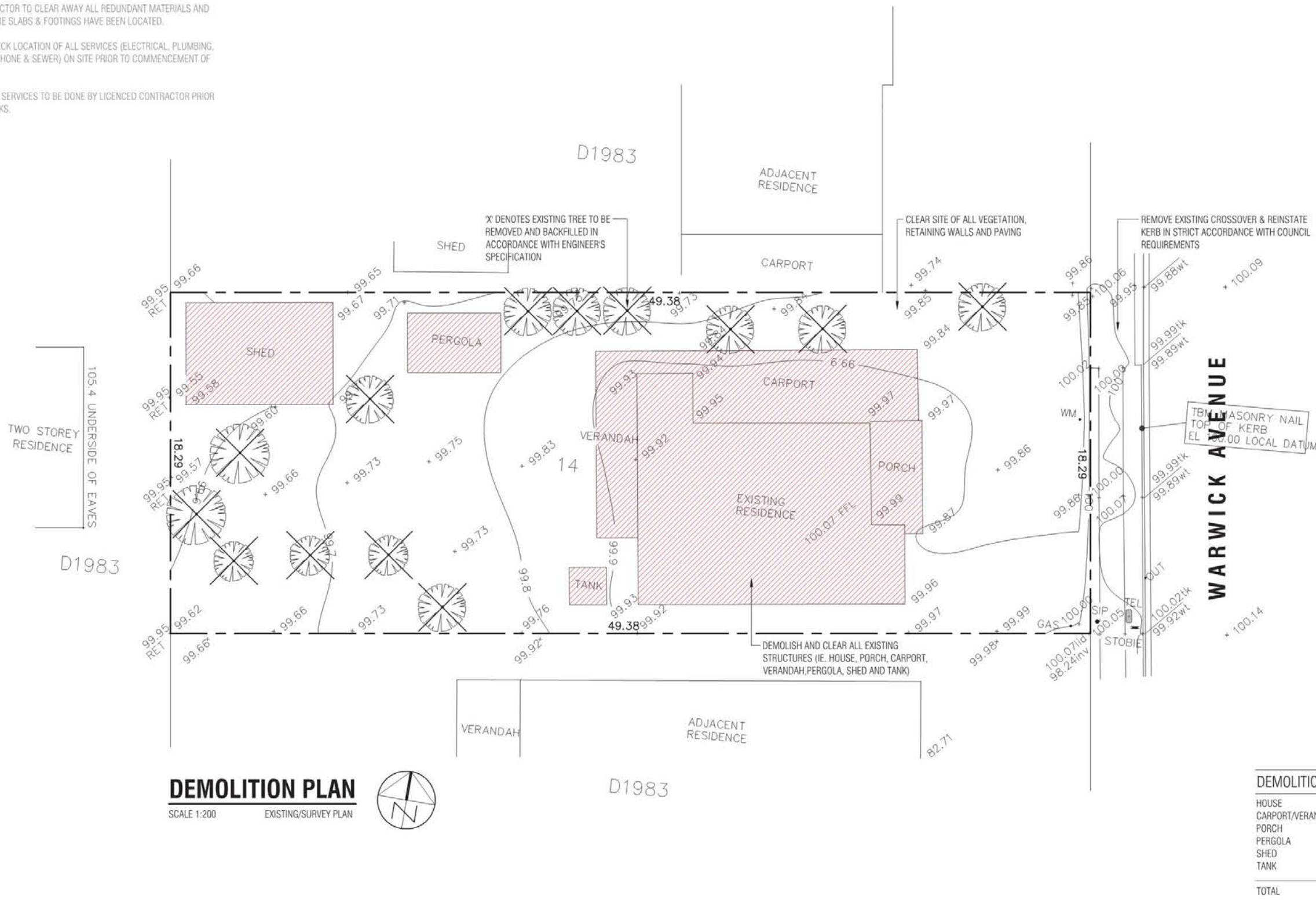


DEMOLITION NOTE:

DEMOLITION CONTRACTOR TO CLEAR AWAY ALL REDUNDANT MATERIALS AND LEVEL GROUND WHERE SLABS & FOOTINGS HAVE BEEN LOCATED.

CONTRACTOR TO CHECK LOCATION OF ALL SERVICES (ELECTRICAL, PLUMBING, STORMWATER, TELEPHONE & SEWER) ON SITE PRIOR TO COMMENCEMENT OF WORK.

ALL TERMINATION OF SERVICES TO BE DONE BY LICENCED CONTRACTOR PRIOR TO DEMOLITION WORKS.



RWT NOTE:

1000 LTS (MIN) RWT COLLECTING 50M<sup>2</sup> ROOF CATCHMENT AREA. PLUMBED TO EITHER TOILET, HOT WATER SYSTEM OR ALL LAUNDRY COLD WATER OUTLETS. RWT MUST BE FITTED WITH OVERFLOW DEVICE. INLET & OVERFLOW MUST BE FITTED WITH MOSQUITO-PROOF SCREENS. MUST COMPLY WITH BCA REQUIREMENTS.

BOUNDARY/SURVEY NOTE:

THIS DRAWING IS INTENDED FOR INDICATIVE BUILDING SETOUT PURPOSES ONLY. REFER TO CIVIL ENGINEER / SURVEYOR DRAWINGS FOR SITE LEVELS, CONTOURS, BENCH MARKS, SERVICE LOCATIONS, & EARTHWORK DESIGN. FINAL BOUNDARY & BUILDING SETOUT SHALL BE CONFIRMED & CERTIFIED BY LICENSED SURVEYOR PRIOR TO ANY CONSTRUCTION.

BRUSH FENCE NOTE:

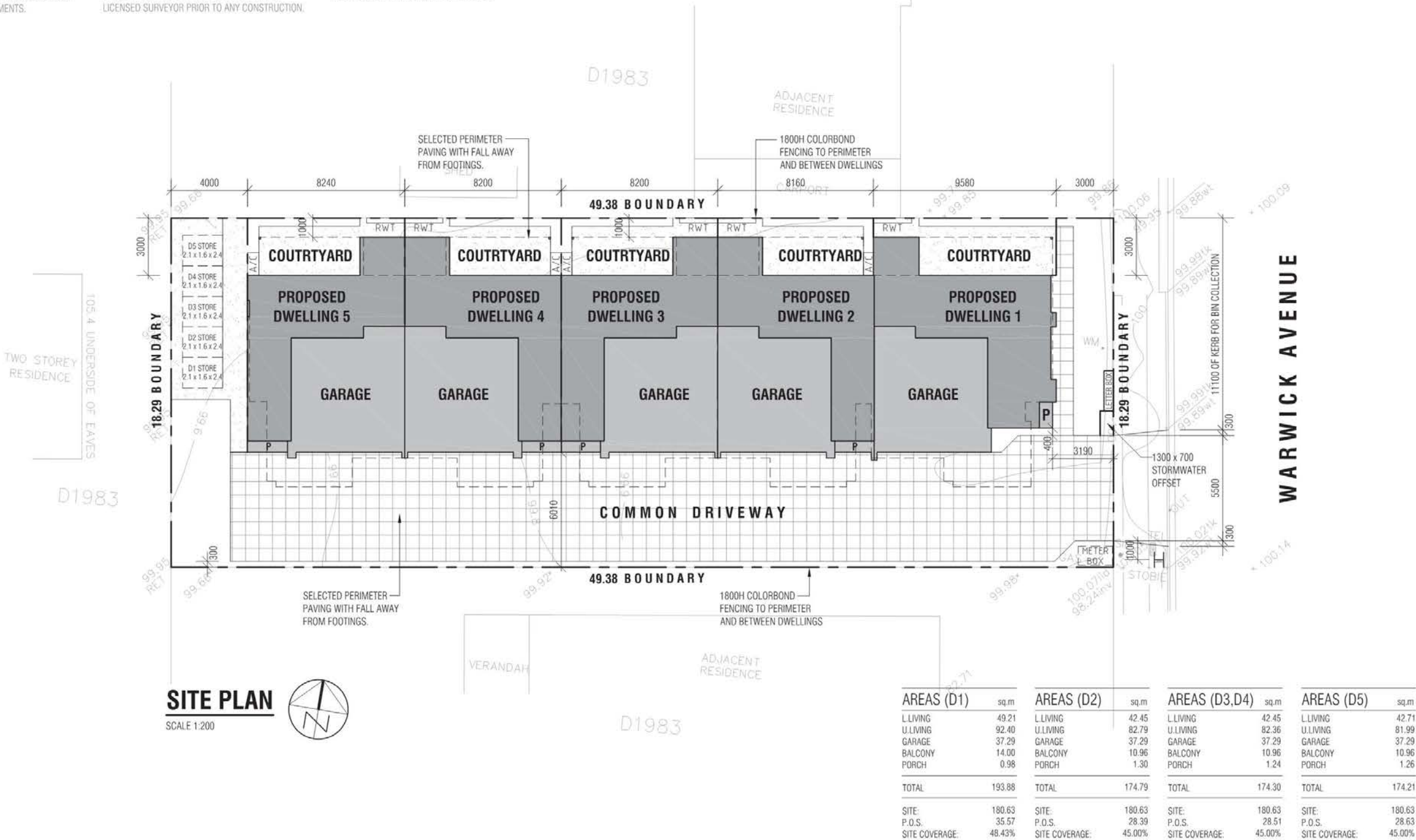
THERE WILL NOT BE ANY BRUSH FENCES WITHIN 3MTRS OF THE PROPOSED BUILDING WORKS. ANY BRUSH FENCES WITHIN 3 METRES OF THE DWELLING ARE TO BE REMOVED (BY OWNER) & REPLACED WITH NON-COMBUSTIBLE MATERIAL. MUST COMPLY WITH BCA REQUIREMENTS.

STORMWATER NOTE:

REFER TO ENGINEER'S DRAINAGE PLAN FOR ALL LEVEL, RETAINING WALLS & STORMWATER DRAINAGE DESIGN.

PARTY WALL NOTE:

COMMON STRUCTURAL PARTY WALL BETWEEN ALL DWELLINGS. PARTY WALL IS A SHARED WALL WITH RECIPROCAL RIGHTS.





LANDSCAPING SCHEDULE

GROUND COVERS



**DIANELLA PRUNINA 'RAINBOW TWIST'**  
A strappy leaved plant with reddish purple leaf edges that twist to expose both sides of the leaf. Very clean foliage with blue flowers followed by a blue berry.



**FICUS PRUNILA 'CREEPING FIG'**  
A vigorous, climbing fig attaches to surfaces by means of aerial roots. Has crinkly, heart-shaped juvenile leaves on a tracery of fine stems which adhere closely to its support. When the plant matures it starts to produce large, leathery adult foliage on horizontal, woody branches. Mature plants also produce yellowish-green, inedible figs.



**LOBELIA ERINUS 'LOBELIA TRAILING'**  
Abundant intense blue or purple flowers, tidy growth habit and graceful cascading stems.



**LOMANDRA 'LIME JET'**  
Lime Jet is a lime green variegated selection of Lomandra longifolia. It provides an exceptional lime contrast when planted in front of dark green backgrounds. It is especially useful to brighten dark or shaded vistas.



**MYOPORUM PARVIFOLIUM 'MYOPORUM'**  
Myoporum is very drought and frost tolerant, and has been very successful in a wide variety of soils. It is a longer lived form with crisp clean foliage. Its thick and woody stems help it to survive longer, these stems are covered by beautiful clean foliage and white flowers.



**THYMUS LONGICAULIS 'MEDITERRANEAN CREEPING THYME'**  
This is a low mat-forming species, with dark green needle-like leaves and clusters of bright-pink flowers during summer. A good choice for hot and sunny sites. Drought tolerant once established. Evergreen.



**CONVOLVULUS MAURITANICUS 'CONVOLVULUS'**  
A small, hardy shrub which provides year round interest with its silver foliage. During the warm summer months there is the added bonus of a profusion of large white, funnel-shaped flowers typical of this plant group.



**DIETES IRIDIOIDES 'DIETES'**  
A clump-forming, rhizomatous perennial from South Africa. It has dark green, strappy foliage, which grows in fan-like rosettes. In spring it produces white, yellow and mauve, iris-like flowers on tall stems. The flowers are followed by 3-celled capsules containing numerous seeds.




**MURRAYA PANICULATA 'MURRAYA HEDGE'**  
These plants are shrubs or trees. The leaves are pinnate, divided into several leaflets, and alternately arranged on the branches. The leaves are glandular, aromatic, and leathery to membranous in texture.




**NANDINA DOMESTICA 'NANDINA'**  
Medium sized dwarf Nandina that produces red new growth in spring and autumn. In winter months, 'Blush™' Nandina turns vivid red all over. It is 20% smaller than Nandina domestica 'Nana', a perfect height for fences, borders or hedging.


TREES



**PRUNUS CERASIFERA 'NIGRA'**  
A small, ornamental flowering plum with very dark purple summer foliage carried on dark branches. Single pink flowers in spring make a spectacular display. Often used as a striking small foliage contrast tree for smaller gardens or as a street tree.
















**PYRUS USSURIENSIS - 'MANCHURIAN PEAR'**  
A well known medium-sized tree that flowers very early in spring and has bright autumn leaf colour that is held well on the branches. Pyrus ussuriensis is suitable for many landscapes, including parks and large gardens.



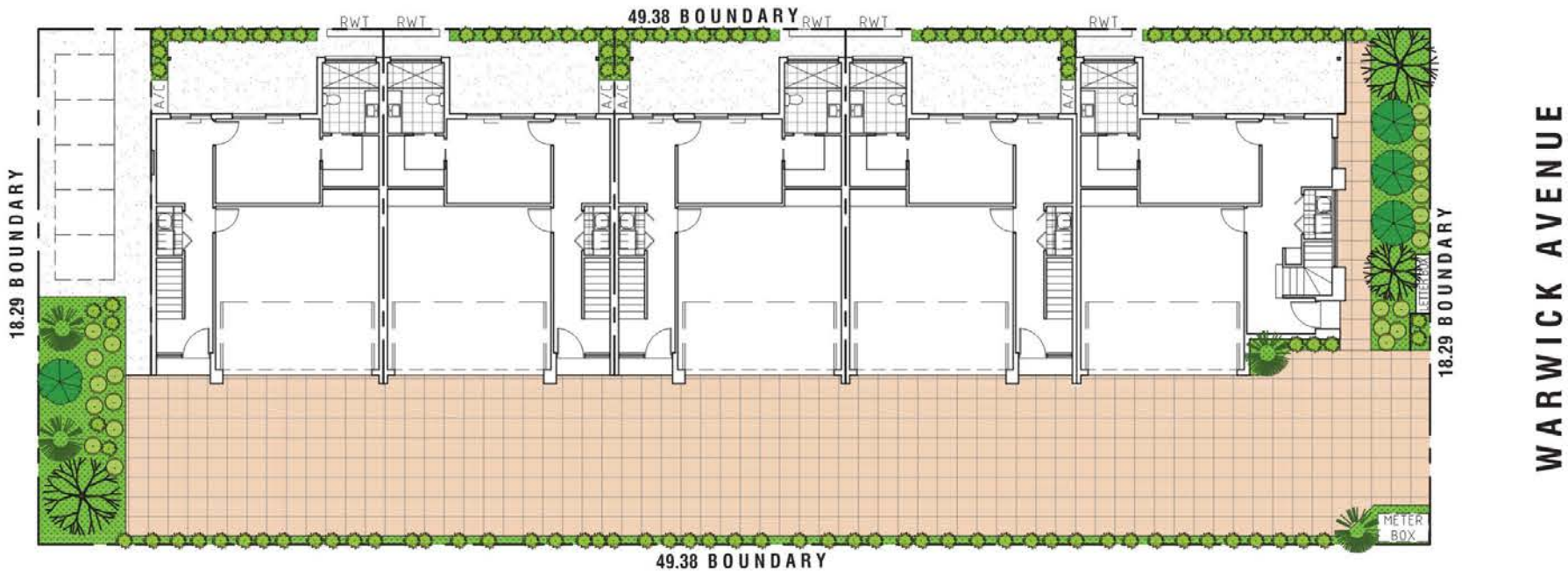
**MORUS - 'PENDULA' (WEeping MULBERRY)**  
A weeping mulberry with long pendulous branches sweeping to the ground that are cloaked with large, glossy leaves. This is a tough small 'special effect' tree suited to park and garden planting and used with good result in shopping malls and avenue plantings.

LANDSCAPING LEGEND:

GROUND COVERS		MATURE HEIGHT/SPREAD (m)	PLANTS	MATURE HEIGHT (m)	MATURE SPREAD (m)	PLANTED HEIGHT (m)
	RAINBOW TWIST Dianella Prunina	SIZE VARIES BETWEEN 50-100mm HIGH WITH A SPREAD OF 1-2 METRES	 CONVOLVULUS Convolvulus Mauritanicus	0.4	0.6	0.2
	CREEPING FIG Ficus Prunila		 DIETES Diets Indifoides	0.5-0.7	0.5-0.7	0.2-0.3
	LOBELIA TRAILING Lobelia Erinus		 MURRAYA HEDGE Murraya Paniculata	1.0-2.5	0.8-1.2	0.2-0.5
	LIME JET Lomandra		 NANDINA Nandina Domestica	0.6-0.7	0.6-0.7	0.2-0.3
	MYOPORUM Myoporum Parvifolium		<b>TREES</b>			
	MEDITERRANEAN CREEPING THYME Thymus Longicaulis	 NIGRA Prunus Cerasifera	5.0	4.0	1.2	
		 MANCHURIAN PEAR Pyrus Ussuriensis	9.0	7.0	1.8	
		 WEEPING MULBERRY Morus Nigra	3.0	4.0	0.9	

ALL SPECIES ARE TO BE CONFIRMED BY OWNER PRIOR TO COMMENCEMENT OF LANDSCAPING. ALL NEW LANDSCAPED AREAS TO BE SERVICED BY POLYPIPE SPRAYERS & DRIPPERS

ALL SPECIES ARE TO BE CONFIRMED BY OWNER PRIOR TO COMMENCEMENT OF LANDSCAPING. ALL NEW LANDSCAPED AREAS TO BE SERVICED BY POLYPIPE SPRAYERS & DRIPPERS



LANDSCAPE PLAN

SCALE 1:200



Contractors to verify all dimensions prior to commencement of any building work. Figured dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the designer immediately. These drawings are the exclusive property of Verrocchi Building Design. Any reproduction without written authority is prohibited. Copyright ©	rev	date	description	notes		project: WARWICK AVE DEVELOPMENT	date: NOV 2017	drawn by: LD
	A	16/11	CONCEPT DRAWINGS					
	B	6/12	AMENDED DRIVEWAY TO SUIT COUNCIL'S DRIVEWAY SETUP					
	C	16/12	PLANNING DRAWINGS					
PLANNING DRAWINGS					128a Mooringe Ave North Plympton 5037 m: 0407 820 946 e: dino.verrocchi@gmail.com	at: 16 WARWICK AVENUE KURRALTA PARK SA 5037	scale: 1:200 project no: client: ZYBEK CONSULTING	drawing no: 03 sheet size: ZYB.073 A3

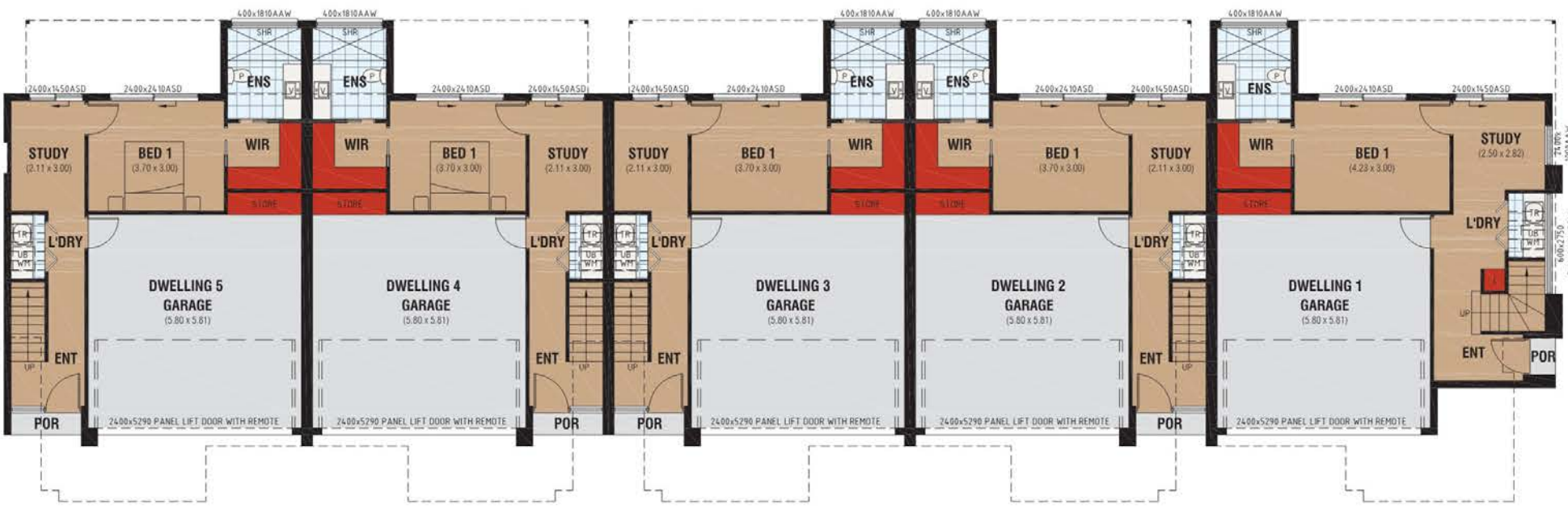


LEGEND:

- S STAINLESS STEEL SINK
- HP HOT PLATE
- UBO UNDER BENCH OVEN
- WD WALL OVEN
- RH RANGE HOOD
- DW DISHWASHER
- F REFRIGERATOR
- FR FREEZER
- PTRY PANTRY
- SHR SHOWER
- B BATH
- FSB FREESTANDING BATH
- L LINEN
- WM WASHING MACHINE
- UBWM UNDER BENCH WASHING MACHINE
- UBDO UNDER BENCH DRYER
- TR TROUGH
- LC LAUNDRY CHUTE
- P TOILET PAN
- V VANITY
- RCH BROOM CUPBOARD
- ST STORAGE
- HA ROOF ACCESS (600x600)
- SL SKY-LIGHT
- SB SWITCH BOARD
- DP DOWNPIPE
- DPS DOWNPIPE WITH SPREADER
- RH RAIN HEAD
- RWT RAIN WATER TANK
- SA SMOKE ALARM HARD WIRED IN ACCORDANCE WITH AS3786 AND TO BE INTERCONNECTED WHEN TWO OR MORE SMOKE ALARMS ARE REQUIRED
- EF EXHAUST FAN - CEILING MOUNTED DUCTED TO OUTSIDE AIR, CONNECTED TO LIGHT SWITCH OPERATION
- CS CONTROL JOINTS AS PER ENGINEERS REPORT

PARTY WALL NOTE:

COMMON STRUCTURAL PARTY WALL BETWEEN ALL DWELLINGS. PARTY WALL IS A SHARED WALL WITH RECIPROCAL RIGHTS.



GROUND FLOOR PLAN

SCALE 1:100



FIRST FLOOR PLAN

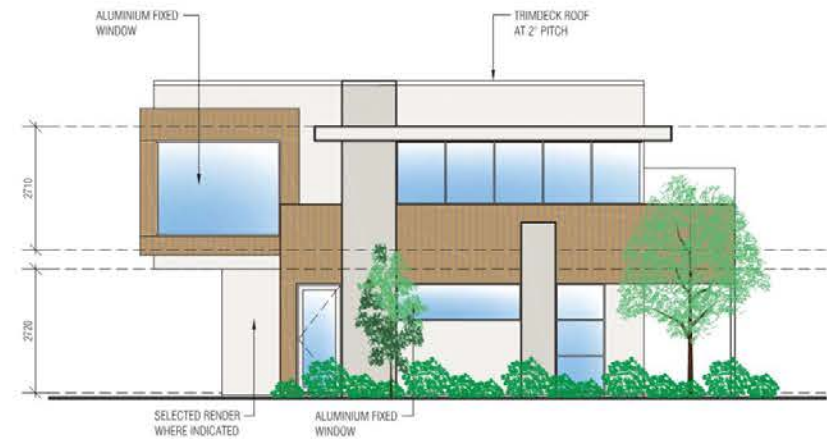
SCALE 1:100



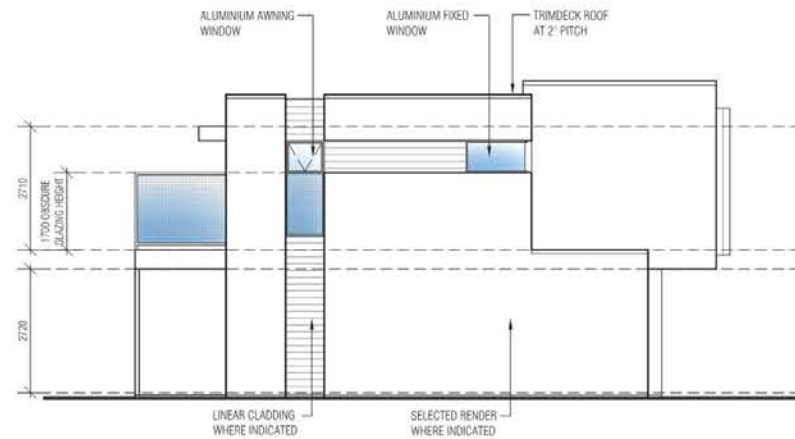
AREAS (D1)		sqm
LIVING		49.21
U/LIVING		92.40
GARAGE		37.29
BALCONY		14.00
PORCH		0.98
TOTAL		193.88
SITE		180.63
P.O.S.		35.57
SITE COVERAGE		48.43%
AREAS (D2)		sqm
LIVING		42.45
U/LIVING		82.79
GARAGE		37.29
BALCONY		10.96
PORCH		1.30
TOTAL		174.79
SITE		180.63
P.O.S.		28.39
SITE COVERAGE		45.00%
AREAS (D3,D4)		sqm
LIVING		42.45
U/LIVING		82.36
GARAGE		37.29
BALCONY		10.96
PORCH		1.24
TOTAL		174.30
SITE		180.63
P.O.S.		28.31
SITE COVERAGE		45.00%
AREAS (D5)		sqm
LIVING		42.71
U/LIVING		81.79
GARAGE		37.29
BALCONY		10.96
PORCH		1.26
TOTAL		174.21
SITE		180.63
P.O.S.		28.63
SITE COVERAGE		45.00%

Contractors to verify all dimensions prior to commencement of any building work. Figures dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the designer immediately. These drawings are the exclusive property of Verrocchi Building Design. Any reproduction without written authority is prohibited. Copyright ©		REV	DATE	DESCRIPTION	NOTES	verrocchi <b>buildingdesign</b> 128a Moorings Ave North Plympton 5037 m 0407 820 946 e dino.verrocchi@gmail.com	project: WARWICK AVE DEVELOPMENT at: 16 WARWICK AVENUE KURRALTA PARK SA 5037 client: ZYBEK CONSULTING	date: NOV 2017 scale: 1:100 drawing no: 04 project no: ZYB.073 sheet size: A2
PLANNING DRAWINGS		A	16/11	CONCEPT DRAWINGS				
		B	6/12	AMENDED DRIVEWAY TO SUIT COUNCIL'S DRIVEWAY SETUP				
		C	16/12	PLANNING DRAWINGS				

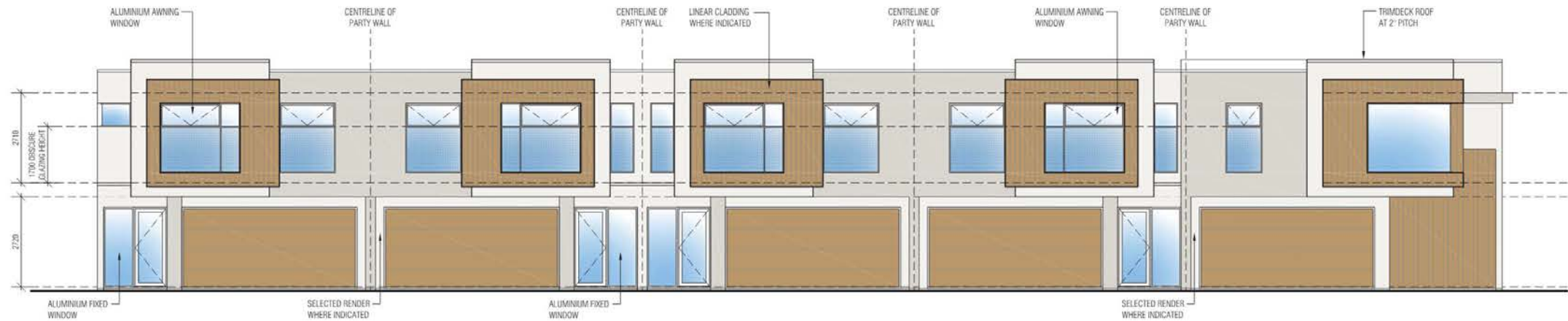




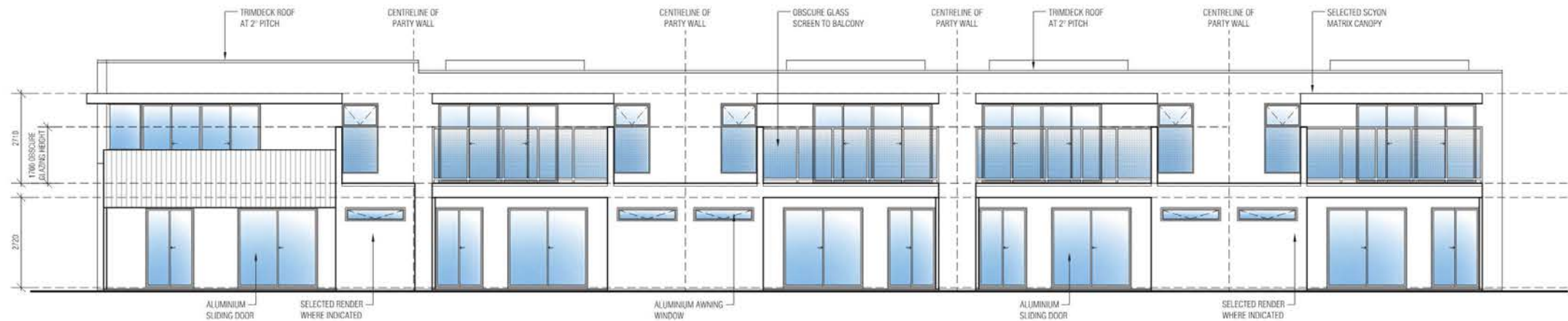
EAST ELEVATION (WARWICK AVE)  
SCALE 1:100



WEST ELEVATION  
SCALE 1:100



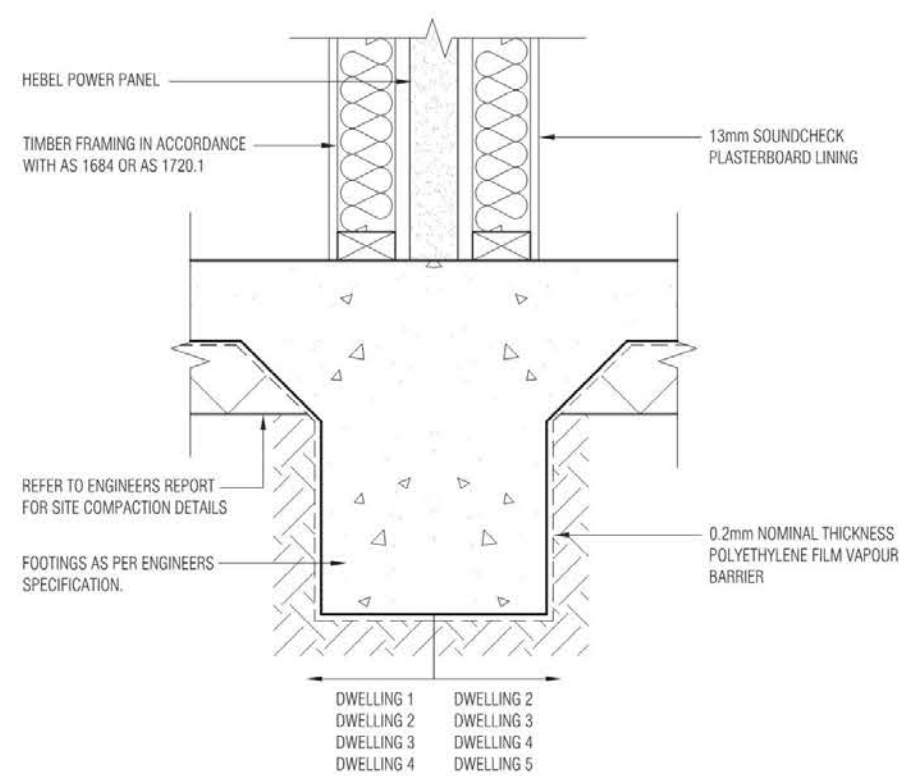
SOUTH ELEVATION (COMMON DRIVEWAY)  
SCALE 1:100



NORTH ELEVATION  
SCALE 1:100

Contractors to verify all dimensions prior to commencement of any building work. Figures dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the designer immediately. These drawings are the exclusive property of Verrocchi Building Design. Any reproduction without written authority is prohibited. Copyright ©	REV	DATE	DESCRIPTION	NOTES
	A	16/11	CONCEPT DRAWINGS	
	B	6/12	AMENDED DRIVEWAY TO SUIT COUNCIL'S DRIVEWAY SETUP	
	C	16/12	PLANNING DRAWINGS	
PLANNING DRAWINGS				
<div>verrocchi<b>buildingdesign</b></div> <div>128a Moorings Ave North Plympton 5037 m 0407 820 946 e dino.verrocchi@gmail.com</div> <div>project: WARWICK AVE DEVELOPMENT at: 16 WARWICK AVENUE KURRALTA PARK SA 5037 client: ZYBEK CONSULTING</div> <div>date: NOV 2017 scale: 1:100 project no: ZYB.073 drawing no: 05 sheet size: A2</div> <div>drawn by: LD</div>				

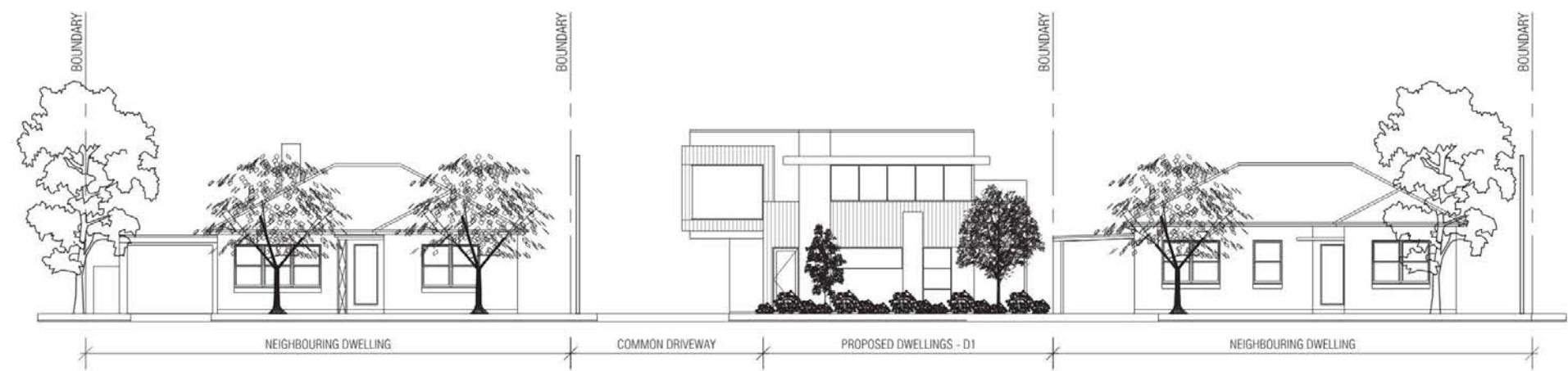
**PARTY WALL NOTE:**  
COMMON STRUCTURAL PARTY WALL  
BETWEEN ALL DWELLINGS. PARTY WALL IS A  
SHARED WALL WITH RECIPROCAL RIGHTS.



**TYPICAL HEBEL PARTY WALL/SLAB DETAIL**

SCALE 1:10

<div>Contractors to verify all dimensions prior to commencement of any building work. Figured dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the designer immediately. These drawings are the exclusive property of Verrocchi Building Design. Any reproduction without written authority is prohibited. Copyright ©</div>	rev	date	description	notes	<div>verrocchi<b>buildingdesign</b></div> <div>128a Mooringe Ave North Plympton 5037 m: 0407 820 946 e: dino.verocchi@gmail.com</div>	project: WARWICK AVE DEVELOPMENT		
	A	16/11	CONCEPT DRAWINGS			date: NOV 2017    drawn by: LD		
	B	6/12	AMENDED DRIVEWAY TO SUIT COUNCIL'S DRIVEWAY SETUP			at: 16 WARWICK AVENUE KURRALTA PARK SA 5037		
	C	16/12	PLANNING DRAWINGS			scale: 1:10    drawing no: 06		
						project no:    sheet size:		
PLANNING DRAWINGS						client: ZYBEK CONSULTING	ZYB.073	A3



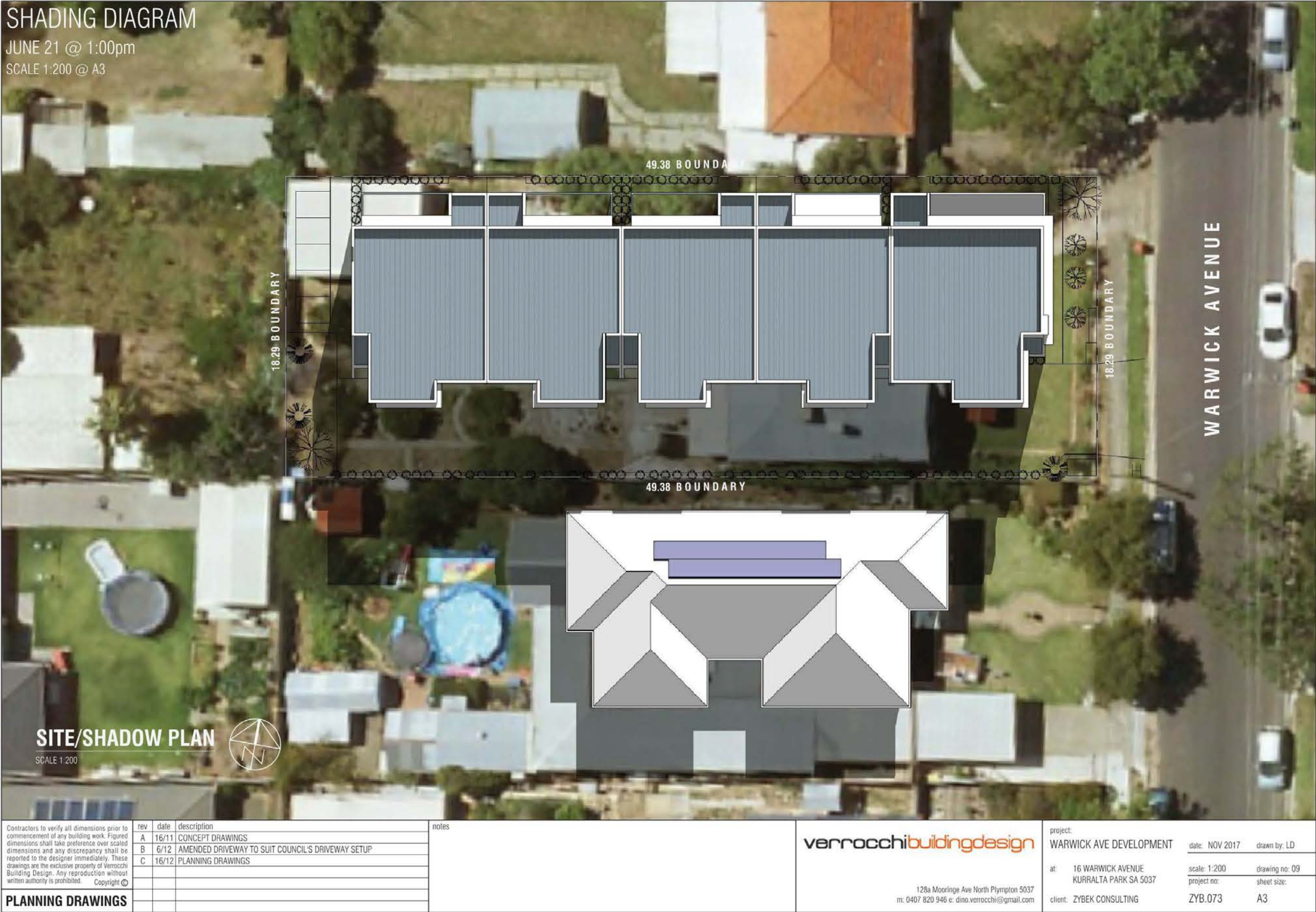
**STREETSCAPE (WARWICK AVENUE)**  
SCALE 1:200

<div>Contractors to verify all dimensions prior to commencement of any building work. Figured dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the designer immediately. These drawings are the exclusive property of Verrocchi Building Design. Any reproduction without written authority is prohibited. Copyright ©</div> <div>PLANNING DRAWINGS</div>	rev	date	description	notes	<div>verrocchi<b>building</b>design</div> <div>128a Mooringe Ave North Plympton 5037 m: 0407 820 946 e: dino.verocchi@gmail.com</div>	project: WARWICK AVE DEVELOPMENT		date: NOV 2017	drawn by: LD
	A	16/11	CONCEPT DRAWINGS			at: 16 WARWICK AVENUE KURRALTA PARK SA 5037	scale: 1:100 project no: drawing no: 07 sheet size:	ZYB.073	A3
	B	6/12	AMENDED DRIVEWAY TO SUIT COUNCIL'S DRIVEWAY SETUP						
	C	16/12	PLANNING DRAWINGS						
						client: ZYBEK CONSULTING			

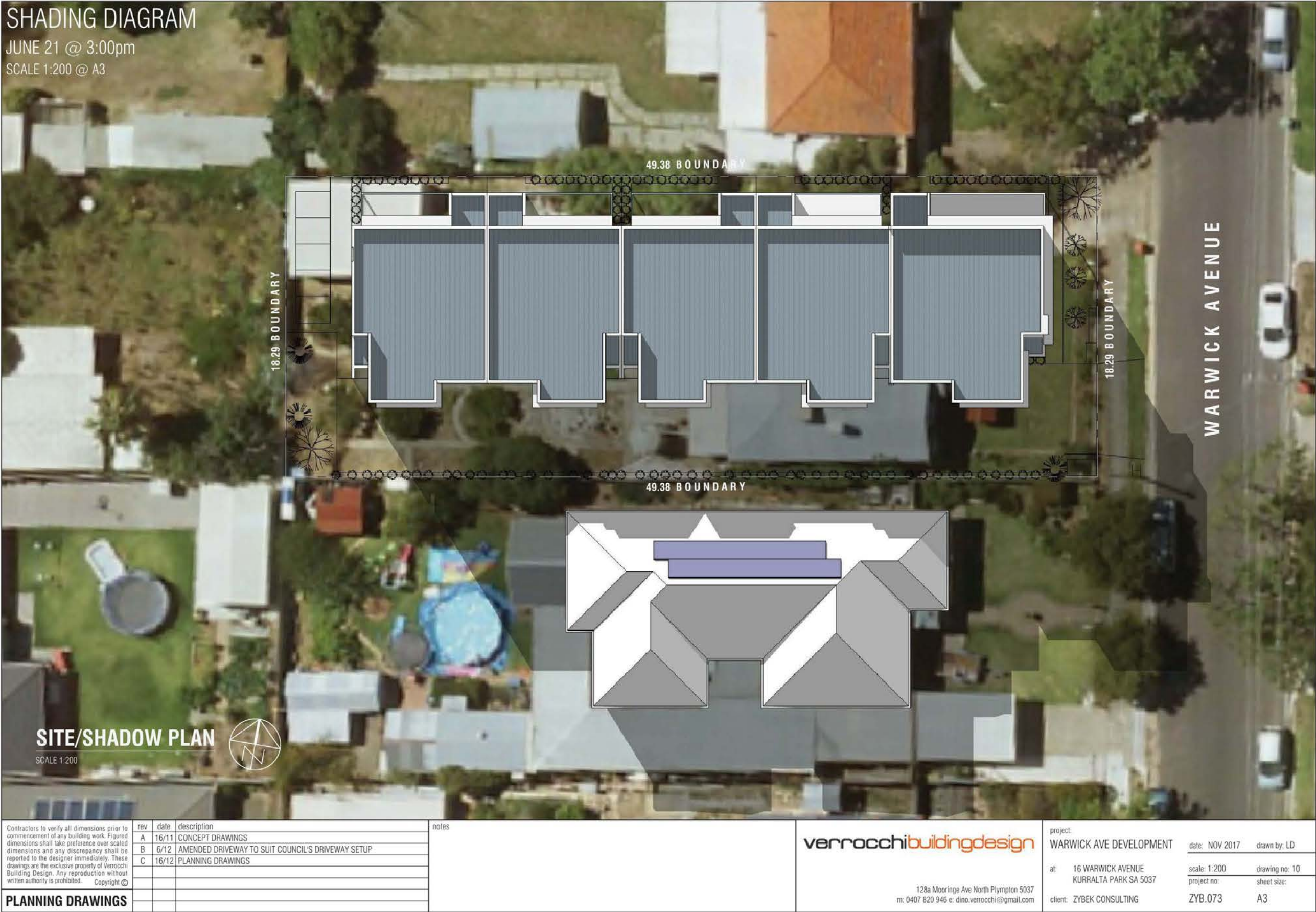




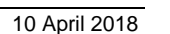




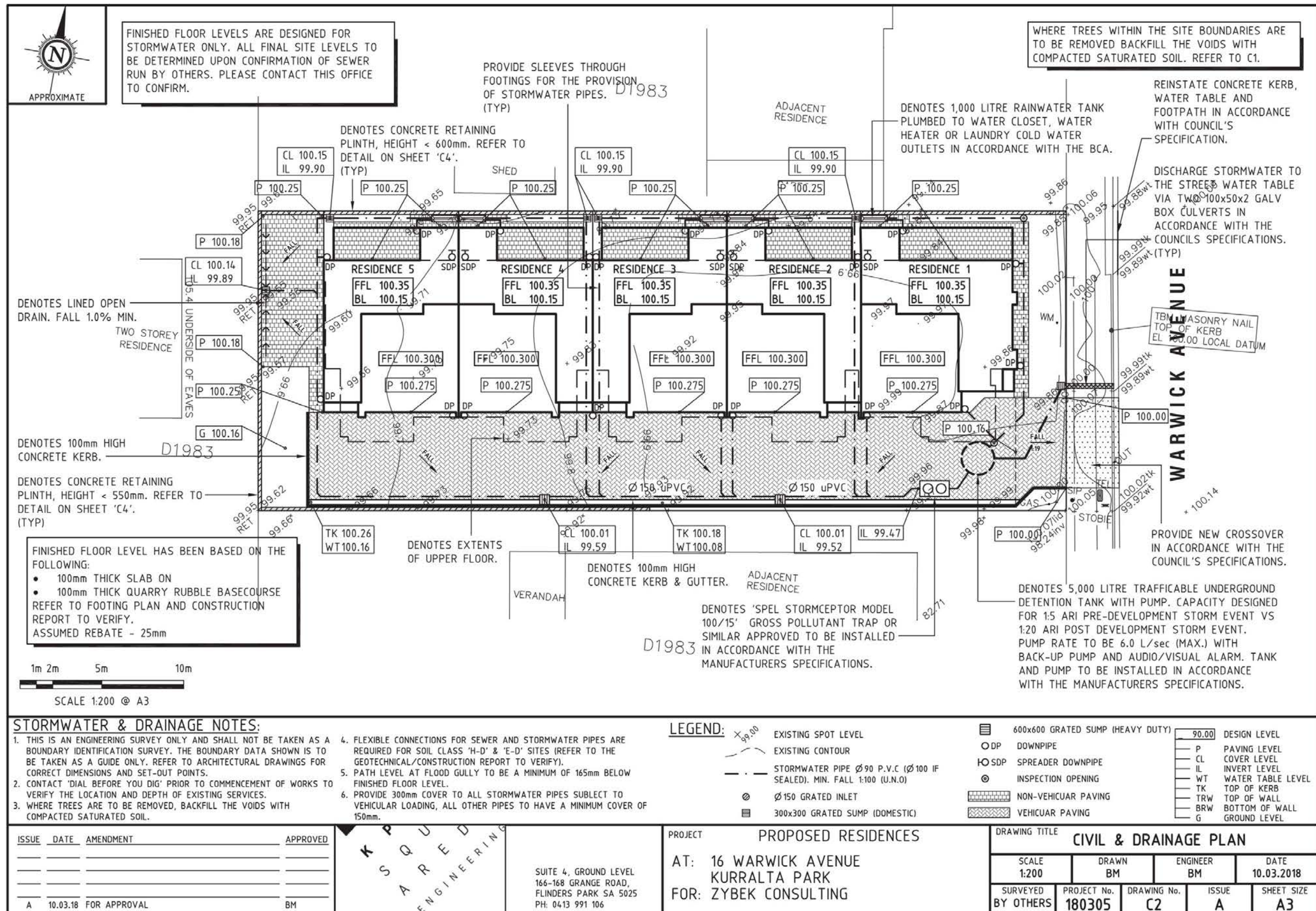








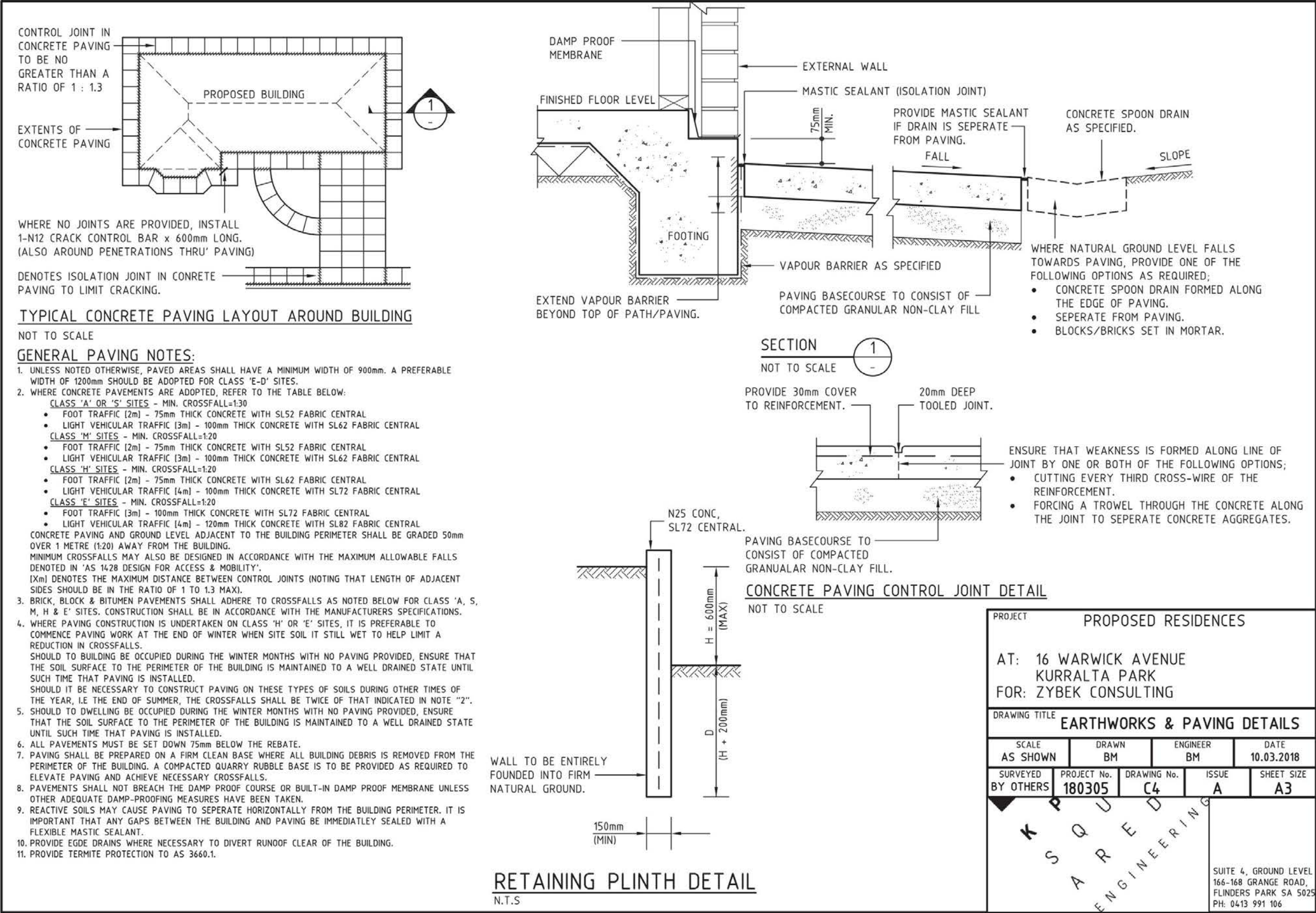




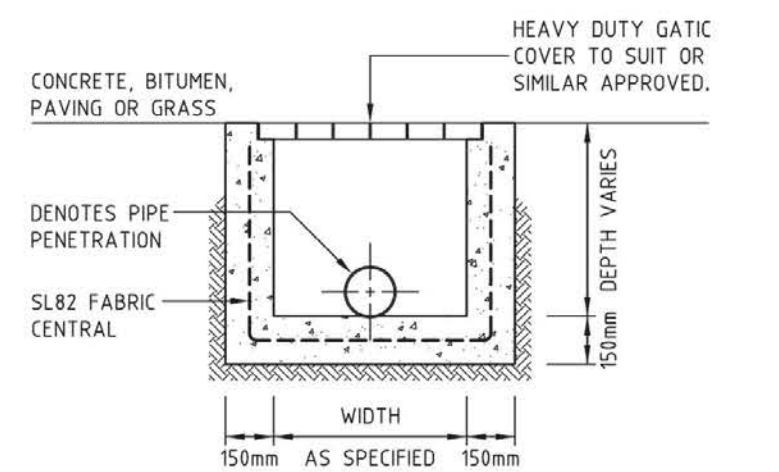












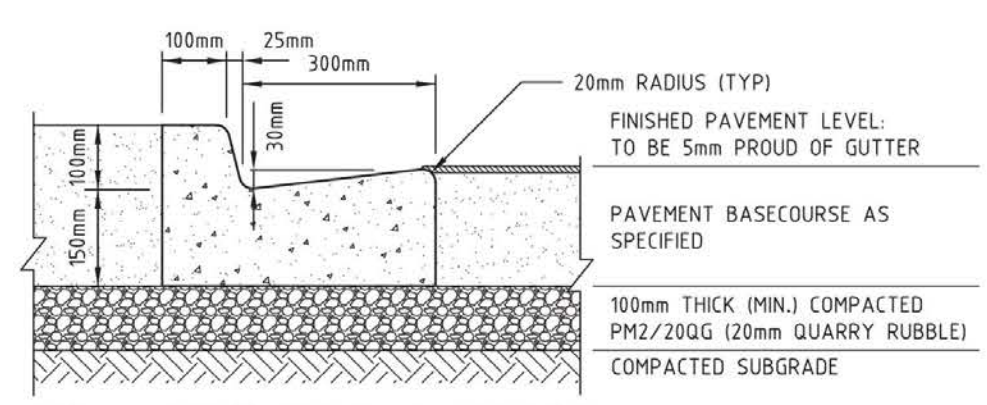
**GRATED SUMP**

- 1:20
- NOTES:
- 1. REFER TO 'STORMWATER PIT SCHEDULE' FOR PIT DIMENSIONS.
  - 2. PENETRATION FOR PIPES SHALL BE APPROX. 50mm GREATER THAN THE OVERALL DIAMETER OF PIPE. PIPES SHALL BE FINISHED FLUSH WITH THE INTERNAL FACE OF THE SUMP. A STIFF MORTAR MIX SHALL BE PACKED INTO THE SPACE FROM BOTH SIDES OF STRUCTURE. THE INTERNAL FACE SHALL BE FINISHED SMOOTH AND A 150mm THICK BAND ON CONCRETE SHALL BE POURED OUTSIDE THE SUMP TO SEA THE PENETRATION.
  - 3. WHERE SUMPS CONSIST OF MORE THAN ONE PRECAST UNIT, THE JOINTS SHALL BE SEALED WITH BUTYL MASTIC FLEXIBLE PIPE SEALANT AROUND THE OUTSIDE AND A 1:3 SAND CEMENT GROUT ON THE INSIDE OF THE SUMP.



**UNIT PAVEMENT**

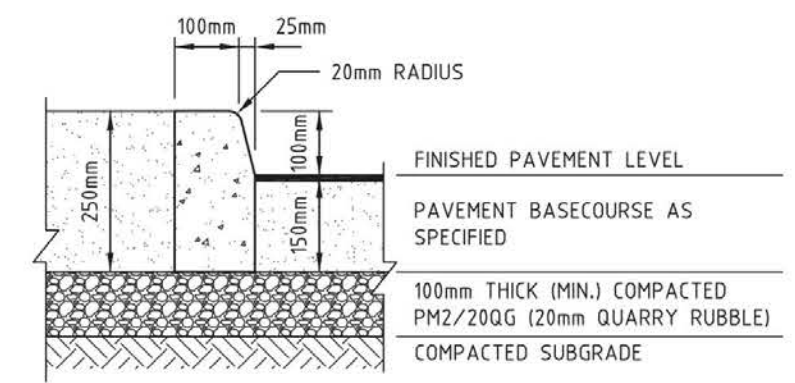
VEHICULAR DUTY  
1:10



**100mm HIGH KERB & GUTTER**

1:10

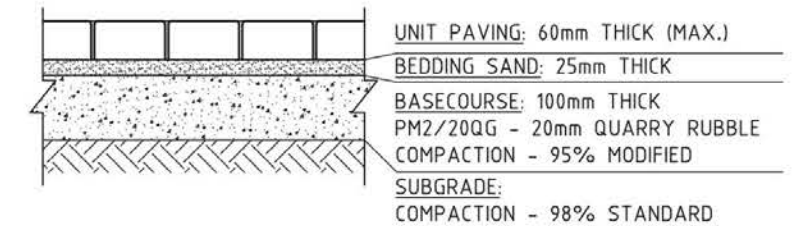
NOTE:  
PROVIDE 10mm WIDE x 20mm DEEP TOOLED JOINTS AT 3.0m CRS (MAX.)  
FORCE A TROWEL THOUGH JOINTS DURING CONCRETE POUR TO SEPARATE AGGREGATES AND INFLUENCE CRACKING AT THESE LOCATIONS.



**100mm HIGH KERB**

1:10

NOTE:  
PROVIDE 10mm WIDE x 20mm DEEP TOOLED JOINTS AT 3.0m CRS (MAX.)  
FORCE A TROWEL THOUGH JOINTS TO SEPARATE AGGREGATES AND INFLUENCE CRACKING AT THESE LOCATIONS.



**UNIT PAVEMENT**

NON-VEHICULAR DUTY  
1:10

PROJECT					PROPOSED RESIDENCES									
AT: 16 WARWICK AVENUE KURRALTA PARK FOR: ZYBEK CONSULTING														
DRAWING TITLE										CIVIL DETAILS				
SCALE		DRAWN		ENGINEER		DATE								
AS SHOWN		BM		BM		10.03.2018								
SURVEYED		PROJECT No.		DRAWING No.		ISSUE		SHEET SIZE						
BY OTHERS		180305		C5		A		A3						
										SUITE 4, GROUND LEVEL 166-168 GRANGE ROAD, FLINDERS PARK SA 5025 PH: 0413 991 106				



## STORMWATER DETENTION DESIGN

Project 16 Warwick Avenue,  
Glengowrie

Page CC1/A  
Job No. 180304  
Date MAR '18  
Eng BM

Total Site Area = 903 m<sup>2</sup>

Pre-development - 1:20 ARI 59.2 mm/hr (10min)

### Pre-Development

Impervious Area - Ai = 360 m<sup>2</sup> Ci = 0.9  
Pervious Area - Ap = 543 m<sup>2</sup> Cp = 0.1  
Cn = [(Ci x Ai) + (Cp x Ap)]/A  
Cn = 0.42  
Flow Rate - Q<sub>s</sub> = (Cn x i x A)/3600  
Q<sub>s</sub> = 6.22 Litres/sec

### Post-Development

Site Area = 903 m<sup>2</sup>  
Impervious Area - Ai = 673 m<sup>2</sup> Ci = 0.9  
Pervious Area - Ap = 230 m<sup>2</sup> Cp = 0.1  
Cn = [(Ci x Ai) + (Cp x Ap)]/A  
Cn = 0.70  
Peak Flow Rate = 1:20 ARI 20 minute duration

**THEREFORE PROVIDE 4,840 LITRES OF DETENTION REQUIRED**

### Orifice Design

Paving area collecting stormwater via grated inlets/sumps - A = 0 m<sup>2</sup>  
Ci = 0.9  
Q<sub>spaving</sub> = 0 Litres/sec  
Q<sub>max</sub> = Q<sub>Total</sub> - Q<sub>spaving</sub>  
Q<sub>max</sub> = 6.22 Litres/sec per Residence  
Q<sub>max</sub> = 0.6 x A √(2 x g x H) where Q<sub>max</sub> = 0.0062 g = 9.81  
A = 0.002703 m<sup>2</sup> A = ? H = 0.75  
A = (π x D<sup>2</sup>)/4  
Diameter = 0.059 m = 58.7 mm

**PROVIDE 4840 LITRES OF DETENTION WITH A Ø 59 mm ORIFICE.**



Ref: 2017-0280

8 August 2017

Ms Janine Lennon  
Manager City Development  
City of West Torrens  
165 Sir Donald Bradman Drive  
**HILTON SA 5033**



Dear Ms Lennon

### **16 Warwick Avenue, Kurralta Park – Residential Flat Building comprising 5 dwellings**

URPS has been engaged by the applicant in the matter described above, to provide planning advice and assistance, and prepare this supporting statement.

A description of the proposal, its land and locality, and an appraisal of the proposal's performance against the most pertinent planning provisions, is set out below.

#### **Proposal**

The proposal is for a Residential Flat Building comprising 5 dwellings each with a garage under the main roof. The development would involve the demolition of existing buildings and structures to accommodate the proposal.

#### **Subject land and locality**

The subject land is one rectangular-shaped allotment with a frontage to Warwick Avenue of 18.29 metres and a depth of 49.38 metres. The subject land therefore has a total area of approximately 903 square metres. Currently a detached dwelling, carport, pergola and outbuilding are situated on the subject land. The land is reasonably flat and does not feature any rights-of-way, easements or other legal encumbrances on the title.

The locality comprises a mix of residential type development including detached dwellings, semi-detached dwellings, group dwellings and residential flat buildings. The era of housing appears to range from the 1950's to 1980's and occasionally comprises Tudor and bungalow style dwellings. Of particular note, some modern replacement dwellings that appear to have been built after the 1980's are also situated within Warwick Avenue.

Further, and of particular relevance, the land is within 400 metres of an Urban Corridor Zone, District Centre Zone and Local Centre Zone, which are situated nearby to the south. Each of these zones envisage some form of commercial type development including shops, offices etc.

shaping great communities



### Procedural matters

As per West Torrens Development Plan (consolidated 30 May 2017), the subject land is situated within the Residential Zone and Medium Density Policy Area 18.

Residential flat buildings are neither “complying” nor “non-complying” forms of development in the zone and therefore the proposal is to be assessed on its merits.

The proposal is for “a building of 2 storeys comprising dwellings” and therefore is a ‘Category 2’ form of development for public notification purposes, in accordance with Schedule 9, clause 18(a) of the *Development Regulations 2008*.

### Planning Assessment

In my view, the most pertinent planning considerations are as follows:

- Land Use and Density.
- Site Areas and Frontage.
- Living Environment.
- Setbacks.
- Building Height.
- Design and Appearance.
- Private Open Space and Landscaping.
- Overshadowing.
- Overlooking.
- Car Parking and Traffic.
- Storage.

#### Land Use and Density

As per Principle 1 within Medium Density Policy Area 18, residential flat buildings are a specifically envisaged dwelling type.

As per the Desired Character within Medium Density Policy Area 18, allotments will be at medium density, accommodating a range of dwelling types including residential flat buildings.

With the allotment being approximately 903 square metres in area, the proposal would achieve an equivalent density to 55.36 dwellings per hectare. The proposal therefore achieves a medium density (that being between 40 and 67 dwellings per hectare) as guided by Principle 4 within Medium Density Policy Area 18.

The proposed land use and density are therefore appropriate for the subject locality.

#### Site Areas and Frontage

Dwellings within a residential flat building should have an average site area of 150 square metres. The complete building should have a minimum frontage to Warwick Avenue of 15 metres.

The Development Plan does not present guidance that explains how site area or frontage should be calculated, however it is common practice to divide the area of the entire allotment by the number of dwellings proposed.

In this circumstance, common property would cover vehicle manoeuvring areas, in particular landscaped spaces and common storage areas. The common property would not act as, or appear to be, a public lane or street. The common property would appear to be a private driveway for occupants and visitors of each dwelling and would typically be used as private space, albeit common for occupants of the development. In addition, common storage areas would be gated for security and would be private for the benefit of dwelling occupants. Given the above circumstances, it appears appropriate to calculate average site areas as an average of the total area of the land, including common property, as these spaces are intended to be privately used by all future occupants or residents of the proposed development.

The intent of a “minimum site area” is considered to ensure that each dwelling is provided with a high quality living environment with appropriate open space and landscaping to prevent the appearance of over-development. The intent of “minimum frontage” is considered to ensure an appropriate streetscape presentation (i.e. appropriate areas for landscaping and spacing between dwellings).

Based on the ‘plan of community division’ prepared by Cavallo Forest Licensed Surveyors (Revision 1), the site has a total area of approximately 903 square metres. Including common property, the proposal would present an average site area of 180.6 square metres per dwelling to satisfy the Development Plan guideline.

The site would have a frontage to Warwick Avenue of 18.36 metres. The frontage width satisfies the Development Plan. While I am aware that the specific proposed width of allotment 1 would have a frontage of 11.85 metres to Warwick Avenue it does feature a total rear width of 14.2 metres. Naturally the allotment narrows at its road frontage to accommodate a communal letterbox and a driveway of suitable width for safe and convenient vehicle movements. Overall the shape of this allotment is functional for its intended purpose for use as a site for medium density housing.

Setting aside the quantitative discussion above, importantly, the proposal presents to the streetscape with a contemporary built form that is well spaced with sufficient boundary setbacks and landscaped areas to enhance ambience and street appeal. Each dwelling achieves a high quality living environment as discussed further below. Even if alternate views are adopted on the numerical standards, any perceived departures are not considered to diminish the overall planning merits of the proposal.

### **High Quality Living Environment**

General Section, residential development, Principle 9 states “residential development should provide a high quality living environment” by ensuring minimum internal floor areas are achieved.

Based on 3 bedrooms, each dwelling should have a minimum 100 square metres of internal floor area excluding balconies and car parking.

Each proposed dwelling would have a floor area exceeding 100 square metres and therefore provides a “high quality living environment” in the context of the Development Plan standards.

### Setbacks

Principle 5 within Medium Density Policy Area 18 guides setback parameters from the primary road frontage and back boundary, which should be 3 and 4 metres respectively.

The building would have a staggered façade being setback 3.3 metres up to 5.5 metres from the primary frontage. Not only does the staggered façade achieve minimum setback parameters it also ensures the street fronting garage element is not dominant, being positioned behind the majority of the façade.

The back boundary setback is to be 4.04 metres and therefore achieve Principle 5.

Principle 11 within the Residential Zone guides side boundary setbacks dependent upon building height. Where the vertical wall height is 3 metres or less, a 1 metre setback is appropriate. Where the vertical wall measures between 3 and 6 metres, a 2 metre setback is appropriate.

The side boundaries are considered the northern and southern boundaries, which also form a side boundary of 14 and 18 Warwick Avenue neighbouring the subject land. Except for the garage servicing dwelling 1, ground floor components of the building would be setback 1 metre from the northern boundary and 6 metres from the southern boundary. Apart from obscured balconies, upper floor level components would be setback 3 metres from the northern boundary and approximately 4.8 metres from the southern boundary.

General section, residential development, Principle 16 allows a garage to be built along a boundary for a length up to 8 metres. The extent of garage on a boundary would be 6.1 metres.

Upper floor level obscured balconies would be setback 1 metre from the northern side boundary. The intent of side setback parameters is to limit overshadowing and visual impact. Being on the northern façade of the building, the balconies would not overshadow neighbouring allotments. Visually, the balconies form a transition to the proposed building's overall height and present good levels of articulation to the north face of the building.

The proposed setbacks are considered to perform very well against the Development Plan.

### Building Height

The desired character within Medium Density Policy Area 18 states *"new buildings will contribute to a highly varied streetscape. Buildings will be up to 3 storeys and provide a strong presence to streets..."*

In addition, Principle 5 within Medium Density Policy Area 18 guides maximum building heights allowing up to three storeys or 12.5 metres.

The proposal is to be two storeys with an approximate building height of 6.8 metres. Although the locality is predominantly characterised by single storey buildings it is important to acknowledge the intended transition to medium density development and the desire for a highly varied streetscape potentially up to three storeys in height.

The proposed building height is appropriate in the context of the Development Plan.



### *Design and Appearance*

The building would have a contemporary appearance with a building height and mass that is expected in Medium Density Policy Area 18. Each elevation would be articulated using a mix of external colours, materials, textures and setbacks.

Windows, doors and design features are proportionate to the proposed building's scale. Entrance doors and garages are clearly legible when viewed from the street.

Balconies have been integrated into the overall form of the building. Private open spaces, including balcony areas and internal living areas, have a northerly aspect to ensure sustained solar access.

The proposal would project back into the allotment at a consistent two storey height. The Development Plan does not impose limitations upon the depth of a building, other than rear setback guidelines. In the context of maximum building height allowances (three storeys), the proposed consistent building height is generally acceptable and supported given the articulation adopted by each dwelling's façade.

### *Private Open Space and Landscaping*

General Section, residential development, principle 19 guides minimum private open space dependant on the site area of each dwelling. In this circumstance, minimum site areas are below 300 square metres and therefore a minimum 24 square metres of private open space should generally be provided. Ground level private open space should have a minimum dimension of 3 metres. 8 square metres of private open space may comprise a balcony provided they have a minimum dimension of 2 metres.

Dwelling 1 would have approximately 34.02 square metres of private open space whilst dwellings 2 to 5 would have 29.38 square metres of private open space. Both ground level courtyards and upper level balconies achieve minimum dimension guidelines.

### *Overshadowing*

Development should ensure that north-facing windows to habitable rooms of existing dwellings receive at least 3 hours of direct sunlight over a portion of their surface between 9:00am and 5:00pm on 21 June (winter solstice). In addition development should ensure ground level open space receives direct sunlight for a minimum of 2 hours between 9:00am and 3:00pm on 21 June.

18 Warwick Avenue is situated north of the proposed development. Given the northern path of the sun, 18 Warwick Avenue would not be overshadowed by the proposal.

14 Warwick Avenue is situated south of the subject land, however the proposed development would be setback 4.8 metres from the shared side boundary and maintains a 2 storey height against a 3 storey guideline.

The proposal would not present unreasonable overshadowing in the context of the Development Plan's height and setback guidelines.

### Overlooking

Side-facing upper level windows and balconies will be obscured up to 1.7 metres above finished floor level to prevent overlooking and maximise visual privacy as guided by general section, residential development, Principle 27.

### Car Parking and Traffic

A residential flat building should provide two car parking spaces per dwelling, one of which is covered, plus an additional 0.25 car parking spaces per dwelling. Based on 5 dwellings, 11 onsite car parking spaces should generally be made available for occupants and visitors for this proposal.

The proposal includes 10 on-site car parking spaces, therefore a 1-space shortfall is identified. Dwellings 1 to 4 would have two covered spaces within proposed double garages. Dwelling 1 would have one covered space and an uncovered space forward of the single car garage. The shortfall only relates to a visitor parking space.

Excluding driveways, the proposal would retain approximately 9 metres of the overall frontage as upright kerb. This length is capable of retaining at least one on-street vehicle parking space. Upon review of intermittent aerial photography, Warwick Avenue is not commonly congested with vehicles parked on-street. It is also worthy to note that the demand for visitor car parking is typically at night time (during the week) and on weekends. In addition, the site is approximately 250 metres from a bus stop and each dwelling is capable of storing a bicycle. As such, the site is well connected and occupants have the option of alternate transportation modes.

The proposed parking shortfall of one space would not impact upon the free-flow of traffic or the safe and efficient movement of anticipated transport modes. The proposed on-site car-parking shortfall is considered acceptable and not fatal in the assessment of the merits of the proposal.

### Storage

General section, residential development, Principle 31 states that *"a dwelling should incorporate a minimum storage area of 8 cubic metres for goods... within... a non-habitable room of the dwelling, a garage, carport or outbuilding"*.

Each dwelling would be provided with a detached storage area at the western end of the development. Each store would have an area of 9.2 cubic metres. In addition, bedrooms would have built-in/walk-in robes and each garage would provide some additional storage space at each end.

### Conclusion

On balance, and understanding the transitional intent of the policy area from low to medium density, the proposal achieves the general intent of the Zone/Policy Area 18.

The proposed allotment sizes reasonably support the proposed dwellings, which are specifically envisaged by the Policy Area and provide a high quality living environment for their future occupants.

The quantitative shortfalls are not of themselves serious planning issues for the reasons set out within this report.

In my view, the proposal exhibits sound planning merit and contributes to the achievement of the zone and policy area objectives without conflict with the Development Plan to a significant degree.

Please do not hesitate to contact the undersigned on 8333 7999 if you wish to discuss this matter further.

Yours sincerely

A handwritten signature in black ink, appearing to be 'M. King', with a stylized flourish extending to the right.

Matthew King  
**Director**



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act, 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

City of West Torrens

17 JAN 2018

City Development

DEVELOPMENT No. 211/731/2017  
 PROPERTY ADDRESS: 16 Warwick Avenue, KURRALTA PARK SA 5037

YOUR FULL NAME	Anton Peter & Rita Rosa Olbrycht
YOUR ADDRESS	14 WARWICK AVE KURRALTA PARK SA 5037
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	owners of property 'next door' to development property <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION	1) the shadow diagram proposed by the developers is grossly misleading. We have a 22 panel solar panel array on the Northern roof facing the development property.  Please see accompanying sheets
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought)	Please see accompanying sheets

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

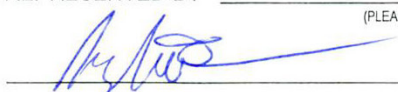
I DO **NOT** WISH TO BE HEARD ☐

I DESIRE TO BE HEARD PERSONALLY ☒

I DESIRE TO BE REPRESENTED BY ☐

(PLEASE SPECIFY)

SIGNED



DATE

14/1/2018

Responsible Officer: Josh Banks  
 Ends: Friday 19 January 2018

If space insufficient, please attach sheets

The developers have depicted the shadow as having no effect on the panels, when in fact, the panels are situated lower than shown. When the panels are shown in the correct position, the shadow covers the bottom half of the array.

THIS IS TOTALLY UNACCEPTABLE!!!

- 2) Lack of Privacy from upper floor
- 3) Increased noise from cars in the driveway, which is on our fence side. We have bedrooms on that side of the house.
- 4) This already exists a 2 story 5 unit development on the block behind this property. If this development is approved, there will be 2 such buildings extending from Wood St through to Warwick Avenue. The visual impact of these ~~two~~ 2 developments together going from street to street is detrimental to the ~~the~~ appearance of the area.  
A COMPLETE EYESORE!!!
- 5) Fear of Decrease in Land Value.
- 6) Fear of the neighbourhood becoming an overcrowded ghetto!!

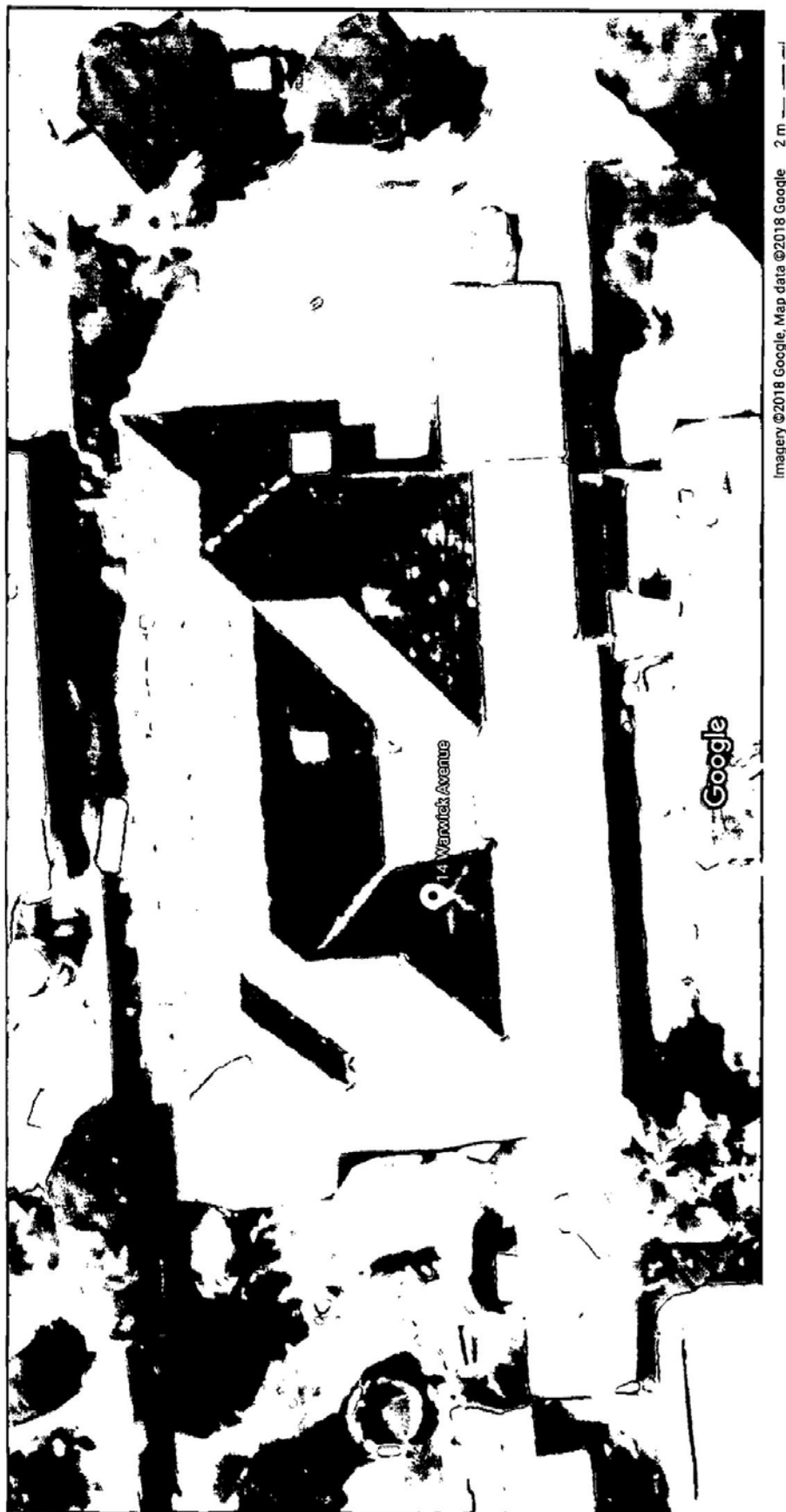
14 Warwick Ave - Google Maps

Page 1 of 2

### Google Maps

#### 14 Warwick Ave

The solar panels cover a much larger area than that shown in the Developers' shadow diagram. They are attempting to misled Council into accepting the proposal by falsifying information.



Imagery ©2018 Google, Map data ©2018 Google 2 m

<https://www.google.com.au/maps/place/14+Warwick+Ave,+Kurralta+Park+SA+5037/@-34.9536101,138.5657981,27m/data=!3m1!1e3!4m5!3m4!...> 15/01/2018



## SOLUTIONS

As far as the solar panels are concerned

- 1) council and developers pay for our electricity bills due to loss of Power generating ability.
- 2) council and developers pay for re-locating the panels to a position of no shadow interference and the installation of a storage battery system
- 3) DON'T BUILD A TWO STOREY DEVELOPMENT !!!

We cannot understand why every development in this area has to be a two storey development.

If the population density is to be increased in this area, it can still be achieved with a single storey development.

The only reason we can see for a multi-storey development is more money for developers and more money for council.

We have owned and lived at 14 Warwick Ave for over 31 years. Why is the council more concerned over developers' financial gain rather than the concerns of local residents. It's time Council stopped allowing developers to rape the Kurralt Park area for their own financial gain. In your responses to our concerns, please do not fob us off with the usual BS. Thank you.

ENOUGH IS ENOUGH!!!!

**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act, 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

City of West Torrens

17 JAN 2018

City Development

DEVELOPMENT No. 211/731/2017  
 PROPERTY ADDRESS: 16 Warwick Avenue, KURRALTA PARK SA 5037

YOUR FULL NAME	Yuenfeng Lam & Teina Gao
YOUR ADDRESS	21 Warwick Ave KURRALTA PARK SA 5037
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	owner of property opposite to development property (eg. Adjoining resident, owner of land in the vicinity etc.)
REASON/S FOR REPRESENTATION	The proposed development of 5x double story flats block will bring some negative effects to Warwick Ave:- - Increase in traffic flow/noise - Shortage of parking - ruin the character of the neighbourhood...
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought)	

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO NOT WISH TO BE HEARD ☐

I DESIRE TO BE HEARD PERSONALLY ☐

I DESIRE TO BE REPRESENTED BY

Anton Peter Olbrych ☒  
 (PLEASE SPECIFY)

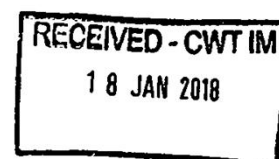
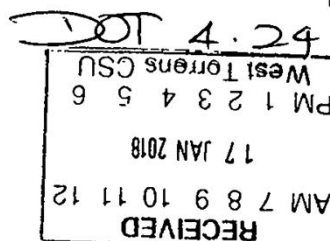
SIGNED

*Yuenfeng Lam*

DATE 15-01-2018

Responsible Officer: Josh Banks  
 Ends: Friday 19 January 2018

If space insufficient, please attach sheets



**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act, 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

DEVELOPMENT No. 211/731/2017  
 PROPERTY ADDRESS: 16 Warwick Avenue, KURRALTA PARK SA 5037

YOUR FULL NAME	Steve and Voula Haliabalas
YOUR ADDRESS	21 Wood St, Kurralta Pk
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	Adjoining Resident <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION	
PLEASE SEE ATTACHED	
MY REPRESENTATIONS WOULD BE OVERCOME BY <small>(state action sought)</small>	
A lesser development of the site. For example 3 single storey dwellings.	

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO **NOT** WISH TO BE HEARD

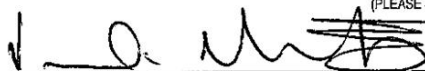
I DESIRE TO BE HEARD PERSONALLY

I DESIRE TO BE REPRESENTED BY \_\_\_\_\_



(PLEASE SPECIFY)

SIGNED



DATE

17/1/18

Responsible Officer: Josh Banks  
 Ends: Friday 19 January 2018

If space insufficient, please attach sheets



## Reasons for Representation

This proposed development will result in an over development of the site which is not in accordance with the desired character for the zone as set out in the Development Plan. The bulk and scale of the proposed development is overbearing. There is a lack of open space, insufficient car parking on the site, insufficient storage, and a lack of adequate vegetation. This development will create overshadowing issues, privacy issues, traffic and noise issues, parking issues to name a few.

No neighbourhood characteristic has been taken into consideration (ie lot size and shape, topography, streetscape, architectural style, landscaping, building form, height, location and size of private open space, front setbacks, side and rear setbacks etc)

### OVERSHADOWING

The height of the proposed development will result in overshadowing . The shadow diagram provided by the applicant is a fabrication. Who in June has swimming pools in their yards???? The overshadowing that will result from such a development will limit the use of our backyard, make solar panels ineffective and will overall impact any vegetation growth obsolete. The loss of light is unacceptable. The overshadowing would be in contradiction of council policy.

### OVERLOOKING & PRIVACY

The proposed development will be overlooking into our home and also that of our surrounding neighbours. The proposed development has in no way been designed to minimise overlooking of our property and that of our surrounding neighbours .This will result in a total invasion of privacy. We will no longer be able to go into our yard without there being onlookers at every angle. This will have a major impact on the way we use our home and garden. We will be no better off than prisoners in a cell.

The council under the Human Rights Act has a responsibility to ensure that we have a right to peaceful enjoyment of all of our possessions which includes our home and gardens.

### OVERDEVELOPMENT

The proposed development is a gross overdevelopment on this site and amounts to serious "cramming" in such an area.. The proposed development not only will have an adverse impact on the character of the neighbourhood but also the residential amenity of the neighbours. The maximum site coverage we believe has exceeded the allowable maximum. The amount of hard surface also impacts on the amount of stormwater runoff. The proposed development involves a total loss of garden land to be replaced with "concrete slabs"

### PRIVATE OPEN SPACE

This development has minimal private open space and we believe is totally inadequate for this development. We believe is in contravention of council policy.

### VISUAL BULK

Visually this development is an eyesore taking away from the richness and character of this beautiful street. The development is very unattractive over bearing, out of scale and out of character in comparison with the current existing development of the street. Such high density in this street is very much not suited, The current form, size and character of established homes in Warwick Avenue has not been taken into account This development surrounds single storey developments. All we will be able to see is massive slabs of concrete. This development makes a mockery of the character and richness of the area. In is not the desired character of the zone, it does not fit in with the streetscape and as such a total inappropriate development.

The proposed development will also result in an excessive building bulk adjacent to existing residential properties, resulting in an un-neighbourly and overbearing impact detrimental to the visual outlook and amenities of us and our neighbours. Such a development is of no benefit to any of the current residents of Warwick Street.

Such a large bulky building will impact on the outlook of our neighbours and us and will dominate our private open space.

### TRAFFIC

The proposed development currently has insufficient car parking spaces. There is no visitor carpark. The residents will not be able to utilise their garages as getting the vehicles in and out will be virtually impossible. This will render them useless and as a result residents will park in the street creating further congestion and parking issues. The street is already congested with surplus cars, adding more cars into the street will just exasperate the current situation. This also equates to another loss of a valuable residential amenity.

The traffic that this proposed development will generate cannot be safely and conveniently accommodated by the existing street networks

### PLANTS AND VEGETATION

Loss of vegetation will significantly impact on neighbourhood character and the landscape and environmental quality of our area. The proposed development does not respect nor reflect the neighbourhood character. The proposed vegetation for this development is minimal at best.

Gone will be the lust green trees and plants only to be replaced with concrete.

Increased urbanisation of the natural environment will impact on existing shrubs and mature trees, destroying the environmental quality of the area, and will also pose a risk to our property and adjoining properties due to a reduction in natural drainage, increasing run-off or seepage and potential structural issues,

### HEATING

The bulk and size of this development and as a result the concrete required will equate to that of a sauna with no trees or plants to detract from this.

### SEWERAGE

The current public sewers are inadequate for such a development and no provisions have been made to amend to accommodate the proposed development.

### STORMWATER

There is no mention of infrastructure or stormwater management. There is no provision for an onsite storm water detention system to reduce storm water discharge. Is the council going to upgrade the current stormwater facilities? The amount of hard surface also impacts on the amount of stormwater runoff.

### STORAGE

The proposed storage is inadequate and insufficient for the proposed development.

### AIR & NOISE POLLUTION

The proposed development will potentially see a further 10 or more vehicles in a 900sqm vicinity. Emissions from so many vehicles in such a confined space will result in a significant source of air pollution. This may adversely affect the health and wellbeing of my children and family and also that of our surrounding neighbours.

The proposed development of five double storey townhouses will result in noise pollution. This will also cause environmental harm. Noise generation the likes of this will result in a significant loss of amenity which we believe do not comply with the standards set up by The Environmental Protection Authority in residential areas of acceptable noise levels.

### SECURITY & SAFETY

The proposed development only allows for a single driveway and does not allow for any "back entry". As a result the safety of our home and that of surrounding homes is greatly compromised especially in the event of a fire or another emergency due to the restricted access.

The proposed development cannot be adequately serviced.

The proposed development is inappropriate would demonstrably harm the amenities enjoyed by local residents, in particular safe and available on-road parking, valuable green space, privacy and the right to enjoy a quiet and safe residential environment. The development that is proposed is suitable to city living only.

The council has to maintain its efforts to protect its heritage built form in the face of the state government's planning reforms that are contrary to the Council's agreed development plan

We implore you to NOT ALLOW the proposed development to proceed for the issues outlined above.



Ref: 2017-0280

7 March 2018

Mr Josh Banks  
Senior Development Officer – Planning  
City of West Torrens  
165 Sir Donald Bradman Drive  
**HILTON SA 5074**



**URPS**

Suite 12  
154 Fullarton Road  
ROSE PARK SA 5067

08 8333 7999  
www.urps.com.au  
ABN 55 640 546 010

Dear Josh

**Response to Representations – 16 Warwick Street, Kurralta Park – Development Number 211/731/2017**

Thank you for your email dated 29 January 2018 advising of the category 2 representation received regarding the above project. I understand that you are now progressing this application.

Following the request of Mr Simon Zybek, I provide you with my response to this representation as follows.

**Summary of Representation**

The following representations were received after public notification.

Representor	Address
V Haliabalias and S Haliabalias	21 Wood Street, Kurralta Park
Y Shan and F Gao	21 Warwick Avenue, Kurralta Park
R R Olbrycht and A P Olbrycht	14 Warwick Ave, Kurralta Park

The representations are addressed in the corresponding subheadings below.

shaping great communities

### Land Use and Density

One of the representors has raised concern with the density of the development.

It is acknowledged that the locality currently has a low-density character, however it must also be acknowledged that the policy area seeks a shift to medium density development.

The emerging medium density character will see smaller allotments and varying building forms that are typically very different to traditional detached dwellings.

As per Principle 1 within the policy area, residential flat buildings are a specifically envisaged dwelling type.

As per the Desired Character within the policy area, allotments will be at medium density, accommodating a range of dwelling types including residential flat buildings.

With respect to the proposal's performance against the relevant density standards:

- The proposal will increase the net density of the site from 11 dwellings per hectare to approximately 55 dwellings per hectare.
- The proposal therefore achieves a medium density (that being between 40 and 67 dwellings per hectare) as guided by Principle 4 within the policy area.

The proposed land use and density are therefore appropriate for the subject locality in accordance with the Development Plan.

With respect to the concern about "congestion" this could relate to a perceived concern about overdevelopment of the land.

In my view the proposal is not an overdevelopment of the land because the proposal:

- Provides a medium density development which satisfies the density standards of the policy area.
- Has an adequate road frontage width for this kind of development.
- Features generally spacious setbacks from side and rear boundaries.
- Features articulated facades that breaks the appearance of lengthy walls.
- Has a building height which is 5.7 metres (45.6%) below that allowed by the policy area.
- Features site coverage of 46.8 percent which is some 23.2 percent lower than the 70 percent site coverage maximum.
- Incorporates sufficient levels of private open space for each dwelling.
- The dwellings are interspersed with landscaping that exceeds 10 percent of the development site.

As elaborated further in this response to representations, the overall form of development is acceptable in that it does not manifest into any serious planning impacts.

### Desired Character

One of the representors has states that *“the development is... out of character in comparison with the current existing development of the street”*.

The land is not situated within a historic character or historic conservation area and no heritage listed buildings are situated within the locality.

The desired character of the policy area states:

**New buildings will contribute to a highly varied streetscape. Buildings will be up to 3 storeys and provide a strong presence to streets, other than in the part of the policy area in Underdale, Ashford (other than allotments adjacent to Residential Character Ashford Policy Area 22) and allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park where buildings will be up to 4 storeys.**

In addition, Principle 5 within the Policy Area guides maximum building heights up to three storeys or 12.5 metres.

The policy area clearly seeks a transition from the existing low-density character. Rather than reflecting the existing character, the policy area seeks:

- A highly varied streetscape.
- Buildings up to 3 storeys (and up to 4 storeys in other areas) with a strong presence to streets.

The proposed building is contemporary and different to traditional building forms within Warwick Avenue and the locality. However, this kind of development specifically satisfies the policy area by presenting variation to the streetscape.

The proposed building will be 2 storeys with an approximate total height of 6.8 metres. The proposed height is therefore 5.7 metres, or 45.6%, below the allowable maximum building height prescribed in the policy area.

### Design and Appearance

One of the representors has stated *“the development is... out of scale and character in comparison to the current existing development of the street”*.

As previously mentioned, the desired character of the policy area states:

**New buildings will contribute to a highly varied streetscape.**

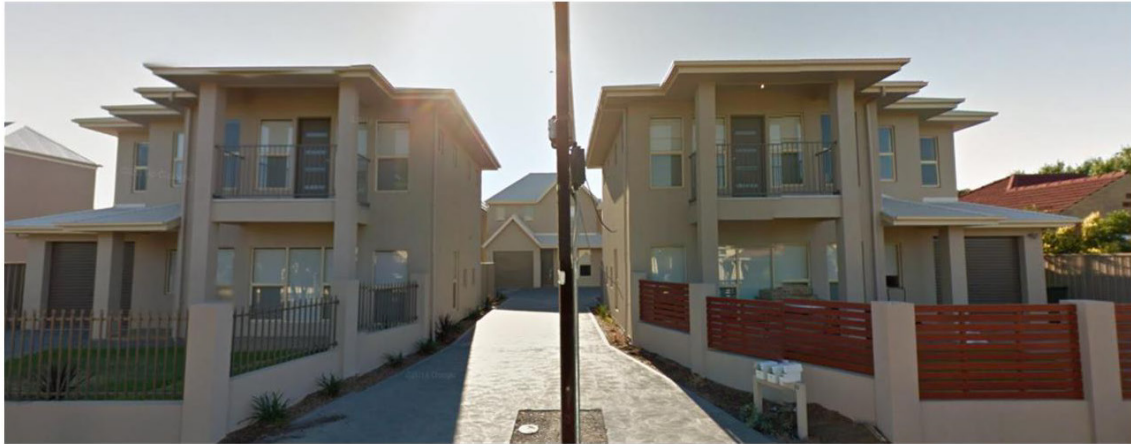
In addition, the land is not situated within a historic character or historic conservation area and no heritage listed buildings are situated within the locality.

The building will have a contemporary appearance. The building height and mass is 5.7 metres (45.6%) lower than what is possible in the Policy Area.

The architectural style is different to traditional detached dwellings within Warwick Avenue however is not dissimilar to modern residential flat buildings of the current era.

Although not traditional, two storey dwellings are situated nearby as per image 1 below.





**Image 1: 35 Warwick Avenue, Kurralta Park.**

As guided by the Development Plan, each elevation of the proposal will be articulated using a mix of external colours, materials, textures and setbacks that will provide shadowing to each facade. Side facing garage doors will not dominate the streetscape and are proportionate to the building and its articulating features.

The proposal satisfies the Development Plan from a design and appearance perspective and is of a scale that is appropriate and consistent with the types and scale of housing envisaged by the policy area.

### **Setbacks**

One of the representors has raised concern with setbacks.

Proposed setbacks are shown in the table below.

	Development Plan Parameter (metres)	Dwelling 1 (metres)	Dwelling 2 (metres)	Dwelling 3 (metres)	Dwelling 4 (metres)	Dwelling 5 (metres)
<b>Setback from Primary Road Frontage</b>	3	3	N/A	N/A	N/A	N/A
<b>Setback from Rear Boundary</b>	4	N/A	N/A	N/A	N/A	4.05
<b>Walls up to 3m - Setback from Side Boundaries</b>	1	1 and 6	1 and 6	1 and 6	1 and 6	1 and 6
<b>Walls between 3m and 6m - Setback from Side Boundaries</b>	2	3 and 4	3 and 4	3 and 4	3 and 4	3 and 4
<b>Satisfies Development Plan?</b>		✓	✓	✓	✓	✓

Front and rear setbacks satisfy the Development Plan.

Ground and upper level wall setbacks satisfy the Development Plan and in fact have a greater setback than that required by the Development Plan.

The balconies will partly protrude from the upper level northern side setback with a total balustrade height that is approximately 4.7 metres above ground level. The edge of each balcony will be setback 1 metre from the northern boundary. The balconies will therefore encroach into the Development Plan setback parameter however are staggered with the majority of the building exceeding Development Plan setback guidelines.

The balconies will not overshadow neighbouring properties or impact upon privacy given the obscured glass balustrades which extend 1.7 metres above the finished floor level of each balcony. The balconies also articulate the northern elevation providing a mix of materials, finishes and setbacks.

The proposed setbacks are considered to be acceptable and are not detrimental to neighbouring amenity or the streetscape, particularly when considered in the context of the development forms envisaged by the policy area.

### Private Open Space

One of the representors has stated that the development has “minimal private open space”.

General Section, Residential Development Principle 19 guides minimum private open space dependant on the site area of each dwelling. In this circumstance, minimum site areas are below 300 square metres and

therefore a minimum 24 square metres of private open space should generally be provided. Ground level private open space should have a minimum dimension of 3 metres. 8 square metres of the private open space may comprise a balcony provided the balcony has a minimum dimension of 2 metres.

Proposed Private Open Space is displayed in the table below.

	Dwelling 1	Dwelling 2	Dwelling 3	Dwelling 4	Dwelling 5
<b>Ground Level Private Open Space (square metres)</b>	19.2	17.4	17.4	17.4	17.4
<b>Minimum Dimension – Ground Level Private Open Space (metres)</b>	3	3	3	3	3
<b>Balcony Private Open Space (square metres)</b>	12.9	10.7	10.7	10.7	10.7
<b>Minimum Dimension – Balcony Private Open Space (metres)</b>	2	2	2	2	2
<b>Total Private Open Space Area (square metres)</b>	32.1	28.1	28.1	28.1	28.1
<b>Satisfies Development Plan?</b>	✓	✓	✓	✓	✓

Each dwelling is provided with private open space that satisfies the Development Plan.

### Landscaping

One of the representations has raised concern with the loss of vegetation.

The land does not comprise regulated trees and the locality does not comprise special environmental qualities. The land is largely covered in manicured grass with very few trees.

General Section, Landscaping, Fences and Walls Principle 4 states:

**A minimum of 10 per cent of a development site should be landscaped.** The development site refers to the land which incorporates a development and all the features and facilities associated with that development, such as outbuildings, driveways, parking areas, landscaped areas, service yards and fences. Where a number of buildings or dwellings have shared use of such features and facilities, the development site incorporates all such buildings or dwellings and their shared features and facilities.

The development provides 92.15 square metres of landscaped area. The total site area is 903 square metres. Therefore 10.2% of the development site will be landscaped.

In the context of the above Development Plan guideline, the proposal provides sufficient space for landscaping.



### Privacy

Some of the representors has raised concern about the lack of privacy presented by the upper level of the proposal.

General Section, Residential Development, Principle 27 states:

**Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.**

All upper level side and rear facing windows and/or balconies will comprise obscured glass up to 1.7 metres above the internal finished floor level to satisfy Principle 27.

The north facing balcony of dwelling 1 has an approximate balustrade height of 1 metre. This is because the balcony overlooks the front garden of 18 Warwick Avenue which is visible to the public from Warwick Avenue and is not private. The applicant is willing to accept a condition that this balcony be obscured to a height of 1.7 metres should the CAP (Council Assessment Panel) feel it is appropriate.

The proposal satisfies the Development Plan with respect to neighbouring privacy.

### Overshadowing

One of the representors has stated that overshadowing will limit the use of backyards and will make existing solar panels ineffective.

General Section, Residential Development Principles 10, 11 and 12 state:

**10 The design and location of buildings should ensure that direct winter sunlight is available to adjacent dwellings, with particular consideration given to:**

- (a) windows of habitable rooms, particularly living areas**
- (b) ground-level private open space**
- (c) upper-level private balconies that provide the primary open space area for any dwelling**
- (d) access to solar energy.**

**11 Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.**

**12 Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:**

- (a) half of the existing ground-level open space**
- (b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).**

The subject land and neighbouring allotments travel east to west from Warwick Avenue.

18 Warwick Avenue is situated north of the proposed development and would not be overshadowed by the proposal given the northern path of the sun.

14 Warwick Avenue is situated south of the subject land and would receive some shadowing on June 21 (winter solstice).

The shadow diagrams originally prepared have been reviewed and amended. It is acknowledged that the solar panels on the roof of 14 Warwick Avenue were inadvertently drawn incorrectly.

The amended shadow diagrams portray the shadow cast as at 21 June (winter solstice) at the following times:

- 9:30am
- 12:00pm
- 2:00pm
- 3:00pm

The shadow diagrams reveal that some of the solar panels are marginally overshadowed at 3:00pm when the sun is low in the sky during the winter solstice (worst case scenario). For the majority of the year, the sun will be higher in the sky and the development will not overshadow the solar panels at all.

It must also be acknowledged that:

- The upper level component is setback 4 metres from the boundary shared with 14 Warwick Avenue. This exceeds the minimum Development Plan guideline by 2 metres or 50 percent.
- The development is within a policy area that allows three-storey development up to 12.5 metres. The proposed building will be 2 storeys with an approximate total height of 6.8 metres. The proposed height is therefore 5.7 metres, or 45.6%, below the allowable maximum building height prescribed in the policy area.

In accordance with the Development Plan and acknowledging the intent of the policy area to provide increased density and building height, direct winter sunlight remains available to 14 Warwick Avenue to provide sufficient access to solar energy.

In addition, the shadow diagrams reveal that 14 Warwick Avenue will receive more than 2 hours of direct sunlight to more than half of the existing ground-level open space.

### Site Coverage

One of the representors has indicated that they believe the proposal exceeds maximum site coverage.

Principle 5 of the Policy Area guides a maximum site coverage of 70 percent. Principle 5 defines site coverage as *"the area of a site covered by the ground floor level of a building, including the dwelling, garage, carport and outbuilding, but excluding unroofed balconies, verandas and pergolas"*.

The relevant floor areas of each dwelling are shown in the table below.

	Dwelling 1	Dwelling 2	Dwelling 3	Dwelling 4	Dwelling 5
<b>Ground Floor Living (square metres)</b>	46.2	43.6	43.3	43.3	43.5
<b>Garage (square metres)</b>	37.6	37.2	37.2	37.2	37.2
<b>Storage (square metres)</b>	3.3	3.3	3.3	3.3	3.3
<b>Total (square metres)</b>	87.1	84.1	83.8	83.8	84

The ground floor area of each dwelling is not large and is reasonable in providing a high quality living environment for future occupants.

The entire building and the associated storage spaces present a total site coverage of 46.8 percent. This is 23.1 percent less than the 70 percent maximum site coverage allowed by Principle 5.

The proposed site coverage is completely appropriate in the context of the policy area.

### **Car Parking**

One of the representors has stated that the development has insufficient car parking.

The proposal has 10 on-site car parking spaces which is suitable for the occupants of each dwelling. It is acknowledged that the Development Plan guides that 1 on-site visitor car parking space should be provided.

With the guidance of council staff, we have amended the proposal to maximise available on-street car parking by removing an independent driveway to service dwelling 1.

In support of the 1 space car parking shortfall, it is worthy to note the following:

- The proposal now retains 11.49 metres of the overall frontage as upright kerb. This is the equivalent of 2 on-street car parking spaces directly in front of the subject land.
- Demand for visitor car parking is typically at night time (during the week) and on weekends.
- The land is approximately 250 metres from a bus stop and therefore public transport usage is viable.
- The land is approximately 188 metres from a District Centre Zone that accommodates a variety of established shops and encourages walking.
- The land is approximately 160 metres from an Urban Corridor Zone.
- Each dwelling is capable of storing a bicycle.

For these reasons, proposed on-site car parking is sufficient.



### Noise and Air Pollution

One of the representors has stated *"emissions from so many vehicles in such a confined space will result in a significant source of air pollution"*.

General Section, Interface between Land Uses, Principle 1 states:

**Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:**

- (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants**
- (b) noise**
- (c) vibration**
- (d) electrical interference**
- (e) light spill**
- (f) glare**
- (g) hours of operation**
- (h) traffic impacts.**

The proposed land use is strictly residential and at a density that is envisaged by the zone and policy area. As such, any noise, odour, smoke, fumes or air pollution will be likened to general domestic uses at the density anticipated by the Development Plan.

Any vehicles accessing the property will be domestic in nature. The driveway will be formed with concrete or paving to ensure a safe and durable surface that is largely free from mud and dust. A fence of a typical height will distinguish the allotment boundaries, separate the land from neighbouring properties and assist in containing vehicle lights and glare. Vehicle movement using the driveway will be very slow.

Any fixed lighting will be directed to prevent light spill.

The land use will not generate vibration or electrical interference.

There is no evidence to suggest that the proposal will present unreasonable emissions or noise levels.

### Sewerage

One of the representors has stated *"the current public sewers are inadequate for such a development"*.

A land division application that reflects the proposal has been lodged with the City of West Torrens. The South Australian Water Corporation is a referral body as part of the land division's assessment. Council has not received advice from the South Australian Water Corporation that suggests existing sewer services are unsuitable for 5 additional allotments as proposed.

The proposed development will be appropriately serviced in accordance with the Development Plan.

### Stormwater Management

A stormwater management plan that suitably satisfies council's guidelines is being prepared by KP Squared Engineering. This will be submitted as soon as possible.

### Storage

One of the representors has stated *"the proposed storage is inadequate and insufficient for the proposed development"*.

General section, residential development, Principle 31 states:

**A dwelling should incorporate a minimum storage area of 8 cubic metres for goods... within... a non-habitable room of the dwelling, a garage, carport or outbuilding.**

Each dwelling would be provided with a detached storage area at the western end of the development. Each store would have an area of 8 cubic metres. In addition, bedrooms would have built-in/walk-in robes and each garage would provide some additional storage space at the far end.

The proposal satisfies the Development Plan with regard to storage.

### Decreased Land Value

An effect upon land values is not contemplated by the Development Plan and therefore is not a valid planning matter in this circumstance.

### Conclusion

I trust I have addressed, in sufficient detail, the concerns raised by the representors. I confirm that I will be available to appear at the CAP (Council Assessment Panel) meeting to respond to the verbal representations.

In the meantime, we look forward to your support acknowledging that this application has sufficient planning merit in order to warrant Development Plan Consent.

If you have any questions in respect of this matter, please feel free to call me on 8333 7999.

Yours sincerely



Philip Harnett

**Associate**



STATE  
COMMISSION  
ASSESSMENT  
PANEL

A COMMITTEE OF THE STATE PLANNING COMMISSION

Contact Planning Services  
Telephone 7109 7016  
Email [didptipdclearanceletters@sa.gov.au](mailto:didptipdclearanceletters@sa.gov.au)

State Commission  
Assessment Panel

2 February 2018

Mr. Terry Buss  
City Manager  
City of West Torrens  
165 Sir Donald Bradman Dr.  
HILTON SA 5033

Level 5  
50 Flinders Street  
Adelaide SA 5000

GPO Box 1815  
Adelaide SA 5001

Dear Sir/Madam

**Re: Proposed Application No. 211/C127/17 (ID 58902) Amended Plan 30.1.18  
for Land Division (Community Title Plan) by Ahmed Panjvani & Ms Renish Satani**

08 7109 7061

I refer to the enclosed application received at this office and advise that the State Commission Assessment Panel has no report to make to Council in accordance with Regulation 29 of the Development Regulations.

The Commission is of the view that there are no planning impacts of State significance associated with the application, and accordingly have only consulted with the SA Water Corporation pursuant to Regulation 29 (3).

While the Commission is making no report on the application, there may be local planning issues which Council should consider prior to making its decision on the application.

I further advise that the Commission has the following requirements under Section 33 (1) (c) of the Development Act. These requirements must be included as conditions of approval on the Council's Decision Notification (should such approval be granted).

1. The financial requirements of the SA Water Corporation shall be met for the provision of water and sewerage services (SA Water H0061890).
2. Payment of \$27320 into the Planning and Development fund (4 lots(s) @ \$6830 /lot). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the State Planning Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person by cheque or card, at Level 5, 50 Flinders Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the State Commission Assessment Panel for Land Division Certificate purposes.

The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

For SA Water to assess this application, the developer must advise SA Water the preferred servicing option. Information can be found at: <http://www.sawater.com.au/developers-and-builders/building-developing-and-renovating-your-property/subdividing/community-title-development-factsheets-and-information> For queries call SAW Land Developments on 74241119

**IT IS ALSO REQUIRED THAT COUNCIL PROVIDE THE STATE COMMISSION ASSESSMENT PANEL WITH:**

- a) the date on which any existing building(s) on the site were erected (if known);
- b) the postal address of the site; pursuant to Regulation 60 (4) (b) (ii).

**IT IS RECOMMENDED THAT THIS INFORMATION BE INCORPORATED INTO COUNCIL'S ADVICE WHEN REPORTING THAT THEIR REQUIREMENTS (IF ANY) HAVE BEEN FULLY SATISFIED.**

**PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.**

Yours faithfully

A handwritten signature in black ink, appearing to read 'Brett Miller'.

Brett Miller  
**TEAM LEADER – PLANNING SERVICES**  
as delegate of the  
**STATE COMMISSION ASSESSMENT PANEL**





02 February 2018

Our Ref: H0061890

The Chairman  
Development Assessment Commission  
136 North Terrace  
ADELAIDE SA 5000

Dear Sir/Madam

**PROPOSED LAND DIVISION APPLICATION NO: 211/C127/17 AT KURRALTA PARK**

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

Yours faithfully

rita demusso

**for MANAGER LAND DEVELOPMENT & CONNECTIONS**

SA Water  
Level 6, 250 Victoria Square  
ADELAIDE SA 5000  
Ph (08) 7424 1119  
Inquiries rita demusso  
Telephone 7424 1119

**From:** Jane Teng  
**Sent:** Thursday, 15 March 2018 9:25 AM  
**To:** Josh Banks  
**Subject:** City Assets feedback on Stormwater Management Plan: : April CAP - 16 Warwick Ave, Kurralta Park

Hi Josh,

I refer to the provided stormwater calculations ( Job No: 180304, Mar' 18) and the Civil & Drainage Plan (KP Squared Engineering, Project No: 180305, Drawing No.C2, Issue A) .

In accordance with the requirement set out in City Assets original feedback referral, the stormwater management considerations for the development has been assessed as satisfactory.

Could we please include the following conditions as part of the planning approval conditions for the site.

- The nominated stormwater water quality device (SPEL Stormceptor Model 100/15) shall be maintained and cleaned in accordance with the manufacturers best practice recommendations.
- A minimum 5KL trafficable underground detention storage tank is to be provided for the site as annotated on the proposed the Civil & Drainage Plan (KP Squared Engineering, Project No: 180305, Drawing No.C2, Issue A) .
- The stormwater discharge from the detention tank to be limited to 6L/s at all times.

Should you have any enquires, please contact me.

Regards,

**Jane Teng**  
**Civil Engineer**  
City of West Torrens  
165 Sir Donald Bradman Drive  
Hilton SA 5033

**6.3 11 Birkalla Terrace, PLYMPTON**

Application No 211/1368/2017

Appearing before the Panel will be:

Representors: **Teresa and Mark Wallace** of 12 Birkalla Terrace, Plympton wish to appear in support of their representation.

Applicant/s **Anthony Donato** of Anthony Donato Architects wishes to appear to respond to the representations.

**DEVELOPMENT APPLICATION DETAILS**

DEVELOPMENT PROPOSAL	Combined application - Community Title land division creating four (4) additional allotments and common property and construction of a two storey residential flat building comprising five (5) dwellings with associated landscaping
APPLICANT	Mr Anthony Donato
APPLICATION NO	211/1368/2017 DAC Reference: 211/C180/17
LODGEMENT DATE	27 October 2017
ZONE	Residential Zone
POLICY AREA	Medium Density Policy Area 18
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 2
REFERRALS	External <ul style="list-style-type: none"> <li>▪ SA Water</li> <li>▪ DAC</li> </ul> Internal <ul style="list-style-type: none"> <li>▪ City Assets</li> </ul>
DEVELOPMENT PLAN VERSION	30 May 2017
MEETING DATE	10 April 2018

**RECOMMENDATION**

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent and Land Division Consent for Application No. 211/1368/2017 by Mr Anthony Donato to undertake a Combined application - Community Title land division creating four (4) additional allotments and common property and construction of a two storey residential flat building comprising five (5) dwellings with associated landscaping at 11 Birkalla Terrace, Plympton (CT 5715/438) subject to the following conditions of consent:

**Development Plan Consent Conditions**

1. That the development shall be undertaken and completed in accordance with the plans and information detailed in this application including the community division plan by Cavallo Forest Licensed surveyor, Ref No 17-289, Rev 02; Site plan by GAMA Consulting, Drawing No 17581-C01 Revision D; Anthony Donato Architects Job No. 4227-17, Sheet No DD01-DD11 Dated Sep. 2017 except where varied by any condition(s) listed below.



2. All driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) and properly drained, and shall be maintained in a reasonable condition at all times.
3. All planting and landscaping will be completed within three (3) months of the occupancy of the development. Any plants that become diseased or die will be replaced with a suitable species.
4. All upper level windows as described on the southern elevation of Dwelling 5 and all upper level windows to the western elevation shall be fitted with fixed obscure glass or raised sill height to a minimum height of 1.7 metres above the upper floor level to minimise the potential for overlooking of adjoining properties, prior to occupation of the building. The glazing in these windows shall be maintained in a reasonable condition at all times to the satisfaction of Council.
5. All stormwater design and construction will be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and for this purpose stormwater drainage will not at any time:
  - a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create unhealthy or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; and not flow across footpaths or public ways.

In addition to the above, Council requires:

- e) installation of a 3,000 litre stormwater collection and re-use tank;
  - f) the stormwater collection and re-use tank to be plumbed to deliver recycled water to all toilets and laundry cold water outlet (can also connect to hot water service);
  - g) a minimum of 90% of the dwelling roof area is to be directed to the stormwater collection and re-use tank;
  - h) mains water backup, pump and plumbing arrangements as typically required to support such an installation are to be compliant with the standard Building Code requirements associated with a compulsory rainwater tank installation; and
  - i) the stormwater collection and re-use system is to be installed and operational prior to the occupancy of the dwelling.
6. Any retaining walls are to be designed to accepted engineering standards, and not of timber construction if retaining a difference in ground level exceeding 200mm.
  7. All external materials and finishes must be non-reflective.
  8. Prior to the issue of certificates to the land division approved herein, all existing buildings and associated structures shall be removed from the subject land.

**Land Division Consent Conditions:**

Nil

## Development Assessment Commission Conditions

9. The financial requirements of the SA Water shall be met for the provision of water supply and sewerage services.

For SA Water to assess this application, the developer must advise SA Water the preferred servicing option. Information can be found at: <http://www.sawater.com.au/developers-and-builders/building-developing-and-renovating-your-property/subdividing/community-title-development-factsheets-and-information>. For queries call SAW Land Developments on 74241119. An investigation will be carried out to determine if connections to the development will be standard or nonstandard.

The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

10. Payment of \$27,320 into the Planning and Development fund (4 lots @ \$6,830/lot). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
11. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.

## BACKGROUND

The development proposal is presented to the Council Assessment Panel (CAP) for the following reasons:

- With regard to residential development and land division applications, where all proposed allotments and/or sites fail to meet, nor are within 5% of, the minimum frontage widths and site areas designated in respective zones and policy areas within the West Torrens Development Plan;
- All Category 2 or 3 applications where a representor has requested to be heard shall be assessed and determined by the CAP.

## PREVIOUS OR RELATED APPLICATION(S)

Development Number	Proposal	Status
211/1175/2017	The construction of a residential flat building comprising five (5) two storey dwellings with rear verandahs and single garages under main roof	Withdrawn and this combined application was lodged

## **SITE AND LOCALITY**

The subject land is a residential property situated at 11 Birkalla Terrace, Plympton. It is formally described in Certificate of Title Volume 5715 Folio 438, comprising allotment 23 in Deposited Plan 3106 in the area named Plympton. The land has a frontage to Birkalla Terrace of 18.69 metres with an overall site area of 928m<sup>2</sup>. A single storey detached dwelling and a number of outbuildings exist on the site. There are a few (non-regulated) trees on the subject land. The land has a rise to the rear of about 300mm.

The locality is an established residential area that is situated between the main arterial corridors of Anzac Highway to the southeast and the Westside Bikeway linear park to the northwest. The built form character is mixed with detached dwellings at low densities, residential flat buildings and semi-detached dwellings. The allotment pattern has been broken up through a variety of older and more recent land divisions within the locality.

The amenity of the locality is considered to be moderate given the mix of allotment sizes and the ageing housing stock. There are few well established trees in the locality other than those lining Anzac Highway.






The subject site and locality are shown on the following maps:









-  School  
 Post Office  
 Other Health Services  
 Railways  
 Local Reserves  
 Development Plan Boundary

**WEST TORRENS COUNCIL**  
Consolidated - 30 May 2017

## PROPOSAL

The proposed development is summarised as follows:

### *Land Division*

A Community Title division is proposed to create four additional allotments and common property. The proposed allotments have the following site areas and frontages:

Lot	Site Area	Frontage
1	165m <sup>2</sup>	10.9 metres
2	122m <sup>2</sup>	Nil
3	120m <sup>2</sup>	Nil
4	120m <sup>2</sup>	Nil
5	137m <sup>2</sup>	Nil
Common property	315m <sup>2</sup>	7.8 metres

### *Built form*

The construction of a two storey residential flat building containing 5 dwellings is also proposed with each dwelling to incorporate a rear verandah and single garage under the main roof.

The floor plan of each dwelling contains a single covered carport and stacked visitor space, an open dining area, living area, kitchen, laundry, toilet and alfresco area on the ground floor, and 3 bedrooms, bathroom, retreat, ensuite and balcony on the upper floor. The total floor area for each dwelling is as follows:

Dwelling 1 - 171.6m<sup>2</sup>

Dwelling 2 - 163.3m<sup>2</sup>

Dwelling 3 - 163.3m<sup>2</sup>

Dwelling 4 - 169.5m<sup>2</sup>

Dwelling 5 - 160.5m<sup>2</sup>

The material finishes of the dwellings consist of rendered hebel, scyon cladding to the balconies, aluminium windows, timber doors, custom orb metal roof sheeting at 22.5 degrees with 350mm wide eaves. Colours are light and dark shades of grey, more specifically Colorbond 'Basalt' for the roof, and paint colours Solver 'Calcium', 'Urban Tribe' and 'Low Cloud' with timber doors and balcony posts.

Vehicular access is to be gained via a common driveway using an existing crossover that will need to be widened.

Landscaping consists of a number of trees (prunus), low shrubs and ground cover covering the area to the front of proposed dwelling 1, along the common driveway and to the rear of the visitor park behind dwelling 5.

A copy of the proposal plans is contained in **Attachment 1**.



## PUBLIC NOTIFICATION

The application is a Category 2 form of development pursuant to Schedule 9, Part 2 (18) (a) of the *Development Regulations 2008*.

<b>Properties notified:</b>	Nine (9) properties were notified during the public notification process.
<b>Representations:</b>	One (1) representation was received.
<b>Persons wishing to be heard:</b>	The one representor identified that they wish to address the Panel. <ul style="list-style-type: none"><li>• Teresa and Mark Wallace</li></ul>
<b>Summary of Representations:</b>	Concerns were raised regarding the following matters: <ul style="list-style-type: none"><li>• privacy being impacted due to dwellings in close proximity;</li><li>• overlooking in their backyard;</li><li>• noise impacting their residence from many dwellings next door to bedrooms.</li></ul>

The Applicant has provided a response to the representation, as summarised below:

- The design incorporates obscure glass to 1.7 metres high above first floor level to all windows facing existing dwellings therefore there will be no privacy issues;
- All walls will be insulated in accordance with building code requirements so there should not be any issues of noise emanating from the development. Any noise will be the same as for other neighbouring properties;
- Council zoning allows for 5 dwellings for this site, therefore it is unreasonable for the neighbour to say that there are too many dwellings proposed for the site.

A copy of the representor's concerns and the applicant's response is contained in **Attachment 2**.

## REFERRALS

### ***Internal***

City Asset's comments are summarised below:

- The driveway servicing the rear of the site needs revision (widened);
- The vehicle manoeuvrability is unsatisfactory;
- Finished floor levels are satisfactory;
- Civil plan needs revision to show a widened crossover;
- Recommend a condition with respect to retention and detention requirements for the site.

Revised plans were submitted to Council addressing these concerns and are to the satisfaction of City Assets.

A copy of the relevant report is contained in **Attachment 3**.

### ***External***

- SA Water

SA Water has raised no concerns with the proposal. The developer will be required to meet the requirements of SA Water for the provision of water and sewerage services. Standard conditions of consent have been recommended should the Panel support the application.

- Development Assessment Commission (DAC)

DAC has raised no concerns with the proposal. Standard conditions of consent have been recommended should the Panel support the application.

A full copy of the relevant reports are contained in **Attachment 4**.

## ASSESSMENT

The subject land is located within the Residential Zone and more specifically the Medium Density Policy Area 18 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Crime Prevention	Objectives	1
	Principles of Development Control	1, 2, 3
Design and Appearance	Objectives	1
	Principles of Development Control	1, 2, 4, 5, 9, 10, 13, 15, 20, 22
Energy Efficiency	Objectives	1
	Principles of Development Control	1, 2, 3, 4
Infrastructure	Objectives	1
	Principles of Development Control	1, 2, 3, 4
Land Division	Objectives	1, 2, 3, 4
	Principles of Development Control	1, 2, 4, 5, 6, 8, 12
Landscaping, Fences and Walls	Objectives	1, 2
	Principles of Development Control	1, 2, 3, 4, 6
Orderly and Sustainable Development	Objectives	1, 2, 3, 4, 5
	Principles of Development Control	1
Residential Development	Objectives	1, 2, 3, 4
	Principles of Development Control	1, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 16, 18, 19, 20, 21, 27, 28, 30, 31
Transportation and Access	Objectives	2
	Principles of Development Control	1, 10, 11, 14, 23, 24, 32, 34, 35, 36, 44, 45
Waste	Objectives	1
	Principles of Development Control	1, 2, 3

<b>Zone: Residential Zone</b>	
<b>Desired Character Statement:</b>	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
<b>Objectives</b>	1, 2
<b>Principles of Development Control</b>	1, 5, 6, 7, 8, 9, 10, 11, 12, 13

<b>Policy Area: Medium Density Policy Area 18</b>	
<b>Desired Character Statement:</b>	
<p><i>Allotments in this policy area will be at medium density, accommodating a range of dwelling types including residential flat buildings, row dwellings, group dwellings, semi-detached dwellings and some detached dwellings on small allotments. Allotment amalgamation to create larger development sites will occur to maximise the density of development while also achieving integrated design outcomes, particularly within a comfortable walking distance of centre zones. Vehicle access will occur from side streets and new rear public and private laneways wherever possible, also supporting the retention of existing street trees.</i></p> <p><i>New buildings will contribute to a highly varied streetscape. Buildings will be up to 3 storeys and provide a strong presence to streets, other than in the part of the policy area in Underdale, Ashford (other than allotments adjacent to Residential Character Ashford Policy Area 22) and allotments bounded by Anzac Highway, Morphett Road and Cromer Street in Camden Park where buildings will be up to 4 storeys. Parking areas and garages will be located behind the front facade of buildings.</i></p> <p><i>Buildings on the edge of the policy area which adjoin residential policy areas at lower densities will pay particular attention to managing the interface with adjoining dwellings, especially in terms of the appearance of building height and bulk, and overshadowing.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
<b>Objectives</b>	1
<b>Principles of Development Control</b>	1, 1, 4, 5, 6



## QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
SITE AREA <i>Medium Density Policy Area 18</i> PDC 6	150m <sup>2</sup> (average)	132.8m <sup>2</sup> <b>Does not Satisfy</b>
SITE FRONTAGE <i>Medium Density Policy Area 18</i> PDC 6	Minimum 15 metres (for complete building)	18.7 metres <b>Satisfies</b>
SITE COVERAGE <i>Medium Density Policy Area 18</i> PDC 5	70% maximum	52.2% <b>Satisfies</b>
BUILDING HEIGHT <i>Medium Density Policy Area 18</i> PDC 5	Maximum of 3 storeys or 12.5 metres	The building is 2 storeys or 7.8 metres <b>Satisfies</b>
STREET SETBACK <i>Medium Density Policy Area 18</i> PDC 5	Minimum 3 metres	2.7 metres <b>Does not Satisfy</b>
SIDE SETBACKS <i>Residential Zone</i> PDC 11	1 metre minimum where the vertical side wall is 3 metres or less	<u>Western setback</u>  Dwelling 1 - 680mm Dwelling 2 - 680mm Dwelling 3 - 680mm Dwelling 4 - 680mm Dwelling 5 - 680mm  <b>Does not Satisfy</b>  <u>Eastern setback (driveway side)</u>  Dwelling 1 - 6.1 metres Dwelling 2 - 4.9 metres Dwelling 3 - 4.9 metres Dwelling 4 - 4.9 metres Dwelling 5 - 6.4 metres  <b>Satisfies</b>

<p><b>SIDE SETBACKS</b>  <i>Residential Zone</i>  <b>PDC 11</b>  <i>(continued)</i></p>	<p>2 metres minimum where the vertical side wall measures between 3 metres and 6 metres</p>	<p><u>Western setback</u></p> <p>Dwelling 1 - 2.2 metres  Dwelling 2 - 3.4 metres  Dwelling 3 - 3.4 metres  Dwelling 4 - 3.4 metres  Dwelling 5 - 1.7 metres</p> <p><b>Does not Satisfy</b></p> <p><u>Eastern setback (driveway side)</u></p> <p>Dwelling 1 - 6.1 metres  Dwelling 2 - 4.9 metres  Dwelling 3 - 4.9 metres  Dwelling 4 - 4.9 metres  Dwelling 5 - 6.4 metres</p> <p><b>Satisfies</b></p>
<p><b>REAR SETBACK</b>  <i>Medium Density Policy Area 18</i>  <b>PDC 5</b></p>	<p>Minimum 4 metres</p>	<p>4.1 metres</p> <p><b>Satisfies</b></p>
<p><b>PRIVATE OPEN SPACE</b>  <i>Residential Development</i>  <b>PDC 19</b></p>	<p>Minimum 24m<sup>2</sup> with a minimum dimension of 3 metres for allotments up to 300m<sup>2</sup></p>	<p>Dwelling 1 - <b>44m<sup>2</sup></b>  Dwelling 2 - <b>29.8m<sup>2</sup></b>  Dwelling 3 - <b>30.6m<sup>2</sup></b>  Dwelling 4 - <b>30.6m<sup>2</sup></b>  Dwelling 5 - <b>54m<sup>2</sup></b></p> <p>These areas include balcony areas</p> <p><b>Satisfies</b></p>
<p><b>LANDSCAPING</b>  <i>Landscaping, Fences &amp; Walls</i>  <b>PDC 4</b></p>	<p>Minimum 10% of the site</p>	<p>21.4%</p> <p><b>Satisfies</b></p>
<p><b>CARPARKING SPACES</b>  <i>Transportation and Access</i>  <b>PDC 34</b></p>	<p>At least 2 car-parking spaces per dwelling, one of which is covered and an additional 0.25 per dwelling</p>	<p>2 per dwelling, both undercover and 1 visitor space</p> <p><b>Satisfies</b></p>

OVERSHADOWING <i>Residential Development</i> PDC 12	Development should ensure that ground-level open space of existing buildings receive direct sunlight for a minimum of two hours between 9.00am and 3.00pm on 21 June to at least the smaller of the following: a) half of the existing ground level open space b) 35m <sup>2</sup> of the existing ground level open space (with at least one of the area's dimensions measuring 2.5 metres)	The overshadowing diagram shows 10 Birkalla Terrace as the main affected property in regards to overshadowing.  More than 50% of the adjacent site is shade free at 12pm and 100% of the site is shade free area at 3pm on 21 June.  <b>Satisfies</b>
DOMESTIC STORAGE <i>Residential Development</i> PDC 31	Minimum 8m <sup>3</sup>	Each dwelling has 8.9m <sup>3</sup> (understair storage)  <b>Satisfies</b>

## QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the most relevant considerations are discussed under the following sub headings:

### Site area

The average site area for a dwelling within a residential flat building should be 150m<sup>2</sup> (PDC 6 of Medium Density Policy Area 18). This development for 5 dwellings falls short of this by 17.2m<sup>2</sup>, which is considered minor. The proposal has appropriately demonstrated that the allotments can be functionally developed whilst providing adequate setbacks, private open space, on-site carparking and landscaping.

### Desired character

Objective 1 and PDC 1 (Form and Character) of Medium Density Policy Area 18 seek development that is consistent with the desired character of the policy area. The proposed development is consistent with this requirement in terms of providing medium density development in the form of a residential flat building with suitable site areas. It contributes to a range of dwelling types within the locality and is a building that will contribute to a highly varied streetscape.

While the development site is not within 400 metres of a Centre Zone, it is adjacent the buffer which extends to the middle of Birkalla Terrace. The site is in close proximity to the Westside Bikeway and Anzac Highway which provide options for alternative modes of transport which would give future residents travel choices that are not reliant on the motor vehicle.

### Built form

The development presents well to the street in terms of achieving articulation, with a variety of building setbacks and eaves that help shade the building. These features are complemented by a mix of colours and materials. At 7.7 metres, the height of the building is considered reasonable and similar to other development in the locality, particularly along Anzac Highway.



The mass and proportion of the building is designed well in terms of having a pitched roof, eave overhang and variation of setbacks at both ground and upper levels with balconies and a mix of colours and materials that provide visual interest. Overall the development is consistent with PDC 4 of the Residential Development section of the Development Plan. Dwelling 1 has a front door that overlooks and is readily identifiable to the street and Westside Bikeway, consistent with PDC 8 of the Residential Development section.

## **Setbacks**

### *Front*

The front setback of a dwelling in the Medium Density Policy Area 18 should be at least 3 metres. There are areas of the front façade of the proposed building that are setback within 3 metres, including a corner of the alfresco area of Dwelling 1 and the balcony element. The minimum setback of these elements is 2.7 metres.

The setback distance increases to the southwest since the front property boundary is angled and not perpendicular to the side boundaries. The corner of the building adjacent the driveway has a front setback of 5.8 metres. Given the angled boundary, the staggered front setback achieves, on average, a distance greater than 3 metres. Accordingly, the front setback is considered satisfactory.

### *Rear*

The residential flat building is oriented toward Birkalla Street with the rear setback toward 329 Anzac Highway. The rear setback is greater than 4 metres and meets the Development Plan requirement (PDC 5 of the Medium Density Policy Area 18). The space between the built form and the rear boundary provides landscaping and a visitor space. The landscaping will assist in softening the southern side of the residential flat building.

### *Side*

PDC 11 of the Residential Zone requires dwellings to achieve side setbacks of at least 1 metre for single storey components and at least 2 metres for two storey components. The side setbacks for the eastern or driveway side comply with this requirement for both the single storey and two storey components.

The western side setback adjacent 10 Birkalla Terrace falls short due to the closer location of the single garages and part of the upper level of dwelling 5. Dwellings 1 to 5 have a staggered setback at ground level with the garage portion closest to the side boundary. However these garages are adjacent the carport, driveway and large shed at 10 Birkalla Terrace and are unlikely therefore to create unreasonable amenity impacts on that property. Considering also the location of the private open space behind the dwelling at 10 Birkalla Terrace, the smaller setbacks on this side of the proposed development are considered reasonable.

The setback of the upper level of Dwelling 5 fails to meet the requirement by 0.3 metres. This shortfall can be attributed to a narrow portion of the covered alfresco area which is also adjacent to the large shed at 10 Birkalla Terrace.

When considering the location of the proposed built form, the staggered side setbacks and how the design addresses privacy and impacts of overshadowing, the ground and upper level side setbacks are considered reasonable.

## **Overlooking**

Visual privacy is dealt with in PDC 27 of the General Section - Residential Development module of the Development Plan. The proposed development has been designed with the western and southern elevations having windows and clear doors that are either recessed back 2 metres from the balcony, or have a sill height of 1.7 metres and / or fixed opaque glass up to 1.7 metres from the finished floor level of the upper storey. As a result, there will be minimal direct overlooking from this development. Passive surveillance looking north toward the reserve across Birkalla Terrace will nonetheless be enhanced by the proposal through the orientation of the balcony areas and their connection to the bedrooms. This is consistent with PDC 2 of the General Section - Crime Prevention module.

## **Stormwater management**

The proposed stormwater management includes the installation of a 3,000 litre rainwater tank for each dwelling which will be plumbed to the laundry and toilets for re-use. It is considered that the proposal meets PDC 9 of the General Section - Natural Resources module in terms of stormwater management and has been conditioned accordingly.

## **Landscaping**

The proposed development meets the minimum landscaping requirement of 10% (PDC 4 of the General Section - Landscaping, Fences and Walls) with a variety of planting to be added to the front of the subject site, along both sides of the proposed driveway and to the rear behind the visitor park. The landscaping will assist in softening the appearance and heat impact of the proposed driveway, fencing and built form. From the street, there will be sufficient planting to complement the built form and enhance the appearance of the two storey building. The plantings around the dwellings will also assist in climate control. This is consistent with PDC 1 of the General Section - Landscaping, Fences and Walls. Further, the proposed landscaping will not restrict solar access on any adjoining property (PDC 3 of the General Section - Landscaping, Fences and Walls).

## **Waste collection**

The proposal can be adequately serviced by Council's domestic on street waste collection with sufficient verge space available for ten (10) waste bins. This meets Council's requirements and complies with Objective 2 of the General Section - Waste module of the Development Plan.

## **SUMMARY**

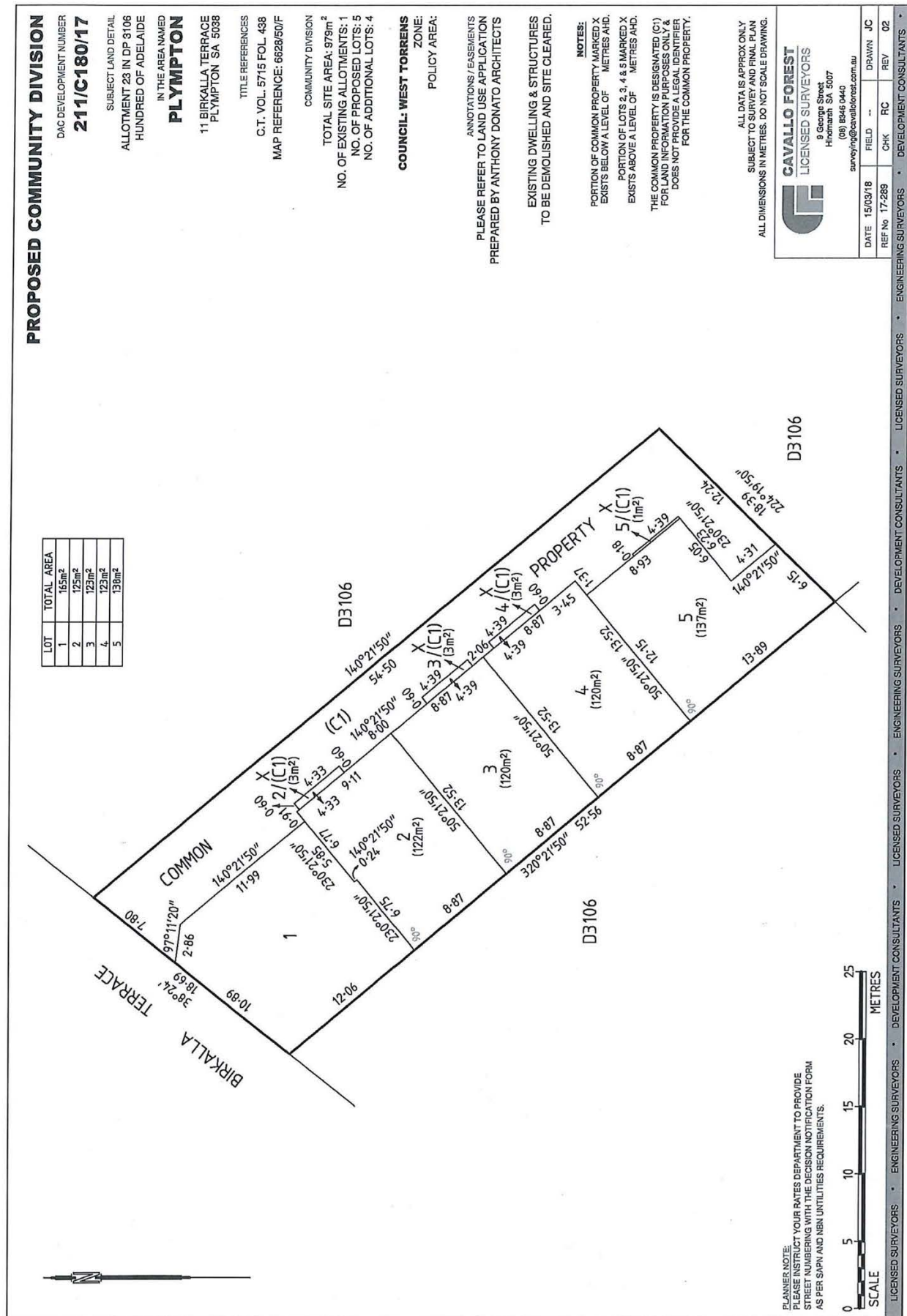
The proposed development satisfies the majority of quantitative provisions of the Development Plan and meets the intent of the Medium Density Policy Area 18. It provides for medium density residential development with a design that appropriately addresses the surrounding locality in terms of building orientation, private open space provision and built form outcome. The proposed design will contribute positively to the streetscape as it is consistent with other dwellings in the locality.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 30 May 2017 and warrants Development Plan Consent and Land Division Consent.

## **Attachments**

1. **Proposal plans**
2. **Representation and response to representation**
3. **Internal referrals**
4. **External referrals**







**EXTERNAL COLOUR SCHEDULE:**

ROOFING	
Sheeting	Colorbond 'Basalt'
Gutters	Colorbond 'Basalt'
Fascias & Barges	Colorbond 'Basalt'
Soffits	Sover 'Calcium'
Gable Ends	Timber finish

## STI/W

Rendered Finish 1	Solver 'Calcaum'
Rendered Finish 2	Solver 'Urban Tribe'
Soyon Stria 1	Solver 'Urban Tribe'
Soyon Stria 2	Solver 'Low Cloud'
Flashing & Cappings	Colorbond 'Basalt'
Fencing	Timber Finish
Columns	Timber Finish

## OPENINGS

Panel Lift Doors	Timber Finish
Front Doors	Timber Finish
Aluminium Frames	Colorbond "Basalt"

**JOB TITLE**  
**PROPOSED RESIDENTIAL DEVELOPMENT**  
**AT: 11 BIRKALLA TERRACE PLYMPTON**

DRAWING TITLE  
MAIN DE  
ED VIEW

## AMENDMENTS

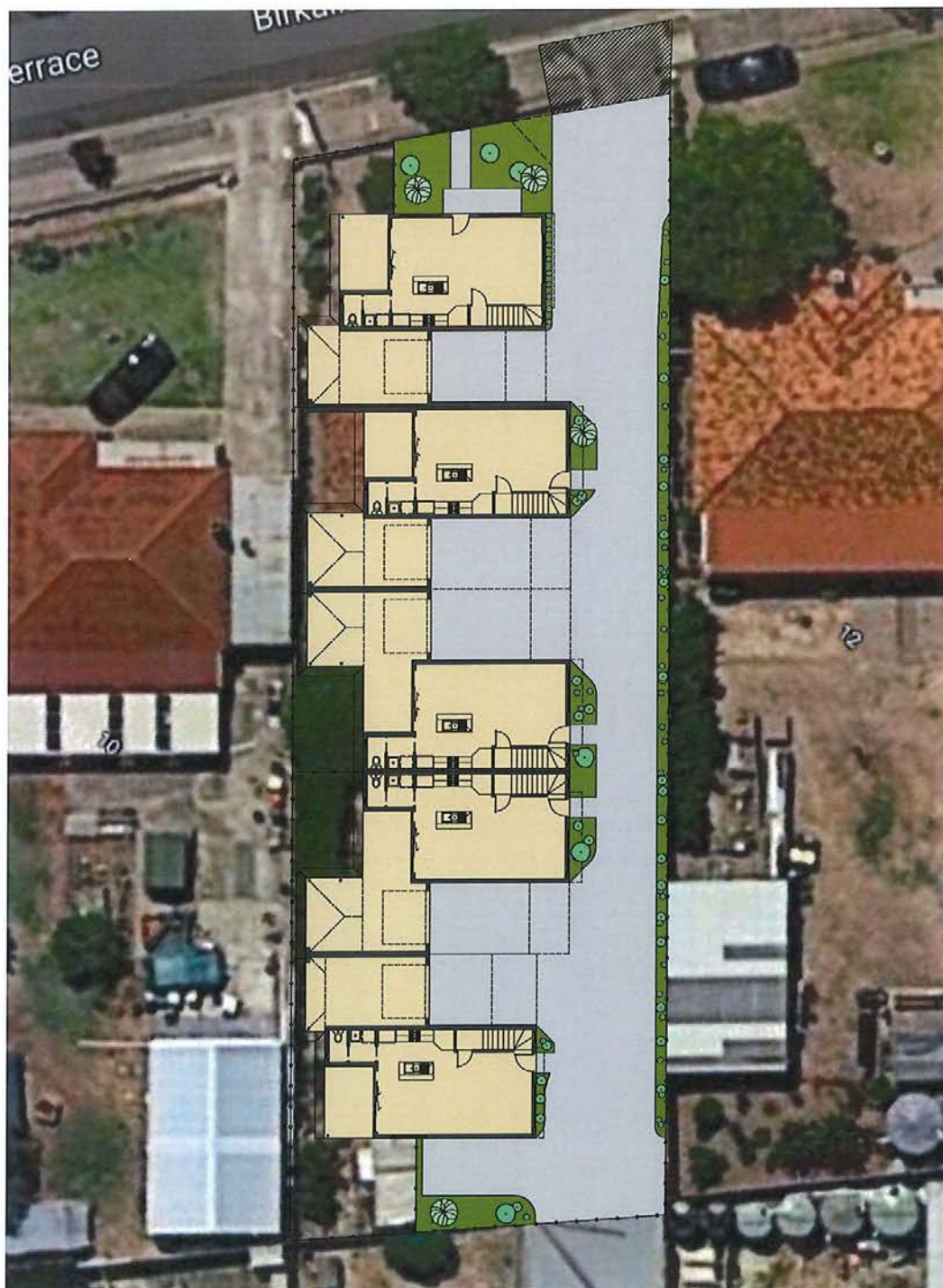
Contractors are to verify all dimensions and levels before commencing any site work or making shop drawings. Figured dimensions shall take preference over dimensioned dimensions and any discrepancy shall be reported to the Architect immediately.



ANTHONY DONATO ARCHITECTS

Suite 6/59 Fullarton Road | Kent Town SA 5087  
 P 08 8364 6888 | F 08 8364 5355 | [www.adarchitects.com.au](http://www.adarchitects.com.au)

JOB NO. 2221-17 SCALE N/A DRAWN DP CHECKED DATE SEP. '17 PAGE A3 SHEET NO. 0001 REVISION	© COPYRIGHT T. 00 8304 8688   T. 00 8304 5055   WWW.AJACITECHSOL.COM.AU
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<p><b>ANTHONY DONATO ARCHITECTS</b>          Suite 603 Fulham Road   Kent Town SA 5007          T 08 8364 6888   F 08 8364 5535   www.donatoads.com.au</p>	<p>Contracting to verify all dimensions and levels before commencing any work or making shop drawings. If any dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the Architect immediately.</p>	<p><b>JOB NO.</b> 4223-TT  <b>SCALE</b> 1:200  <b>DRAWN</b> DP  <b>CHECKED</b> A3  <b>DATE</b> SEP. '17  <b>PAGE</b> A3  <b>SHEET NO.</b> DD02  <b>REVISION</b></p>
<p><b>JOB TITLE</b>          PROPOSED RESIDENTIAL DEVELOPMENT          AT: 11 BIRKALLA TERRACE PLYMPTON</p>	<p><b>DRAWING TITLE</b>          LOCATION PLAN</p>	<p><b>AMENDMENTS</b></p>



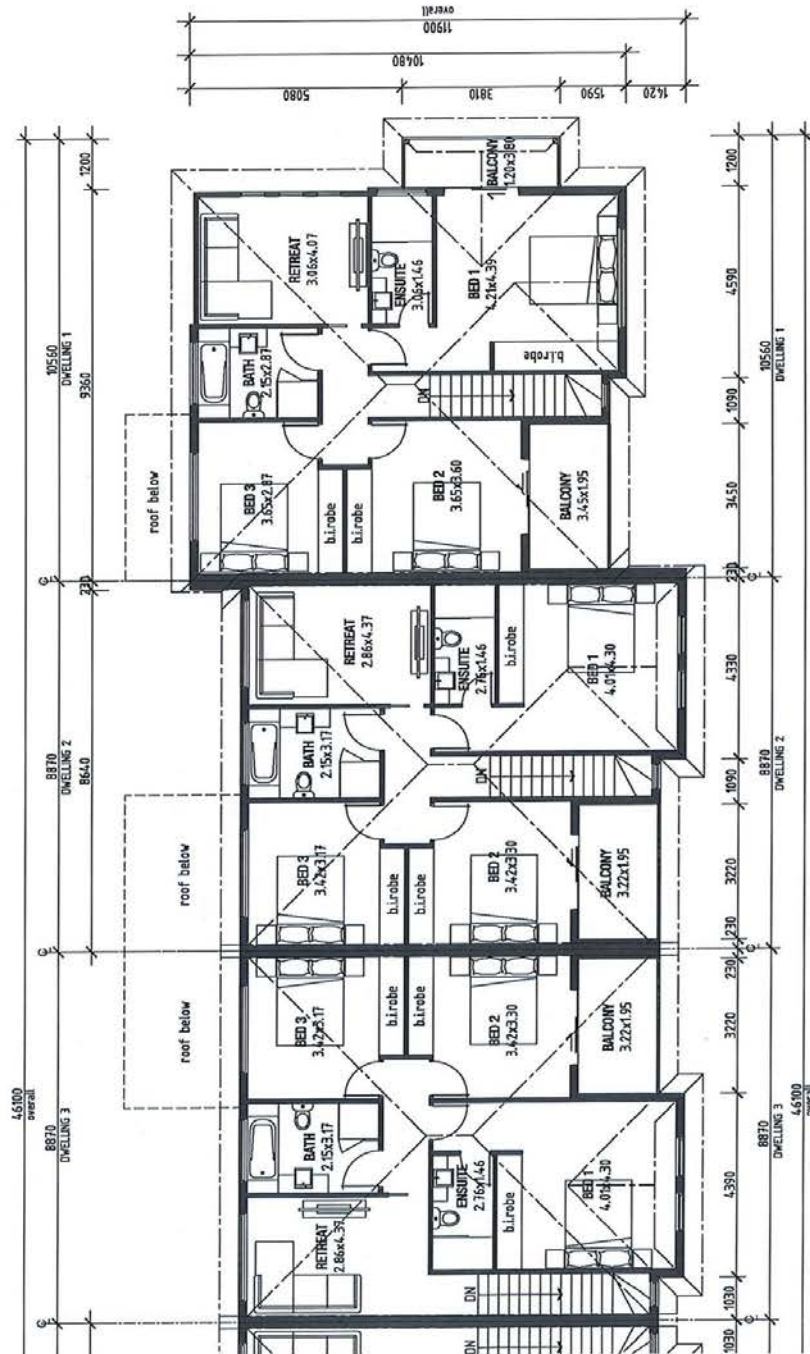






JOB TITLE PROPOSED RESIDENTIAL DEVELOPMENT LOT: 11 BIRKALLA TERRACE PLYMPTON	DRAWING TITLE DWELLINGS 1-3 GROUND FLOOR PLANS	REVISIONS	CONTRACTORS ARE TO VERIFY ALL DIMENSIONS OF THE WORK. ANY DISCREPANCY CONCERNING ANY OF THE WORKING DIMENSIONS OF THE DIMENSIONS SHALL TAKE PRECEDENCE OVER THE DIMENSIONS OF THE DIMENSIONS AND ANY DISCREPANCY SHALL BE REPORTED TO THE IMMEDIATELY.									
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**DWELLING 1.**Building Areas: (m<sup>2</sup>)

Lower Living..... 48.5  
 Upper Living..... 88.3  
 Carport..... 22.8  
 Alfresco..... 8.8  
 Balcony 1..... 4.6  
 Balcony 2..... 7.2

Total Building Area: ..... 180.2

**DWELLINGS 2**Building Areas: (m<sup>2</sup>)

Lower Living..... 46.4  
 Upper Living..... 85.3  
 Carport..... 21.8  
 Alfresco..... 8.3  
 Balcony..... 6.7

Total Building Area: ..... 168.5

**DWELLING 3 & 4**Building Areas: (m<sup>2</sup>)

Lower Living..... 46.4  
 Upper Living..... 85.4  
 Carport..... 22.5  
 Alfresco..... 7.6  
 Balcony..... 6.7

Total Building Area: ..... 168.6

**DWELLINGS 1-3****FIRST FLOOR PLAN**

Scale 1:100 @ A3

Contractors are to verify all dimensions and levels before commencing work or making shop drawings. Figure dimensions shall take preference over scaled dimensions and any discrepancy shall be reported to the architect immediately.

REVISIONS

DRAWING TITLE  
DWELLINGS 1-3  
UPPER FLOOR PLANSJOB TITLE  
PROPOSED RESIDENTIAL DEVELOPMENT  
AT: 11 BIRKALLA TERRACE PLYMPTON**ANTHONY DONATO ARCHITECTS**Suite 8269 Fullarton Road | Kent Town SA 5087  
t 08 8264 6868 | f 08 8264 5255 | www.adarchitects.com.au

JOB NO.	SCALE	DRAWN	CHECKED	DATE	PAGE	SHEET NO.	REVISION
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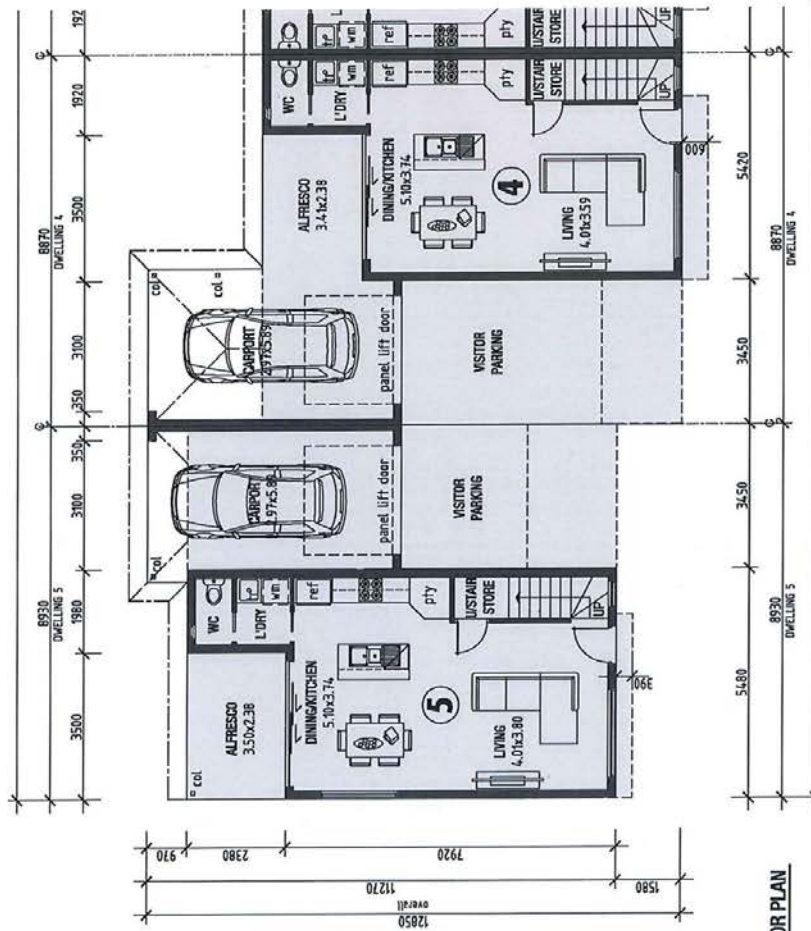
**DWELLING 3 & 4**Building Areas: (m<sup>2</sup>)

Lower Living.....	46.4
Upper Living.....	85.4
Carport.....	22.5
Alfresco.....	7.6
Balcony.....	6.7
<b>Total Building Area:</b> .....	<b>168.6</b>

**DWELLING 5**Building Areas: (m<sup>2</sup>)

Lower Living.....	48.2
Upper Living.....	86.2
Carport.....	21.3
Alfresco.....	8.3
Balcony.....	7.5
<b>Total Building Area:</b> .....	<b>171.5</b>

Upper level shown shaded



**DWELLINGS 4-5**  
**GROUND FLOOR PLAN**  
 Scale 1:100 @ A3

JOB TITLE PROPOSED RESIDENTIAL DEVELOPMENT AT: 11 BIRKALLA TERRACE PLYMPTON	DRAWING TITLE DWELLINGS 4 & 5 GROUND FLOOR PLANS	REVISIONS	 <b>ANTHONY DONATO ARCHITECTS</b> Suite 6/59 Fullarton Road   Kent Town SA 5087 T: 08 8364 6888   F: 08 8364 6205   www.anthonydonato.com.au © COPYRIGHT							
			CONTRACTORS are to verify all dimensions and levels on site prior to commencing any work or making shop drawings. If any discrepancies shall take preference over the dimensions and levels on this drawing, the contractor shall be reported to the Architect immediately.	JOB NO. 4221-17	SCALE 1:100	DIAMN DP	CHECKED SEP '17	PAGE A3	SHEET NO. 0006	REVISION

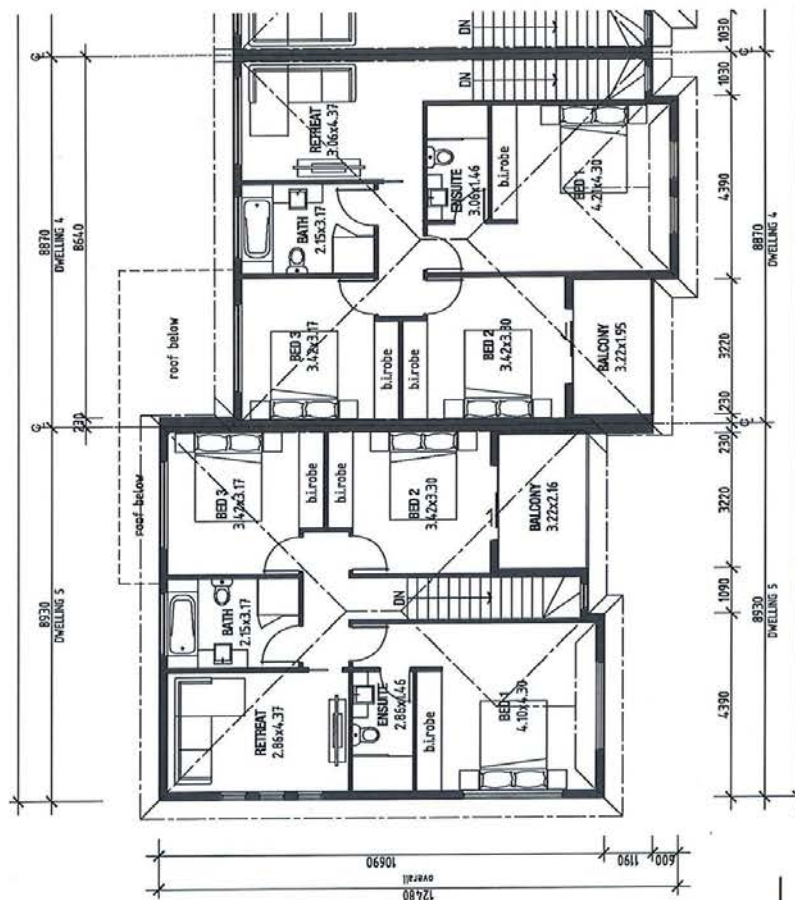


**DWELLING 3 & 4**

Building Areas: (m <sup>2</sup> )	
Lower Living.....	46.4
Upper Living.....	85.4
Carport.....	22.5
Alfresco.....	7.6
Balcony.....	6.7
<b>Total Building Area:</b> .....	<b>168.6</b>

**DWELLING 5**

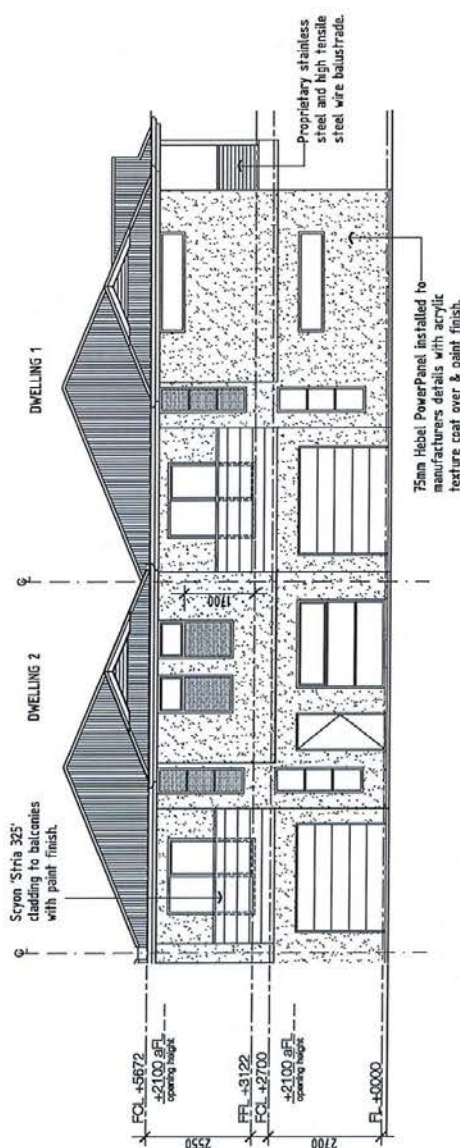
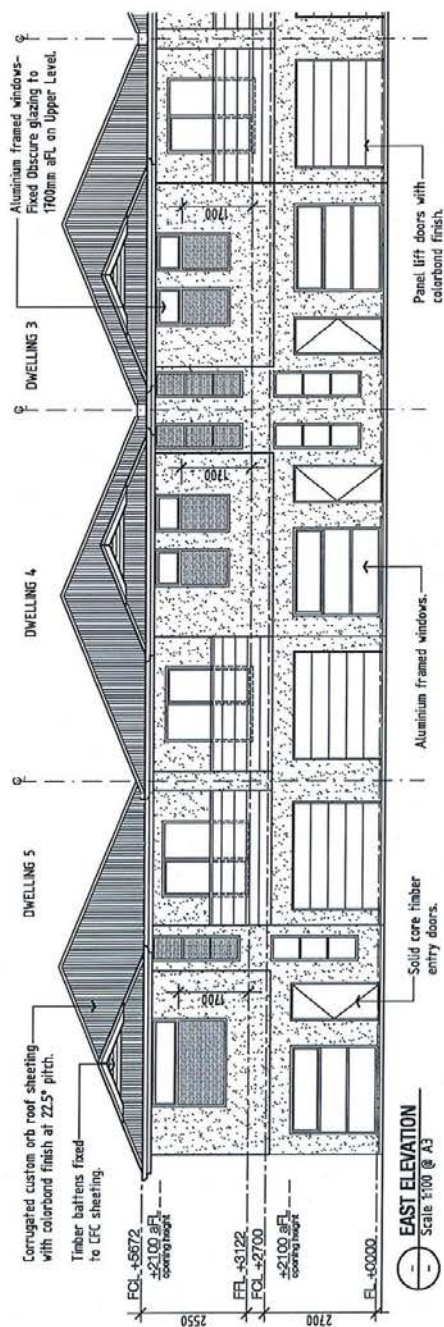
Building Areas: (m <sup>2</sup> )	
Lower Living.....	48.2
Upper Living.....	86.2
Carport.....	21.3
Alfresco.....	8.3
Balcony.....	7.5
<b>Total Building Area:</b> .....	<b>171.5</b>



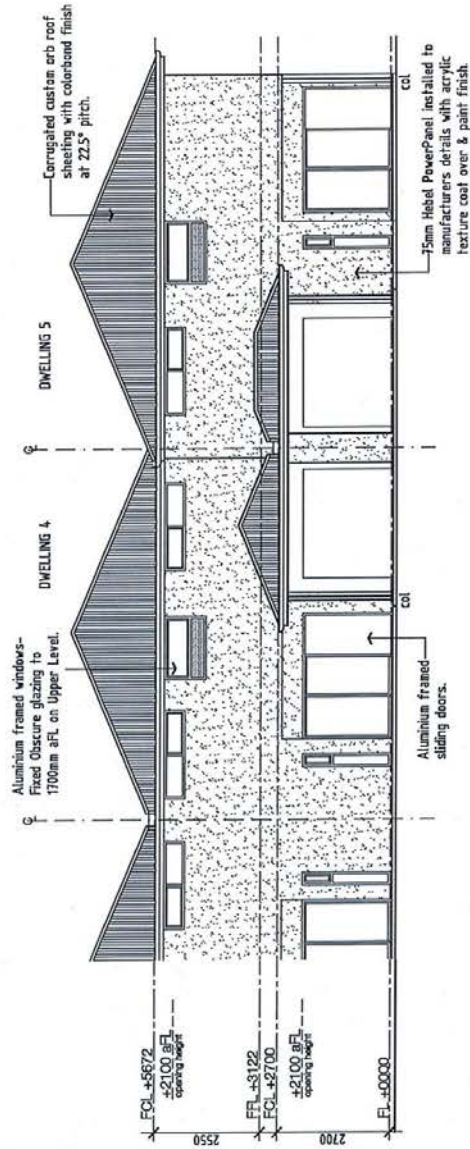
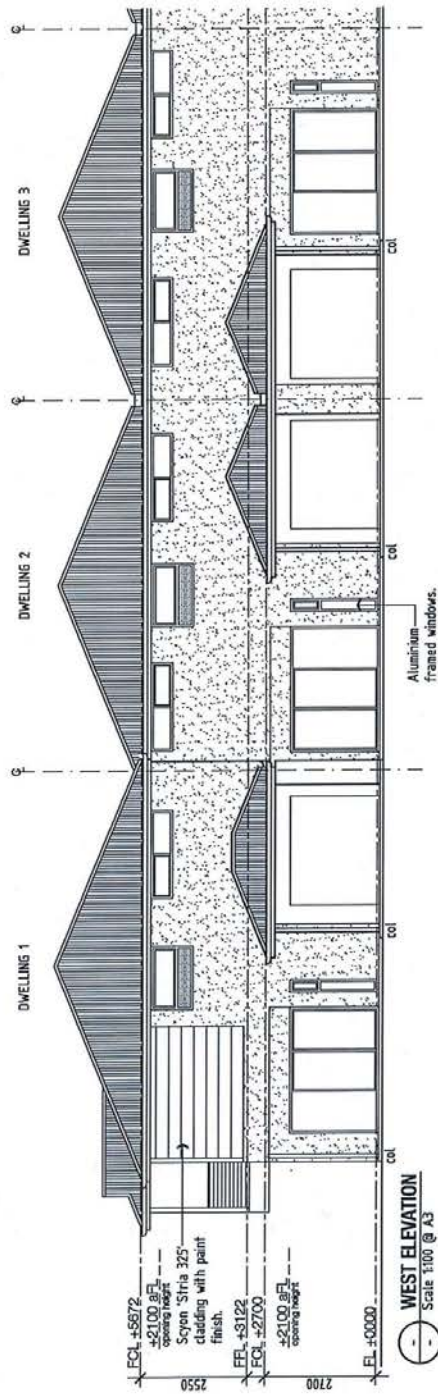
**DWELLINGS 4-5**  
**FIRST FLOOR PLAN**  
 Scale 1:100 @ A3

JOB TITLE PROPOSED RESIDENTIAL DEVELOPMENT AT: 11 BIRKALLA TERRACE PLYMPTON	DRAWING TITLE DWELLINGS 4 & 5 UPPER FLOOR PLANS	ARCHITECTS	<p>Customers are to verify all dimensions and levels before commencing any site work or making shop drawings. Fenced boundaries shall take preference over unenclosed dimensions and any discrepancy reported to the architect immediately.</p> <p>Scale: 1:100 Date: 17 SEP '17 Page: A3 Sheet No: 0007</p>	ANTHONY DONATO ARCHITECTS	
				<p>100 NO. SCALE DRAWN DATE CHECKED SHEET NO. REVISION</p> <p>4221-17 1:100 DP 17 SEP '17 A3 0007</p>	

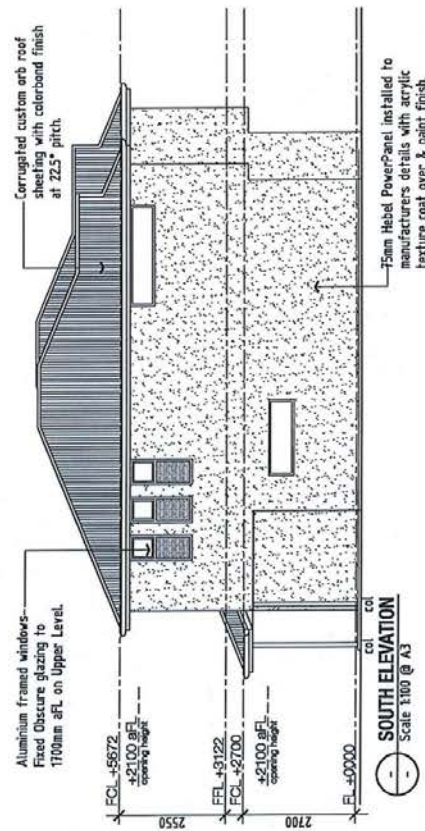
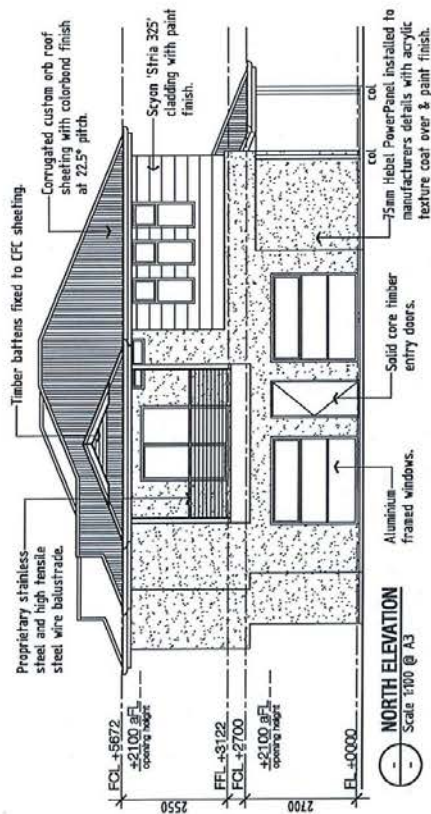




<b>ANTHONY DONATO ARCHITECTS</b> Suite 5/59 Fullarton Road   Kent Town SA 5007 T: 08 8364 6888   F: 08 8364 9305   www.adarchitects.com.au	Architects are to supply all dimensions and levels before commencing any site work or making shop drawings. Figural dimensions shall take preference over verbal dimensions and any discrepancy shall be reported to the architect immediately. © COPYRIGHT	JOB NO. SCALE DRAWN CHECKED DATE PAGE SHEET NO. REVISION	PROPOSED RESIDENTIAL DEVELOPMENT AT: 11 BIRKALLA TERRACE PLYMPTON	DRAWING TITLE EAST ELEVATION	COMMENTS

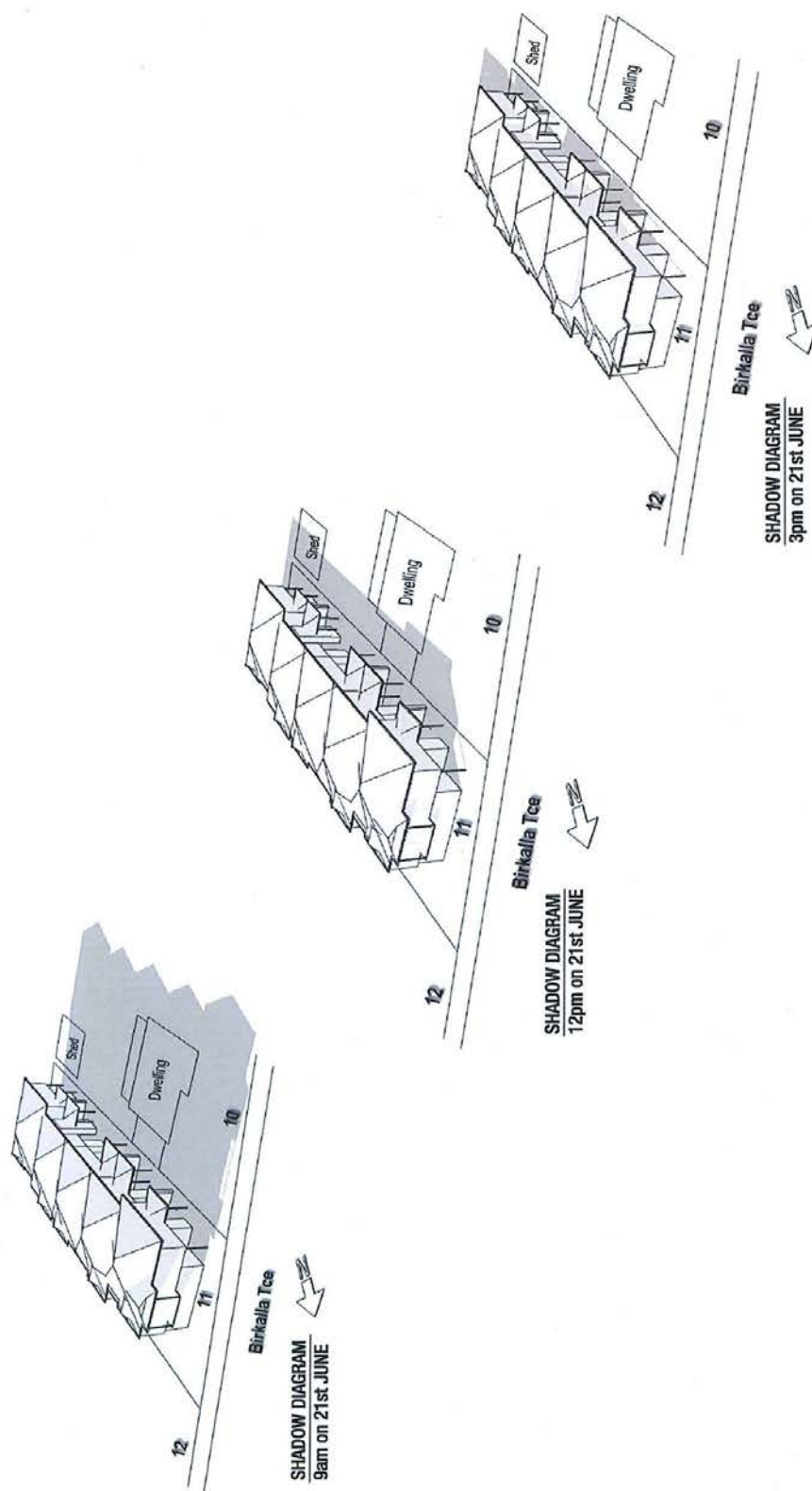


JOB TITLE	DRAWING TITLE	REVISIONS	ANTHONY DONATO ARCHITECTS
PROPOSED RESIDENTIAL DEVELOPMENT	WEST ELEVATION		
AT: 11 BIRKALLA TERRACE PLYMPTON			
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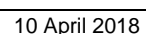


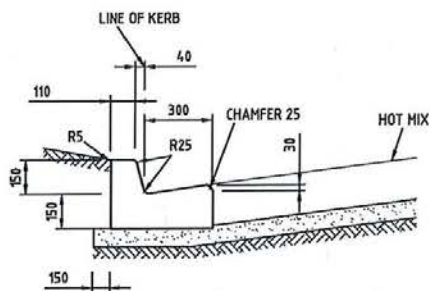
JOB TITLE PROPOSED RESIDENTIAL DEVELOPMENT AT: 11 BIRKALLA TERRACE PLYMPTON	DRAWING TITLE NORTH & SOUTH ELEVATIONS	ARCHITECTS ANTHONY DONATO ARCHITECTS Suite 6/50 Fullarton Road   Kent Town SA 5087 T: 08 8364 6888   F: 08 8364 6205   www.audonato.com.au © COPYRIGHT	<div></div>				REVISION		
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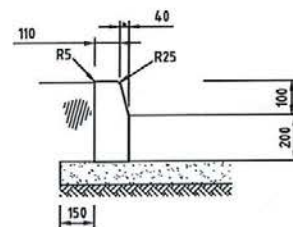
JOB TITLE PROPOSED RESIDENTIAL DEVELOPMENT AT: 11 BIRKALLA TERRACE PLYMPTON	DRAWING TITLE SHADOW DIAGRAMS	AMENDMENTS	<div data-bbox="1283 210 1410 703"> <p>  <b>ANTHONY DONATO ARCHITECTS</b>            Suite 6/39 Fullarton Road   Kent Town SA 5007            T 08 8264 6868   F 08 8264 5255   www.adarchitects.com.au            E info@adarchitects.com.au         </p> </div> <p>           Conditions are to verify all dimensions and measurements are correct. The work or making shop drawings. If any dimensions shall take preference over scaled dimensions and any discrepancy shall be referred to the architect immediately.         </p> <div> <div>JOB NO. 4221-TF</div> <div>SCALE N/A</div> <div>DRAWN DP</div> <div>CHECKED SEP '17</div> <div>DATE A3</div> <div>SHEET NO. 0011</div> <div>REVISION</div> </div>
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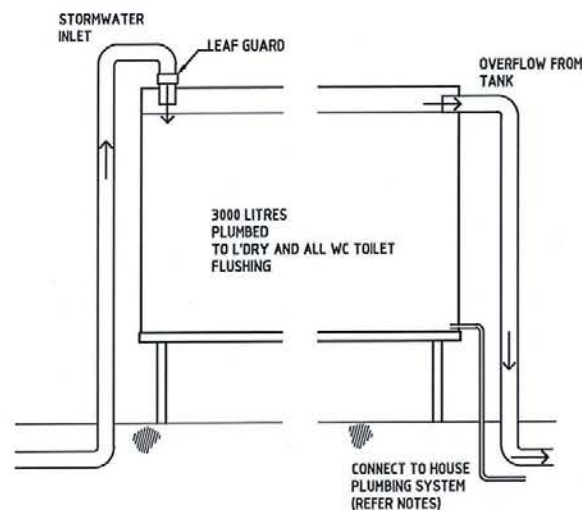
### BARRIER KERB AND WATER TABLE (NTS)

PROVIDE CONTRACTION JOINTS AT MAXIMUM 3m CENTRES



### BARRIER KERB (NTS)

PROVIDE CONTRACTION JOINTS AT MAXIMUM 3.0m CENTRES



### DETENTION TANK DETAIL (NTS)

#### NOTES:

1. RAINWATER TANK SHALL BE INSTALLED AND OPERATIONAL PRIOR TO OCCUPANCY OF DWELLING.
2. MAINS WATER BACKUP, PUMP AND PLUMBING ARRANGEMENTS AS TYPICALLY REQUIRED ARE TO BE COMPLAINT WITH BUILDING CODE REQUIREMENTS ASSOCIATED WITH A COMPULSORY RAINWATER TANK INSTALLATION.



GAMA CONSULTING  
engineers and project management  
a: Suite 7, No.128 Fvdlarton Rd,  
Norwood, SA 5047  
m: 0438 662 888  
0407 777 988  
admin@gamaconsulting.com.au  
www.gamaconsulting.com.au

PROJECT PROPOSED DEVELOPMENT  
11 BIRKALLA TERRACE  
PLYMPTON, SA  
CLIENT APS HOMES

C	RAINWATER TANK DETAIL AMENDED	02/03/2018	GS
B	RE-ISSUE FOR PLANNING APPROVAL	28/02/2018	GS
A	ISSUE FOR PLANNING APPROVAL	28/09/2017	GS
REV	DESCRIPTION	DATE	CHECKED
STATUS PRELIMINARY		DRAWN CT	DESIGN CT
COUNCIL WEST TORRENS		DRAFT CHECK CT	DESIGN CHECK GS
DESCRIPTION SITE PLAN DETAILS		DRAWING No.	17581-C02
		SHEET SIZE	A3
		REVISION	C



SCANNED

4 DEC 2017  
REF No. A2083936

# STATEMENT OF REPRESENTATION

## Pursuant to Section 38 of the Development Act, 1993

TO Chief Executive Officer  
City of West Torrens  
165 Sir Donald Bradman Drive  
HILTON 5033

City of West Torrens

14 DEC 2017

City Development

RECEIVED - CWT IM  
15 DEC 2017

DEVELOPMENT No. 211/1368/2017  
PROPERTY ADDRESS: 11 Birkalla Terrace, PLYMPTON SA 5038

YOUR FULL NAME	Teresa & Mark Wallace
YOUR ADDRESS	12 Birkalla Terrace Plympton SA 5038
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	Adjoining resident (eg. Adjoining resident, owner of land in the vicinity etc.)
REASON/S FOR REPRESENTATION I have concerns with our privacy being impacted due to dwellings being close to the boundaries & overlooking in our backyard. Also noise impacting our residence from so many dwelling next door to bedrooms	
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought) Not so many dwellings being built	

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO NOT WISH TO BE HEARD ☐I DESIRE TO BE HEARD PERSONALLY ☒I DESIRE TO BE REPRESENTED BY \_\_\_\_\_ ☐

(PLEASE SPECIFY)

SIGNED



DATE 12/12/17

Responsible Officer: Sonia Gallarello

Ends: Wednesday 27 December 2017

If space insufficient, please attach sheets

**Sonia Gallarello**

---

**From:** Council Enquiries  
**Sent:** Monday, 15 January 2018 9:59 AM  
**To:** Development  
**Subject:** FW: Dev No. 211/1368/2017 11 Birkalla Terrace, Plympton

-----Original Message-----

From: Anthony Donato  
Sent: Saturday, 13 January 2018 12:25 PM  
To: Council Enquiries <csu@wtcc.sa.gov.au>  
Cc: 'Surveying | Cavallo Forest' <surveying@cavalloforest.com.au>; 'Tony Schnaars'  
; 'filip'  
Subject: Dev No. 211/1368/2017 11 Birkalla Terrace, Plympton

Att: Sonia Gallarello  
Senior Planner  
City of West Torrens

Dev No. 211/1368/2017 11 Birkalla Terrace, Plympton

In response to representation from neighbour at 12 Birkalla Tce, I wish to make the following comments:

The design proposal incorporates obscure glass to 1700mm above first floor level to all windows to all units facing side and rear boundaries so there will not be any privacy issues with residents from this development looking into neighbouring properties.

All walls to all dwellings will be insulated according to BCA Energy & Acoustic requirements so there should not be any issues of noise emanating from this development to neighbouring properties. External noise will be the same as any other development proposed for this site.

The Council zoning policy for this site allows for 5 Dwellings to be designed for this site so it is not reasonable for neighbour to say that there are too many dwellings proposed for this site.

Kind regards,

Anthony Donato  
Director (RAIA)

Suite 5/59 Fullarton Road | Kent Town SA 5067 t. 08 8364 6888 | f. 08 8364 5355 | [www.adarchitects.com.au](http://www.adarchitects.com.au)

City of **West Torrens**

Between the City and the Sea

## Memo

To Sonia Gallarello  
From Jane Teng  
Date 20/02/2018  
Subject 211/1368/2017, 11 Birkalla Terrace, PLYMPTON SA 5038

Sonia Gallarello,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

### 1.0 Traffic Comments

- 1.1 As the access driveway will service more than one property at the rear, the driveway corridor to the site will require widening to a minimum of 5.5m wide pavement width (+ 300mm offset from fences/walls/boundary) for the first 5.0m into the site to permit the passing of entering and exiting traffic. The proposed driveway falls short of this requirement. I note that in this instance, the 300mm offset is not provided to the southern fence/boundary.

**It is recommended that the driveway servicing the rear of the subject site be revised to the required dimensions indicated above. Revised plans showing a driveway that satisfies the above provisions should be provided to Council.**

- 1.2 Elements of the vehicle manoeuvrability within this development appear to be unsatisfactory in consideration of the requirements of the relevant Australian Standard (AS/NZS 2890.1:2004).

The driveway aisleway width is indicated as 3.59m at the minimum. This does not appear to allow for suitable vehicle manoeuvrability into the open 'visitor' spaces (in tandem with carport spaces).

In the revision of the traffic manoeuvrability design, it is required that information be provided to clearly demonstrate the compliance of manoeuvrability for critical parking spaces and movements using suitable techniques as outlined within AS/NZS 2890.1:2004.

**It is recommended that the applicant seek advice from someone suitably experienced in traffic design, to assist them in producing a vehicle manoeuvrability design for this site which complies with the Australian Standard requirements, preferably through the use of either**





**“AutoTrack” or “AutoTURN” demonstration. Reports and drawings should then be submitted to Council.**

## **2.0 FFL Consideration – Finished Floor Level (FFL) Requirement**

- 2.1 In accordance with the provided ‘Site Plan’ (Gama consulting - Drawing No. 17581-C01), the FFLs of the proposed development (99.90 minimum) have been assessed as satisfying minimum requirements (99.80) in consideration of street and/or flood level information.

## **3.0 Verge Interaction**

- 3.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically required to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram ramps, etc. (as measured at the front property boundary).

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council’s Technical Officer (Arboriculture).

- 3.2 The current civil plan does not show the widened crossover necessary to service the proposed two-way driveway access point.

**It is recommended that revised plans indicating satisfaction to the above requirements should be provided to Council.**

Should you require further information, please contact Jane Teng on the following direct extension number 8416 6296

Regards

**Jane Teng**  
**Civil Engineer**

City of **West Torrens**

Between the City and the Sea

---

## Memo

To Sonia Gallarello  
From Jane Teng  
Date 19/03/2018  
Subject 211/1368/2017, 11 Birkalla Terrace, PLYMPTON SA 5038

Sonia Gallarello,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

### 1.0 Traffic Comments

- 1.1 As the access driveway will service more than one property at the rear, the driveway corridor to the site will require widening to a minimum of 5.5m wide pavement width (+ 300mm offset from fences/walls/boundary) for the first 5.0m into the site to permit the passing of entering and exiting traffic. The proposed driveway falls short of this requirement. I note that in this instance, the 300mm offset is not provided to the southern fence/boundary.

**It is recommended that the driveway servicing the rear of the subject site be revised to the required dimensions indicated above. Revised plans showing a driveway that satisfies the above provisions should be provided to Council.**

- 1.2 Elements of the vehicle manoeuvrability within this development appear to be unsatisfactory in consideration of the requirements of the relevant Australian Standard (AS/NZS 2890.1:2004).

The driveway aisleway width is indicated as 3.59m at the minimum. This does not appear to allow for suitable vehicle manoeuvrability into the open 'visitor' spaces (in tandem with carport spaces).

In the revision of the traffic manoeuvrability design, it is required that information be provided to clearly demonstrate the compliance of manoeuvrability for critical parking spaces and movements using suitable techniques as outlined within AS/NZS 2890.1:2004.

**It is recommended that the applicant seek advice from someone suitably experienced in traffic design, to assist them in producing a vehicle manoeuvrability design for this site which complies with the Australian Standard**



**requirements, preferably through the use of either "AutoTrack" or "AutoTURN" demonstration. Reports and drawings should then be submitted to Council.**

**Editor Notes: 19/ 03/2018**

Based on proposed 'Site Plan' (ANTHONYDONATOARCHITECTS- Job no: 4227-17 amended 23.02.18), there are no major concern of further information required or recommendations required in association with this element of development proposal.

**2.0 FFL Consideration – Finished Floor Level (FFL) Requirement**

- 2.1 In accordance with the provided 'Site Plan' (Gama consulting - Drawing No. 17581-C01), the FFLs of the proposed development (99.90 minimum) have been assessed as satisfying minimum requirements (99.80) in consideration of street and/or flood level information.

**Editor Notes: 19/ 03/2018**

There are no major concern of further information required or recommendations required in association with this element of development proposal.

**3.0 Verge Interaction**

- 3.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically required to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram ramps, etc. (as measured at the front property boundary).

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council's Technical Officer (Arboriculture).





City of **West Torrens**

Between the City and the Sea

- 3.2 The current civil plan does not show the widened crossover necessary to service the proposed two-way driveway access point.

**Editor Notes: 19/ 03/2018**

Based on propose 'Site Plan' (ANTHONYDONATOARCHITECTS- Job no: 4227-17 amended 23.02.18), there are no major concern of further information required or recommendations required in association with this element of development proposal.

**4.0 Stormwater Management**

Based on proposed 'Site Plan' (GAMA CONSULTING - DRAWING No: 17581-C01- Rev D-Amended 02.03.18 and - DRAWING No: 17581-C02- Rev C-Amended 02.03.18), there are no major concern of further information required or recommendations required in association with this element of development proposal and such the following considerations are proposed to be incorporated as part of the planning conditions for the site.

- Installation of a 3,000 litre rainwater tank.
- Rainwater tank plumbed to deliver recycled water all toilets and laundry cold water outlet. (Can also be connected to Hot Water Service if desired).
- A minimum of 90% of the dwelling roof area is to have its stormwater runoff directed to the rainwater tank.
- Mains water backup, pump and plumbing arrangements as typically required to support such an installation are to be compliant with the standard Building Code requirements associated with a compulsory rainwater tank installation.
- The stormwater collection and re-use system is to be installed and operational prior to occupancy of the dwelling.
- Prior to occupancy of a dwelling, the 3000 litre stormwater collection & reuse tank and associated plumbing to service all toilets and laundry is to be installed and operational.

Should you require further information, please contact Jane Teng on the following direct extension number 8416 6296.

Regards

**Jane Teng**  
**Civil Engineer**



06 November 2017

Our Ref: H0065709

The Chairman  
Development Assessment Commission  
136 North Terrace  
ADELAIDE SA 5000

Dear Sir/Madam

**PROPOSED LAND DIVISION APPLICATION NO: 211/C180/17 AT PLYMPTON**

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

For SA Water to assess this application, the developer must advise SA Water the preferred servicing option. Information can be found at: <http://www.sawater.com.au/developers-and-builders/building,-developing-and-renovating-your-property/subdividing/community-title-development-factsheets-and-information> For queries call SAW Land Developments on 74241119. An investigation will be carried out to determine if connections to the development will be standard or nonstandard.

The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

Yours faithfully

JOSIE BONNET

**for MANAGER LAND DEVELOPMENT & CONNECTIONS**

Contact Lands Titles Office  
Telephone 7109 7016



*Development  
Assessment Commission*

06 November 2017

The Chief Executive Officer  
City of West Torrens

Dear Sir/Madam

**Re: Proposed Application No. 211/C180/17 (ID 59854)  
for Land Division by Mr Anthony Donato**

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 31 October 2017, I advise that the Development Assessment Commission has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the Development Assessment Commission has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.  
For SA Water to assess this application, the developer must advise SA Water the preferred servicing option. Information can be found at: <http://www.sawater.com.au/developers-and-builders/building,-developing-and-renovating-your-property/subdividing/community-title-development-factsheets-and-information> For queries call SAW Land Developments on 74241119. An investigation will be carried out to determine if connections to the development will be standard or nonstandard.  
The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.
2. Payment of \$27320 into the Planning and Development Fund (4 allotment(s) @ \$6830/allotment).  
Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

**PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING  
COUNCIL'S DECISION.**

A handwritten signature in black ink, appearing to read 'P. Hodgson'.

Phil Hodgson

**Unit Manager**

**Lands Titles Office**

as delegate of

**DEVELOPMENT ASSESSMENT COMMISSION**



**6.4 2 - 4 Bray Avenue, TORRENSVILLE**

Application No 211/1430/2017

Appearing before the Panel will be:

Representors: **Dan Xiong** of 1 Bray Avenue, Torrensville wishes to appear in support of the representation.

Applicant/s **Han Xue** of 2 - 4 Bray Avenue, Torrensville wishes to appear to respond to the representation.

**DEVELOPMENT APPLICATION DETAILS**

DEVELOPMENT PROPOSAL	Change of use from motor vehicle repair station to service trade premises (second hand motor vehicle dealership) with associated advertising signage (Unit 5)
APPLICANT	Han Xue
APPLICATION NO	211/1430/2017
LODGEMENT DATE	23 November 2017
ZONE	Industry
POLICY AREA	N/A
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 3
REFERRALS	Internal <ul style="list-style-type: none"> <li>▪ City Assets</li> </ul> External <ul style="list-style-type: none"> <li>▪ Nil</li> </ul>
DEVELOPMENT PLAN VERSION	30 May 2017
MEETING DATE	10 April 2018

**RECOMMENDATION**

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/1430/2017 by Han Xue to undertake a change of use from motor vehicle repair station to service trade premises (second hand motor vehicle dealership) with associated advertising signage (Unit 5) at 2-4 Bray Avenue, TORRENSVILLE (CT 5447/161 & 5431/18) subject to the following conditions of consent:

**Development Plan Consent Conditions:**

1. The development must be undertaken, completed and maintained in accordance with the plan(s) and information detailed in this application except where varied by any condition(s) listed below.
2. That the hours of operation shall not exceed the following:
  - Monday to Friday: 9am to 5pm; and
  - Saturday: 9am - 4pm.
Any variation to these hours of operation will require further consent from Council.
3. The maximum number of staff on the site shall be limited to three (3) at any one time.

4. That driveways, parking and manoeuvring areas and footpaths shall remain formed (surfaced with concrete, bitumen or paving) and be properly drained and maintained in a reasonable condition at all time.
5. That all carparking spaces on site shall be line marked, in accordance with the approved plans and in accordance with Australian Standards Association Code AS 2890.1, 2004 Parking Facilities, Part 1, Off Street Carparking, prior to the occupation of the proposed development.
6. That all loading and unloading of goods and merchandise shall be carried out upon the subject land and NO loading of any goods or merchandise shall be permitted to be carried out in the street.
7. The storage of 'car stock' shall be limited to within the showroom area only, with no cars being displayed for sale within the carpark or on or about the curtilage.
8. All landscaping shall be planted in accordance with the approved plans within three (3) months of commencement of the use of the development and maintained in good health at all times. Any landscaping which may become diseased or die shall be replaced with the same or like species/variety to the reasonable satisfaction of Council.
9. That the advertisement, herein approved, shall be maintained in good repair at all times and if damaged will be replaced with a like sign within one (1) month.
10. That the advertisements, herein approved, shall not be internally illuminated and incorporate red, yellow, green or blue lighting, will not move, will not flash and will not reflect light.
11. All lighting on site shall be designed and installed so that light is directed downwards and towards the site to prevent light spill beyond the site boundaries and so as to not create glare and nuisance to adjoining properties and to motorists and pedestrians on adjoining roads in accordance with the relevant Australian Standards.
12. The washing of vehicles is not to be undertaken inside or outside of the building at any time without the prior consent of Council.
13. No body repairs or mechanical repairs of any type are to be undertaken within the building or on the site without the prior consent of Council.
14. All vehicles shall enter and exit the site in a forward direction.
15. That all waste generated by land uses within the proposed buildings is to be stored in dedicated rubbish skips/bins and must comply with the following requirements at all times:
  - a) to be covered to prevent the entry of stormwater or dispersal of waste by wind;
  - b) to be sealed to prevent leakage;
  - c) to be located on hard standing areas which are secure;
  - d) to be screened from view; and
  - e) collection of skips/bins are to undertaken in accordance with *Environment Protection Act* and the *Environment Protection (Noise) Policy 2007* at all times.

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## BACKGROUND

The development proposal is presented to the Council Assessment Panel (CAP) for the following reason:

- All Category 2 or 3 applications where a representor has requested to be heard shall be assessed and determined by the CAP.

**PREVIOUS OR RELATED APPLICATION(S)**

Nil

**SITE AND LOCALITY**

The subject land is formally described as Allotment 49 Deposited Plan 3359, Volume 5431 Folio 18 and Allotment 50 Deposited Plan 3359, Volume 5447 Folio 161 in the area named Torrensville Hundred of Adelaide, more commonly known as 2 - 4 Bray Avenue, Torrensville. The subject site is rectangular in shape consisting of four contiguous allotments and extends from Bray Avenue through to Howie Avenue. The subject site is located within the Industry Zone however it borders Residential Policy Area 21 to the east.

The site currently contains an industrial building comprising a number of separately tenanted units. The proposed use seeks to occupy unit 5 within the building. The site has a 38 metre-wide frontage to Howie Avenue and provides for 6 off-street carparks and 2 on-street carparking spaces.

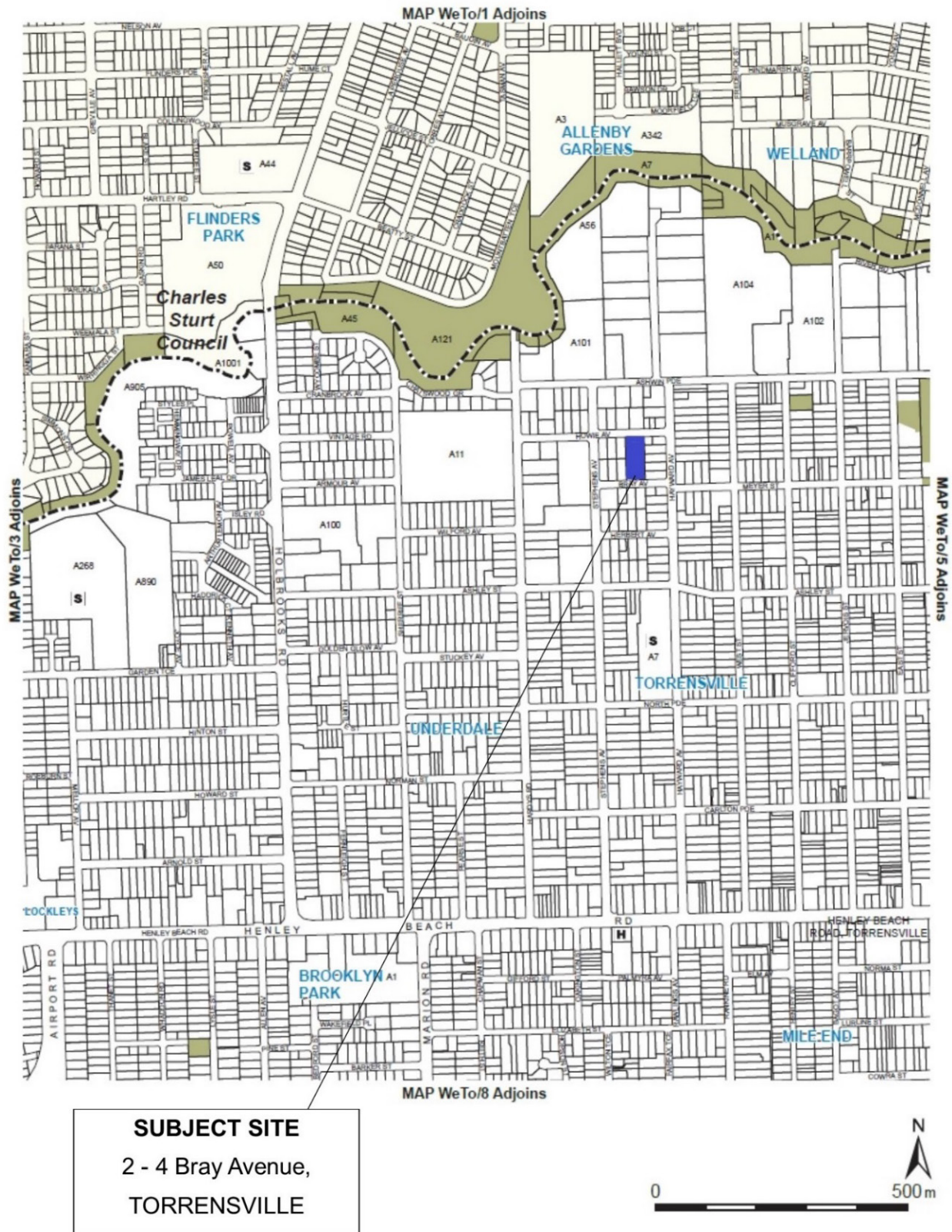
The locality is mixed use in nature comprising industrial, commercial and residential land uses. Commercial and industrial land uses are located to the north, south and west of the subject site. To the east and further south are residential properties comprising single and two storey detached dwellings.

The site and locality are shown on the following maps:









## Location Map WeTo/4

WEST TORRENS COUNCIL  
Consolidated - 30 May 2017

## PROPOSAL

The proposal seeks a change of land use from a motor vehicle repair station to a service trade premises or more specifically a second hand motor vehicle dealership, associated office and advertising signage.

The office component has an approximate floor area of 77 square metres and the showroom comprises an approximate floor area of 369 square metres. The showroom is proposed to accommodate up to 18 vehicles at any one time, with vehicular access through an existing roller door from Howie Avenue. The six carparks currently on the site and two on-street carparks will remain.

The applicant has indicated that a maximum number of three staff members will be on site at any one time. The operating hours have been confirmed to be Monday to Friday 9am to 5pm and on Saturday 9am to 4pm.

The site will be used to undertake administrative tasks relating to the business and conduct trade-ins and sales of vehicles. No machinery or chemicals will be used or stored on the subject site. All mechanical work relating to the vehicles will be carried out off-site.

The applicant has proposed to clean up the existing garden and garden bed fronting Howie Avenue and plant native Australian plants to visually enhance the existing building. The two existing trees in front of the building will remain with knobby club rush to be planted around these trees.

Signage is also proposed to the front façade of the building. The sign measures 10m x 1.2m and is predominantly black with white and orange writing depicting the name of the business and its contact details. The proposed sign will not be internally illuminated.

A copy of the plans is contained in **Attachment 1**.

## PUBLIC NOTIFICATION

The application is a Category 3 form of development pursuant to Section 38 and Schedule 9 of the *Development Act 1993* and *Development Regulations 2008*. That application was not considered to be minor pursuant to Schedule 9, Part 1 (2)(g) of the Regulations due the interface with the Residential Zone.

<b>Properties notified:</b>	37 properties were notified during the public notification process.
<b>Representations:</b>	1 representation was received.
<b>Persons wishing to be heard:</b>	1 representor identified that they wish to address the Panel. <ul style="list-style-type: none"> <li>Dan Xiong</li> </ul>
<b>Summary of Representation:</b>	Concerns were raised regarding the following matters; <ul style="list-style-type: none"> <li>Air pollution of increased number of cars;</li> <li>Noise pollution due to increased number of cars and additional customers;</li> <li>Off-street parking limitations;</li> <li>Increase in crime rate;</li> <li>Proposed use will devalue residential properties; and</li> <li>Streetscape will be compromised.</li> </ul>



The Applicant did not provide a response to the representation.

A copy of the representation is contained in **Attachment 2**.

## REFERRALS

Internal

### *City Assets*

- The proposed change in use from an office/warehouse to office/car showroom is not expected to result in a notable change in parking requirements.
- On balance, the proposed change of use would be acceptable from a traffic and parking perspective.

A full copy of the relevant report is contained in **Attachment 3**.

External

Nil.

## ASSESSMENT

The subject land is located within the Industry Zone as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
<i>Advertisements</i>	<i>Objectives</i>	1, 2 & 3
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23 & 24
<i>Design and Appearance</i>	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24 & 25
<i>Industrial Development</i>	<i>Objectives</i>	1, 2, 3, 4 & 5
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11 & 12
<i>Interface between Land Uses</i>	<i>Objectives</i>	1, 2 & 3
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12 & 13
<i>Landscaping, Fences and Walls</i>	<i>Objectives</i>	1 & 2
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5 & 6
<i>Transportation and Access</i>	<i>Objectives</i>	1, 2, 3, 4 & 5
	<i>Principles of Development Control</i>	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46 & 47
<b>Zone: Industry</b>		
<i>Objectives</i>		1
<i>Principles of Development Control</i>		1, 3, 10, 11, 12

**QUANTATITIVE ASSESSMENT**

<b>DEVELOPMENT PLAN PROVISIONS</b>	<b>STANDARD</b>	<b>ASSESSMENT</b>
<b>CARPARKING</b> <i>Transportation and Access</i> <i>PDC 34</i>	Used car lot: 2 carparks per 100m <sup>2</sup> of the total site area  Office: 4 carparks per 100m <sup>2</sup> of total floor area	Used car lot: 369m <sup>2</sup> Requires: 7.4 carparks  Office: 77m <sup>2</sup> Requires: 3.1 carparks  A minimum of 10.5 carparks required, a shortfall of 2.5 carparks.  <b>Does Not Satisfy</b>
<b>LANDSCAPING</b> <i>Landscaping, Fences and Walls</i> <i>PDC 4</i>	Min. of 10% of the development site should be landscaped.	30m <sup>2</sup> of landscaping provided  <b>Does Not Satisfy by 3%</b>
<b>SIGNAGE</b> <i>Advertisements</i> <i>PDC 8</i>	Total advertising area to building surface does not exceed 20%.	Building surface area: 88m <sup>2</sup>  Existing & proposed sign: 14.2m <sup>2</sup> (16% of building surface area)  <b>Satisfies by 4%</b>

**QUALITATIVE ASSESSMENT**

In assessing the merits or otherwise of the application, the proposed development will be discussed in accordance with the relevant provisions of the Development Plan under the following sub headings:

**Land Use**

The proposal is for a change of use to a service trade premises for the purposes of second-hand motor vehicle sales. Principle of Development Control (PDC) 1 of the Industry Zone envisages a service trade premises to be an appropriate land use for the zone.

**Interface - Amenity**

The subject site is located along the interface of the Residential Zone - Low Density Policy Area 21. It is considered that the proposal will not unreasonably impact the amenity of the adjacent residential properties due to the nature of the existing access and parking areas, the proposed hours of operation, and the anticipated number of vehicular movements. It should be highlighted that the proposal is only for the portion of the building that faces Howie Avenue, and not the portion that faces Bray Avenue. It is understood that existing uses - some of which are industrial in nature - occupy these remaining areas of the building.

The proposed operating hours are 9am to 5pm Monday to Friday, and 9am to 4pm on Saturdays. All business/administrative dealings are to be contained within the existing building and conducted during standard business hours from Monday to Friday. Whilst Saturday is outside of standard business hours, the proposed operating hours are not considered to detrimentally affect the adjacent residential properties. The proposed use will not utilise heavy machinery or other noise generating activities. Furthermore, the Saturday operating hours are considered to be limited and reasonable so as not to negatively impact daily human activities and sleeping patterns.

There are no concerns in relation to noise or odours arising from the proposed use. The applicant has confirmed that no machinery will be used on site and, for these reasons, the adjacent properties will not be detrimentally impacted by the proposed use, satisfying Objective 2 and PDC 1 of General Section - Interface between Land Uses. As a side note, Council has no records of a received complaint in the past five years in regards to noise or odour from the subject site.

### **Landscaping**

A landscaping plan has been submitted by the applicant to Council. In accordance with PDC 4 of General Section - Landscaping, Fences and Walls, 10% of the subject site should be landscaped. The proposal falls 3% short of the requirement however this is not considered fatal to the application. The plantings detailed on the landscaping plan are predominantly located along the northern boundary of the subject site. The plantings will visually enhance the site as seen from Howie Avenue and will assist in softening the extent of hard paved surfaces of the development as well as screening the development from view, satisfying PDC 1 of General Section - Landscaping, Fences and Walls. The plants detailed are drought tolerant species which are native to Australia and thrive in Australian climates, requiring minimal maintenance, satisfying PDC 2 of General Section - Landscaping, Fences and Walls.

### **Parking & Access**

The site currently utilises the existing access and sealed carpark from Howie Avenue. No change to this access arrangement is proposed.

*Table WeTo/2 - Off Street Vehicle Parking Requirements* of the Development Plan prescribes that an office requires 4 car parks per 100 square metres of total floor area and a used car lot requires 2 carparks per 100 square metres of total floor area. Accordingly, the parking demand for the proposal is as follows:

- Office: 3.1 car parks for the 77 square metres of floor area; and
- Used car lot: 7.4 car parks for the 369 square metres of floor area.

Accordingly, a total of 10.5 carparks will be required to satisfy the requirements of the Development Plan, meaning that there is a shortfall of 2.5 carparks associated with the proposal.

City Assets has accepted that a commonly used guide, the '*Planning Bulletin - Parking provisions for selected land uses*', is appropriate for calculating parking demand in this instance. A parking rate of 1.5 spaces per 100m<sup>2</sup> for a used car lot is specified in the Bulletin. If this rate is used, the used car lot would require 5.5 carparks rather than the 7.4 carparks prescribed in the Development Plan. The revised carparking requirement for the proposal would therefore be reduced to 8.6 carparks, a shortfall of 0.6 carparks. This shortfall is considered to be minor and the proposed use is considered to satisfy PDC 34 of General Section - Transportation and Access.

It is worth noting that the previous use, a motor repair station, has the same prescribed requirements as a used car lot within *Table WeTo/2*. Accordingly, the proposed use is not expect to result in any notable change to the parking requirement based on the previous land use of the subject site.



## **Advertising**

The proposed sign will be in addition to the existing sign located on the main façade of the building fronting Howie Avenue. The proposed sign, which is not internally illuminated, is considered large at 10 metres x 1.2 metres but is of a scale that is appropriate for the existing building. The sign has been designed in a manner that is simple yet effective in providing information about the proposed business satisfying PDC 4 and 11 of General Section - Advertisements. In addition, the proposed sign will 'break up' the expanse of white colorbond cladding of the building, providing an element of visual interest satisfying Objective 3 and PDC 1 of General Section - Advertisements. The proposed (and existing) signs on the building do not present any clutter, disorder or driver distraction which could occur with multiple signs being located to the main façade of buildings, satisfying PDC 2 of General Section - Advertisements. Overall the proposed sign provides clear information about the proposed use to the public whilst avoiding any disorder or driver distraction impacts.

## **SUMMARY**

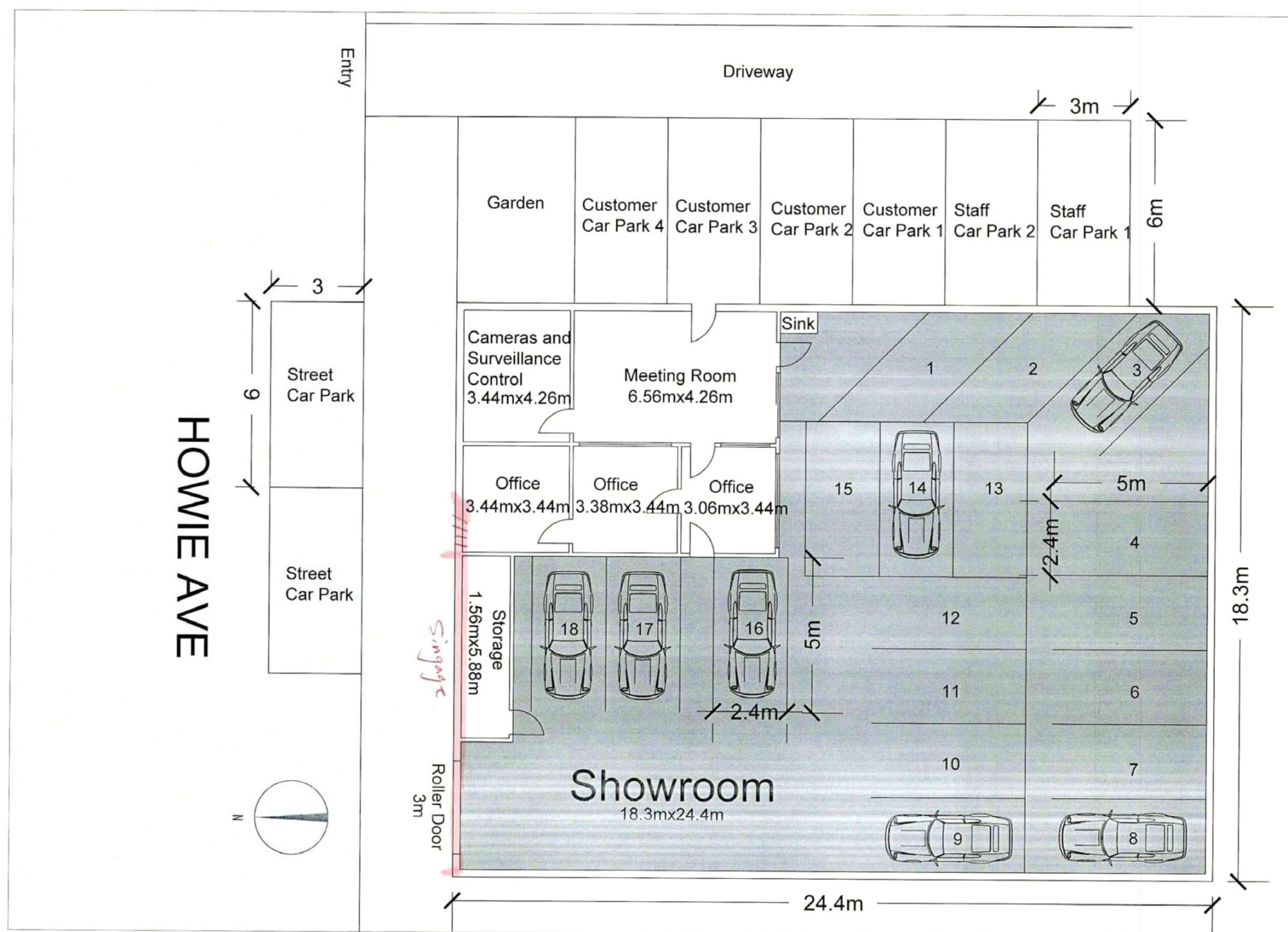
The subject site is located within the Industry Zone where a service trade premises is an envisaged land use. The proposal seeks to utilise a building previously used as a motor repair shop. It is considered that the proposed use will have less noise emissions than the previous use. Although the site is on the interface of the Residential Zone, it is considered that the proposed use will not detrimentally impact the amenity of the area by the way of noise, fumes or excessive traffic movements.

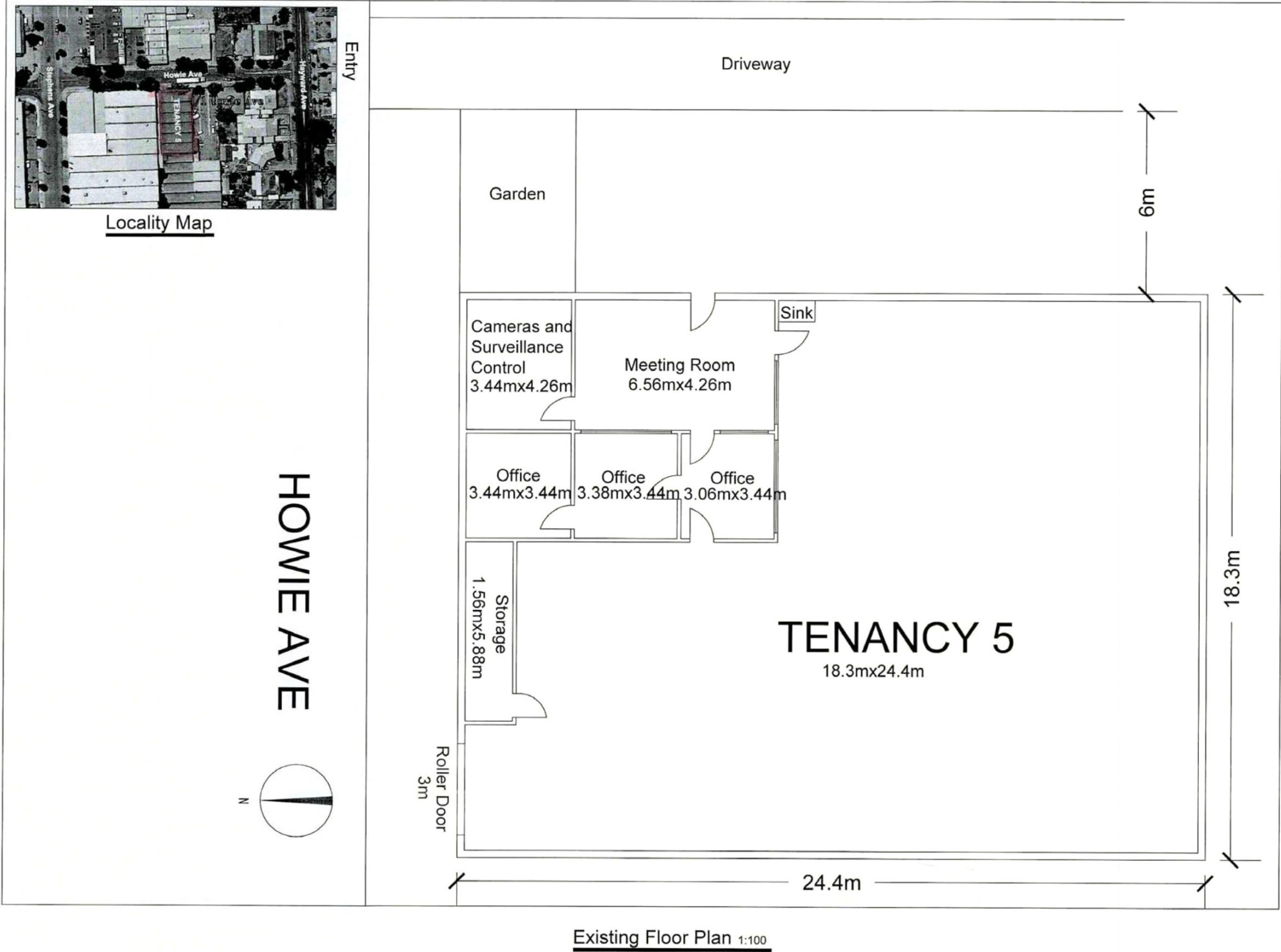
Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 30 May 2017 and warrants Development Plan Consent.

## **Attachments**

- 1. Plan Set**
- 2. Public Notification Representation**
- 3. City Assets Referral Report**

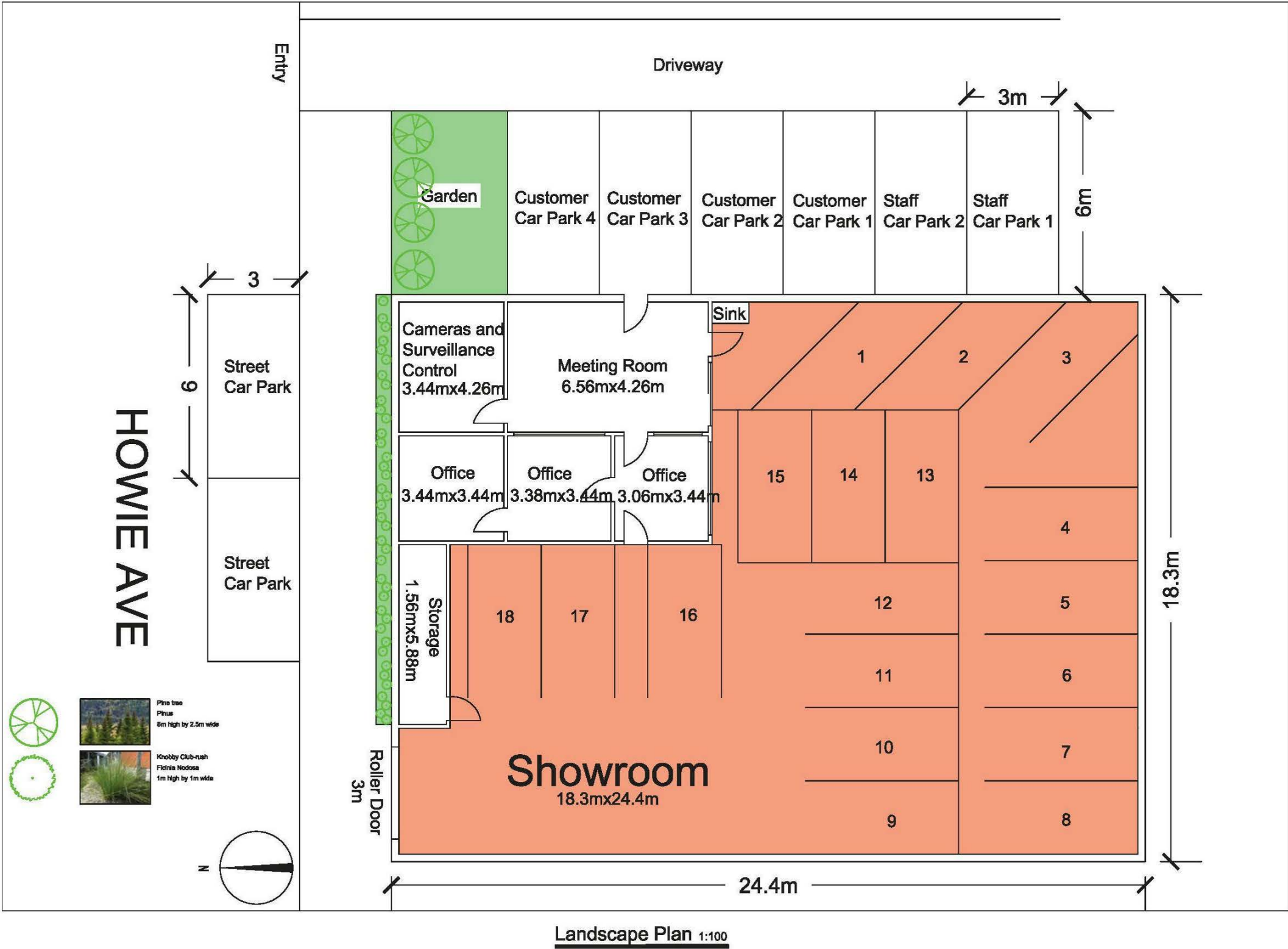




TENANCY 5, 2-4 BRAY AVE, TORRENSVILLE 5031

SO EASY AUTO  
Date: 19 Nov 2017





TENANCY 5, 2-4 BRAY AVE, TORRENSVILLE 5031

SO EASY AUTO  
Date: 19 Nov 2017



Elevation

Tenancy 5, 2-4 Bray Ave, Torrensville 5031 SA  
SO EASY AUTO

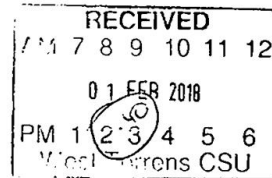
**STATEMENT OF REPRESENTATION**  
**Pursuant to Section 38 of the Development Act 1993**

TO Chief Executive Officer  
 City of West Torrens  
 165 Sir Donald Bradman Drive  
 HILTON 5033

City of West Torrens

02 FEB 2018

City Development



DEVELOPMENT No. 211/1430/2017  
 PROPERTY ADDRESS: 2-4 Bray Avenue, TORRENSVILLE SA 5031

YOUR FULL NAME	DAN XIONG
YOUR ADDRESS	1 BRAY AVE. TORRENSVILLE 5031
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	(eg. Adjoining resident, owner of land in the vicinity etc.)
REASON/S FOR REPRESENTATION	I'm resident of the neighbor. We don't want more noisy here. we want change to resident's zone. I don't want open the door, face to a car yard. too many cars drive around !!!
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought)	Reject this applications please !!!

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission:

I DO NOT WISH TO BE HEARD

I DESIRE TO BE HEARD PERSONALLY

I DESIRE TO BE REPRESENTED BY \_\_\_\_\_  
 (PLEASE SPECIFY)



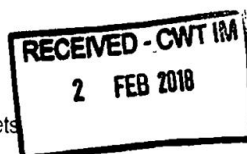
Please see  
 the attached  
 letter for further  
 reasons. →

SIGNED

DATE 30/01/18

Responsible Officer: Amelia DeRuvo

Ends: Friday 9 February 2018



Thank You!

If space insufficient, please attach sheets



I would ask that council for the city of west torrens reject this application for the following reasons. I am a neighbouring residence living directly across in front from the location for the proposed car yard. We are within 10 metres distance of the proposed location. We have lived here 18 years and intend to continue living here.

- 1) Pollution would be a primary concern. Given that this is a small suburban street the level of smog from the number of cars going in and out would be apparent and have negative effects on the local air quality particularly for neighbouring houses. We used to live opposite a furniture manufacturer and we suffered a long time due to the pollution and chemical outflow from the building. They finally moved out and the air quality in that area improved significantly. There would also likely be more rubbish placed around the location such as car parts and repair supplies.
- 2) Kids Safety. We have children from nearby houses who often ride their bikes down this street. The increased daily traffic would make this street much less safe which would be unfair given that this is a suburban street and not a main road.
- 3) The noise level for nearby houses would increase due to the import of cars or customers that come. This would be unfair for the neighbours as well as discomforting
- 4) If this car dealership also opens on the weekend then the neighbours who would otherwise be resting during this time would have their rest disrupted by any happenings at the dealership
- 5) When friends or family come over to the neighbouring houses for special occasions or barbecues there would be less space for parking.
- 6) Some of our residents do shift work. If the dealership is opened seven days with customers and car imports coming in and out, the noise would prevent us from getting as much rest. This would be incredibly stressful for not just us but others as well as they would have to go to work without proper rest.
- 7) There is potentially the risk for an increased crime rate with people breaking into the cars at the dealership or stealing parts. This may then increase the chance of other peoples property getting vandalised being so close to a dealership. Even now we have occasionally had incidents involving vandalism and break ins and having a car lot would probably increase the likelihood of further crime.
- 8) As the proposal is asking to setup a second hand car dealership, there is the potential that the clientele seeking to buy these cars may be of suspect origins
- 9) There would potentially be an increase in traffic on the weekends which again would disrupt peoples rest for those who actually live right next to the location
- 10) A dealership right in the middle of a suburban area would severely devalue our properties in this area which would be completely unreasonable. A dealership is more suitable for a main commercial road and not next to suburban homes.
- 11) This proposal would also have a severe impact on my nearby rental properties as the rental value would also drop because no one would want to live near a car dealership due to the aforementioned reasons.
- 12) Having a car dealership in front of my home would also compromise the nearby environment and general view from the front of my house.
- 13) We have also had a recent city council meeting regarding the rezoning of our area. We want to change our zone to a residential zone. This proposal would harm this idea and make it less liveable in the future.

## Preliminary Traffic, Flooding & Stormwater Assessment

**Development Application No:** 211/1430/2017

**Assessing Officer:** Amelia DeRuvo  
**Site Address:** 2-4 Bray Avenue, TORRENSVILLE SA 5031  
**Certificate of Title:** CT-5443/413, CT-5447/190, CT-5431/18, CT-5447/161  
**Description of Development:** Change of use from motor vehicle repair station to service trade premises (second hand motor vehicle dealership) with associated advertising signage (Unit 5)

### TO THE TECHNICAL OFFICER - CITY ASSETS

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Amelia DeRuvo**

**DATE 22 January, 2018**



## Memo

To Amelia DeRuvo  
From Jane Teng  
Date 22 Jan 2018  
Subject 211/1430/2017, 2-4 Bray Avenue, TORRENSVILLE SA 5031

Amelia DeRuvo,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

### 1.0 Traffic Comments

The following comments are provided by Council's Traffic Consultant

I note that the existing use of the tenancy is unclear from documentation. However, the existing building layout appears conducive to a warehouse/office type of building. Indeed, other tenancies within the subject site include warehouse/office land uses already.

As the building floor plan does not appear to be changing, the assessment of existing use rights would be appropriate to determine the required car parking provision.

The existing floor areas assumed to be the following:

- Office - 77m<sup>2</sup>
- Warehouse - 369m<sup>2</sup>

Based on the above floor areas, the existing parking requirement would be **9 spaces**.

The new floor areas are the following:

- Office - 77m<sup>2</sup> (4 spaces per 100m<sup>2</sup>)
- Car Showroom - 369m<sup>2</sup> (2 spaces per 100m<sup>2</sup>)

Based on the above floor areas and parking rates, the new parking requirement would be **10 spaces (rounded down)**, an increase in demand of 1 space. However, I note that the 'Planning Bulletin - Parking provisions for selected land uses' (a parking guide commonly used), specifies a parking rate of 1.5 to 2 spaces per 100m<sup>2</sup> for a car showroom land use. If the rate of 1.5 spaces per 100m<sup>2</sup> is used, then the parking requirement would be **9 spaces (rounded up)**. That is, no change from the existing approved use.

In regards to the servicing of the site by larger vehicles (such as deliveries), there would be no change from the existing use rights of the subject site. That is, there





City of **West Torrens**

Between the City and the Sea

would be no change from the existing arrangement. It is likely that deliveries could be dropped off in the main car park with stock being relocated inside the showroom by staff.

Consequently, the proposed change in use from an office/warehouse to office/car showroom is not expected to result in a notable change in parking requirements of servicing of the site based on the existing use rights of the subject site. Therefore, on balance, the proposed change of use would be acceptable from a traffic and parking perspective.

If an approval is granted for the application it is advised that a condition be included to restrict the storage of car 'stock' to within the showroom area only (that is, no storage of cars for sale in the car park).

Should you require further information, please contact Jane Teng on the following direct extension number 8416 6296

Regards

**Jane Teng**  
Civil Engineer

**6.5 46 Mooringe Avenue, NORTH PLYMPTON**

Application No 211/1178/2017

**DEVELOPMENT APPLICATION DETAILS**

DEVELOPMENT PROPOSAL	Combined Application - Community Title land division creating two (2) additional allotments and common property and construction of one (1) single storey detached dwelling and two (2) single storey group dwellings and associated landscaping
APPLICANT	KT Property Investments Pty Ltd
APPLICATION NO	DA: 211/1178/2017 DAC No: 211/C165/17
LODGEMENT DATE	3 October 2017
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 20
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> <li>▪ City Assets</li> <li>▪ City Operations</li> </ul> External <ul style="list-style-type: none"> <li>▪ SA Water</li> <li>▪ DAC</li> </ul>
DEVELOPMENT PLAN VERSION	30 May 2017
MEETING DATE	10 April 2018

**RECOMMENDATION**

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent and Land Division Consent for Application No. 211/1178/2017 by KT Property Investments Pty Ltd to undertake a Combined Application - Community Title land division creating two (2) additional allotments and common property and construction of one (1) single storey detached dwelling and two (2) single storey group dwellings and associated landscaping at 46 Mooringe Avenue, North Plympton (CT 5503/260) subject to the following conditions of consent:

**Development Plan Consent Conditions**

1. That the development shall be undertaken and completed in accordance with the plans and information detailed in this application including the community division plan by State Surveys, Ref No 17440, Drawn by ADL; Site plan and elevation plan by Russo Design, Drawing Numbers PA01 and Site Plan by GAMA Consulting Engineers and Project Management, Drawing Number 17385-C01, Revision B Dated except where varied by any condition(s) listed below.
2. That all driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) and properly drained, and shall be maintained in a reasonable condition at all times.

3. The common driveway shall remain clear of visitor parking to allow vehicles parked within the garages associated with Dwellings 2 and 3 to be able to exit in a forward direction and in an unobstructed manner.
4. That all planting and landscaping will be completed within three (3) months of the occupancy of the development. Any plants that become diseased or die will be replaced with a suitable species.
5. That all stormwater design and construction will be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and for this purpose stormwater drainage will not at any time:
  - a. Result in the entry of water into a building; or
  - b. Affect the stability of a building; or
  - c. Create unhealthy or dangerous conditions on the site or within the building; or
  - d. Flow or discharge onto the land of an adjoining owner; and not flow across footpaths or public ways.
6. That any retaining walls be designed to accepted engineering standards, and not of timber construction if retaining a difference in ground level exceeding 200mm.
7. All external materials and finishes must be non-reflective.
8. That prior to the issue of certificates to the division approved herein, all existing buildings and associated structures shall be removed from subject land.

**Land Division Consent Conditions:****Council Conditions**

Nil

**Development Assessment Commission Conditions**

9. The financial requirements of the SA Water shall be met for the provision of water supply and sewerage services.

The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner / applicant.

10. Payment of \$13,660 into the Planning and Development fund (2 lots @ \$6,830/lot). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
11. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.



## BACKGROUND

The development proposal is presented to the Council Assessment Panel (CAP) for the following reason:

- With regard to residential development and land division applications, where all proposed allotments and or sites fail to meet, nor are within 5% of, the minimum frontage widths and site areas designated in respective zones and policy areas within the West Torrens Council Development Plan.

## PREVIOUS OR RELATED APPLICATION(S)

Development Number	Description	Decision
211/768/2017	Demolition of existing dwelling and associated structures and the construction of three (3) group dwellings	Refused due to a lack of information

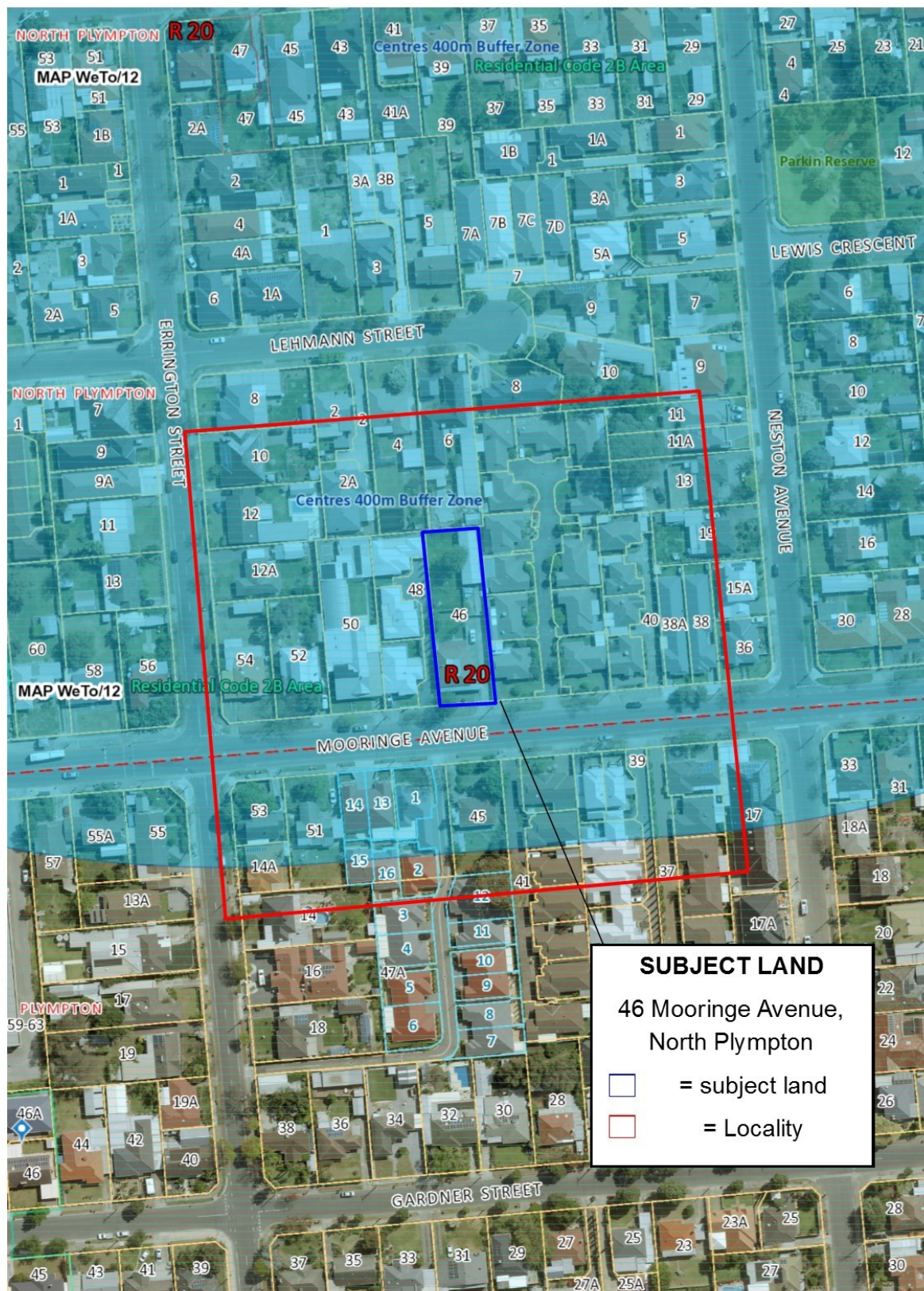
## SITE AND LOCALITY

The subject site is commonly known as 46 Mooringe Avenue, North Plympton. It is formally described in Certificate of Title, Volume 5503, Folio 260 comprising allotment 64 with a frontage to Mooringe Avenue of 18.29 metres. The subject site is a rectangular shaped allotment totalling 1081.8m<sup>2</sup> in area and is relatively flat with no easements.

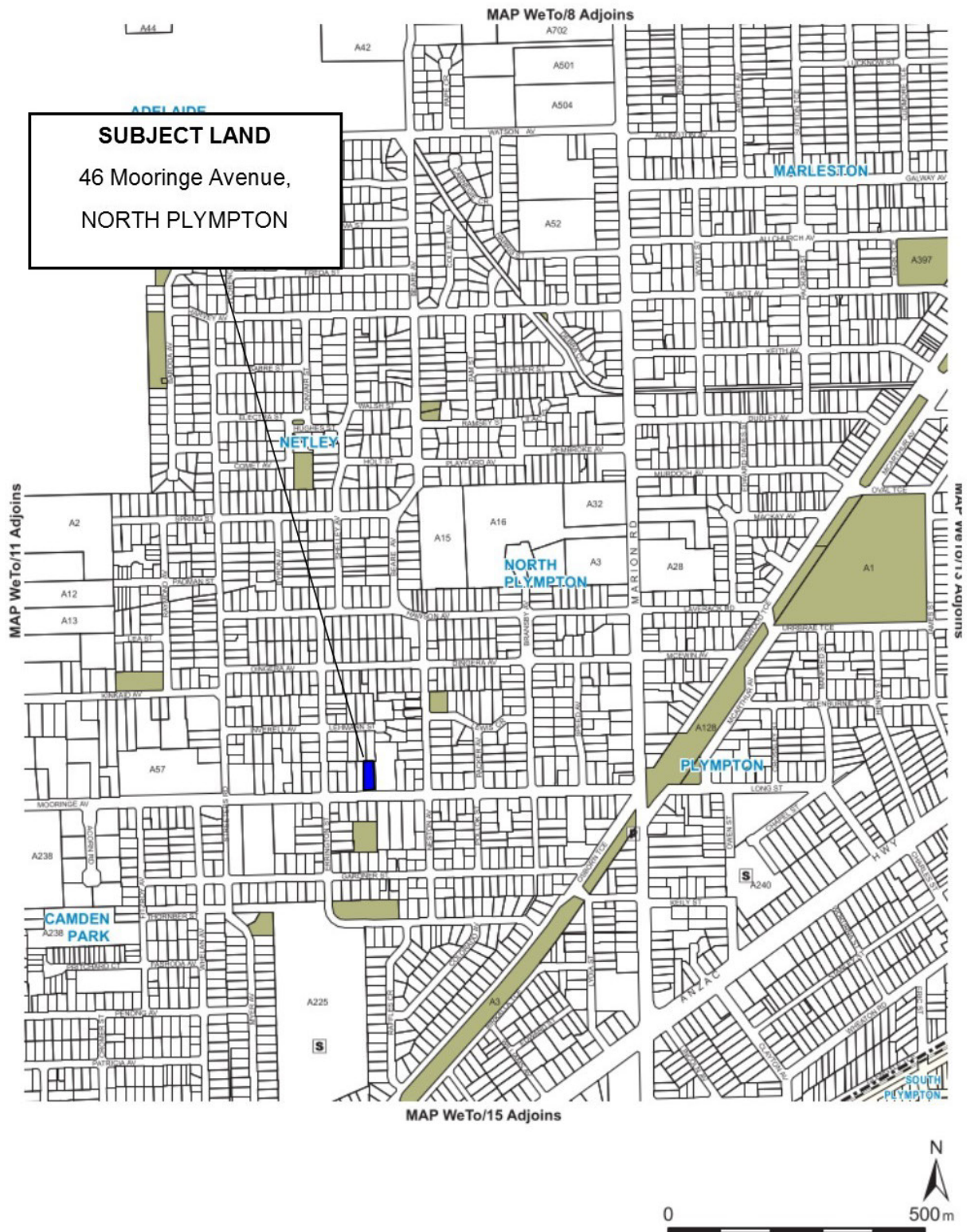
The site currently contains a single storey detached dwelling with an attached carport and a shed in the rear yard. There are no regulated trees on the subject site or on adjoining land that would be affected by the proposed development. Access is currently achieved via a double width crossover on the eastern side of the allotment.

The locality consists of 1930s housing stock with more recent medium density infill development evident consisting of detached dwellings on smaller allotments, group dwellings and residential flat buildings post 2000. The site is within 400 metres of a Centre Zone on the corner of Hawson Avenue and Shelley Avenue. This centre contains a small group of local convenience shops and a number of dwellings.

The subject land and locality map are provided below:







## Location Map WeTo/12

**WEST TORRENS COUNCIL**  
Consolidated - 30 May 2017

## PROPOSAL

The proposed development is best described as:

### *Land Division*

A Community Title land division to create two additional allotments and common property. The proposed site areas and frontages for each allotment are shown in the table below:

Lot	Site Area	Frontage
701	317m <sup>2</sup>	11.2 metres
702	235m <sup>2</sup>	N/A
703	255m <sup>2</sup>	N/A
Common property	274m <sup>2</sup>	7.1 metres

### *Dwellings*

The construction of three (3) single storey dwellings with associated fencing and landscaping.

Dwelling 1 fronts Mooringe Avenue while Dwellings 2 and 3 are group dwellings which front the common driveway.

Each dwelling contains 3 bedrooms, a double garage, internal living areas and an outdoor alfresco area. The dwellings have a modern appearance with a 22.5 degree roof pitch, portico at the front and vertical windows on the front elevations. External materials include brick, render and metal cladding.

A copy of the plan of division and dwelling plans is contained in **Attachment 1**.

## PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Part 1, 2 (a) (ii) of Schedule 9 of the *Development Regulations 2008* therefore public notification was not required.

## REFERRALS

### *Internal*

- City Assets

The following comments were provided:

- A minimum finished floor level of 100.35 is required. This has been addressed satisfactorily by the Applicant.
- Verge interaction should be considered. The development requires the removal of a street tree and 1 metre separation should be achieved from the stobie pole. The proposed distance of around 850mm is satisfactory.
- Stormwater outlets and connections to meet Council's requirements have been addressed satisfactorily by the Applicant.
- The driveway access meets Council's requirements in terms of offsets, width and flaring to the kerb.
- Garage dimensions are a minimum of 5.8 metres achieving the requirement.
- Manoeuvring would not work if the driveway were used for visitor parking - this is reflected on the proposed plan of division and is therefore addressed.
- Stormwater detention is required to be addressed. The Applicant has added details on the civil plan including detention tank details that are considered satisfactory.



- City Operations

Having considered the health, structure, form, useful life expectancy and age of the affected street tree, its removal can be supported. The Applicant will need to pay \$888.00 to compensate for the loss of the tree should development approval be granted.

A copy of the relevant reports are contained in **Attachment 2**.

*External*

- SA Water

SA Water has raised no concerns with the proposal. The developer will be required to meet the requirements of SA Water for the provision of water and sewerage services. Standard conditions of consent have been recommended.

- Development Assessment Commission

The Development Assessment Commission has raised no concerns with the proposal. Standard conditions of consent have been recommended.

A copy of the relevant reports are contained within **Attachment 3**.

## ASSESSMENT

The subject land is located within the Residential Zone, Low Density Policy Area 20 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Crime Prevention	Objectives	1
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8
Design and Appearance	Objectives	1, 2
	Principles of Development Control	1, 2, 3, 4, 10, 12, 13, 15, 20, 21
Energy Efficiency	Objectives	1, 2
	Principles of Development Control	1, 2, 3
Infrastructure	Objectives	1, 2, 3
	Principles of Development Control	1, 3, 4, 5, 6, 8, 9, 10, 14, 16
Land Division	Objectives	1, 2, 3, 4
	Principles of Development Control	1, 2, 4, 5, 6, 7, 8, 10, 12, 16, 17
Landscaping, Fences and Walls	Objectives	1, 2
	Principles of Development Control	1, 2, 3, 4, 6
Orderly and Sustainable Development	Objectives	1, 2, 3, 4, 5
	Principles of Development Control	1, 3, 5, 6, 7, 8
Residential Development	Objectives	1, 2, 3, 4
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 14, 15, 16, 18, 19, 20, 21, 28, 30, 31
Siting and Visibility	Objectives	
	Principles of Development Control	7, 8

Transportation and Access	Objectives	1, 2, 3, 4
	Principles of Development Control	1, 2, 8, 10, 11, 14, 16, 18, 23, 24, 30, 32, 34, 35, 36, 37, 40, 41, 43, 44, 45
Waste	Objectives	1, 2
	Principles of Development Control	

<b>Zone: Residential Zone</b>		
<b>Desired Character Statement:</b>		
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>		
Objectives	1, 2, 3, 4	
Principles of Development Control	1, 5, 6, 7, 8, 11, 12, 13	

<b>Policy Area: Low Density Policy Area 20</b>		
<b>Desired Character Statement:</b>		
<p><i>Allotments in the policy area will be at low density, accommodating predominantly detached dwellings and some other dwellings types such as semi-detached and group dwellings. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.</i></p>		
Objectives	1	
Principles of Development Control	1, 2, 4	

**QUANTITATIVE ASSESSMENT**

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

<b>DEVELOPMENT PLAN PROVISIONS</b>	<b>STANDARD</b>	<b>ASSESSMENT</b>
<b>SITE AREA*</b> <i>Low Density Policy Area 20</i> <i>PDC 4</i>  *exclusive of common property	Minimum 300m <sup>2</sup> (detached and group dwellings)	Dwelling 1 - 317m <sup>2</sup>  <b>Satisfies</b>  Dwelling 2 - 235m <sup>2</sup> Dwelling 3 - 255m <sup>2</sup>  <b>Does not Satisfy</b>
<b>SITE FRONTAGE</b> <i>Low Density Policy Area 20</i> <i>PDC 4</i>	Minimum 9 metres	Dwelling 1 - 11.2 metres  <b>Satisfies</b>  Dwelling 2 - N/A Dwelling 3 - N/A
<b>BUILDING HEIGHT</b> <i>Residential Zone</i> <i>PDC 6</i>	Maximum of 2 storeys (above natural ground level)	Dwellings 1-3 are single storey  <b>Satisfies</b>
<b>STREET SETBACK</b> <i>Residential Zone</i> <i>PDC 8</i>	At least the average setback of the adjacent buildings.	48 Mooringe Avenue street setback is 4.5 metres; 42-44 Mooringe Avenue street setback is 6.5 metres  Average is 5.5 metres  5.0 metres  <b>Does not Satisfy</b>



<p><b>SIDE SETBACKS (DWELLING)</b>  <i>Residential Zone</i>  PDC 11</p>	<p>1 metre minimum where the vertical side wall is 3 metres or less</p>	<p><u>Eastern setback</u></p> <p>Dwelling 1 - 1.2 metres  Dwelling 2 - nil  Dwelling 3 - 900mm</p> <p><u>Western setback</u></p> <p>Dwelling 1 - 3.6 metres  Dwelling 2 - 3.6 metres  Dwelling 3 - nil</p> <p><b>Does not Satisfy</b></p>
<p><b>WALL ON BOUNDARY (GARAGE)</b>  <i>Residential Zone</i>  PDC 13(b) (i) and (ii)</p>	<p>Side boundary wall less than 3 metres in height</p> <p>Side boundary wall maximum length of 8 metres</p>	<p>Dwelling 1 - 2.7 metres  Dwelling 2 - 2.7 metres  Dwelling 3 - 2.7 metres</p> <p><b>Satisfies</b></p> <p>Dwelling 1 - 5.8 metres  Dwelling 2 - 5.7 metres  Dwelling 3 - 5.7 metres</p> <p><b>Satisfies</b></p>
<p><b>REAR SETBACKS</b>  <i>Residential Zone</i>  PDC 11</p>	<p>Minimum 3 metres</p>	<p>3 metres</p> <p><b>Satisfies</b></p>
<p><b>PRIVATE OPEN SPACE</b>  <i>Residential Development</i>  PDC 19</p>	<p>60m<sup>2</sup> with a minimum dimension of 2 metres for site areas between 300m<sup>2</sup> and 500m<sup>2</sup></p> <p>Minimum 24m<sup>2</sup> with a minimum dimension of 3 metres for allotments up to 300m<sup>2</sup></p>	<p>Dwelling 1 - 76.3m<sup>2</sup> with a minimum dimension of 3 metres</p> <p>Dwelling 2 - 56m<sup>2</sup> with a minimum dimension of 3 metres</p> <p>Dwelling 3 - 75m<sup>2</sup> with a minimum dimension of 3 metres</p> <p><b>Satisfies</b></p>
<p><b>LANDSCAPING</b>  <i>Landscaping, Fences &amp; Walls</i>  PDC: 4</p>	<p>Minimum 10% of the site</p>	<p>11.7%</p> <p><b>Satisfies</b></p>

CARPARKING SPACES <i>Transportation and Access</i> PDC 34	At least 2 car-parking spaces per dwelling, one of which is covered and an additional 0.25 per dwelling	2 per dwelling (both undercover) and 1 visitor space  <b>Satisfies</b>
DOMESTIC STORAGE <i>Residential Development</i> PDC 31	Minimum 8m <sup>3</sup>	Dwelling 1 - 4m <sup>3</sup> Dwelling 2 - 4m <sup>3</sup> Dwelling 3 - 4m <sup>3</sup>  <b>Does not Satisfy</b>

## QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the most relevant considerations are discussed under the following sub headings:

### Land division

The development provides a land division that creates three additional parcels of land, each accommodating a dwelling, and common land that will facilitate provision of a driveway, landscaping and common services. The division is consistent with the built form and is considered orderly and economic. The development complies with Objectives 1 and 2 of the Land Division module of the General Section.

The three allotments that will be created allow for north facing private open space. This complies with Objective 3 of the Land Division module that encourages the energy efficient orientation of buildings.

In terms of site areas, the two group dwellings fall short of the Development Plan requirement (Low Density Policy Area 20, PDC 4) of 300m<sup>2</sup> while Dwelling 1 meets the requirement. Dwelling 2 is proposed on a site of 235m<sup>2</sup> and Dwelling 3 on a site of 255m<sup>2</sup>. These two rear dwellings nonetheless provide adequate private open space, setbacks, carparking and on-site manoeuvring areas. The siting of the dwellings on these smaller allotments has not compromised the overall design and space around each dwelling with the proposed allotments considered suitable for their intended use.

The locality accommodates a variety of smaller allotment sizes, including the properties adjacent the subject site at 48 Mooringe Avenue and 42-44 Mooringe Avenue where the allotment sizes are similar to the proposed development. Additionally, the site is in close proximity to shops and public transport where higher density development is encouraged. While the proposed division achieves a slightly higher density than envisaged in Low Density Policy Area 20, it nonetheless creates allotments that are of a suitable size for their intended residential use. Accordingly, it is considered that the proposal is consistent with PDC 5 of Land Division provisions of the General Section.

### Desired character

The desired character of Low Density Policy Area 20 seeks low density, predominantly detached dwellings but with some group dwellings. The proposed detached and group dwellings are therefore considered an appropriate form of development and generally consistent with Objective 1 of the policy area. While the proposed development of 3 dwellings on a site of 1081.8 m<sup>2</sup> is defined as 'medium' density in the Development Plan, these dwellings are nonetheless single storey, located within 400 metres of a Centre Zone and adjacent to higher density residential development. In addition, the garages are not in front of the proposed dwellings nor do they dominate the streetscape. For these reasons the proposed development is considered to be consistent with the desired character of the policy area and the Residential Zone more generally.

**Site frontage**

Site frontage is required to be a minimum of 9 metres. Dwelling 1 achieves this. The driveway width is 7 metres at the Mooringe Avenue entrance and while this meets the engineering requirement to accommodate functional access and egress to and from the proposed dwelling sites, it does not meet the site frontage requirement. However, there are several examples of narrow site frontages to allow access for group dwellings in the immediate locality, including the neighbouring property at 48 Mooringe Avenue, so this pattern of development is not unreasonable.

**Street setback**

The primary face of Dwelling 1 is setback 5 metres from Mooringe Avenue. This is marginally short of the average setback of the two adjoining dwellings. The front of this dwelling is staggered with the porch set slightly forward of the dwelling, the garage at 6 metres and bedroom 1 at 6.3 metres from the street. Despite being slightly forward of the average of the two adjoining dwellings, the street setback of the proposed development is considered satisfactory.

**Side setbacks and garages on boundary**

Each of the proposed dwellings have double garages positioned on the boundary. The height and length of these garages comply with PDC 13 (b) (i) and (ii) of the Residential Zone and do not dominate the streetscape. Dwelling 1 has its garage located on the eastern boundary which abuts the neighbouring garage at Unit 1/42-44 Mooringe Avenue. Accordingly, it is unlikely to have any adverse effect on the neighbouring property.

Similarly, the proposed garage for Dwelling 3 will abut a double garage at 3/48 Mooringe Avenue and will therefore have a minimal impact on that property.

Dwelling 2 or the middle dwelling of the proposed development has its double garage on the eastern boundary adjacent 3/42-44 Mooringe Avenue for a length of 6.9 metres. This is adjacent the western area of private open space of the adjacent dwelling. The impact of the garage in this location is considered to be negligible given the extensive covered area of the rear yard of this dwelling.

**Rear setback**

Proposed Dwelling 3 has a rear setback that is consistent with the Development Plan requirement (PDC 11 of the Residential Zone). The rear setback is adjacent 6 Lehmann Street to the north which includes a deep private open space area and, more specifically, a swimming pool and shed. Accordingly the rear setback of the proposed development is considered to be satisfactory.

**Domestic storage**

The three dwellings have 4m<sup>3</sup> of storage space within the roof space of the garage, accessible via a ladder. While this fails to meet the Development Plan requirement (PDC 31 of the Residential Development section) each of the proposed dwellings have areas for additional storage. Dwelling 1 has laundry cupboards, Dwelling 2 has a linen cupboard and cupboards within the garage, and Dwelling 3 has a hallway cupboard and several laundry cupboards. Given the allowance for storage within each dwelling and in the roof space of the garages, it is considered that the proposal provides sufficient domestic storage.

**Access and parking**

The driveway entrance to the rear two dwellings meets Council requirements with a minimum area of 6.1 metres x 5 metres. Adequate parking is provided for the two group dwellings and the manoeuvring has been assessed as satisfactory by City Assets. The garage lengths for Dwellings 2 and 3 have been extended to a minimum of 5.8 metres which comfortably accommodates a car (and a laundry in Dwelling 2).

Vehicle manoeuvrability will not work if visitor parking occurs forward of the garages associated with Dwellings 2 and 3. For this reason a condition has been added requiring this space to remain unrestricted.

**Design and appearance**

The development presents well to the street with a single storey detached dwelling with a 25 degree roof pitch. The front elevation of Dwelling 1 provides a double garage, combination of materials and a porch over the front door that helps to break up the design. The proposal also includes landscaping and a front fence that is generally consistent with the local streetscape.

**Landscaping**

The development meets the landscaping requirement of 10% with a variety of planting to the front of Dwelling 1 and along the common driveway.

The proposed landscaping areas are considered to be consistent with *Landscaping, Fences and Walls PDCs 1, 2, 4, 5 and 6* in that landscaping has been incorporated as part of the design and utilises appropriate species that will assist in softening the driveway, fencing and built form of the development.

**Stormwater management**

The proposed method of stormwater disposal includes a combined 800 litres for on-site detention and 1000 litres to be plumbed to the laundry or toilet. The proposed system is considered to adequately cater for and manage stormwater arising from the development whilst preventing overflow to adjoining owners and the street. This is in accordance with *Natural Resources PDC 9* in having adequate provision to control stormwater run-off.

**SUMMARY**

The proposed development provides for 3 dwellings on a site that has sufficient width and area to accommodate a development of this density. The development presents well to Mooringe Avenue, provides sufficient private open space, internal living areas, driveway width and carparking and satisfies most of the quantitative provisions of the Development Plan.

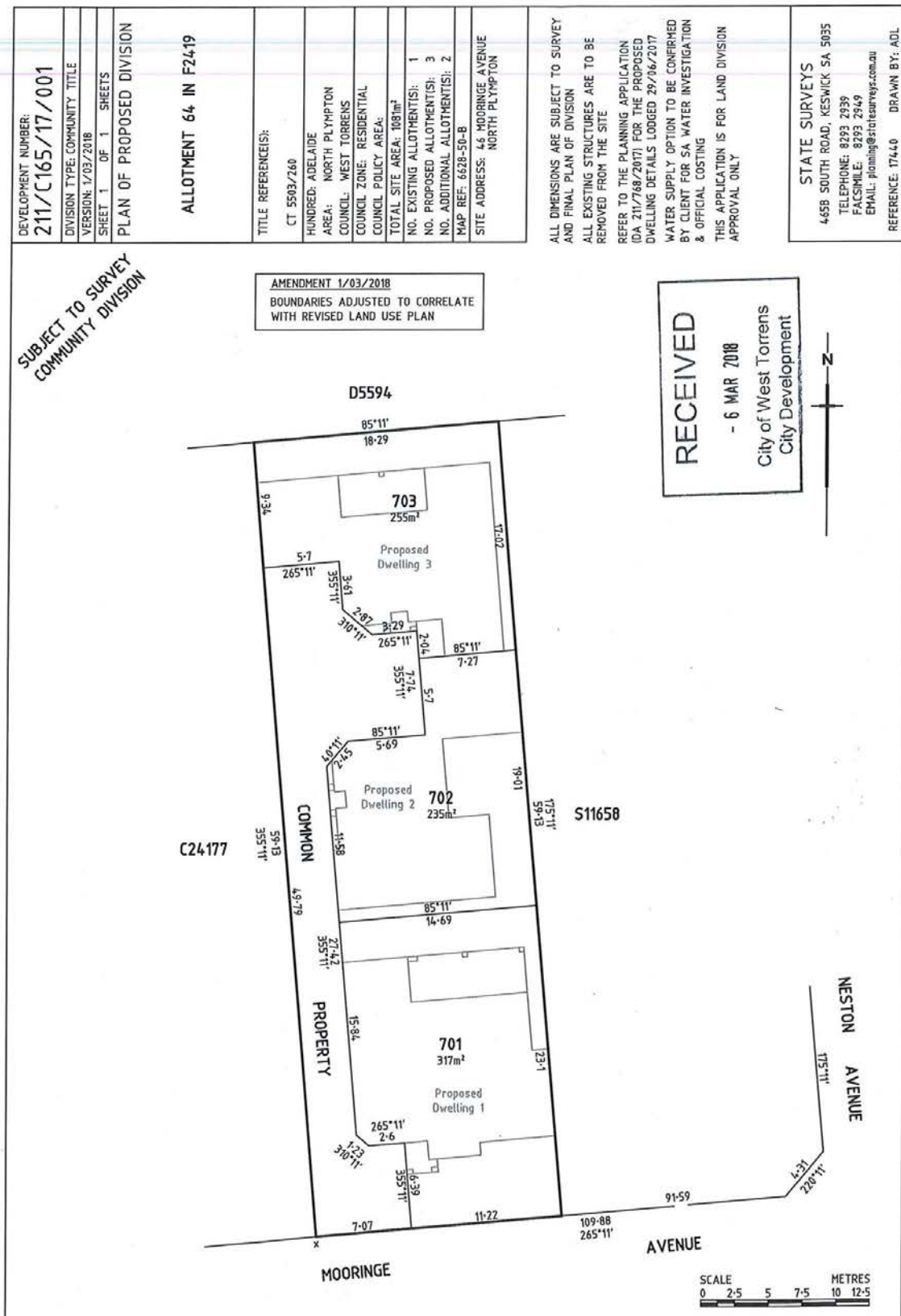
Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 30 May 2017 and warrants Development Plan Consent and Land Division Consent subject to conditions.

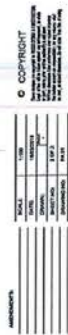
**Attachments**

1. **Proposal Plans**
2. **Internal Referrals**
3. **External Referrals**

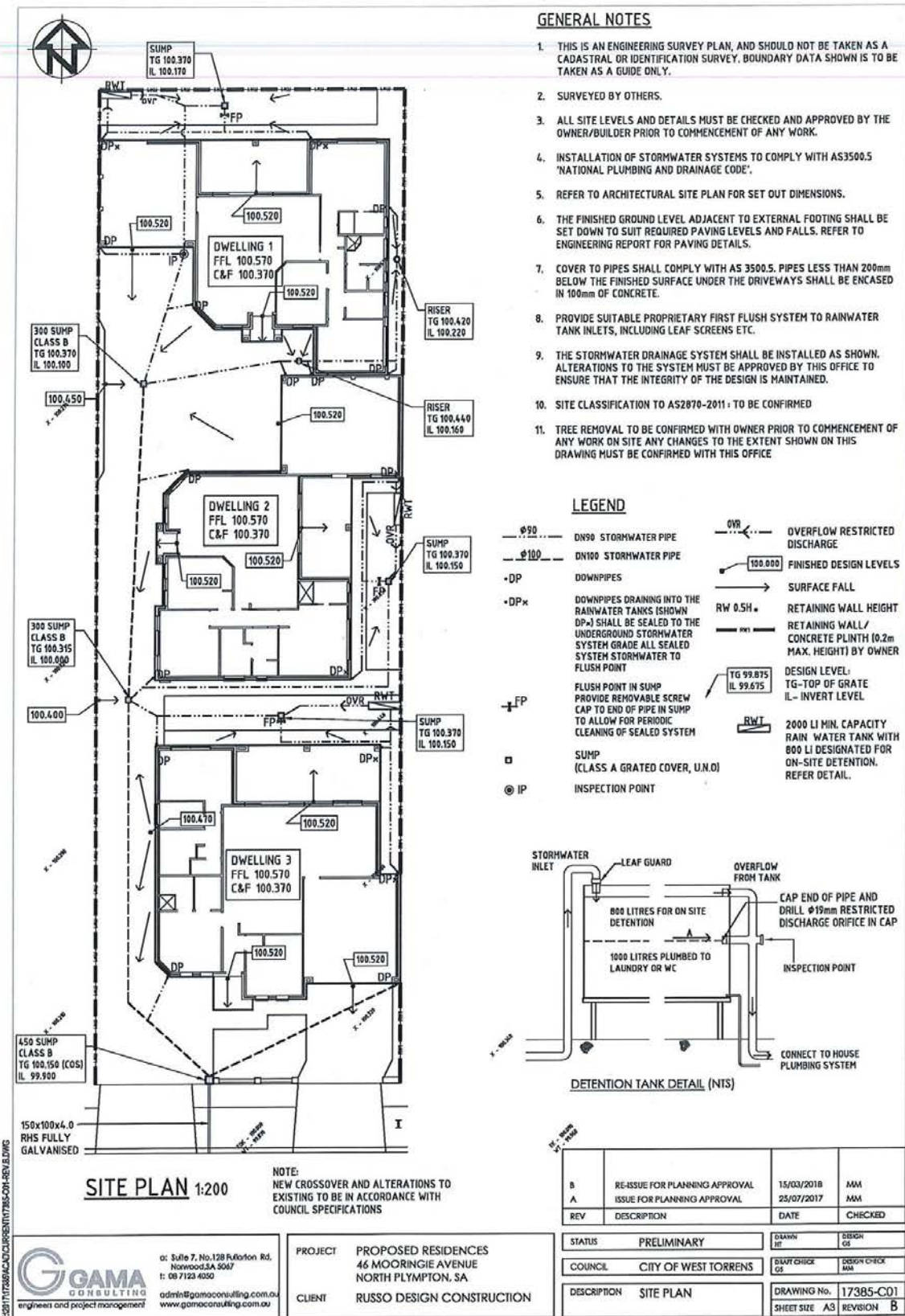














## Preliminary Traffic, Flooding & Stormwater Assessment

**Development Application No:** 211/1178/2017

**Assessing Officer:** Brendan Fewster  
**Site Address:** 46 Mooringe Avenue, NORTH PLYMPTON SA 5037  
**Certificate of Title:** CT-5503/260  
**Description of Development:** Combined Application: Land division - Community Title; SCAP No. 211/C165/17 (Unique ID 59567), Create two (2) additional allotments; and Construction of one (1) single storey detached dwelling and two (2) single storey group dwellings, all with front porticos, rear verandahs and double garages under main roof

### TO THE TECHNICAL OFFICER - CITY ASSETS

Please provide your comments in relation to:

- ☐ Site drainage and stormwater disposal
- ☐ Required FFL
- ☐ On-site vehicle parking and manoeuvrability
- ☐ New Crossover
- ☐ Your advice is also sought on other aspects of the proposal as follows:

.....  
.....

**PLANNING OFFICER - Brendan Fewster**

**DATE 27 November, 2017**



## Memo

To Brendan Fewster  
From Jane Teng  
Date 5/12/2017  
Subject 211/1178/2017, 46 Mooringe Avenue, NORTH PLYMPTON SA 5037

Brendan Fewster,

The following City Assets Department comments are provided with regards to the assessment of the above development application:

### **1.0 General Finished Floor Level (FFL) Consideration**

It is noted that there are discrepancy in the site survey levels between proposed site plan undertaken by RDC (Sheet 1, Council received 6/11/2017) and site plan undertaken by GAMA Consulting ( Dwg No: 17385-01, Rev A). A conservative assessment has been adopted in the determination of the required minimum FFL level for the site.

Based on the survey information provided on 'Proposed Site Plan', (RDC-Sheet 1, Council received 6/11/2017), a minimum FFL of 100.35 would be required.

**It is recommended that revised plans indicating the required minimum FFL be provided to Council.**

### **2.0 Verge Interaction (with street tree)**

2.1 In association with new development, driveways and stormwater connections through the road verge need to be located and shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties. Any new driveway access shall be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.

New driveways and stormwater connections are typically required to be located a minimum 1.0 metre offset from other existing or proposed driveways, stormwater connections, stobie poles, street lights, side entry pits and pram ramps, etc. (as measured at the front property boundary).

These new features are also desired to be located a minimum of 2.0 metres from existing street trees, although a lesser offset may be



City of West Torrens

Between the City and the Sea

acceptable in some circumstances. If an offset less than the desired 2.0 metres is proposed or if it is requested for the street tree to be removed, then assessment for the suitability of such will be necessary from Council's Technical Officer (Arboriculture).

**It is recommended that revised plans indicating satisfaction to the above requirements should be provided to Council and advice from the City Arborist is requested in regards to the removal of existing street tree fronting the site.**

- 2.2 Stormwater outlet to Residence 3 shall be positioned to the eastern side of the driveway at 1.0m offset distance.

Whilst the common stormwater outlet to the rear dwellings (Res 1 and Res 2) shall be repositioned to the western side of the main driveway mimicking the layout as depicted in the provided Sketch in Section 3.1.

It should also be nominated for the stormwater connection through the road verge area to be constructed of shape and material to satisfy Council's standard requirements.

- 100 x 50 x 2mm RHS Galvanised Steel or
- 125 x 75 x 2mm RHS Galvanised Steel or
- Multiples of the above.

**It is recommended that revised plans clearly and accurately indicating satisfaction of the above criteria be provided to Council.**

### **3.0 Driveway Access**

#### **3.1 Rear Dwellings Res 1 and Res 2**

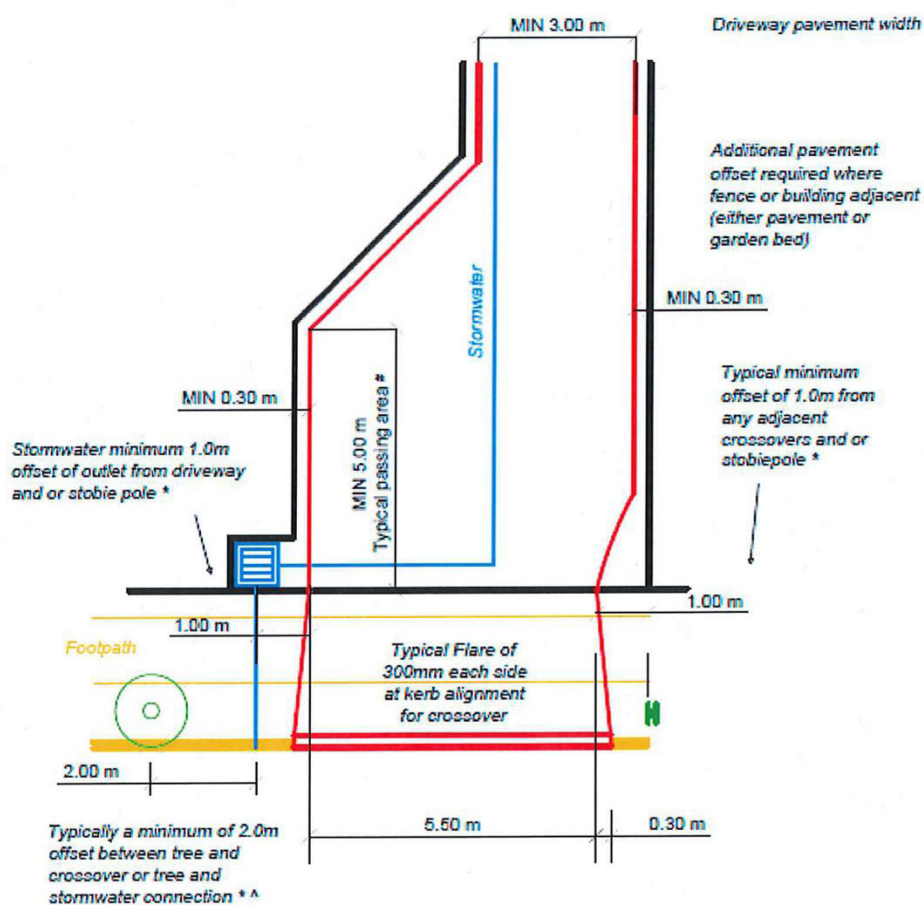
As the access driveway will service more than one property at the rear, the driveway corridor to the site will require widening to a minimum of 5.5m wide pavement width (+ 300mm offset from fences/walls/boundary) for the first 5.0m into the site to permit the passing of entering and exiting traffic. The proposed driveway falls short of this requirement. Please refer to the attached sketch for a typical layout.

**It is recommended that the driveway servicing the rear of the subject site be revised to the required dimensions indicated above. Revised plans showing a driveway that satisfies the above provisions should be provided to Council.**





## ACCESS ARRANGEMENT SERVICING TWO REAR PROPERTY



### NOTES:

- \* Distance as measured along alignment of front property boundary.
- ^ Must be deemed to comply by Council's Technical Officer (Amenity).
- # No aboveground structure(s) such as letterboxes, service meters or similar are to be installing within the common driveway entrance and passing area.

It is also important to ensure that the functionality of this driveway entrance and passing area is not compromised by the ultimate installation of letterboxes, above ground service metres or similar. Provided plans should indicate location for letterboxes and meters which would satisfy this requirement.





It is recommended that any approval associated with this development included a condition of similar wording to the following;

"No aboveground structure(s) such as letterboxes, service meters or similar are to be installed within the common driveway entrance and passing area."

### **3.2 Dwelling Res 3**

City Assets supports the driveway access arrangement for Res 3 of 5.20m at the front property boundary.

### **4.0 Garage Dimensions and Setback**

The internal garage length proposed as 5.80m meets the minimum internal length of 5.80m as specified in the traffic engineering best practice guides of an enclosed garage or enclosed carport space on all Dwellings.

The garage setback of 5.5m minimum length from the property boundary has also been maintained on Res 3.

### **5.0 Traffic Comments (Res 1 and Res 2)**

Traffic manoeuvrability for vehicles in the double garage systems for Res 1 and Res 2 have been assessed as acceptable in accordance with the site layout shown in 'Floor Plan & Site PL' (Russo Design Construction – Plan Set received by Council 6/11/2017, No Revision and Date Observed). No additional car parking spaces will be required for the Res 1 and Res 2 as each dwelling has provided two parking spaces which are covered, a requirement required under the City's development plan.

A simplistic internal manoeuvrability checks indicates that should 2 vehicles are parked behind the garage doors of Res 1 and Res 2, vehicles would not be able to exit the site in the forward direction. As such, the common driveway area of the battle axe allotment would only be sufficient vehicles to enter, turnaround and exit the site in the forward direction. Any allocation to use common driveway area as visitor parking spaces would not be feasible as this would impede vehicles parked in the garage systems to reserve out onto the common area.

Hence, it is cognisant that the driveway pavement as depicted on the floor plan & site plan is shown only as a common driveway and reflected accordingly on the proposed plan of division for the site. The visitor carparks to Res 1 and 2 are recommended for removal on the proposed development plans of this application.



## 6.0 Stormwater Detention

As the size of allotment(s) being affected by the proposed development totals between 1000 and 4000 square metres, stormwater detention measures will be required to be undertaken to restrict the total discharge from the total development site to a maximum of 20 litres per second for the site critical 20 year ARI storm event.

In calculating the stormwater detention requirements, runoff from any existing structures and buildings to be maintained must be taken into consideration.

**It is recommended that an indication of how the storage is to be provided and calculations supporting the nominated volume be submitted to Council.**

It is noted that the stormwater detention measures are in addition to the compulsory Building Code of Australia (BCA) stormwater re-use requirement that is necessary for the new dwellings. For clarity the BCA required rainwater re-use storage should also be indicated on the plans.

To encourage improved Water Sensitive Urban Design measures within the proposed development, once the necessary extent and distribution of detention storage has been acceptably calculated, Council will permit this storage to be traded (on a one to one basis) and added to the compulsory BCA active stormwater re-use storage.

Should you require further information, please contact Jane Teng on the following direct extension number 8416 6333.

Regards

**Jane Teng**  
**Civil Engineer**

## Arboricultural Assessment of Street Trees

Development Application No: 211/1178/2017

### REFERRAL DUE DATE:

Assessing Officer:	Brendan Fewster
Site Address:	46 Mooringe Avenue, NORTH PLYMPTON SA 5037
Certificate of Title:	CT-5503/260
Description of Development	Combined Application: Land division - Community Title; SCAP No. 211/C165/17 (Unique ID 59567), Create two (2) additional allotments; and Construction of one (1) single storey detached dwelling and one (1) single storey residential flat building comprising two (2) dwellings, all with front porticos, rear verandahs and double garages under main roof

### TO THE TECHNICAL OFFICER – CITY ASSETS

Please provide your comments in relation to:

- ☐ **The removal of or impact upon the Street Tree**
- ☐ **Species of Tree:**
- ☐ **Your advice is also sought on other aspects of the proposal as follows:**

PLANNING OFFICER - Brendan Fewster

DATE 16 November 2017

### FROM THE TECHNICAL OFFICER

**I have examined the plans as requested and provide comments as follow.**

As with all development applications it must be proven beyond reasonable doubt that all alternatives have been explored so not to hinder the progress of any street tree(s).

Any proposed development that does not consider "AS4970 Protection of Trees on Development Sites", is likely to require revision until all plans accurately correspond with the specific tree information detailed in this standard.

Verge interaction must consider all services that cross council land including stormwater outlets (and other) which will need to be maintained a minimum of 2.0m from any existing street tree (unless existing or otherwise negotiated).

**A site investigation has revealed and together with the information provided that the location of the proposed crossover for residence 3 will directly impact on an existing *Jacaranda mimosifolia* street tree.**



With reference to the City of West Torrens, Fees and Charges Document 2017-2018 "Tree removal for driveway construction", once Council has assessed all circumstances and considered it acceptable that a street tree can be removed, a fee is calculated based on Council's standard schedule of fees and charges.

The fee is used to offsets the loss of the asset (street tree) to the community, with funds received invested in Council's annual Greening Program.

**As a result of the proposed crossover for Residence 3 on Mooringe Avenue, City Operations has considered the health, structure, form, useful life expectancy, and age of the street tree and will support the removal.**

**A fee of \$888.00 will be required prior to the commencement of any work.**

Please note, under no circumstances is any individuals other than council staff permitted to interfere with a street tree. If pruning etc. is required, council must be notified via the appropriate request, and council staff will perform all works associated with the community asset.

Final crossover locations will be confirmed once appropriate consultation has been received from the applicant and they have submitted "an application to construct a vehicle crossing place(s) across council land".

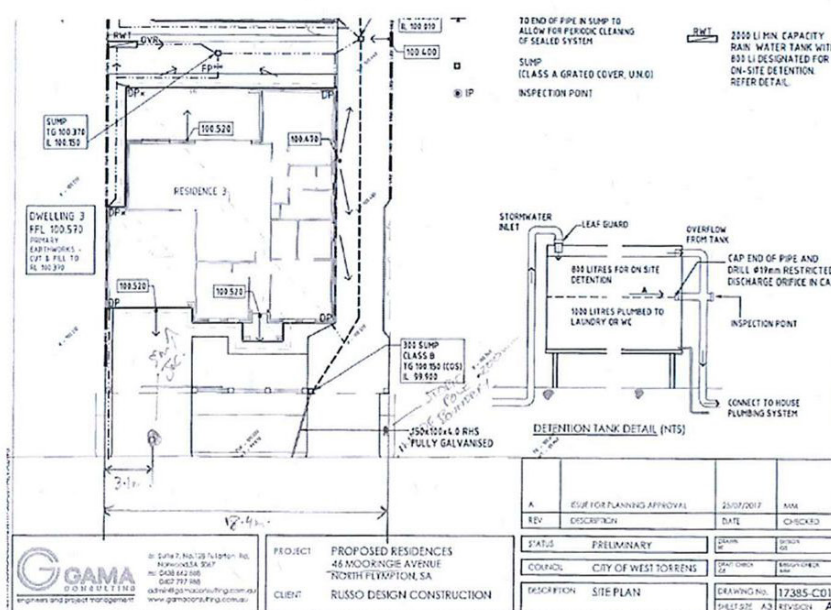
**Rick Holmes**  
**Arboriculture Officer**  
**165 Sir Donald Bradman Drive**  
**Hilton SA 5033**

**Telephone: 8416 6333**

**Fax: 8443 5709**

**Email: [rholfmes@wtcc.sa.gov.au](mailto:rholfmes@wtcc.sa.gov.au)**

**DATE: 24/11/2017**













11 October 2017

Our Ref: H0064660

The Chairman  
Development Assessment Commission  
136 North Terrace  
ADELAIDE SA 5000

Dear Sir/Madam

**PROPOSED LAND DIVISION APPLICATION NO: 211/C165/17 AT NORTH PLYMPTON**

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.

Yours faithfully

Wendy Hebbard

**for MANAGER LAND DEVELOPMENT & CONNECTIONS**

SA Water  
Level 6, 250 Victoria Square  
ADELAIDE SA 5000  
Ph (08) 7424 1119  
Inquiries Wendy Hebbard  
Telephone 7424 1119



Contact - Lands Titles Office  
Telephone 7109 7016



11 October 2017

The Chief Executive Officer  
City of West Torrens

Dear Sir/Madam

**Re: Proposed Application No. 211/C165/17 (ID 59567)  
for Land Division  
(Community Title Plan) by KT Property Investments Pty Ltd**

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 29 September 2017, I advise that the Development Assessment Commission has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the Development Assessment Commission has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.  
The developer must inform potential purchasers of the community lots of the servicing arrangements and seek written agreement prior to settlement, as future alterations would be at full cost to the owner/applicant.
2. Payment of \$13660 into the Planning and Development Fund (2 allotment(s) @ \$6830/allotment).  
Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

**PURSUANT TO REGULATION 60(4)(b)(ii), SHOULD THIS APPLICATION BE APPROVED, COUNCIL MUST PROVIDE THE DEVELOPMENT ASSESSMENT COMMISSION WITH:**

- (a) the date on which any existing building(s) on the site were erected (if known),
- (b) the postal address of the site

It is recommended that this information be incorporated into the Decision Notification Form.

**PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.**

Yours faithfully

A handwritten signature in black ink, appearing to read "P. Hodgson", written over a horizontal line.

Phil Hodgson  
Unit Manager  
Lands Titles Office  
as delegate of  
**DEVELOPMENT ASSESSMENT COMMISSION**



**6.6 2 Aldridge Terrace, MARLESTON**

Application No 211/1421/2017

**DEVELOPMENT APPLICATION DETAILS**

DEVELOPMENT PROPOSAL	Combined Application: Land division - Torrens Title; Create one (1) additional allotment; and Demolition of Existing Dwelling and Construction of two (2) Single Storey Detached Dwellings and the Removal of Two (2) Regulated Trees.
APPLICANT	Mr Carl Poultney
APPLICATION NO	211/1421/2017
LODGEMENT DATE	8 November 2017
ZONE	Residential Zone
POLICY AREA	Medium Density Policy Area 19
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> <li>▪ City Assets</li> <li>▪ Calypso Tree Company</li> </ul> External <ul style="list-style-type: none"> <li>▪ Nil</li> </ul>
DEVELOPMENT PLAN VERSION	30 May 2017
MEETING DATE	10 April 2018

**RECOMMENDATION**

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent and Land Division Consent for Application No. 211/1421 /2017 by Mr Carl Poultney to undertake Land division - Torrens Title; Create one (1) additional allotment; and the Demolition of Existing Dwelling and Construction of two (2) Single Storey Detached Dwellings and removal of two (2) Regulated Trees at 2 Aldridge Terrace, Marleston (CT5313/927) subject to the following conditions of consent:

**Development Plan Consent Conditions**

1. Development is to take place in accordance with the plans prepared by Weber Frankiw and Associates and Stannard Family Homes relating to Development Application No. 211/1421/2017 (SPAC 211/D187/17).
2. Driveways, parking and manoeuvring areas shall be formed (surfaced with concrete, bitumen or paving) and properly drained, and shall be maintained in a reasonable condition at all times.
3. Landscaping will be planted in accordance with the Site Plan prepared by Stannard Family Homes, dated 14/3/2018) within three (3) months of the occupancy of the development. Any person(s) who have the benefit of this approval will cultivate, tend and nurture the landscaping and shall replace any plants which may become diseased or die.

4. All stormwater design and construction will be in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and for this purpose stormwater drainage will not at any time:
  - a) Result in the entry of water into a building; or
  - b) Affect the stability of a building; or
  - c) Create unhealthy or dangerous conditions on the site or within the building; or
  - d) Flow or discharge onto the land of an adjoining owner; and not flow across footpaths or public ways.
5. Within four (4) weeks of the date of tree removal, payment of \$350.00 shall be made into the State Government's Planning and Development Fund, in accordance with Section 42(6) of the *Development Act 1993* and Regulation 117(4) of the *Development Regulations 2008*.

Please contact the State Commission Assessment Panel on 1800752664 (option 5) for more information on making payment.

6. Receipt of payment shall be provided to Council within one (1) week of payment into the State Government's Planning and Development Fund.
7. Prior to the issue of certificates for the division of land approved herein, all existing buildings and structures shall be removed from the proposed Lot 1 and Lot 2.

### **Land Division Consent Conditions**

#### **Council Requirements**

Nil

#### **State Commission Assessment Panel Requirements**

8. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services. Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non-standard fees.

On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

9. Payment of \$6830 into the Planning and Development Fund (1 allotment @ \$6830/allotment). Payment may be made by credit card via the internet at [www.edala.sa.gov.au](http://www.edala.sa.gov.au) or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
10. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

## BACKGROUND

The development proposal is presented to the Council Assessment Panel (CAP) for the following reason:

- With regard to residential development and land division applications, where all proposed allotments and / or sites fail to meet, nor are within 5% of, the minimum frontage widths and site areas designated in respective zones and policy areas within the West Torrens Council Development Plan.

## PREVIOUS OR RELATED APPLICATION

DA 211/828/2010 - Removal of Significant Tree *Corymbia Citriodora* (Lemon Scented Gum)

## SITE AND LOCALITY

The site is a rectangular shaped allotment on the western side of Aldridge Avenue. The allotment is relatively flat with a frontage width of 18.29 metres, a depth of 44.20 metres and a total site area of 808m<sup>2</sup>.

The land contains a detached dwelling with a carport along the northern side and a rear verandah together with a detached outbuilding located along the rear boundary. There are two regulated trees located within the rear yard which are proposed to be removed. The land is located within a 400 metre radius of a centre zone.

The locality comprises a mix of residential and commercial uses which are concentrated along Richmond Road. Residential development is located to the south, east and west of the subject land whilst the land to the immediate north and west are occupied by commercial uses (refer images 1 and 2 below). These abutting commercial properties are built to the common boundary resulting in very high masonry walls along those boundaries. The residential pattern of development is characterised by regular shaped allotments with a mix of dwelling types.





Image 1: Commercial premises to the north-east of the subject land



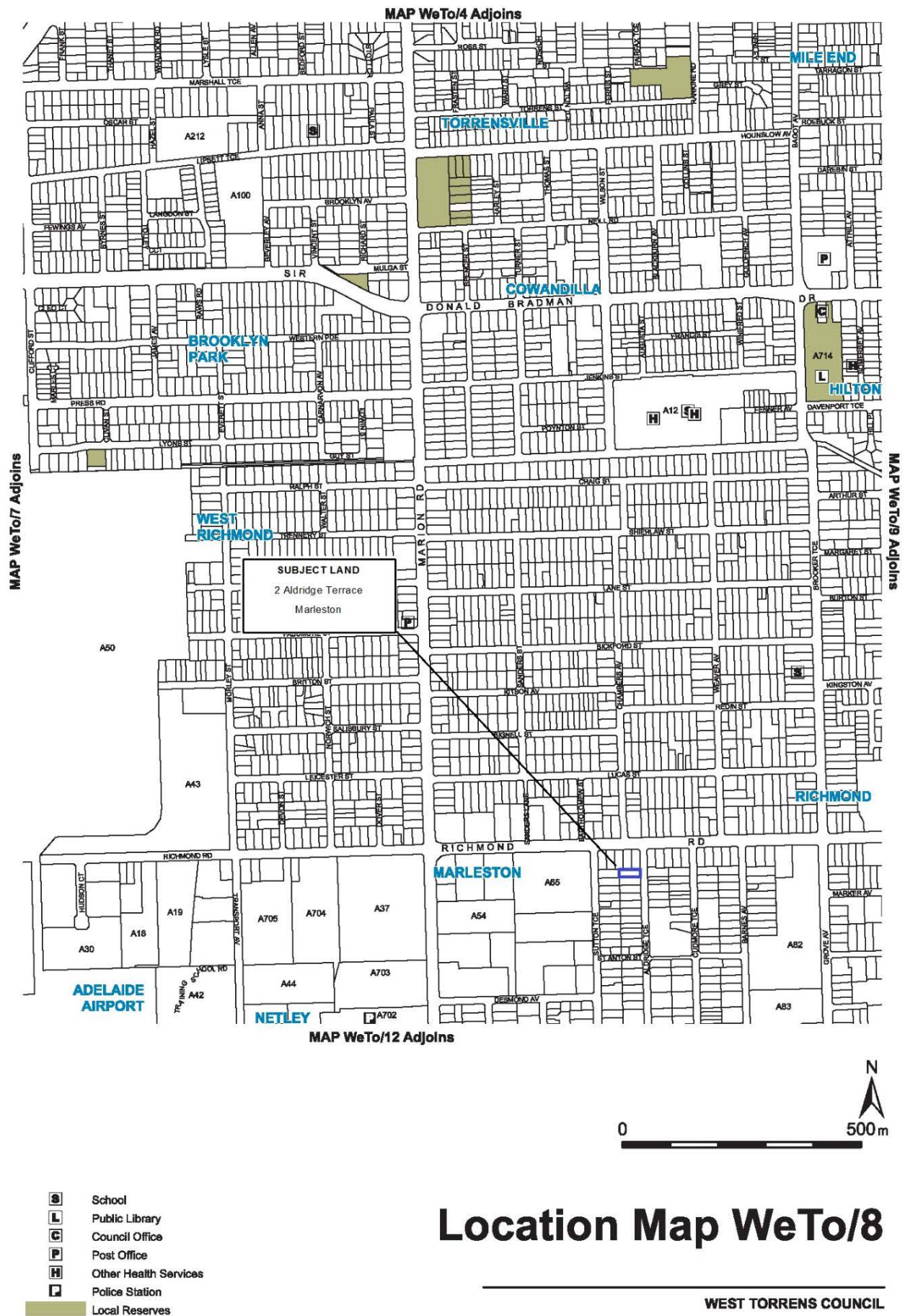
Image 2: Commercial premises to the immediate north of the subject land.

The subject site and locality are shown on the following maps.









## PROPOSAL

### ***Land Division***

The proposal includes the demolition of the existing dwelling and associated outbuildings on the land and the creation of two allotments in a battleaxe configuration. The existing allotment has a frontage of 18.29 metres and site area of 808m<sup>2</sup>.

The front allotment (Lot 1) proposes a frontage of 14.29 metres to Aldridge Terrace and a depth of 24.29 metres having an overall area of 345m<sup>2</sup>. The rear allotment (Lot 2) has a battleaxe configuration. The head of the proposed lot exclusive of the handle is to have an area of 365m<sup>2</sup> and an overall area of 463m<sup>2</sup>.

### ***Land Use - Dwellings***

It is proposed to construct a detached dwelling on each of the allotments. Both dwellings are of modern design with double garages under the main pitched colorbond roof with a 25 degree pitch. Each dwelling comprises three bedrooms, living areas and an undercover alfresco area. Landscaping is proposed to the front and rear of Dwelling 1, along both sides of the driveway and to the rear of Dwelling 2.

It is also proposed to remove two regulated trees that are located at the rear of the existing dwelling. The trees have been assessed by Council's Arborist as discussed below.

A copy of the proposal plans is contained in **Attachment 1**.

## REFERRALS

### *Internal*

- City Assets

Concerns were raised regarding the following matters;

- A minimum FFL of 100.24 is required. As such the FFL for House 2 would need to be revised to the minimum FFL specified.
- That the proposed site plan by Stannard Family Homes- Dated 01/09/17) to be updated to reflect the RCI Consulting Engineers Siteworks Plan.

### *Council's Arborist - Calypso Tree Company*

- Both Regulated Trees are recommended for removal to accommodate reasonable development of the site.

A full copy of the relevant reports are contained in **Attachment 2**.



## ASSESSMENT

The subject land is located within the Residential Zone Medium Policy Area 19 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Crime Prevention	Objectives	1
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9 & 10
Design and Appearance	Objectives	2
	Principles of Development Control	1, 2, 3, 4, 5, 6, 12, 13, 14, 15
Energy Efficiency	Objectives	1 & 2
	Principles of Development Control	1, 2, 3 & 4
Land Division	Objectives	1, 2, 3 & 4
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8 & 12,
Landscaping, Fences and Walls	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4 & 6
Natural Resources	Objectives	1
	Principles of Development Control	1
Orderly and Sustainable Development	Objectives	1, 2, 4 & 5
	Principles of Development Control	1 & 5
Regulated Trees	Objectives	1 & 2
	Principles of Development Control	1, 2 & 3
Residential Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55 & 56
Transportation and Access	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	8, 10 & 23

Zone: Residential Zone
Desired Character Statement:
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p>

*Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a **Historic Conservation Area**.*

*Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.*

<b>Objectives</b>	1, 2, 3 & 4
<b>Principles of Development Control</b>	1, 5, 7, 8, 11, 12, 13, 17, 18, 22,

**Policy Area: Medium Density Policy Area 19**

**Desired Character Statement:**

*Allotments in this policy area will be at medium density, accommodating a range of dwelling types including semi-detached, row and group dwellings, as well as some residential flat buildings and some detached dwellings on small allotments. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones.*

*New buildings will contribute to a highly varied streetscape. Buildings will be up to 2 storeys, except for allotments fronting Brooker Terrace, Marion Road and Henley Beach Road, and overlooking the Westside Bikeway, where buildings will be up to 3 storeys in height and provide a strong presence to streets. Garages and carports will be located behind the front facade of buildings.*

*Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.*

<b>Objectives</b>	1
<b>Principles of Development Control</b>	1, 2, 3 & 5

## QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
SITE AREA <i>Medium Density Policy Area 19</i> <i>PDC 5</i>	250m <sup>2</sup> minimum	Dwelling 1 - 345m <sup>2</sup> Dwelling 2 - 365m <sup>2</sup> (not including the handle)  <b>Satisfies</b>
SITE FRONTAGE <i>Medium Density Policy Area 19</i> <i>PDC 5</i>	9m minimum	Dwelling 1 - 14.29m Dwelling 2 - 4m  <b>Does Not Satisfy</b>

SITE COVERAGE <i>Medium Density Policy Area 19</i> PDC 3	60% maximum	Dwelling 1 - 40%  Dwelling 2 - 49%  <b>Satisfies</b>
PRIMARY STREET SETBACK <i>Medium Density Policy Area 19</i> PDC 3	3 metres minimum	5 metres  <b>Satisfies</b>
SIDE/REAR SETBACKS <i>Medium Density Policy Area 19 :</i> PDC 3	Side 1m minimum - vertical side wall is 3 metres or less.  Rear 6 metres minimum	>1 metre setback  <b>Satisfies</b>  Dwelling 1 - 4.3 metres Dwelling 2 - 3.2 metres  <b>Does Not Satisfy</b>
PRIVATE OPEN SPACE <i>Residential Development</i> PDC 19	Allotments between 300-500 m <sup>2</sup> - 60m <sup>2</sup> min	Dwelling 1 - 77m <sup>2</sup> Dwelling 2 - 100m <sup>2</sup>  <b>Satisfies</b>
LANDSCAPING <i>Landscaping, Fences &amp; Walls</i> PDC: 4	10% minimum	At least 13% of development site is to be landscaped  <b>Satisfies</b>
CARPARKING SPACES <i>Transportation and Access</i> PDC .34	2 car-parking spaces required	2 covered and 1 visitor space per dwelling provided  <b>Satisfies</b>

## QUALITATIVE ASSESSMENT

The key issues arising from the assessment of this application include:

- The appropriateness of the proposed land division creating a battleaxe allotment in this location;
- Whether the location, size, design and appearance of the dwellings is appropriate within the locality;
- Access and egress arrangements to and from the site;
- Whether the removal of the regulated trees on the site is appropriate.



## Land Division

The key consideration with a battleaxe form of land division is whether the resultant layout and configuration of allotments is appropriate within the locality and Policy Area in terms of the prevailing the pattern of development.

Residential Zone Policy Area 19 does not speak against battleaxe allotments as is the case in other Policy Areas. Battleaxe allotments are clearly envisaged in certain areas as demonstrated in General Section Land Division PDC 7 which prescribes the following:

Allotments in the form of a battleaxe configuration should:

- a) have an area of at least the minimum site area specified by the zone, policy area or precinct (excluding the area of the 'handle' of such an allotment);
- b) provide for an access onto a public road, with the driveway 'handle' being not less than:
  - i. 4 metres in width to facilitate landscape planting along the driveway, and
  - ii. 5.5 metres for at least the first 5 metres of the driveway for an allotment accommodating two or more dwellings to allow vehicles to pass safely.

On examination of the locality it is evident that there exists a range of medium density developments with a mix of dwelling types ranging from detached dwellings on individual allotments to residential flat buildings and group dwellings.

Although there are no other battleaxe allotments with detached dwellings in the locality, there are several examples of residential flat buildings and group dwellings on community title arrangements which have a similar layout and presentation to the street as battleaxe allotments. The distinction between them when viewed from the street is nominal and, for the most part, indiscernible in terms of the land tenure.

Accordingly, the proposal is considered to satisfy the Desired Character for the Policy Area and the intent of Principle of Development Control 1 of Medium Density Policy Area 19.

## Site Area and Frontage

Principle of Development Control (PDC) 5 of Medium Density Policy Area 19 prescribes a minimum site area of 250m<sup>2</sup> for a detached dwelling where located within 400 metres of a centre zone. As the development site is within 400 metres of a centre zone the minimum site area required is 250m<sup>2</sup>, which should be considered when assessing both applications.

The land division proposal indicates that proposed Lot 1 (Dwelling 1) would have a site area of 345m<sup>2</sup> and Lot 2 (Dwelling 2) would have a site area of 365m<sup>2</sup>. Accordingly, both proposed lots exceed the minimum site area standard. The proposed dwellings have been designed and sited in a manner that reasonably satisfies the relevant quantitative requirements relating to building height and scale, boundary setbacks, landscaping, private open space, site coverage and vehicle access and car parking.

Whilst the allotment frontage of Lot 2 only achieves a width of 4 metres, this is inherent with battleaxe allotments by their very nature (with have a narrow driveway 'handle' and a larger 'head' for the siting of a dwelling). In considering the appropriateness of the 4 metre frontage it is necessary to consider General Section Land Division PDC 7 which describes the nature of battleaxe allotments in terms of site areas and frontages.

Allotments in the form of a battleaxe configuration should:

- a) Have an area of at least the minimum site area specified by the zone, policy area or precinct (excluding the area of the 'handle' of such an allotment);
- b) Provide for an access onto a public road, with the driveway 'handle' being not less than:
  - i. 4 metres in width to facilitate landscape planting along the driveway, and
  - ii. 5.5 metres for at least the first 5 metres of the driveway for an allotment accommodating two or more dwellings to allow vehicles to pass safely.

When the above principle is considered, Allotment 2 achieves the minimum width to facilitate access prescribed in the Development Plan and also exceeds the minimum site area prescribed for detached dwellings in Policy Area 19. On this basis the frontage is considered reasonable and appropriate and will not unduly impact the prevailing streetscape character.

### **Boundary Setbacks**

#### *Front Setback*

PDC 3 of Medium Density Policy Area 19 requires dwellings to be setback at least 3 metres from the primary road frontage. The main front wall of Dwelling 1 is setback at a distance of 5 metres with the garage component setback at 5.8 metres. The front setback is considered to accord with Medium Density Policy Area 19 Principle of Development Control 3.

#### *Rear Setback*

PDC 3 of Medium Density Policy Area 19 requires dwellings to be setback at least 6 metres from the back boundary. Proposed Dwelling 2 achieves a rear setback to the dwelling component of 5.4 metres and 2.6 metres to the garage.

Notwithstanding the rear setback deficiency, the siting of the proposed dwelling in relation to the rear boundary is considered acceptable given that the majority of rear boundary is comprised of a 5.5 - 6 metre high masonry wall. In addition, the rear site is occupied by a non-residential use. Accordingly, the reduced rear setback is not considered to have any measurable impacts on the adjoining western property.



Extent of the existing rear wall along the western boundary.



Extent of wall along the side northern boundary of the site

### Regulated Trees

There are two regulated trees located on the site which are proposed to be removed as part of the proposal (see image below).



Tree 1: *Agonis flexuosa* (Willow Myrtle)      Tree 2: *Citharexylum fruticosum* (Fiddlewood)

Tree 1 is an *Agonis flexuosa* (Willow Myrtle) and Tree 2 is a *Citharexylum fruticosum* (Fiddle Wood).

The application was referred to Council's consulting arborist for comment and has been assessed against the following provisions of the Development Plan:



### General Section Regulated Trees

Objective 1: The conservation of regulated trees that provide important aesthetic and/or environmental benefit.

Objective 2: Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:

- a) significantly contributes to the character or visual amenity of the locality
- b) indigenous to the locality
- c) a rare or endangered species
- d) an important habitat for native fauna.

The trees do not contribute to the character or visual amenity of the locality, are not indigenous, are not rare or endangered and are not considered to be important habitat for native fauna.

Having established the above, the trees must then be assessed against Principle of Development Control 2 which states:

PDC 2: A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:

- a) the tree is diseased and its life expectancy is short
- b) the tree represents a material risk to public or private safety
- c) the tree is causing damage to a building
- d) development that is reasonable and expected would not otherwise be possible
- e) the work required for the removal of dead wood, treatment of disease or is in the general interests of the health of the tree.

In relation to PDC 2, the *Agonis flexuosa* is considered to be in poor health. The sparse canopy contains large amounts of deadwood, as well as tip-die back and yellowing of the foliage.



Tree 1 *Agonis Flexuosa* Willow Myrtle located along the southern boundary



The presence of Longicorn Beetles (borers) is confirmed by the accumulation of small oval shaped exit holes on the main stem. When trees are stressed they emit higher levels of nitrogen, which attracts boring insects. Pests often capitalise on a tree's ill health, which further accelerates the tree's decline.

This mature tree is considered 'fully grown' in its current growing environment and due to the symptoms present it is considered to be at the end of its useful life expectancy.

Having given consideration to the plans provided, and observations made of the tree, it is clear that the sustainable retention of this regulated tree is not achievable. Accordingly, the application for its removal to enable reasonable development within this area is supported.

In relation to Tree 2, the arboriculture assessment concluded the following:

- the tree has no major structural defects
- tree does not represent an unacceptable risk to public and private safety
- the tree does not make an important contribution to the character or amenity of the local area
- the tree is not indigenous to the locality
- the tree does not provide an important habitat for native fauna.



Tree 2 *Cithareylum fruticosum* (Fiddlewood) located along the northern side boundary

As an introduced species Tree 2 does not provide suitable nesting hollows, is not part of a wildlife corridor and has limited habitat value. It is not important to the maintenance of biodiversity in the local environment and does not form a notable visual element within the local landscape.

Other than achieving a regulated trunk measurement, the subject tree does not achieve any other aesthetic or environmental attributes that qualify it as a regulated tree.

Having given consideration to the plans provided, and observations made of the tree, it is concluded that the *Citharexylum fruticosum* is not suitable for retention in its current urban environment. Its removal to enable reasonable development within this area is supported.

### **Stormwater management**

The site works and civil plan illustrates the proposed method of stormwater disposal. It is considered that the proposed system shall adequately cater for and manage stormwater associated with the development and will prevent overflow to adjoining owners and the street.

### **Landscaping**

The development meets the minimum landscaping requirement of 10% with a variety of planting to the front of the Dwelling 1, along both sides of the driveway and to the rear of Dwelling 2 proposed.

The proposed landscaping areas are considered to be acceptable and consistent with the relevant provisions of the Development Plan in that landscaping has been incorporated as an integral part of the design and utilises appropriate species that will assist in softening the driveway, fencing and built form of the development.

### **Transport and Access**

The proposed dwellings are each provided with two undercover car parking spaces plus additional parking in front of the garages.

The access arrangements for each dwelling has been designed in accordance with Council Policy relating to the separation of driveways and distance from street trees and infrastructure. Access to the rear dwelling accords with *Land Division PDC 7* by the inclusion of a 4 metre-wide driveway allowing for landscaping and a 3 metre-wide apron. Vehicles associated with the rear dwelling have a sufficient turning area to allow vehicles to exit the site in a forward direction.

The proposal is considered to accord with the requirements of *Transportation and Access PDCs 34, 35 and 36*.

### **SUMMARY**

When balanced against the existing site and locality characteristics and the Desired Character for Medium Density Policy Area 19, the proposed division of land and associated dwellings is considered to be a desirable, orderly and appropriate form of development.

The dwelling density and allotment layout of the proposal sufficiently accords with the Desired Character and is compatible with the established pattern and built form characteristics of the locality. The proposed development is also unlikely to negatively impact on the amenity of adjoining properties.




The proposed dwellings are designed and sited on the subject land in a manner that is appropriate for the streetscape and will not detrimentally affect adjoining land by way of unreasonable bulk or massing, overlooking or overshadowing.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 30 May 2017 and warrants Development Plan Consent and Land Division Consent subject to conditions.

### **Attachments**

- 1. Application Plans**
- 2. Referral Documents**



Search | Configure Notification Email | Log Out |

### Application Form Details - DA. 211/D187/17 (Unique Id. 59978)

[Cancel](#)

**NB \*** represents a mandatory field.

Application Type :	<input type="text" value="D - Conventional Land Division"/>	*
What Consents do you wish to apply for? :	<input type="radio"/> Provisional Development Plan Consent Only	
	<input checked="" type="radio"/> Provisional Development Plan Consent with Land Division Consent	
Certificate of Approval Fee:	<input checked="" type="radio"/> Pay Now	
	<input type="radio"/> Pay Later	
Is this a combined Land Division/Land Use application? :	<input checked="" type="radio"/> Yes	
	<input type="radio"/> No	
Council :	<input type="text" value="City of West Torrens (211)"/>	*
Submitting Agents OrganisationId :	<input type="text" value="430"/>	
Submitting Agents Name :	<input type="text" value="Weber, Frankiw &amp; Associates"/>	
Submitted By UserId :	<input type="text" value="541"/>	
Submitted By Details :	<input type="text" value="Scott Vasey, Weber, Frankiw &amp; Associates"/>	
Agents Reference :	<input type="text" value="7546V"/>	

---

#### APPLICANT DETAILS :

Applicant Salutation :	<input type="text" value="Mr"/>	*
Applicant First Name :	<input type="text" value="Carl"/>	*
Applicant Last Name :	<input type="text" value="Poultney"/>	*
Applicant Organisation Name :	<input type="text"/>	*(Mandatory if Salutation, First, and Last Name have not been completed)
Postal Address Line 1 :	<input type="text" value="C/O"/>	*
Postal Address Line 2 :	<input type="text" value="Agent"/>	*
Postal Address Line 3 :	<input type="text"/>	
State :	<input type="text" value="South Australia"/>	
Country :	<input type="text" value="AUSTRALIA"/>	*

---

#### OWNER DETAILS :

Owner Salutation :	<input type="text" value="Mr"/>	*
Owner First Name :	<input type="text" value="Carl"/>	*
Owner Last Name :	<input type="text" value="Poultney"/>	*
Owner Organisation Name :	<input type="text"/>	*(Mandatory if Salutation, First, and Last Name have not been completed)
Postal Address Line 1 :	<input type="text" value="C/O"/>	*
Postal Address Line 2 :	<input type="text" value="Agent"/>	*
Postal Address Line 3 :	<input type="text"/>	
State :	<input type="text" value="South Australia"/>	
Country :	<input type="text" value="AUSTRALIA"/>	*

[https://www.cdala.sa.gov.au/edala/Edala.aspx?PageMode=ApplicationMaintain\\_CSB&ApplicationId=59978](https://www.cdala.sa.gov.au/edala/Edala.aspx?PageMode=ApplicationMaintain_CSB&ApplicationId=59978)[22/11/2017 11:19:19 AM]



**OWNER DETAILS :**

Owner Salutation : Mrs \*

Owner First Name : Tracey \*

Owner Last Name : Poultney \*

Owner Organisation Name : \* (Mandatory if Salutation, First, and Last Name have not been completed)

Postal Address Line 1 : C/O \*

Postal Address Line 2 : Agent \*

Postal Address Line 3 : \*

State : South Australia \*

Country : AUSTRALIA \*

**CONTACT DETAILS :**

Contact Salutation : Mr \*

Contact First Name : Scott \*

Contact Last Name : Vesey \*

Postal Address Line 1 : The Centre \*

Postal Address Line 2 : 178 Main Road \*

Postal Address Line 3 : McLaren Vale 5171 \*

State : South Australia \*

Country : AUSTRALIA \*

Telephone 1 : 8323 8991 \*

Telephone 2 : \*

Fax 1 : 8323 9686 \*

Fax 2 : \*

Email : survey@weberfrankiw.com.au \*

Mobile : \*

Existing Use :

Residential \*

Description of Proposed Development :

1 in to 2 Land Division \*

**SUBJECT PROPERTY DETAILS :**

House Number : \*

Lot Number : \*

Street : Aldridge \*

Suburb/Town : Marlestone \*

Hundred : Adelaide \*

Reference Section : \*

**TITLE DESCRIPTION DETAILS :**

Title Reference Type : CT - Certificate of Title \*

Volume : 5313 \*

Folio : 927 \*

[https://www.cdala.sa.gov.au/cdala/Edala.aspx?PageMode=ApplicationMaintain\\_CSB&ApplicationId=59978\[22/11/2017 11:19:19 AM\]](https://www.cdala.sa.gov.au/cdala/Edala.aspx?PageMode=ApplicationMaintain_CSB&ApplicationId=59978[22/11/2017 11:19:19 AM])

APPLICATION DOCUMENTS							Hide
Document Title	Document Type	Version #	State	File Size (Kb)	Date Uploaded	Notes	
7546div	Proposed Plan of Division	1	Uploaded	50.3500000	08 Nov 2017	Show	

**PLAN PARCEL DETAILS :**

Plan Type :  \*

Deposited Plan Number :  \*

Parcel Number :  \*

---

Does either schedule 21 or 22 of the Development Regulations 1993 apply? : ☐ Yes  
☒ No

Notes :

Lodgement Date :

Acknowledgement : I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the Development Regulations 1993  
☒ Yes  
☐ No

**CONVENTIONAL LAND DIVISION :**

Total Area of Land to be Divided :  Square metres \*

Reserve Area :  Square metres \*

Number of existing allotments :  \*

Number of proposed allotments :  \*

Number of additional allotments :

---

**COMBINED LAND DIVISION / LAND USE :**

Do you wish to provide builder information? : ☐ Now  
☒ Later

---

Building Rules Classification sought? : ☐ Yes  
☒ No

Present Classification :

If class 5,6,7,8,9 is sought, state the proposed number of employees :  
 Male :   
 Female :

If class 9a is sought, state the proposed number of persons for whom accommodation is provided :

Is classification 9b sought? : ☐ Yes  
☒ No

Has the Construction Industry Training Fund Act 1993 levy been paid? : ☐ Yes  
☒ No

Development Cost - \$ (do not include fit-out cost) :  \*

---

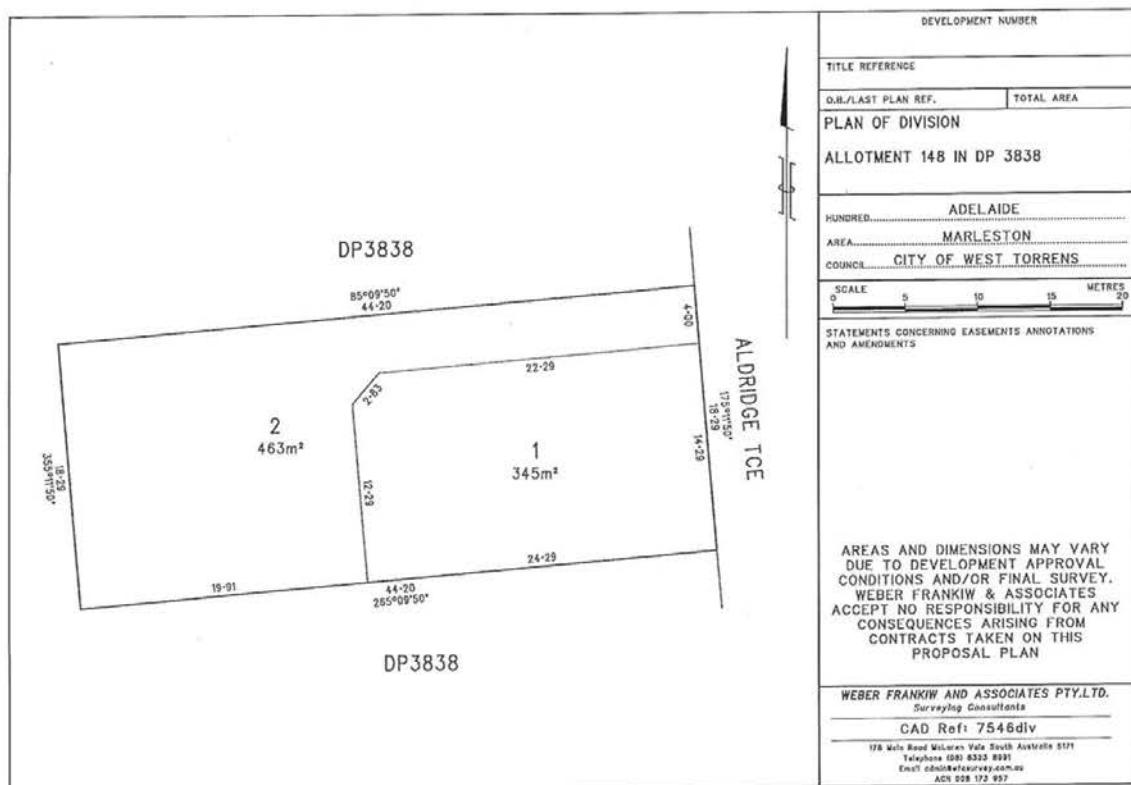
[https://www.cdala.sa.gov.au/cdala/Edala.aspx?PageMode=ApplicationMaintain\\_CSB&ApplicationId=59978\[22/11/2017 11:19:19 AM\]](https://www.cdala.sa.gov.au/cdala/Edala.aspx?PageMode=ApplicationMaintain_CSB&ApplicationId=59978[22/11/2017 11:19:19 AM])

CT 5313/927	Certificate of Title/Lease	1	Uploaded	18.9600000	08 Nov 2017	Show
Lodgement Fee Receipt	Miscellaneous	1	Uploaded	29.3800000	08 Nov 2017	Show
24033 Prelim Footing report New	Miscellaneous	1	Uploaded	1618.4700000	08 Nov 2017	Show
2AldT Prelim Plan New	Miscellaneous	1	Uploaded	712.6300000	08 Nov 2017	Show
Location plan - enlargement New	Miscellaneous	1	Uploaded	158.9900000	09 Nov 2017	Show
Location plan New	Miscellaneous	1	Uploaded	283.9100000	09 Nov 2017	Show
LOTS Admin Interests New	Miscellaneous	1	Uploaded	14.3000000	09 Nov 2017	Show
DAC Regulation 29 Letter New	Miscellaneous	1	Uploaded	2.1100000	09 Nov 2017	Show

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59978 e

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Date created: November 9, 2017



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PLB Pro - developed by DPTI GIS Office

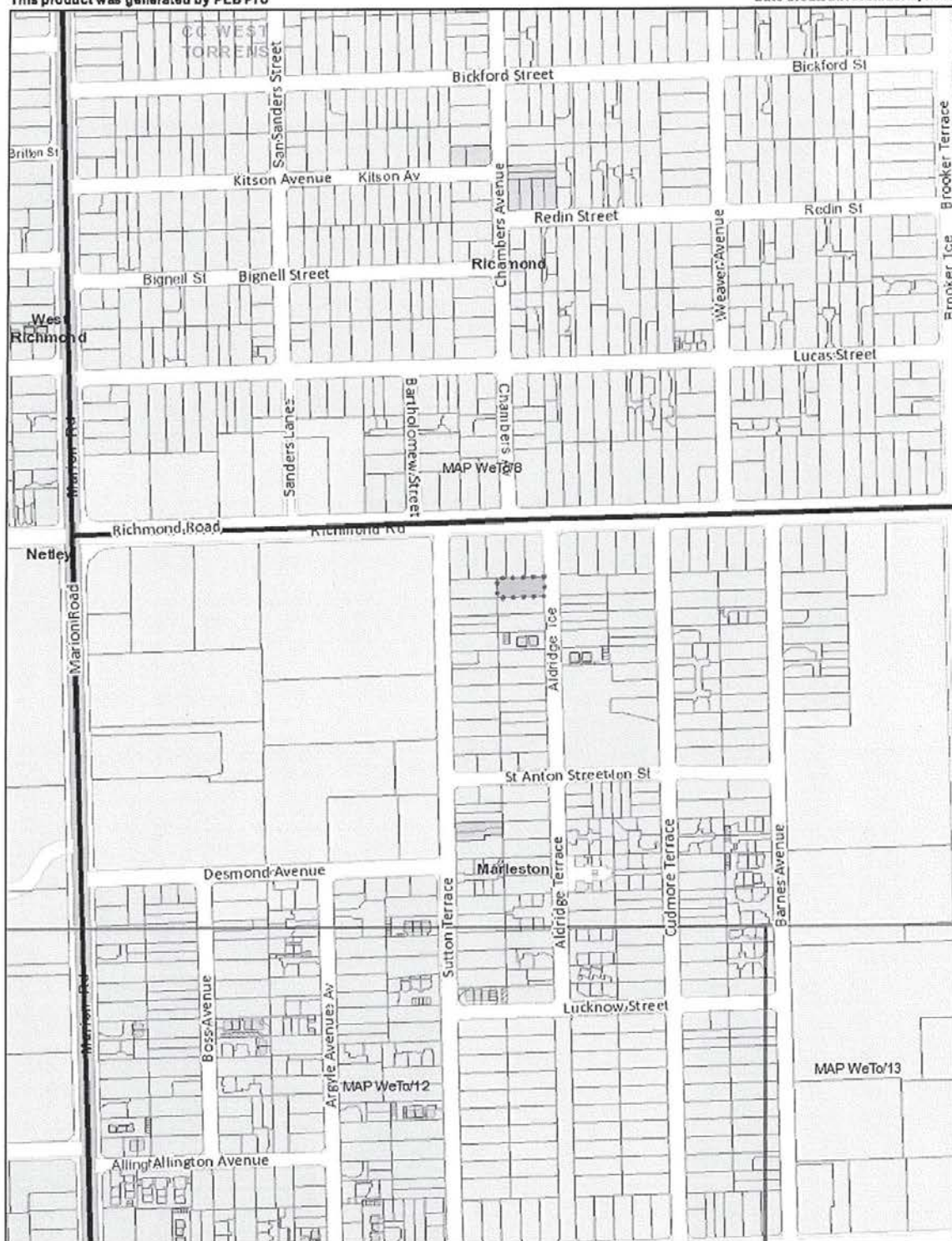
[DPTI.GISOoffice@sa.gov.au](mailto:DPTI.GISOoffice@sa.gov.au)

Government of South Australia  
Department of Planning,  
Transport and Infrastructure

59978

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Date created: November 9, 2017



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Government of South Australia  
Department of Planning,  
Transport and Infrastructure

**Government of South Australia**Department of Planning,  
Transport and Infrastructure

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**Tax Invoice / Receipt**

**ABN:** 92 366 288 135  
**Agency:** Department of Planning, Transport and Infrastructure  
**Address:** PO Box 1815, Adelaide SA 5001  
**Phone:** 7109 7018

Fee Description	GST excl.	GST	GST Incl.
Lodgement Fee (additional allotment)	\$209.50	\$0.00	\$209.50
Land Division Fee (additional allotment)	\$158.00	\$0.00	\$158.00
Land Division Fee (per Additional Allotment)	\$14.90	\$0.00	\$14.90
Statement of Requirements Fee (additional allotment)	\$417.00	\$0.00	\$417.00
Certificate of Approval Fee (additional allotments)	\$347.00	\$0.00	\$347.00
DAC Consultation Report Fee (additional allotments)	\$208.00	\$0.00	\$208.00
<b>Total</b>	<b>\$1354.40</b>	<b>\$0.00</b>	<b>\$1354.40</b>

---

**APPLICATION DETAILS**

**Unique Id:** 59978  
**Development Number:** 211/D187/17  
**Agents Reference:** 7546V  
**Applicant:** Mr Carl Poultney  
**Owner:** Mr Carl Poultney  
**Type:** CreditCard  
**Agent:** Weber, Frankiw & Associates  
**Address:** The Centre 178 Main Road McLaren Vale 5171  
South Australia

---

**TRANSACTION DETAILS**

**Received:** Wednesday, 08 Nov 2017  
**Receipt Ref. No:** 78397936914  
**Process Id:** DEV5997820171108111838704  
**Amount Paid:** \$1354.40  
**Payment Method:** Credit Card





Government of South Australia  
Department of Planning,  
Transport and Infrastructure

<b>Product</b>	Title Details
<b>Date/Time</b>	08/11/2017 11:02AM
<b>Customer Reference</b>	
<b>Order ID</b>	20171108003804
<b>Cost</b>	\$10.00

## Certificate of Title

**Title Reference** CT 5313/927  
**Status** CURRENT  
**Easement** NO  
**Owner Number** 17514913  
**Address for Notices** 18 TANDINA WAY KINGSLEY WA 6026  
**Area** 810M<sup>2</sup> (APPROXIMATE)

## Estate Type

Fee Simple

## Registered Proprietor

CARL STEVEN POULTNEY  
 TRACEY LOUISE POULTNEY  
 OF 11 LILLY PARADE BAYNTON WA 6714  
 AS JOINT TENANTS

## Description of Land

ALLOTMENT 148 DEPOSITED PLAN 3838  
 IN THE AREA NAMED MARLESTON  
 HUNDRED OF ADELAIDE

## Last Sale Details

**Dealing Reference** Transfer (T) 9699142  
**Dealing Date** 07/10/2003  
**Sale Price** \$234,500  
**Sale Type** Transfer for full monetary consideration

## Constraints

### Encumbrances

Dealing Type	Dealing Number	Beneficiary
MORTGAGE	12781116	NATIONAL AUSTRALIA BANK LTD. (ACN: 004 044 937)

### Stoppers

NIL

## Valuation Numbers

Valuation Number	Status	Property Location Address
2129418001	CURRENT	2 ALDRIDGE TERRACE, MARLESTON, SA 5033

## Notations

Land Services

Page 1 of 2

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Government of South Australia  
Department of Planning,  
Transport and Infrastructure

<b>Product</b>	<b>Title Details</b>
<b>Date/Time</b>	08/11/2017 11:02AM
<b>Customer Reference</b>	
<b>Order ID</b>	20171108003804
<b>Cost</b>	\$10.00

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**Dealings Affecting Title**

NIL

**Notations on Plan**

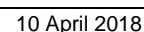
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**Registrar-General's Notes**

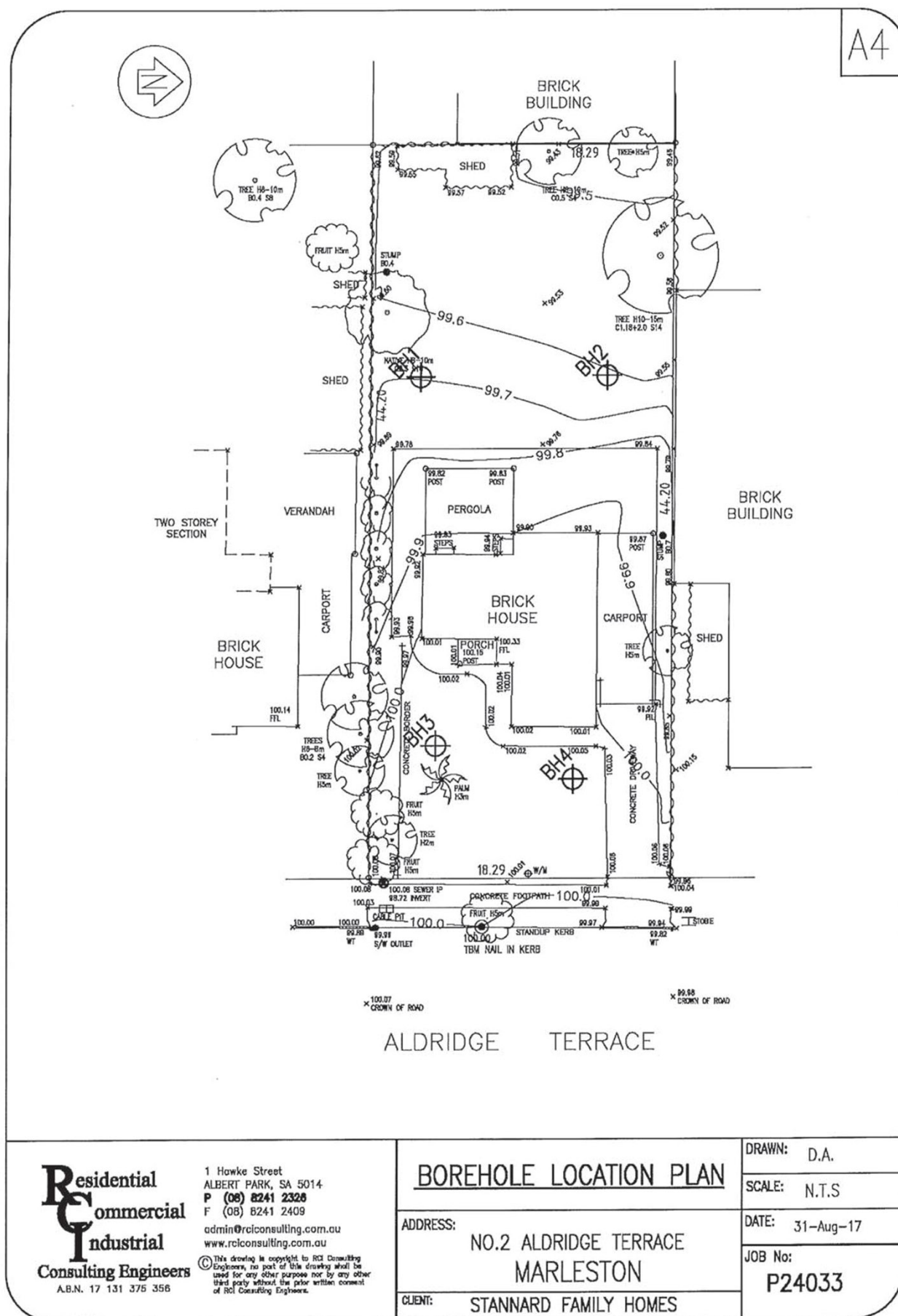
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**Administrative Interests**

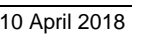
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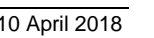


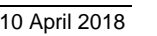


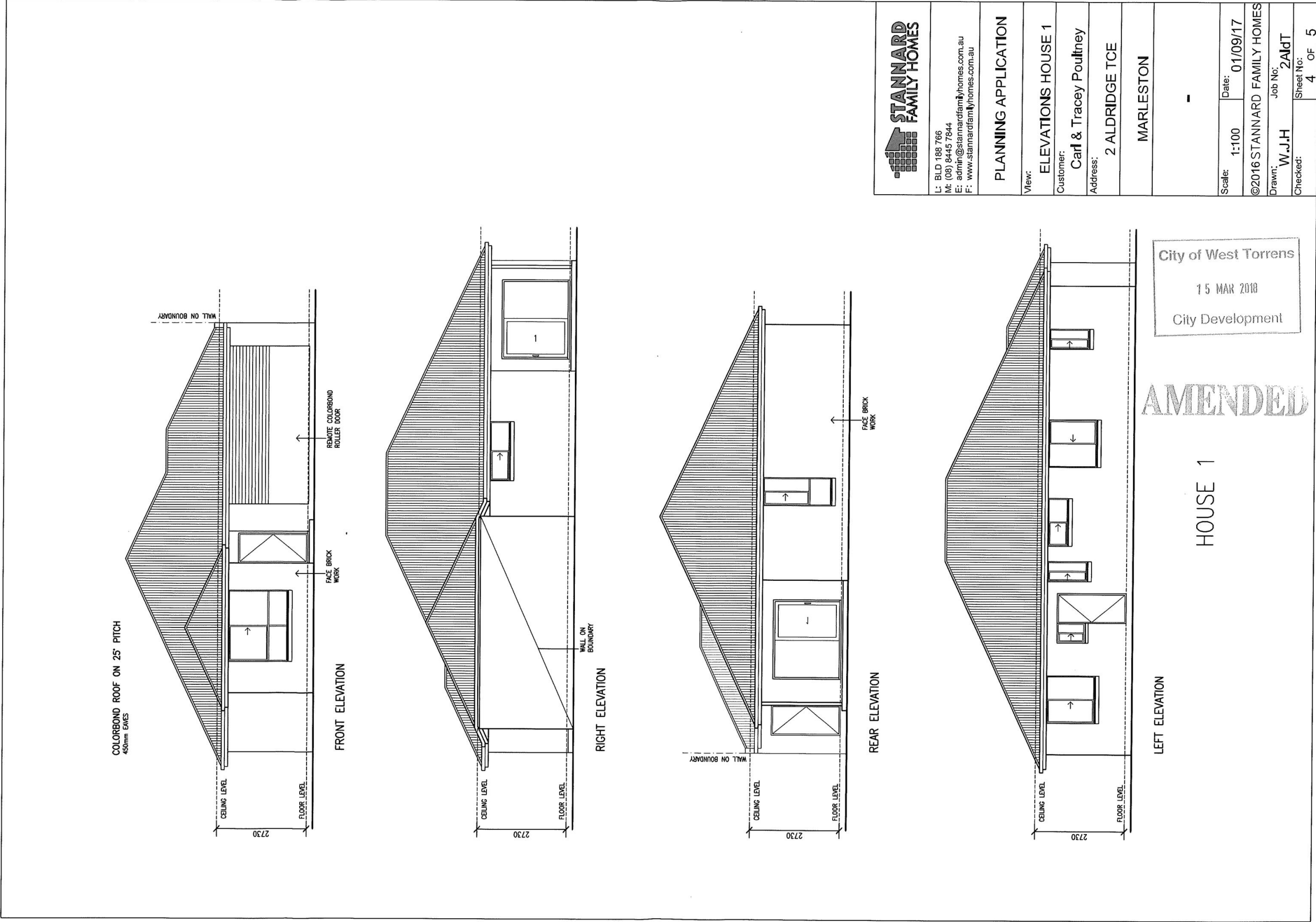




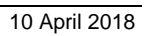












## Arboricultural Assessment of Regulated Trees

Development Application No: **211/1421/2017**

**Referral Due Date:** 29 March 2018

**Assessing Officer:** Claudio Barone

**Site Address:** 2 ALDRIDGE TERRACE, MARLESTON SA 5033

**Certificate of Title:** CT-5312/927

**Description of Development:** Combined Application: Land division - Torrens Title; SCAP No. 211/D187/17 (Unique ID 59978), Create one (1) additional allotment; and Construction of one (1) single storey group dwelling to rear of existing dwelling, with verandah and double garage under main roof and the removal of two (2) regulated trees

*To be completed by:* **CONSULTANT ARBORIST**

**SPECIES & COMMON NAME:** *Agonis flexuosa* (Willow Myrtle)

**TOTAL CIRCUMFERENCE:** 2340mm

**MULTI-TRUNK:** No

The following comments are provided with regards to the relevant Objectives and Principles of Development Control of the General Section, Regulated Tree Section of the West Torrens Council Development Plan:

**OBJECTIVE 1:**

*The conservation of regulated trees that provide important aesthetic and/or environmental benefit.*

**OBJECTIVE 2:**

*Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:*

- |   |           |
|---|-----------|
| <i>(a) Significantly contributes to the character or visual amenity of the locality</i> | <b>No</b> |
| <i>(b) Indigenous to the locality</i>   | <b>No</b> |
| <i>(c) A rare or endangered species</i>   | <b>No</b> |
| <i>(d) An important habitat for native fauna</i>  | <b>No</b> |

**PDC 1:**

*Development should not have minimum adverse effects on regulated trees.*

**PDC 2:**

*A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:*

- |  |            |
|--|------------|
| <i>(a) The tree is diseased and its life expectancy is short</i>   | <b>Yes</b> |
| <i>(b) The tree represents a material risk to public or private safety</i>   | <b>No</b>  |
| <i>(c) The tree is causing damage to a building</i>  | <b>No</b>  |
| <i>(d) Development that is reasonable and expected would not otherwise be possible</i>   | <b>Yes</b> |
| <i>(e) The work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree</i> | <b>No</b>  |

**PDC 3:**

*Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.*

**ADDITIONAL COMMENTS:**

---

I have examined the plans as requested and provide comments as follow:

The mature *Agonis flexuosa* has a substantial trunk and a compact symmetrical canopy which extends over the rear yard and neighbouring property to the south.

Upon inspection it was clear that the specimen is in a predisposed condition. The sparse canopy contains large amounts of deadwood, as well as tip-die back and yellowing of the foliage.

The presence of Longicorn Beetles (borers) is confirmed by the accumulation of small oval shaped exit holes on the main stem. When trees are stressed they emit higher levels of nitrogen, which attracts boring insects. Pests often capitalize on a trees low vigour, which further accelerates its decline.

This mature tree is considered 'fully grown' in its current growing environment and due to the symptoms present; I believe it is reaching the end of its useful life expectancy.

Having given consideration to the plans provided, and observations made of the tree, it is clear that the sustainable retention of this regulated tree is not expected to be achievable. I therefore support the application for its removal to enable reasonable development within this area.

**RECOMMENDATION: REMOVE**

**ADDITIONAL PHOTOGRAPHS:**

---





**SPECIES & COMMON NAME:** *Citharexylum fruticosum* (Fiddlewood)

**TOTAL CIRCUMFERENCE:** 3900mm

**MULTI-TRUNK:** Yes **AVERAGE CIRCUMFERENCE:** 1950mm

The following comments are provided with regards to the relevant Objectives and Principles of Development Control of the General Section, Regulated Tree Section of the West Torrens Council Development Plan:

**OBJECTIVE 1:**

*The conservation of regulated trees that provide important aesthetic and/or environmental benefit.*

**OBJECTIVE 2:**

*Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:*

- |  |           |
|--|-----------|
| (e) Significantly contributes to the character or visual amenity of the locality | <b>No</b> |
| (f) Indigenous to the locality   | <b>No</b> |
| (g) A rare or endangered species   | <b>No</b> |
| (h) An important habitat for native fauna  | <b>No</b> |

**PDC 1:**

*Development should not have minimum adverse effects on regulated trees.*

**PDC 2:**

*A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:*

- |   |            |
|---|------------|
| (f) The tree is diseased and its life expectancy is short   | <b>No</b>  |
| (g) The tree represents a material risk to public or private safety   | <b>No</b>  |
| (h) The tree is causing damage to a building  | <b>No</b>  |
| (i) Development that is reasonable and expected would not otherwise be possible   | <b>Yes</b> |
| (j) The work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree | <b>No</b>  |

**PDC 3:**

*Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.*

**ADDITIONAL COMMENTS:**

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This dual-stemmed tree is identified as a mature *Citharexylum fruticosum* which are not native to Australia. Natural distribution occurs throughout southern U.S and the Caribbean. It is deemed fully grown in its existing environment.

No major structural defects have been noted and the tree does not represent an unacceptable risk to public and private safety. However, the tree does not make an important contribution to the character or amenity of the local area, it is not indigenous and does not represent an important habitat for fauna.

As an introduced species it does not provide suitable nesting hollows, is not part of a wildlife corridor and has limited habitat value. It is not important to the maintenance of biodiversity in the local environment and does not form a notable visual element within the local landscape.

Other than achieving a regulated trunk measurement, the subject tree does not achieve any other aesthetic or environmental attributes that qualify it as a regulated tree.

Having given consideration to the plans provided, and observations made of the tree, I conclude that the *Citharexylum fruticosum* is not suitable for retention in its current urban environment. I therefore support the application for its removal to enable reasonable development within this area.

**RECOMMENDATION:       REMOVE**



JARRAD ALLEN  
*Dip. Arb.*  
Consulting Arborist

DATE: 15/03/18

**ADDITIONAL PHOTOGRAPHS:**

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## Memo

To Sonia Gallarello  
From Jane Teng  
Date 6/12/2017  
Subject 211/1421/2017, 2 Aldridge Terrace, MARLESTON SA 5033

Sonia Gallarello,

The following City Assets Department comments are provided with regards to the assessment of the above development application. The following commentaries are based by the provided 'Proposed Siteworks Plan', (RCI Consulting Engineers – Job no. C24033 – Date: 5-Sep-17). I defer the aspects of proposal consistencies to the City planner to consider.

### 1.0 FFL Consideration – Finished Floor Level (FFL) Requirement

Based on the survey information provided on 'Proposed Siteworks Plan', (RCI Consulting Engineers – Job no. C24033 – Date: 5-Sep-17), a minimum FFL of 100.24. As such the FFL for House 2 would be required to be revised to the minimum FFL required.

**It is recommended that revised plans indicating the required minimum FFL for House 2 be provided to Council.**

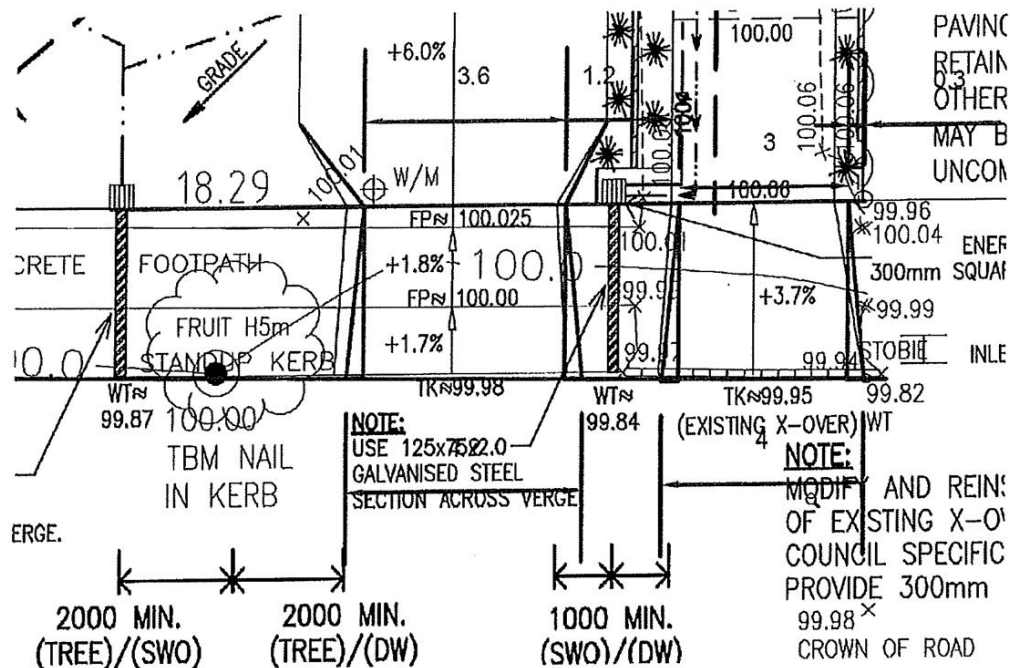
### 2.0 Verge Interaction

Verge interaction has been assessed as acceptable in accordance with the site layout shown on 'Proposed Siteworks Plan', (RCI Consulting Engineers – Job no. C24033 – Date: 5-Sep-17) subject to the following:

- That the driveway access to House 1 and rear property House 2 are updated to reflect the alterations proposed in Sketch 1.0 below.
- That the proposed site plan and elsewhere (Stannard Family Homes-Dated 01/09/17) are updated to reflect the revised RCI Consulting Engineers Siteworks Plan.

**It is recommended that revised plans indicating satisfaction to the above requirements should be provided to Council.**





### 3.0 Traffic Comments and Residential Parking Requirement

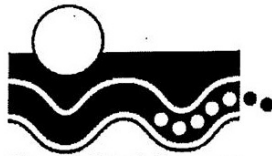
The vehicle manoeuvrability within this development is satisfactory in consideration of the requirements of the relevant Australian Standard (AS/NZS 2890.1:2004).

Each dwelling has provided with **two covered parking spaces** which meets the minimum requirement as per the requirements in the development plan. The driveway area would allow only one visitor to park behind the garage system, turnaround and exit the site in the forward direction.

### 4.0 Garage Dimensions and Setback

Although not specified in the relevant Australian Standards (AS/NZS 2890.1:2004), the internal garage length proposed as 5.80m meets the minimum internal length of 5.80m as specified in the traffic engineering best practice guides of an enclosed garage or enclosed carport space.

The garage setback of House 1 has maintained 5.5m minimum length from the property boundary.



City of **West Torrens**

Between the City and the Sea

Should you require further information, please contact Jane Teng on the following direct extension number 8416 6333.

Regards

**Jane Teng**  
**Civil Engineer**

**6.7 51 Watson Street NETLEY**

Application No 211/140/2018

**DEVELOPMENT APPLICATION DETAILS**

DEVELOPMENT PROPOSAL	Construction of Two (2) Single Storey Dwellings
APPLICANT	Mr Reece Mundy
APPLICATION NO	211/140/2018
LODGEMENT DATE	19 February 2018
ZONE	Residential Zone
POLICY AREA	Policy Area 20
APPLICATION TYPE	Complying Development (Schedule 4 2(B))
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal ▪ Nil External ▪ Nil
DEVELOPMENT PLAN VERSION	6 February 2018
MEETING DATE	10 April 2018

**RECOMMENDATION**

The Council Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/140 /2018 by Mr Reece Mundy to undertake The Construction of Two (2) Single Storey Dwellings at 51 Watson Street Netley (CT5629/783).

**BACKGROUND**

The development proposal is presented to the Council Assessment Panel (CAP) for the following reason:

- With regard to sites where the Development Assessment Panel or CAP has previously refused an application within the last five years, all similar applications on the site shall be assessed and determined by the CAP.

Previous applications on the site for similar proposals have been refused by the CAP with the most recent application for a combined land division and two detached dwellings being refused for the following reasons:

- Resulting allotments will not be suitable for their intended use;
- Resulting dwellings will not provide sufficient storage;
- The development would not contribute to the Desired Character of the Policy Area; and
- The development would not meet minimum allotment widths.

Having considered the reasons for refusal the applicant has since amended the proposal which now meets all of the complying development criteria set out in Schedule 4 Clause 2(B) of the *Development Regulations 2008*.

Importantly it is noted that statutory timeframes cannot be met as Council delegations require the application to be decided by the CAP.

This application is a complying form of development. Statutory timeframes for deciding complying forms of development is two weeks from the receipt of the application. Clearly timeframes cannot be achieved in this instance based on current delegations.

### **PREVIOUS OR RELATED APPLICATIONS:**

- DA 211/163/2017 Combined Application Land Division and Construction of Two Detached Dwellings.
- DA 211/1427/2015 Demolition of Existing Dwelling and Construction of Two Detached Dwellings.
- DA 211/1311/2015 Land Division, Create One (1) Additional Allotment.

### **SITE AND LOCALITY**

The subject land is formally described as Allotment 6 Deposited Plan 5900, Volume 5629 Folio 783 in the area named Netley Hundred of Adelaide, more commonly known as 51 Watson Avenue Netley. The subject site is rectangular in shape with a frontage of 18.59m, a depth of 37.80m and a site area of 703 square metres.

A single storey detached dwelling built during the mid-20<sup>th</sup> century and an outbuilding in the rear yard of the allotment currently occupy the site. The allotment has an existing single width crossover for vehicle access located on the western side of the allotment. There is one mature street along the rear boundary which does not qualify as either a regulated or significant tree.



Image 1: View of the Subject land





Image 2: View of commercial building directly across from the subject land



Image 3: View looking east along Watson Avenue

The locality is residential in nature, predominantly comprising detached dwellings. Dwellings in the locality are set back to allow for generously landscaped front yards. The land to the north of the site is part of the Adelaide Airport with a large commercial building located directly opposite the subject land.

Overall the wider locality provides a high level of visual amenity to residents but this is overshadowed by the activities associated with the Adelaide Airport.

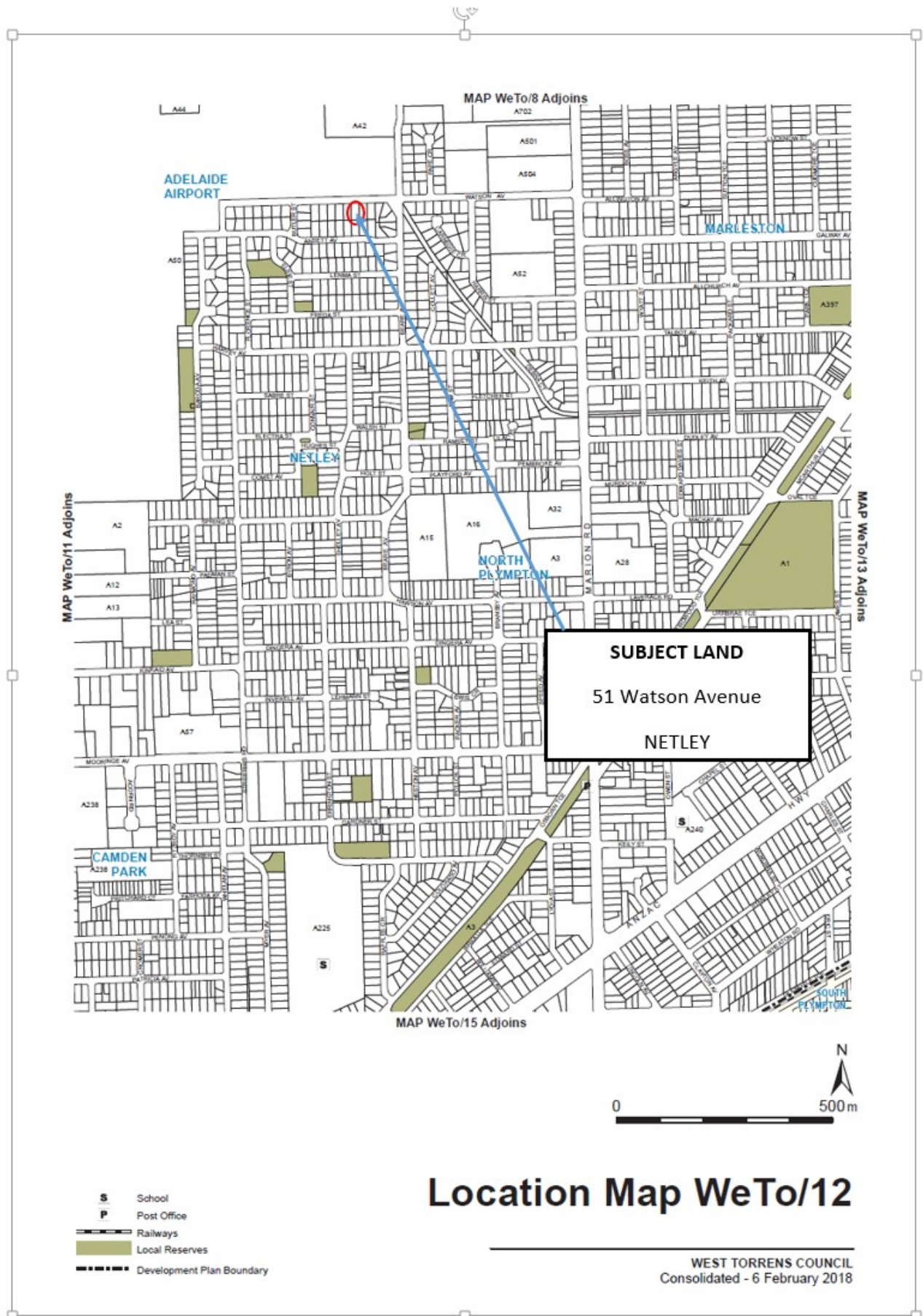
The immediate and wider locality is designated as a Residential Code Area allowing for new dwellings to be constructed in accordance with the criteria set out in Schedule 4 Clause 2B of the Development Regulations.



The subject site and locality are shown on the following maps.







## PROPOSAL

The proposal is for the construction of two single storey dwellings. The dwellings are proposed to be constructed in the form of detached dwellings but are not defined as such given that they do not fit the description of detached dwellings in Schedule 1 of the Development Regulations 2008, in that each of the dwellings is not on a site that is held exclusively with that dwelling.

No land division has been lodged with the land use application. However Schedule 4 of the Development Regulations contemplates proposed “sites” within an “allotment”. That is two or more dwellings (and their respective sites) on a single allotment are able to be assessed.

Schedule 4 recognises that two dwellings on a single allotment cannot legally be defined as detached dwellings until such time that the allotments are formally created. To ensure that the dwellings are not erroneously assessed as detached dwellings, the Development Regulations were amended in November 2017 with the following important changes being made.

### *Current Development Regulations (Clause 2B)*

(6) Construction of or in relation to a new dwelling, other than where the dwelling is to be situated on a battle-axe allotment (or as indicated in a preceding subclause)

### *Previous Development Regulations (Clause 2B)*

(6) Construction of or in relation to a new detached or new semi-detached dwelling, other than where the dwelling is to be situated on a battle-axe allotment (or as indicated in a preceding subclause)

Accordingly, the current Development Regulations contemplate the land use application preceding a land division application. Clause 2B of the Development Regulations would otherwise be redundant.

On consideration of the relevant Clause 2B design criteria it is evident that the two proposed dwellings constitute a residential code complying development as they satisfy all required criteria set out in Schedule 4 Clause 2B.

A full copy of the plans and documentation is contained in **Attachment 1**

## PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations and Residential Zone, Procedural Matters of the West Torrens Council Development Plan. As noted, the proposal is a complying form of development.

## REFERRALS

### *Internal*

- Nil

### *External*

- Nil



## ASSESSMENT

For the benefit of the CAP, the following checklist is provided to assess the proposal against the relevant complying criteria set out in Schedule 4, 2B of the Development Regulations.

## COMPLYING CHECKLIST

*Development Regulations 2008 Schedule 4, 2B - New Dwellings*

The subject site:

- does not contain a State or Local Heritage listed item;
- is not located within a Historic Conservation Zone;
- is not flood affected; and
- is not (or has not been) subject to site contamination as a result of a previous use of the land or a previous activity on the land or in the vicinity of the land.

Further, the proposed development will not cause any tree damaging activity.

SITE AREA AND FRONTAGE			
1.	The minimum site area and any minimum frontage requirements specified in the relevant Development Plan apply in relation to the site and any balance of the allotment	<b>Site area</b> 351m <sup>2</sup>  <b>Frontage width</b> 9.25m.	<b>Site area</b> 350m <sup>2</sup>  <b>Satisfies</b>  <b>Frontage width</b> 9m  <b>Satisfies</b>
FRONT SETBACK			
2.	<p>The construction will not result in the dwelling or any part of the dwelling being;</p> <p>Nearer to an existing boundary of the primary street for the dwelling than any distance that applies in respect of setbacks under the relevant Development Plan in relation to any road or portion of a road that constitutes the primary street frontage; or more than 1 metre in front of:-</p> <p>the average setbacks of any existing dwellings on any adjoining allotments with the same primary street frontage (or, if there is only 1 such dwelling, the setback of that dwelling); or</p> <p>if, on any adjoining allotments with the same primary street frontage, there are only existing buildings other than dwellings—the average setbacks of the buildings (or, if there is only 1 such building, the setback of that building); or</p>	The average setback of adjoining dwellings is estimated to be 7 metres. Proposed dwellings have a minimum setback of 7.7 metres	<b>Satisfies</b>

<b>SECONDARY STREET SETBACK</b>			
3.	within 900 millimetres of a boundary of the allotment with a secondary street or, if a dwelling on any adjoining allotment is closer to the secondary street than 900 millimetres, the distance of that dwelling from the boundary with the secondary street (being, if relevant, the lesser of the 2 distances).		
<b>REAR SETBACK</b>			
4.	If the size of the site is less than 301 square metres—in relation to the ground floor of the dwelling—within 3 metres of the rear boundary of the site (measured from the closest solid wall).	N/A	N/A
5.	If the size of the site is 301 square metres or more—  in relation to the ground floor of the dwelling—within 4 metres of the rear boundary of the site (measured from the closest solid wall).	Rear setback of both dwellings is 5.5m	<b>Satisfies</b>
6.	In relation to any other storey of the dwelling—within 6 metres of the rear boundary of the site; and	N/A	N/A
<b>SIDE SETBACK</b>			
7.	If any side wall of the dwelling will exceed 3 metres in height when measured from the top of the footings—the wall will be set back at least 900 millimetres from the boundary of the site plus a distance equal to one-third of the extent to which the height of the wall exceeds 3 metres from the top of the footings;	No side wall over 3 metres in height	<b>Satisfies</b>
8.	In relation to any dwelling wall to be located on a side boundary of the site associated with the development—  The wall will not exceed 3 metres in height when measured from the top of the footings; and(B) the wall will not exceed 8 metres in length; and	Garage wall on boundary is 3m in height and 6.56m in length	<b>Satisfies</b>
9.	The wall, when its length is added to the length of any other relevant dwelling walls or structures located on that boundary—  will not result in all such walls and structures exceeding a length equal to 45% of the length of the boundary; and  will not be within 3 metres of any other relevant wall or structure located along the boundary; and	No walls on adjoining properties.	<b>Satisfies</b>

10.	<p>if any side wall of the dwelling that faces south and the development includes building work in relation to an upper storey, other than where the boundary on that side of the building is with a secondary street, the setback of any upper storey component is to be—</p> <p>at least the same as the setback required under that provision plus 1 metre; or</p> <p>in any other case—at least 1 metre from the side wall; and</p>	N/A	N/A												
BUILDING HEIGHTS															
11.	<p>The dwelling is not constructed so that—</p> <p>any part of the dwelling will exceed 9 metres in height when measured from the top of the footings; or</p>	Dwelling height is 5.4m	Satisfies												
12.	<p>Any wall height will exceed 6 metres when measured from the top of the footing.</p>	No wall height greater than 3m	Satisfies												
PRIVATE OPEN SPACE															
13.	<p>The construction will not result in a contravention of the following minimum private open space requirements in respect of the site</p> <table><tr><th>Site Area</th><th>Minimum Area of Private Open Space in Site Area</th><th>Minimum Area if Private Open Space at Rear or Side of Relevant Dwelling</th></tr><tr><td>More than 501m<sup>2</sup></td><td>80m<sup>2</sup></td><td>24m<sup>2</sup></td></tr><tr><td>Between 301m<sup>2</sup> and 501m<sup>2</sup> (inclusive)</td><td>60m<sup>2</sup></td><td>24m<sup>2</sup></td></tr><tr><td>Less than 301m<sup>2</sup></td><td>24m<sup>2</sup></td><td>24m<sup>2</sup></td></tr></table>	Site Area	Minimum Area of Private Open Space in Site Area	Minimum Area if Private Open Space at Rear or Side of Relevant Dwelling	More than 501m <sup>2</sup>	80m <sup>2</sup>	24m <sup>2</sup>	Between 301m <sup>2</sup> and 501m <sup>2</sup> (inclusive)	60m <sup>2</sup>	24m <sup>2</sup>	Less than 301m <sup>2</sup>	24m <sup>2</sup>	24m <sup>2</sup>	A minimum of 70m <sup>2</sup> of Private Open Space has been provided for each dwelling.	Satisfies
Site Area	Minimum Area of Private Open Space in Site Area	Minimum Area if Private Open Space at Rear or Side of Relevant Dwelling													
More than 501m <sup>2</sup>	80m <sup>2</sup>	24m <sup>2</sup>													
Between 301m <sup>2</sup> and 501m <sup>2</sup> (inclusive)	60m <sup>2</sup>	24m <sup>2</sup>													
Less than 301m <sup>2</sup>	24m <sup>2</sup>	24m <sup>2</sup>													
14.	<p>Any area at ground level at the front of the proposed dwelling or any existing dwelling on the site will not be included; and</p>	All Private Open Space is located to the rear	Satisfies												
15.	<p>In the case of private open space at ground level—the area of any verandah, pergola, patio or any other covered outdoor area may comprise up to</p>	No more than 50% covered	Satisfies												

	50% of the private open space; and		
16.	Each private open space area must have a width of at least 2.5 metres; and	Minimum dimension of 6m	<b>Satisfies</b>
17.	any balcony must have a width of at least 2 metres; and	N/A	N/A
18.	A dwelling wall will have a setback of at least 900 millimetres on at least 1 side boundary of the site;	Both dwellings setback 900mm from one side boundary	<b>Satisfies</b>
<b>SITE COVERAGE</b>			
19.	The development will not result in the total roofed area of all buildings on the allotment exceeding 60% of the total area of the allotment; and	48.67% of the subject site is covered.	<b>Satisfies</b>
<b>EXCAVATION</b>			
20.	The development does not involve— excavation exceeding a vertical height of 1 metre; or filling exceeding a vertical height of 1 metre, and if the development involves both excavation and filling, the total combined excavation and filling must not exceed a vertical height of 2 metres; and	No excavation greater than 1m	<b>Satisfies</b>
<b>CARPARKING</b>			
21.	In relation to any proposed garage or carport, the garage or carport—  will be set back at least 5.5 metres from the primary street; and	Garages setback 8.2m	<b>Satisfies</b>
22.	Is or will be situated so that no part of the garage or carport will be in front of any part of the building line of the dwelling; and	Located behind the main face of the dwelling	<b>Satisfies</b>
23.	Will not have an opening or openings for vehicle access facing a street frontage that exceed, in total, 7 metres in width; and	Garage opening is 3m	<b>Satisfies</b>
24.	Is not designed or located so as to provide vehicle access from an alley, lane or right of way that is less than 6.2 metres wide along the boundary of the allotment; and	N/A	N/A



25.	Is located so that vehicle access— will use an existing driveway or a driveway authorised under section 221 of the <i>Local Government Act 1999</i> (including a driveway for which consent under the Act has been granted as part of an application for the division of land); or	Section 221 permit granted by Council	<b>Satisfies</b>
26.	Will use a driveway that—  is not located within 6 metres of an intersection of 2 or more roads or a pedestrian actuated crossing;	N/A	N/A
27.	Will not interfere with an item of street furniture (including directional signs, lighting, seating and weather shelters), other infrastructure, or a tree; or	No items or street trees are affected	<b>Satisfies</b>
28.	Will be via a kerb that is designed to allow a vehicle to roll over it; and	N/A	N/A
29.	Is located so that the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport when work is completed is not steeper than 1:4 on average; and	Gradient is less than 1:4	<b>Satisfies</b>
30	The development—the dwelling will have at least 2 car parking spaces of which— 1 or more— • must be, or must be able to be, enclosed or covered; and • must comply with the requirements set out in paragraph (k) in relation to garages and carports; and 1 may comprise a driveway	1 space undercover and 1 in the driveway	<b>Satisfies</b>
31.	The dwelling will have at least 1 habitable room window facing the primary street;	Dwellings have living room facing the primary frontage	<b>Satisfies</b>

## SUMMARY

As demonstrated, the development meets all the relevant criteria set out within Schedule 4, 2B, of the *Development Regulations 2008*. The development must therefore be granted Development Plan Consent, insofar as it is developed in accordance with the plans submitted.

## Attachments

### 1. Proposal plans

RESIDENTIAL CODE "LIMITED ASSESSMENT"

# Residential Code Dwellings and Additions (Sch.4 Cl. 2A & 2B) Development Application Form



Government of South Australia  
Department of Planning,  
Transport and Infrastructure

Please tick the appropriate box to confirm the decision you are seeking:

Planning Consent: Private Certification ☐ or DAC/Council ☒

Building Rules Consent: Private Certification ☐ or DAC/Council ☐

**APPLICANT:** Reece Mundy C/- Heynen Planning

Postal Address: 9A Stanley Street PLYMPTON SA 5038

**OWNER:** Reece Mundy

Postal Address: 9A Stanley Street PLYMPTON SA 5038

**BUILDER:** TBA

**LICENCE No:**

Postal Address:

**CONTACT PERSON FOR FURTHER INFORMATION:** Gregg Jenkins

Email: gregg@heynenplanning.com.au

Ph (work): 8272 1433

Ph (home):

**PREVIOUS USE:** Vacant Land ☐ or Residential ☒ or Other (please describe) ☐:

**DESCRIPTION OF PROPOSED DEVELOPMENT:** Dwelling ☐ or Other (please describe) ☒:

Two Single Storey Dwellings

## LOCATION OF PROPOSED DEVELOPMENT:

House No: 51

Lot No: 6

Street: Watson Street

Suburb: NETLEY

Lot No: 6

Section No. (full/part):

Hundred:

Certificate of Title - Volume: 5629

Folio: 783

**DEVELOPMENT COST** (do not include any fit-out costs): \$375200.00

**HAS THE CONSTRUCTION INDUSTRY TRAINING FUND ACT 1993 LEVY BEEN PAID?** Yes ☐ or No ☒

## SITE DECLARATIONS - NEW DWELLINGS ONLY:

Was the allotment created on or after 1 September 2009?

Yes ☐ or No ☒

Does the site have connection to or is capable of being connected to a sewage system or waste control system which complies with the *Public and Environmental Health Act, 1987*?

Yes ☒ or No ☐

Was the site, to the best of your knowledge and belief, subject to site contamination as a result of a previous use of the land or a previous activity on the land?

Yes ☐ or No ☒

## SITE DECLARATIONS - ALL DEVELOPMENT:

Does a regulated tree exist on the site or on adjoining land which might be affected (including damage to tree roots) by the proposed development?

Yes ☐ or No ☒

If the proposed building includes a garage/carport, does it gain driveway access from:

☐ (if not applicable)

(i) an existing driveway or authorised access point; or

Yes ☒ or No ☐

(ii) a mountable or rollover kerb; or

Yes ☐ or No ☐

(iii) a driveway access point illustrated as part of an approved land division; or

Yes ☐ or No ☐

(iv) a driveway access point that is not located within 6 m of an intersection or a pedestrian actuated crossing and will not interfere with a tree, street furniture, or other infrastructure?

Yes ☒ or No ☐

I acknowledge that copies of this application and supporting documentation may be provided to interested persons in accordance with the *Development Act, 1993*.

**SIGNATURE:**

Dated: 02/01/2018

# Powerline Clearance Declaration form

## Development Regulations 2008

Civic Centre: 165 Sir Donald Bradman Drive, Hilton SA 5033. Office hours: Mon - Fri 8.30am - 5pm.  
Phone: (08) 8416 6333. Email: [development@wtcc.sa.gov.au](mailto:development@wtcc.sa.gov.au). Web: [westtorrens.sa.gov.au](http://westtorrens.sa.gov.au)



Date of Application: 1 / 2 / 18			
Applicant:	Given Name: Reece	Family Name: Mundy	
Address:	Lot No: 51	House No: 6	Street: Watson Street
	Suburb: Netley		P/Code: 5037
Volume:	Folio:		
Nature of proposed development:			
Two Single Storey Dwellings			
<p>I <u>Gregg Jenkins</u> being the applicant / a person acting on behalf of the applicant (delete the inapplicable statement) for the development described above declare that the proposed development will involve the construction of a building which would, if constructed in accordance with the plans submitted, not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i>. I make this declaration under clause 2A(1) of Schedule 5 of the <i>Development Regulations 2008</i>.</p>			
Date: 1 / 2 / 18			
Signature:			

Last updated 13 Sept 2017



Government of South Australia  
Department of Planning,  
Transport and Infrastructure

Product	Register Search (CT 5629/783)
Date/Time	31/01/2018 01:25PM
Customer Reference	Netley
Order ID	20180131006994
Cost	\$28.25

REAL PROPERTY ACT, 1999



South Australia

The Registrar-General certifies that this Title Register Search displays the records maintained in the Register Book and other notations at the time of searching.



## Certificate of Title - Volume 5629 Folio 783

Parent Title(s)	CT 2816/6		
Creating Dealing(s)	CONVERTED TITLE		
Title Issued	01/03/1999	Edition 4	Edition Issued 04/11/2016

### Estate Type

FEE SIMPLE

### Registered Proprietor

REECE LINDSAY MUNDY  
OF 9A STANLEY STREET PLYMPTON SA 5038

### Description of Land

ALLOTMENT 6 DEPOSITED PLAN 5900  
IN THE AREA NAMED NETLEY  
HUNDRED OF ADELAIDE

### Easements

NIL

### Schedule of Dealings

Dealing Number	Description
12615883	MORTGAGE TO NATIONAL AUSTRALIA BANK LTD. (ACN: 004 044 937)

### Notations

Dealings Affecting Title	NIL
Priority Notices	NIL
Notations on Plan	NIL
Registrar-General's Notes	NIL
Administrative Interests	NIL

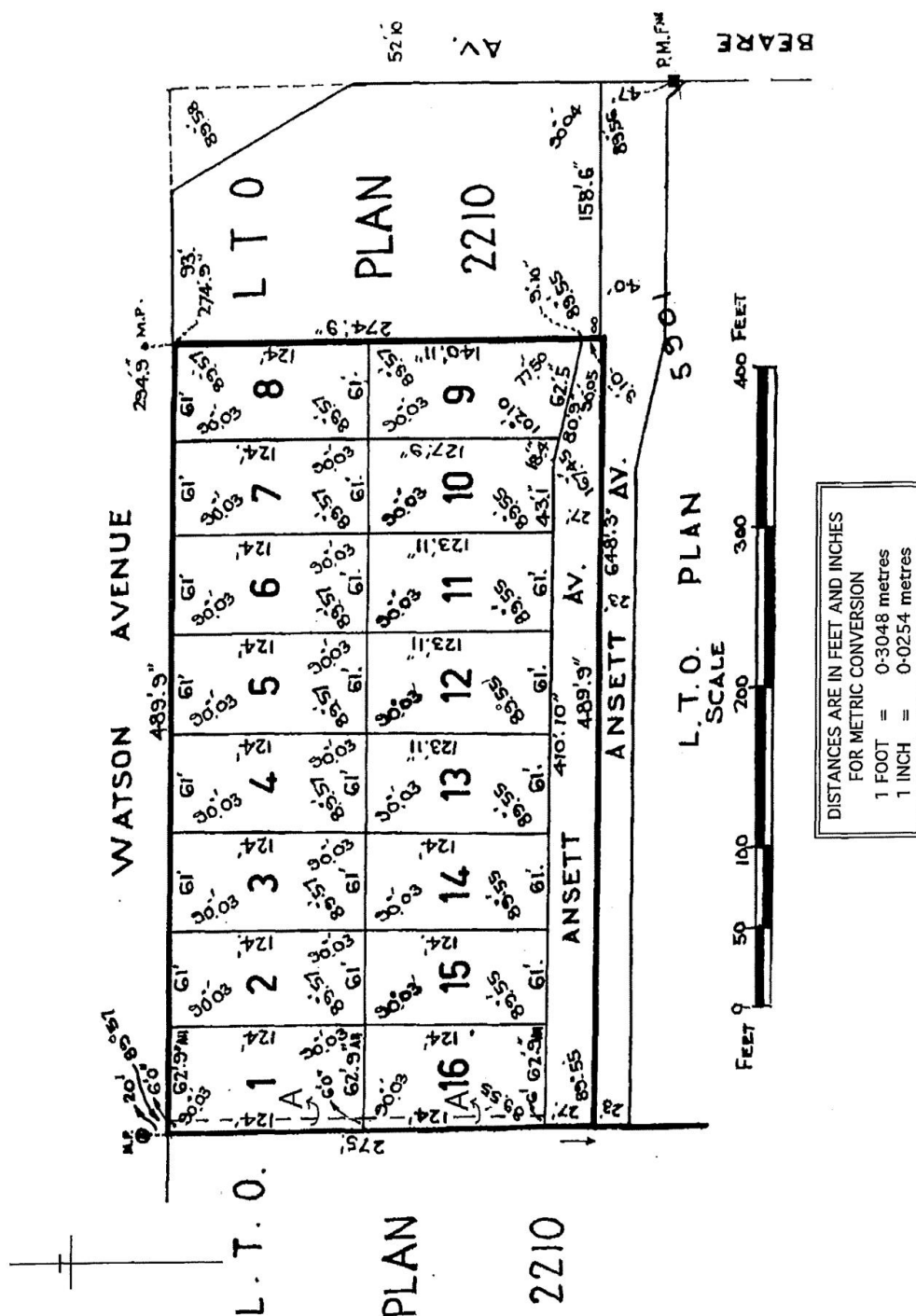




Government of South Australia  
Department of Planning,  
Transport and Infrastructure

Product
Date/Time
Customer Reference
Order ID
Cost

Register Search (CT 5629/783)  
31/01/2018 01:25PM  
Netley  
20180131006994  
\$28.25



HEYNEN  
PLANNING CONSULTANTS

T 08 8271 7944  
F 08 8357 0507  
E garth@heyenplanning.com.au  
PO BOX 523  
KENSINGTON PARK SA 5068

ABN 54 159 265 022

5 February 2018

City of West Torrens  
ATT: Planning Department  
165 Sir Donald Bradman Drive  
HILTON SA 5033

To Whom It May Concern:

**RE: 51 WATSON AVENUE NETLEY**

Please find attached the following documents pertaining to two dwellings proposed for 51 Watson Avenue Netley:

- o Development Application Form;
- o Electricity Declaration Form;
- o Planning drawings prepared by City and Country Construction Pty Ltd;
- o City of West Torrens Internal Memorandum (prepared by Jane Teng), dated 16 May 2017; and
- o Section 221 Permit - Approval to construct a vehicle crossover, dated 23 November 2015.

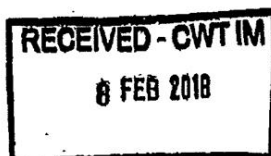
Further, to the application by City and Country Construction Pty Ltd, I confirm the applicant sought my opinion on the merit of proposed development. Accordingly, I seek Council's assessment of the application having regard to my opinion as follows.

*Preliminary Comment*

Having reviewed the *Development Regulations* I confirm that the subject land is located within the "determined area" for the purposes of Schedule 4, Clause 2B. The relevance of this designation is that the proposed dwellings have the potential to either be assessed pursuant to the provisions of the West Torrens Council Development Plan (consolidated 19 December 2017) or Clause 2B of Schedule 4 ("residential code complying development").

Upon consideration of the relevant Clause 2B design criteria it is my opinion that the two dwellings constitute a form of "limited assessment" residential code complying development.

I also note, having reviewed the plans, the proposed vehicle access for dwellings 1 and 2 are consistent with the previous requirements outlined within the Internal Memorandum (as prepared by Jane Teng dated 16 May 2017). Accordingly, the planning drawings within ensure consistency with the Internal Memo provided by The City of West Torrens and the prior Section 221 permit. In this regard, the proposed vehicle access (provided as per Council's previous correspondence) achieves consistency with Council's previous position and will comply in full with Clause 2B (6)(k)(v)(B).



### Assessment

In reaching the conclusion about the planning merit of the application three important questions arise:

- (a) How closely do the proposed dwellings match the Schedule 4, Clause 2B design criteria?
- (b) What assessment process is required?
- (c) When assessed “on merit”, does the one departure accord with the West Torrens Council Development Plan?

Please find my responses below to these matters.

- (a) How closely do the proposed dwellings match the Schedule 4, Clause 2B design criteria?

Having reviewed the various “exception clauses” (e.g. local heritage, historic conservation, hills face, flood management area, contamination and battle-axe allotments), the designated area status applying to the subject land remains.

In relation to the design of the dwellings, I have considered the various other design criteria within Schedule 4 Clause 2B and the proposed dwellings satisfy all other “standards”<sup>1</sup>, save for the width of the proposed street frontages (9.296 metres) (sub clause (5)(a)). In this regard, a width of 10 metres is preferred by the Development Plan (i.e. 704 mm greater than that proposed).

Accordingly, the proposed dwellings satisfy all but one design standard prescribed by Schedule 4, Clause 2B (i.e. the width of the street frontages)<sup>2</sup>.

- (a) What assessment process is required?

The *Development Act, 1993* prescribes the following in relation to development that satisfies all but one design standard:

s35(1c) If a proposed development meets all but 1 criteria necessary for the development to be *complying* development, the aspect or aspects of the development that are consistent with the development being *complying* development must be regarded accordingly and the balance of the development will be assessed as *merit* development.

Accordingly, this sets a process whereby the Council is limited to assess only the question of the merit of the proposed street frontage width. Should it be concluded that the proposed street frontages display planning merit when assessed against the Development Plan a decision to grant Development Plan consent is warranted.

- (b) When assessed “on merit”, does the one departure accord with the Development Plan?

The key question therefore pertains to whether the street frontage width as a “sole issue” displays sufficient merit to warrant Development Plan consent, or put another way, whether the street frontage width creates any “consequences” that are contrary to the Development Plan.

<sup>1</sup> Compliance with the other various design standards is self-evident.

<sup>2</sup> Assuming that the Council again supports the proposed driveway crossovers.

Before considering this point, I am conscious that a quantitative departure should not automatically prove “fatal” to the planning merit of development. In this regard the matter of *AG Building and Developments Pty Ltd v City of Holdfast Bay & Tanti* [2009] SASC 11 is highly instructive.

*“...it does not follow that because some minimum quantitative standards or guidelines are not met, the proposal must be rejected.”*

In relation to the proper assessment process in the *AG Building* matter it was also noted:

*“It required an assessment not of particular issues in watertight compartments, but rather as part of a single complex planning problem -- whether the proposal as a whole should be approved.”*

In the first instance, the proposed dwellings are complying in all other respects. That is, when considering the development “as a whole” the proposed site frontages specifically do not manifest as an issue with respect to setbacks from side boundaries, the supply of private open space, the front boundary set back and the general space around the dwellings.

Turning to the Development Plan, the following provisions are relevant to an assessment of the street frontage widths:

#### **Residential Zone – Low Density Policy Area 20**

PDC 3 dwelling should have a minimum site area and a frontage to a public road not less than that shown in the following table:

- (a) when located 400 metres or more from a centre zone, or
- (b) when located within 400 metres of the Neighbourhood Centre Zone on Marion Road

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	340 minimum	10
Semi-detached	340 minimum	10
Group dwelling	340 minimum	10

#### **General - Residential**

PDC 1 The following forms of development are envisaged specifically in the policy area:

- detached dwelling

As previously discussed, the proposed frontages depart from PDC 3 of Policy Area 20 by a mere 704 mm. I note that the 10.00 metre frontage preferred associated with PDC 3 is couched as a “should” criteria, rather than a must. That is, PDC 3 recognises that planning policies are typically guidelines and not mandatory statutes. In considering this the matter of *Telstra Corporation Ltd v Corporation of the City of Mitcham* [2001] SASC 166; (2001) 79 SASR 509, provides the following guidance on the matter: (my underlining added for emphasis)

*“The Court has repeatedly stated that the provisions of the Development Plan are not to be construed like a statute; see, for example, St Ann’s College v Corporation of City of Adelaide [1999] SASC 479. A development plan is a planning document couched in the language of planning objectives and principles, rather than that of legal obligation. It uses language appropriate to the expressions of goals and guiding principles, rather than the expression of legal mandates...”*



Accordingly, a departure from PDC 3 does not automatically manifest itself as an inappropriate development. It is apparent therefore that dwellings can display planning merit, despite a lesser street frontage.

In considering the merit of the proposed street frontage width, the Desired Character Statement recognises that each policy area seeks "*distinction between policy areas*" with the following extract provided below for clarity.<sup>3</sup>

**Residential Zone**

**Desired Character Statement**

The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas (my underlining added)

Accordingly, consideration of how the proposal aligns within its distinct policy area is provided below in tabular form, with the minimum site area and site frontage of further policy areas provided for consideration.<sup>4</sup>

<i>Policy Area</i>	<i>Minimum Site Area detached dwelling (m<sup>2</sup>)</i>	<i>Minimum Site Frontage detached dwelling (metres)</i>
<i>Medium Density Policy Area 18</i>	250	9
<i>Medium Density Policy Area 18*</i> <i>Dwelling constituting affordable housing</i>	150	No quantitative minimum
<i>Medium Density Policy Area 19</i>	270	9
<i>Medium Density Policy Area 19* Located within 400 metres of a centre zone</i>	250	9
<i>Medium Density Policy Area 19*</i> <i>Dwelling constituting affordable housing</i>	230	No quantitative minimum
<i>Low Density Policy Area 20 (subject site)</i>	340	10
<i>Proposed development (within Policy Area 20 and not within 400 m of a Centre Zone)</i>	<b>351</b>	<b>9.296</b>
<i>Low Density Policy Area 20 Located within 400 metres of a centre zone</i>	300	9
<i>Low Density Policy Area 21</i>	420	12
<i>Low Density Policy Area 21 located within 400 metres of a centre zone</i>	350	9
<i>Low Density Policy Area 21 Dwelling constituting affordable housing</i>	300	No quantitative minimum

**Table 1: Policy Area Allotment Size Analysis**

It is apparent that the proposed site area of 351.3 m<sup>2</sup> achieves compliance with the Development Plan guideline of 340 m<sup>2</sup> within Policy Area 20.

Having reviewed Policy Area 18, 19, 20 and 21, and in further considering the distinct character sought for each policy area, the 351.3 m<sup>2</sup> sites are greater in area than that sought within Policy Area 18 and 19 and less than that anticipated within Policy Area 21.

<sup>3</sup> Further distinction between policy areas will be delivered if qualitative provisions are considered, particularly for Policy Area 22 through to Policy Area 33.

<sup>4</sup> As Policy Area 22 through to Policy Area 33 provide specific "Character" and "Conservation" provisions, these policy areas have been omitted from Table 1

Beyond the simple approach of assessing the site area minimum provisions and the role they play in maintaining the distinct character of a Policy Areas, further consideration of the forms of residential development envisaged within the specific policy areas is warranted, see Table 2 below:

<i>Policy Area</i>	<i>Detached</i>	<i>Semi-detached</i>	<i>Row</i>	<i>Group</i>	<i>Residential Flat Building</i>
<i>Medium Density Policy Area 18</i>	✓	✓	✓	✓	✓
<i>Medium Density Policy Area 19</i>	✓	✓	✓	✓	✓
<i>Low Density Policy Area 20 (subject site)</i>	✓	✓	✗	✓	✗
<i>Low Density Policy Area 21</i>	✓	✓*	✓*	✗	✗
<i>Proposal</i>	✓				

**Table 2: Envisaged Forms of Residential Development within Policy Areas**

\* within 400 metres of an existing centre zone

Clearly, the proposal seeks two detached dwellings and does not seek for a type of dwelling not envisaged within Policy Area 20. That is, the uniqueness of Policy Area 20, which does not seek row dwellings or residential flat buildings will ensure the proposal does not introduce an inappropriate form of housing in Policy Area 20.

In this regard, the proposed site area undoubtedly ensures the hierarchy of policy areas is maintained while the provision of two detached dwellings is an envisaged form of development.

In view of how the site frontages “fit” within the hierarchy of policy areas, I note that the 9.296 metre allotment width is approximately 300 mm greater in width than the minimum allotment width sought for within Policy Area 18 and 19. Consequently, the proposed 9.296 metre proposal does not seek to create an allotment width less than (or equal to) the 9.0 metre allotment width sought for detached dwellings in Policy Area 18 and 19. Likewise, the 9.296 metre frontage will be approximately 300 mm greater than that sought for allotment widths within Policy Area 20 (when located within 400 m of a Centre Zone (other than the Neighborhood Centre Zone on Marion Road)).

In considering further development options within the separate policy areas, the 9.296 metre frontage widths will continue to maintain the distinction sought within Policy Area 20, noting the 7.0 metre minimum frontage width for group dwellings and 5 metre frontage width for row dwelling.

Patently, the proposed frontage widths will continue to uphold the hierarchy of policy areas despite the minor numeric departure and it follows that the development will continue the distinction between Policy Areas.

In further considering the density of the proposal and the “flow on effect” of the two detached dwellings, consideration should be given to the density of the development site in terms of the total space around the development. Accordingly, the development will continue to provide a side boundary setback of 900 mm for each dwelling, conforming in full with the complying standard, while the front and rear boundary setbacks also achieve the complying standard.

That is, the combined side boundary setbacks totalling 1.8 metres will provide a sense of spaciousness when the development is viewed from the streetscape further reinforcing the appropriateness of the proposal, while the appropriate front and rear boundary setbacks continue to be achieved.

In further considering the spaciousness around the proposal a single “complying” detached dwelling can “as of right” provide a dwelling on one side boundary of the site and a 900 mm setback from the opposite side boundary.

Consequently, the impacts often associated with overdevelopment such as a lack of space around dwellings, garage dominance and boundary to boundary development is not encountered.

Finally, I note the landscaping schedule on the site plan provides for generous landscaping, noting the provision of a mature tree to the front of each dwelling, with all cultivars native to the City of West Torrens. The open front garden, noting the absence of formal solid fencing, will retain the open appearance of the development whilst assisting in providing increased passive surveillance to the street.

In this regard, the development achieves compliance with the following Development Plan provisions:

**Residential Zone - Policy Area 20**

**Desired Character Statement**

Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.

**General Section**

***Landscaping, Fences and Walls***

PDC 2 Landscaping should:

- (a) include the planting of drought tolerant species, including locally indigenous species where appropriate
- (b) be oriented towards the street frontage

Accordingly, the following Development Plan provisions will be upheld by the proposal:

**General Section**

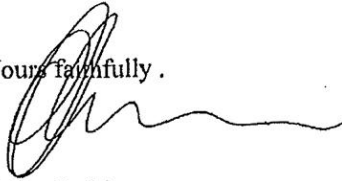
***Residential Development***

PDC 4 Building appearance should be compatible with the desired character statement of the relevant zone, policy area or precinct, in terms of built form elements such as:

- (a) building height
- (b) building mass and proportion
- (c) external materials, patterns, textures, colours and decorative elements
- (d) ground floor height above natural ground level
- (e) roof form and pitch
- (f) facade articulation and detailing and window and door proportions
- (g) verandas, eaves and parapets
- (h) driveway crossovers, fence style and alignment.

In conclusion, I am of the opinion that an assessment of the only departure associated with the development confirms that the development "as a whole" displays substantial planning merit and warrants the grant of Development Plan consent.

Yours faithfully .



Gregg Jenkins  
**BUrb&RegPlan(Hons)**  
0475 933 823  
8272 1433  
gregg@heynenplanning.com.au



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165 Sir Donald Bradman Drive  
Milton, SA 5033  
Tel 08 8416 6333  
Fax 08 8443 5709  
Email: [cs@wtcc.sa.gov.au](mailto:cs@wtcc.sa.gov.au)  
Web: [westtorrens.sa.gov.au](http://westtorrens.sa.gov.au)



23 November 2015

RYAN MUNDY  
CITY & COUNTRY CONSTRUCTION PTY LTD  
PO BOX 9  
BROOKLYN PARK SA 5032

Dear Sir,

**Re: Application to Construct a Vehicular Crossing Place Over Council Land  
– 51 Watson Avenue, Netley - DA211/1311/2015**

I refer to your application for the construction of two vehicular crossing places over Council land from the property located at 51 Watson Avenue (two dwellings), Netley dated 23 November 2015.

Pursuant to Section 221(2) (a) of the Local Government Act 1999, authorisation is granted for the construction of two vehicular crossing places, subject to compliance with the following Conditions and Council Specifications as per Standard Drawing No. 8685-300/330;

- 1 Two crossing places shall be constructed to an invert width of 3.0m each at the kerb line as per the attached plan.
- 2 The crossing places shall be constructed in their entirety including the pedestrian walkway and verge sections of the property frontage, i.e. the pedestrian walkway must be re-concreted as part of the vehicular crossing place construction.
- 3 The crossing places shall be constructed of natural concrete from the property boundary to the kerb line.
- 4 A driveway access must be constructed as near as practicable to 90 degrees to the kerb alignment (unless specifically approved otherwise) and must be situated wholly within the property frontage.
- 5 All cutting of roadways and of concrete or bitumen footpaths must be carried out using a saw cut in straight lines.
- 6 This authorisation and authorisation for any associated works is valid for 12 months from the date of this correspondence.
- 7 Any redundant vehicular crossing places or any damage caused to the existing footpaths and driveways shall be reinstated by the owner/applicant at their own expense (in this instance the existing vehicular crossing place on the Southern boundary). Re-instatement of concrete to footpaths must be between existing control joints and finished to appropriate standards, failure to do so will result in Compliance action by Council.

*Printed on Enviro Recycled, 50/50 which is certified Carbon Neutral and Australian Made.*

Should you require further information please contact Tim Koch by telephone on 8416 6333.

Yours sincerely,

A handwritten signature in black ink, appearing to read 'Erik Stopp', written over a dashed line.

**Erik Stopp**  
**Traffic Engineer - City Assets**

*Encl: Copy of application and specification sheets*



**7. Special notice - Street trees**

Where an application to 'Construct a vehicular crossing place across Council land' necessitates the removal of a street tree, the tree in question will require assessment by Council's Arborist to consider whether it is appropriate to be removed. Should approval be granted, a fee will be generated and must be paid prior to removal (in most circumstances). The fee is calculated based on Council's standard schedule of fees and charges, which is used to offset the loss of the asset (street tree) to the community, with the funds received invested in Council's annual Greening Program.

Under no circumstances is an individual, other than a member of Council's arboricultural staff (or agent acting on the behalf of Council), permitted to interfere with any street tree(s). Street trees will only be removed once the applicant has obtained written confirmation from Council.

Vehicular crossing place applicants, who wish to have a new street tree(s) planted in the verge, must apply by contacting Council on 8416 6333.

Council policy directs that where individual street tree(s) are planted by Council at a resident's written request, they shall be planted on the understanding that it is the resident's responsibility to water the tree(s) until established (for approximately 3 years).

**8. Declaration**

I undertake to be responsible for the works approved under this application, and undertake to effect reinstatement of the footway and road pavements, as may be required, to a standard equivalent to that existing prior to the commencement of this work.

I am aware of the general conditions of authorisation as contained herein.

I agree to any/all special conditions that the Council may determine and attach to this authorisation.

I am aware of the administration fee to be paid prior to approval of this application.

Signature of property owner or applicant on behalf of the property owner:

Dated the 29 day of Oct 2015.

**OFFICE USE ONLY**

Application approved:

Yes ☒

No ☐

Tree removal approved:

Yes ☒

No ☐

Work site inspected:

Yes ☒

No ☐

Tree removal fee

\$ \_\_\_\_\_

Administration fee(s)

\$ \_\_\_\_\_

Total amount payable

\$ \_\_\_\_\_

Application approved by:

(authorised officer of Council)

Date: 23/11/15

Reinstatement Inspected by:

Date: 1 / 1

☐

Date entered into Dataworks:

Initials:

Date:



# Aon Risk Services Australia Limited

Aon Risk Services Australia Limited  
ACN 000 434 720 ABN 17 000 434 720

Mr R Mundy  
City & Country Construction Pty Ltd  
PO Box 9  
BROOKLYN PARK SA 5032

## CLIENT COVERAGE SUMMARY

If you have any queries please contact :  
Aon Risk Solutions  
PO Box 550  
HINDMARSH SA 5007

Your contact is Joshua Ingall

Phone 08 8229 5100 Fax 08 8340 7599

Client Name : City & Country Construction Pty Ltd

Class of Insurance : HIA ANNUAL TURNOVER

Period of Insurance : From - 9th September 2015

: To - 8th September 2016

## COVERING (Summary Only)

### General Information

#### INSURED

CITY & COUNTRY CONSTRUCTION PTY LTD  
and others as may be defined in the policy document

ABN NUMBER 28163012753

#### REGISTERED BUSINESS ADDRESS

1/14 FARROW CIRCUIT  
SEAFORD SA

Postcode: 5169

This policy does not include Worker's Compensation Insurance.  
Employers are reminded that Worker's Compensation Insurance is  
compulsory and that they should obtain separate insurance cover for  
Worker's Compensation.

#### COOLING OFF PERIOD

If you decide that you do not wish to continue with this policy,  
you have twenty-one days after the commencement of this insurance  
to request cancellation. We will provide you with a full refund  
of premium paid, provided you have not made a claim under the  
policy.

## Construction Works

### BUSINESS DESCRIPTION

Reference: AON ZAD C2230 A715077/003

11.09.15

Page No. 1

**CLIENT COVERAGE SUMMARY**City & Country Construction Pty Ltd  
2014 ANNUAL TURNOVER

d) Expediting Expenses	10 %
e) Government and other fees	5 %
f) Fire extinguishment costs	5 %
g) Escalation Allowance	15 %
h) Materials in Storage ( off site )	\$ 100,000
i) Transit	\$ 50,000
j) Mitigation Expenses	\$ 75,000

Plant machinery equipment and tools - whilst at the worksite only  
 Limit any one loss \$ 20,000

Existing property - limit any one loss  
 Building \$Not Insured

**DEDUCTIBLE(S)**

Malicious damage and theft	\$ 500
Upper storey extensions and additions	\$ 1,000
All other claims	\$ 500
Cyclone deductible	\$ 2,500
Damage to underground services	\$ 1,000

**EXCLUSIONS**

The following exclusions apply to this policy, please refer to your Policy Wording for full details:

Electronic Data  
 Terrorism  
 Asbestos

Please refer to the Policy Wording and the requirements relating to:  
 Underground Services

**CLAUSES:****General Information:**

\$1,000 COMMERCIAL EXCESS

Interested Party:

**Public & Products Liability****BUSINESS DESCRIPTION**

HOUSE CONSTRUCTION NOC  
 and incidental activities

ANZSIC: 411105

**COVERING (SUMMARY ONLY)**

The Insurer will pay to or on behalf of the Insured all sums which

Reference: AON ZAD C2230 A715077/003

11.09.15

Page No. 3

**CLIENT COVERAGE SUMMARY**City & Country Construction Pty Ltd  
HIA ANNUAL TURNOVER**ENDORSEMENT**

9.1 Queensland Workcover Amendment - only applicable to Queensland projects, please refer to your Policy Wording for details.

**EXCLUSIONS**

The following exclusions apply to this policy, please refer to your Policy Wording for full details:

Electronic Data

Terrorism

Pollution

Asbestos

Please refer to the Policy Wording and the requirements relating to:  
Underground Services  
Cutting, Heating, Welding

**CLAUSES:****General Information:**

\$1,000 COMMERCIAL EXCESS

**Interested Party:****INSURER**

QBE INSURANCE (AUSTRALIA) LIMITED  
A.B.N. 78 003 191 035  
LEVEL 5, 2 PARK STREET  
SYDNEY NSW 2000

**POLICY NUMBER**

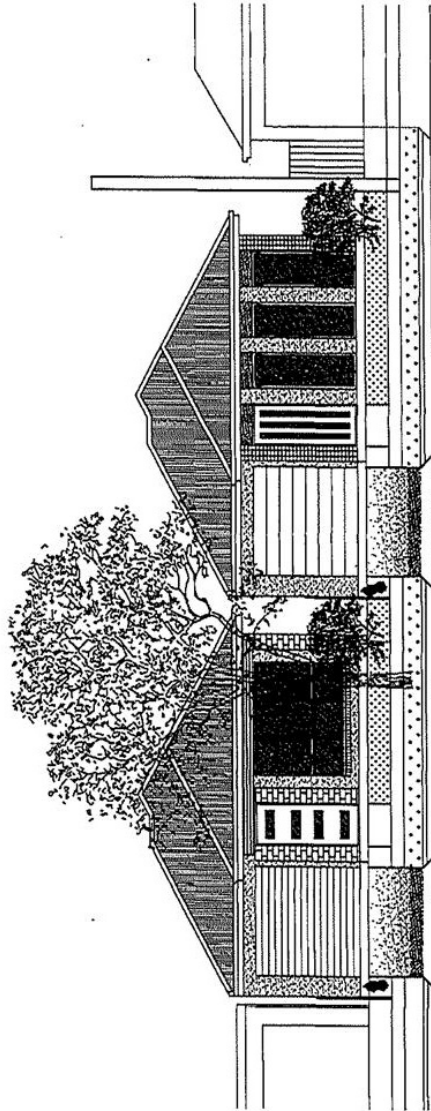
1328001936CAR

**PROPORTION**

100.0000%







W A T S O N   A V E N U E

PROPOSED DEVELOPMENT

51 WATSON AVE, NETLEY

DRAWING LIST  
51WAT-01  
51WAT-02  
51WAT-03  
51WAT-04  
51WAT-05  
51WAT-06

COVER SHEET + CONTEXT DRAWING (THIS SHEET)  
NORTHWEST ELEVATIONS  
SOUTH/EAST ELEVATIONS  
SITE PLAN  
FLOOR PLAN  
SITE DRAINAGE

City & Country  
Construction Pty Ltd

PO BOX 465, EDWARDSTOWN 5039  
RECEPTION@BULDCO.COM.AU  
+61 421 347 893  
+61 401 875 633

PROJECT LOCATION

RES 1 + RES 2  
51 WATSON AVE  
NETLEY, SA

PROJECT

51 WATSON AVENUE

DRAWING

51WAT-01

SCALE / PAPER SIZE

PAPER SIZE A3

AUTHOR

RYAN MUNDY

DATE

9/1/2018

NOTES

**NORTH ELEVATION**

1:100

**RESIDENCE 1**

Colorbond Woodland Grey orb sheets

Colorbond Woodland Grey Mossman gutter profile

Colorbond Woodland Grey fascias

65x16 aluminium powder coated slats (Knotwood Ironbark)

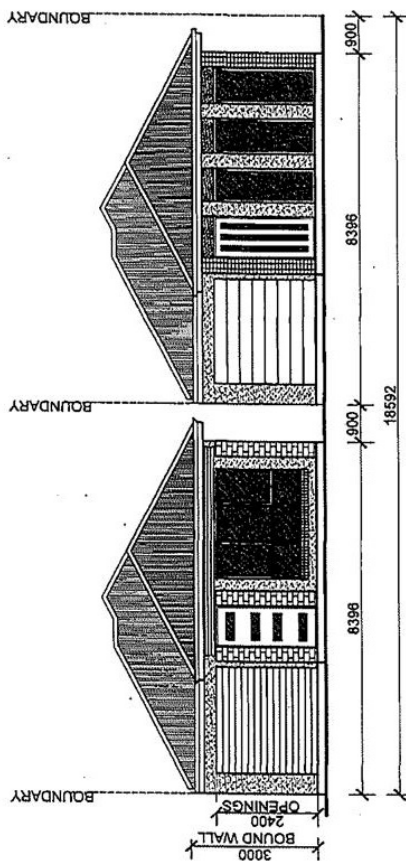
Littlehampton Block Tuscan blocks to front three pillars

Meranti timber frames to front elevation only

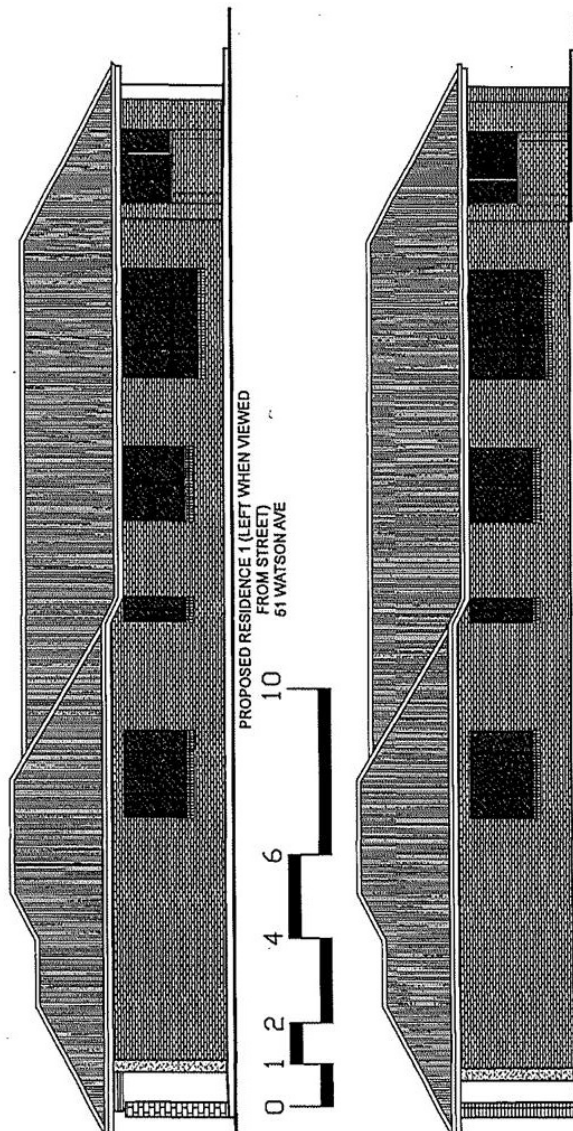
Front entry door Hume XNS (timber, not primed) at 2400 high with Gainsborough Trilock

Facade other than pillars is rough brick rendered then painted with Weather Guard in Colorbond Classic Cream

Steel-Line garage door coloured Ironbark with slimline profile



**RESIDENCE 2**  
Colorbond Shale Grey orb sheets  
Colorbond Shale Grey D-gutter profile  
Colorbond Shale Grey fascias  
Southern Star Shale Grey window frames to entire house  
Front entry door Hume XNS (primed and painted Shale Grey) at 2400 high with Gainsborough Trilock  
Facade other than pillars is rough brick rendered then painted with Weather Guard low sheen bond white  
Steel-Line garage door coloured Shale Grey with flat-line profile  
Pillars brick with Travertine veneer



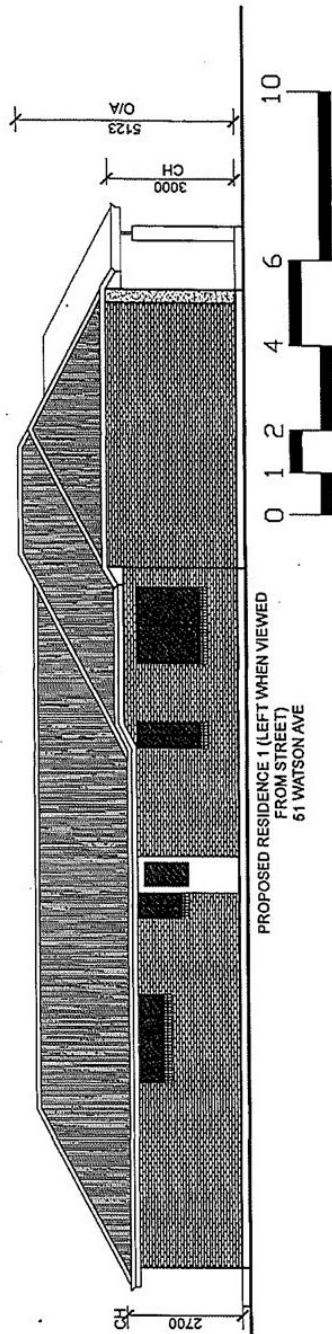
**WESTERN ELEVATION**  
1:100

<b>City &amp; Country Construction Pty Ltd</b>	
PO BOX 465, EDWARDSTOWN 5039 RECEPTION@BUILDCCC.COM.AU +61 421 347 899 +61 401 876 833	
PROJECT LOCATION	RES 1 + RES 2
PROJECT	51 WATSON AVE NETLEY, SA
DRAWING	51 WATSON AVENUE
SCALE / PAPER SIZE	61WAT-02
AUTHOR	1:100 ON PAPER SIZE A3
DATE	RYAN MUNDY
NOTES	9/1/2018

**GENERAL NOTES**

**RES 1**  
Littlehampton Tuscan bricks all elevations except facade  
Southern Star Woodland Grey window frames all elevations except facade

**RES 2**  
Littlehampton Textured Cream bricks all elevations except facade  
Southern Star Shale Grey window frames all elevations  
Continue 'stacker bond' of brick veneer from front blade wall down to the first control joint

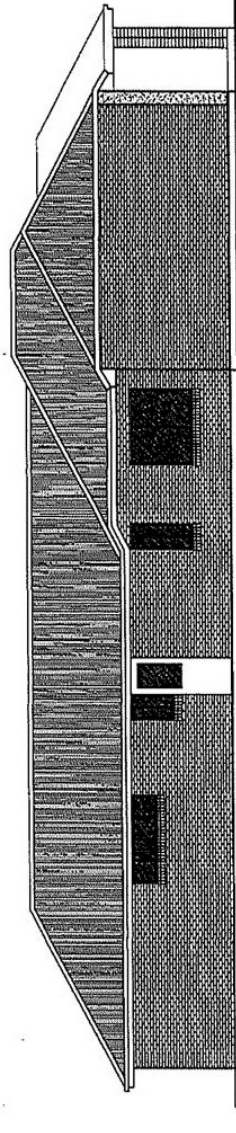


## GENERAL NOTES

Both residences 3020  
ceilings at front reducing to  
2720 at rear. Refer to Floor  
Plan for further details

RES 1  
Half glass laundry door  
painted Woodland Grey

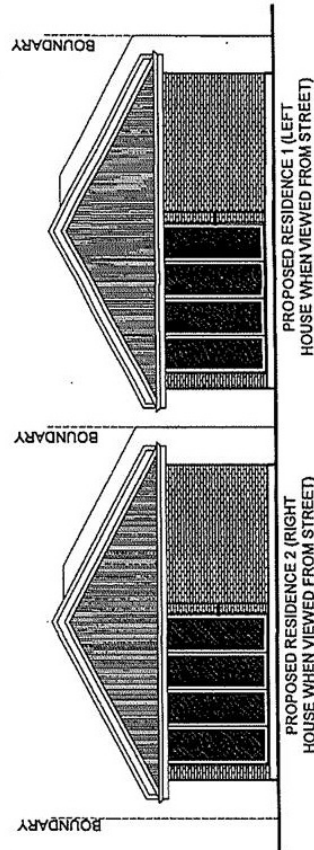
RES 2  
Half glass laundry door  
painted Shale Grey



## EASTERN ELEVATION

1:100

PROPOSED RESIDENCE 2 (RIGHT WHEN VIEWED  
FROM STREET)  
51A WATSON AVE



## SOUTHERN ELEVATION

1:100

City & Country  
Construction Pty Ltd

PO BOX 465, EDWARDSTOWN 5038  
RECEPTION@BUILDCCC.COM.AU  
+61 421 347 898  
+61 401 876 633

## PROJECT LOCATION

RES 1 + RES 2  
51 WATSON AVE  
NETLEY, SA

## PROJECT

51 WATSON AVENUE

## DRAWING

51WAT-03

## SCALE / PAPER SIZE

PAPER SIZE A3

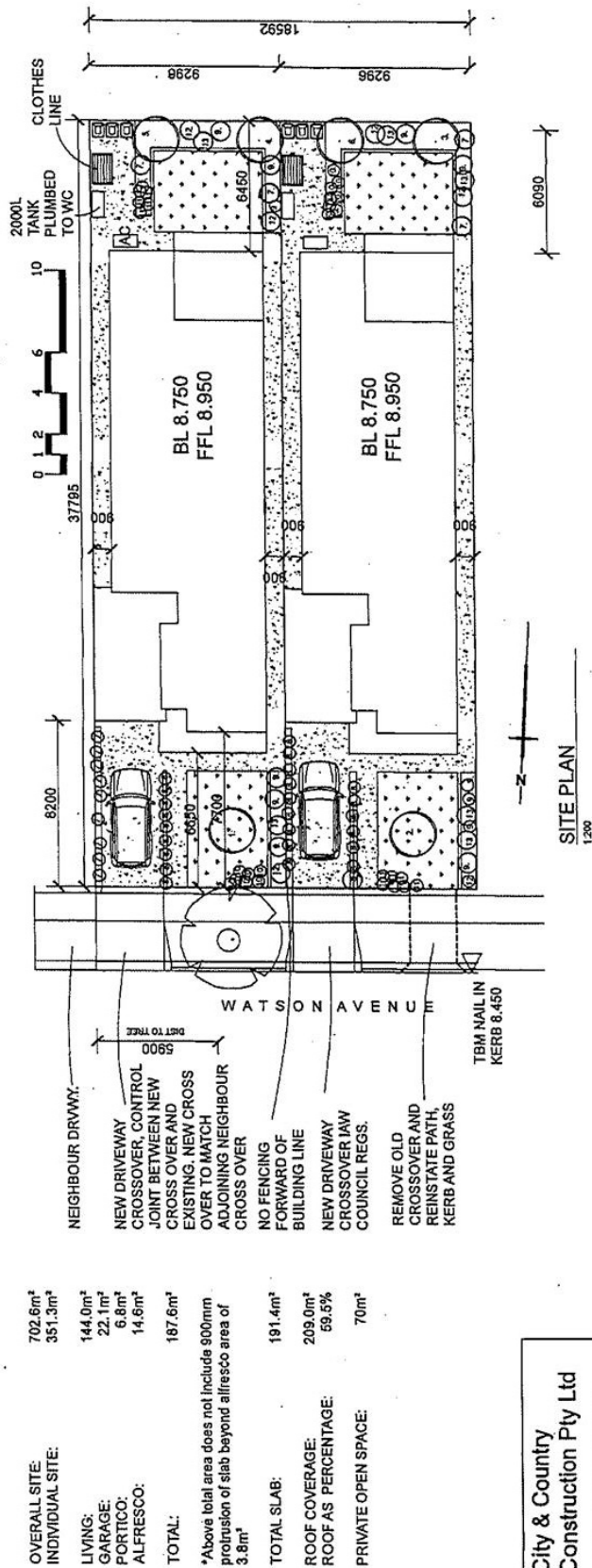
## AUTHOR

RYAN MUNDY

## DATE

9/1/2018

## NOTES



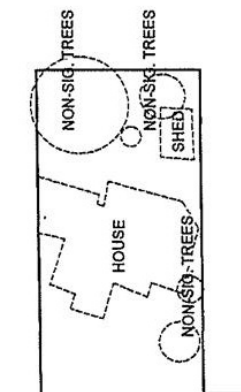
TREE LIST (S: SHRUB, GC: GROUND COVER, T: TREE, GR: GRASS)

1.	SILVER BANKSIA	BANKSIA MARGINATA	T	7M	x1
2.	SA BLUE GUM	EUCALYPTUS LEUCOXYLON	T	7M	x1
3.	ROUND LEAF WATTLE	ACACIA CINACIA	S	2M	x1
4.	RIVER BOTTLEBRUSH	CALLISTEMON SIEBERTII	S	3M	x1
5.	SILKY TEA TREE	LEPTOSPERMUM LANGERUM	S	3M	x1
6.	SWEET BURROBURY	WESTRINGIA LANCEolata	S	3M	x1
7.	HARD WATTLE	ACACIA LANCEOLATA	S	3M	x1
8.	EVERLASTING	DAISYCHRYSOCEPHALUM	GR	0.5M	x13
9.	GARLAND	LILYCALOSTEMMA PURPUREUM	GR	0.4M	x25
10.	FEATHER SPEAR	GRASS STIPA ELEGANTISSIMA	GR	0.3M	x15
11.	ROUND-LEAF	PIRACACIA CRASSIFOLIUM	GC	0.4M	x13
12.	CUSHION BUSH	LEUCOPHYTTA BROWNII	S	1.2M	x6

## DEMOLITION PLAN

1:500

REMOVE HOUSE AND SHED AND ASSOCIATED FOOTINGS  
REMOVE ALL CONCRETE DRIVEWAYS AND PATHS  
REMOVE ALL NON-SIGNIFICANT TREES  
RETURN BLOCK TO NATURAL CONTOUR  
CONTRACTOR ATTENTION DRAWN TO AS2601  
CODE OF PRACTICE: DEMOLITION WORK  
CODE OF PRACTICE: SAFE REMOVAL OF ASBESTOS



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PROJECT LOCATION	
RES 1 + RES 2	
51 WATSON AVE	
NETLEY, SA	
PROJECT	
51 WATSON AVENUE	
DRAWING	
51WAT-04	
SCALE / PAPER SIZE	
PAPER SIZE A3	
AUTHOR	
RYAN MUNDY	
DATE	
9/1/2018	
NOTES	



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PROJECT LOCATION

RES 1 + RES 2  
51 WATSON AVE  
NETLEY, SA

PROJECT

61 WATSON AVENUE

DRAWING

20 JUL 1952

51WAI-05

SCALE / PAPER SIZE

PAPER SIZE A3

AUTHOR

REVATI M

RYAN

DATE \_\_\_\_\_

9/11/20

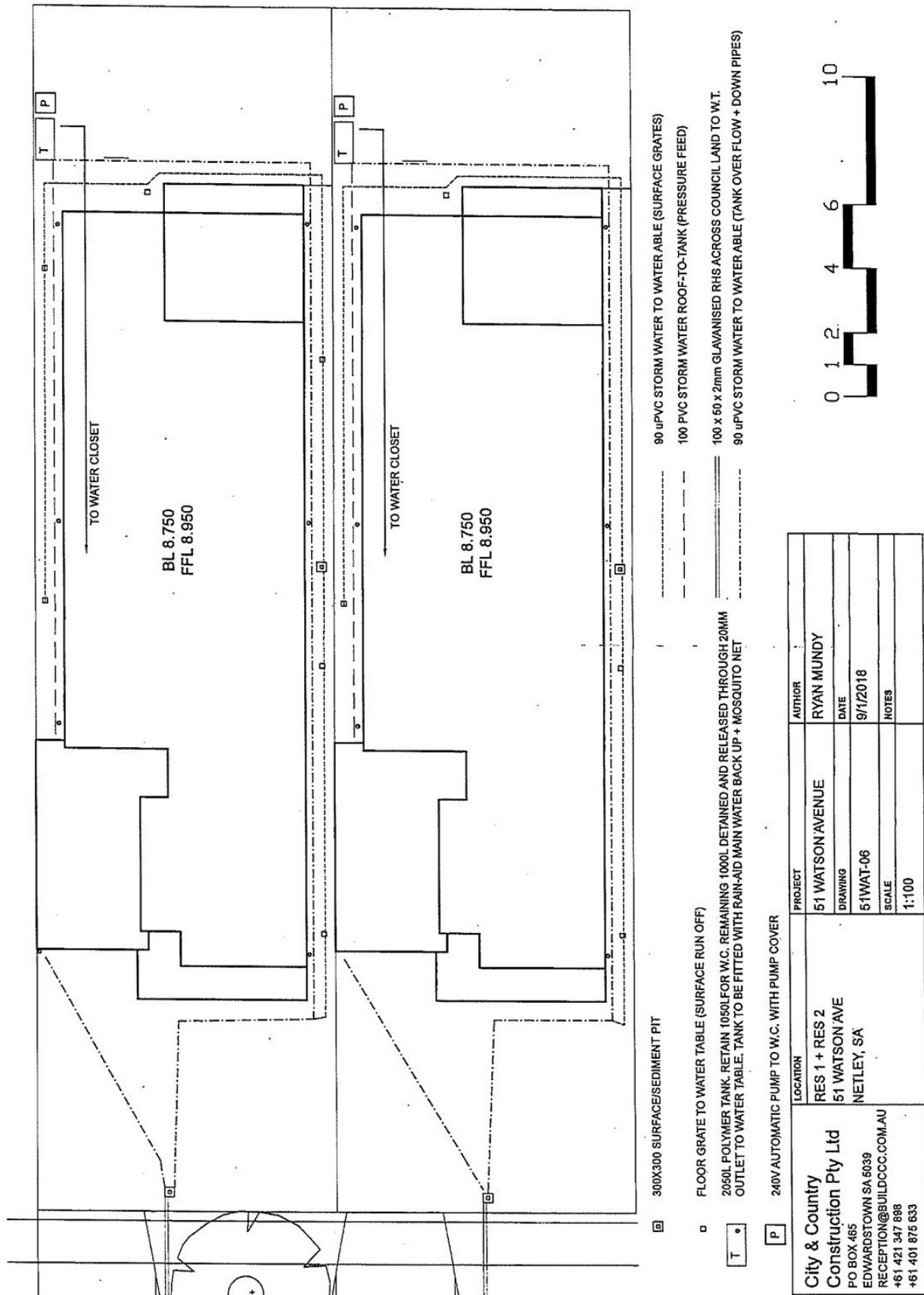
WATER

**NOTES**

RESIDENCE 1 (51 WATSON AVE) - LEFT WHEN VIEWED FROM STREET IS DISPLAYED ABOVE  
 RES 1 AND 2 ARE TECHNICALLY IDENTICAL EXCEPT FOR PILLAR AND WINDOW CONFIGURATION ON FACADE

FLOOR PLAN RES 1 + 2





## 7 CONFIDENTIAL REPORTS OF THE ASSESSMENT MANAGER

Nil

## 8 SUMMARY OF COURT APPEALS

### 8.1 Summary of SCAP, ERD Court and deferred CAP matters - April 2018

#### Brief

This report presents information in relation to:

1. any matters being determined by the State Commission Assessment Panel (SCAP);
2. any planning appeals before the Environment, Resources and Development (ERD) Court; and
3. any deferred items previously considered by the Council Assessment Panel.

#### RECOMMENDATION

The Council Assessment Panel receive and note the information.

#### Matters pending determination by SCAP that have been received by Council

Reason for referral	DA number	Address	Description of development
Section 49	211/911/2017	8 & 8A West Thebarton Road, THEBARTON	Land division - Boundary re-alignment
Section 49	211/40/2018	Lot 102 Anderson Avenue, WEST BEACH	Installation of Solar Photovoltaic cells (PV)
Section 49	211/44/2018	Lot 52 Military Road, WEST BEACH	Swimming pool upgrade
Schedule 10	211/740/2017	192 ANZAC Highway, GLANDORE	Eight-storey building, 36 dwellings
Schedule 10	211/961/2017	79 Port Road, THEBARTON	Nine-storey mixed-use building, commercial tenancy and 28 dwellings
Schedule 10	211/9/2018	4 Selby Street, KURRALTA PARK	Six storey residential flat building with ground level car park
Major Project	211/61/2018	292-304 Anzac Highway, PLYMPTON	Land division - Boundary Re-alignment

**Development Application appeals before the ERD Court**

DA Number	Address	Reason for Appeal	Description of Development	Status
211/738/2017	432-434 Sir Donald Bradman Drive, BROOKLYN PARK	Applicant appealed CAP refusal	Construct a childcare centre with associated car parking and landscaping	Compromise plans presented to CAP at March meeting.  Conference scheduled for 29 March.
211/1159/2017; 211/1309/2017	37 & 39 Malurus Avenue, LOCKLEYS	Applicant appealed CAP refusal	Create one additional allotment and construct 3 dwellings	Conference scheduled for 9 April.
211/1103/2017	69 Ashley Street, TORRENSVILLE	Applicant appealed CAP refusal	Alterations and additions (2 storey) and swimming pool	Conference scheduled for 9 April.

**Deferred CAP Items**

DAP/CAP Meeting	DA number	Address	Description of development	Reason for CAP's deferral
9 August 2016	211/796/2016	22 Lindsay Street, CAMDEN PARK	Create 2 additional allotments and construct 3 two-storey dwellings within a residential flat building	Acoustic report, tree assessment report
16 January 2018	211/704/2017; 211/399/2017	17 Keith Street, NORTH PLYMPTON	Create 4 additional allotments and common property and construct a residential flat building comprising 5 two-storey dwellings	Landscaping, car parking, over shadowing impact, bulk and scale, number of dwellings
13 February 2018	211/475/2017; 211/916/2017	37 Daly Street, KURRALTA PARK	Create 3 additional allotments and common property	Land division, landscaping, transportation and access, vehicle parking for residential development, light access to dwellings and covered private open space



13 March 2018	211/1128/2017 211/922/2017	32 Dudley Avenue, NORTH PLYMPTON	Create 3 additional allotments and construct 2 two storey residential flat buildings	Site coverage, private open space, landscaping, manoeuvrability, bulk and scale, rear setback
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**Conclusion**

This report is current as at 22 March 2018.

**Attachments**

Nil

**9 MEETING CLOSE**