

CITY OF WEST TORRENS



Notice of Panel Meeting

NOTICE IS HEREBY GIVEN in accordance with Section 56A(19) of the *Development Act 1993*, that a meeting of the

DEVELOPMENT ASSESSMENT PANEL

of the

CITY OF WEST TORRENS

will be held in the George Robertson Room, Civic Centre
165 Sir Donald Bradman Drive, Hilton

on

**TUESDAY, 11 JULY 2017
at 5.00pm**

**Terry Buss
Chief Executive Officer**

City of West Torrens Disclaimer
Development Assessment Panel

Please note that the contents of this Development Assessment Panel Agenda have yet to be considered and deliberated by the Development Assessment Panel and officer recommendations may be adjusted or changed by the Development Assessment Panel in the process of making the formal Development Assessment Panel decision.

Note: The plans contained in this Agenda are subject to copyright and should not be copied without authorisation.

INDEX

1	Meeting Opened.....	1
	1.1 Evacuation Procedures	
2	Present	1
3	Apologies	1
4	Confirmation of Minutes.....	1
5	Disclosure Statements	1
6	Reports of the Chief Executive Officer.....	2
	6.1 15 Broadmore Avenue, FULHAM.....	2
	6.2 76 Daly Street, KURRALTA PARK.....	13
	6.3 4 Layton Street, FULHAM	22
	6.4 101-105 Hardys Road, UNDERDALE	33
	6.5 5 Ramsey Street, NETLEY.....	149
	6.6 294 Marion Road, NETLEY	164
7	Confidential Reports of the Chief Executive Officer	200
	7.1 23A Rowells Road, LOCKLEYS	200
	7.2 57 McArthur Avenue, PLYMPTON	201
8	Summary of Court Appeals.....	202
	8.1 Summary of Court Appeals	202
9	Meeting Close	204

1 MEETING OPENED

1.1 Evacuation Procedures

2 PRESENT

3 APOLOGIES

Panel Members:

W Stokes

4 CONFIRMATION OF MINUTES

RECOMMENDATION

That the Minutes of the meeting of the Development Assessment Panel held on 13 June 2017 be confirmed as a true and correct record.

5 DISCLOSURE STATEMENTS

The following information should be considered by Development Assessment Panel Members prior to a meeting:

Action to be taken prior to consideration of a matter

Sections 2(4)(5) of the Minister's Code of Conduct - Section 21A of the *Development Act 1993* requires that:

"If you consider that you have, or might reasonably be perceived to have an interest in the matter before the panel, you must clearly state the nature of that interest in writing to the presiding member before the matter is considered.

If you consider that you have a personal interest which may be in conflict with your public duty to act impartially and in accordance with the principles of the Act, you must declare a conflict of interest as above."

Action to be taken after making a declaration of interest:

Section 2(6) of the Minister's Code of Conduct - Section 21A of the *Development Act 1993* requires that:

"If you have an interest in a matter, you must not partake in any of the assessment processes involving the matter. You must leave the room at any time in which the matter is discussed by the panel including during the hearing of any representations or during any vote on the matter. You must not vote on the matter and you must not move or second any motion or participate in any discussion through the consensus process."

If an interest has been declared by any member of the panel, the presiding member must record the nature of the interest in the minutes of meeting.

6 REPORTS OF THE CHIEF EXECUTIVE OFFICER

6.1 15 Broadmore Avenue, FULHAM

Application No 211/306/2017

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Land division, Torrens Title (211/D042/17, Unique ID 57350) create one additional allotment
APPLICANT	Minh Tran
APPLICATION NO	211/306/2017
LODGEMENT DATE	9 March 2017
ZONE	Residential
POLICY AREA	Low Density Policy Area 21
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> ▪ City Works (Amenity Officer) - impact on street tree External <ul style="list-style-type: none"> ▪ Development Assessment Commission (DAC) ▪ SA Water
DEVELOPMENT PLAN VERSION	5 May 2016
MEETING DATE	11 July 2017

RECOMMENDATION(S)

The Development Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/306/2017 by Minh Tran to undertake land division, Torrens Title (211/D042/17, Unique ID 57350) create one additional allotment at 15 Broadmore Avenue (CT5737/36) subject to the following conditions of consent (and any subsequent or amended condition that may be required as a result of the consideration of reserved matters under Section 33(3) of the Development Act):

DEVELOPMENT PLAN CONSENT

Council Conditions

1. Development is to take place in accordance with the plans prepared by Cavallo Forest & Associates Surveying and Land Division Consultants relating to Development Application No. 211/306/2017 (DAC 211/D042/17).

LAND DIVISION CONSENT

Council Conditions

1. Prior to the issue of clearance to this division, existing structures must be removed from the allotments approved herein. For this purpose a separate application for demolition shall be submitted for the consideration and determination by Council.

Development Assessment Commission Conditions

2. Payment of \$6676 into the Planning and Development Fund (1 allotment(s) @ \$6676/allotment). Payment may be made by credit card via the internet at www.edala.sa.gov.au or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, Level 5, 50 Flinders Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.
4. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services. The alteration of internal drains to the satisfaction of SA Water is required. Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees. On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

BACKGROUND

The development proposal is presented to the Development Assessment Panel (DAP) for the following reason:

- With regard to residential development and land division applications, where all proposed allotments and or sites fail to meet, nor are within 5% of, the minimum frontage widths and site areas designated in respective zones and policy areas within the West Torrens Council Development Plan.

PREVIOUS OR RELATED APPLICATION(S)

Nil

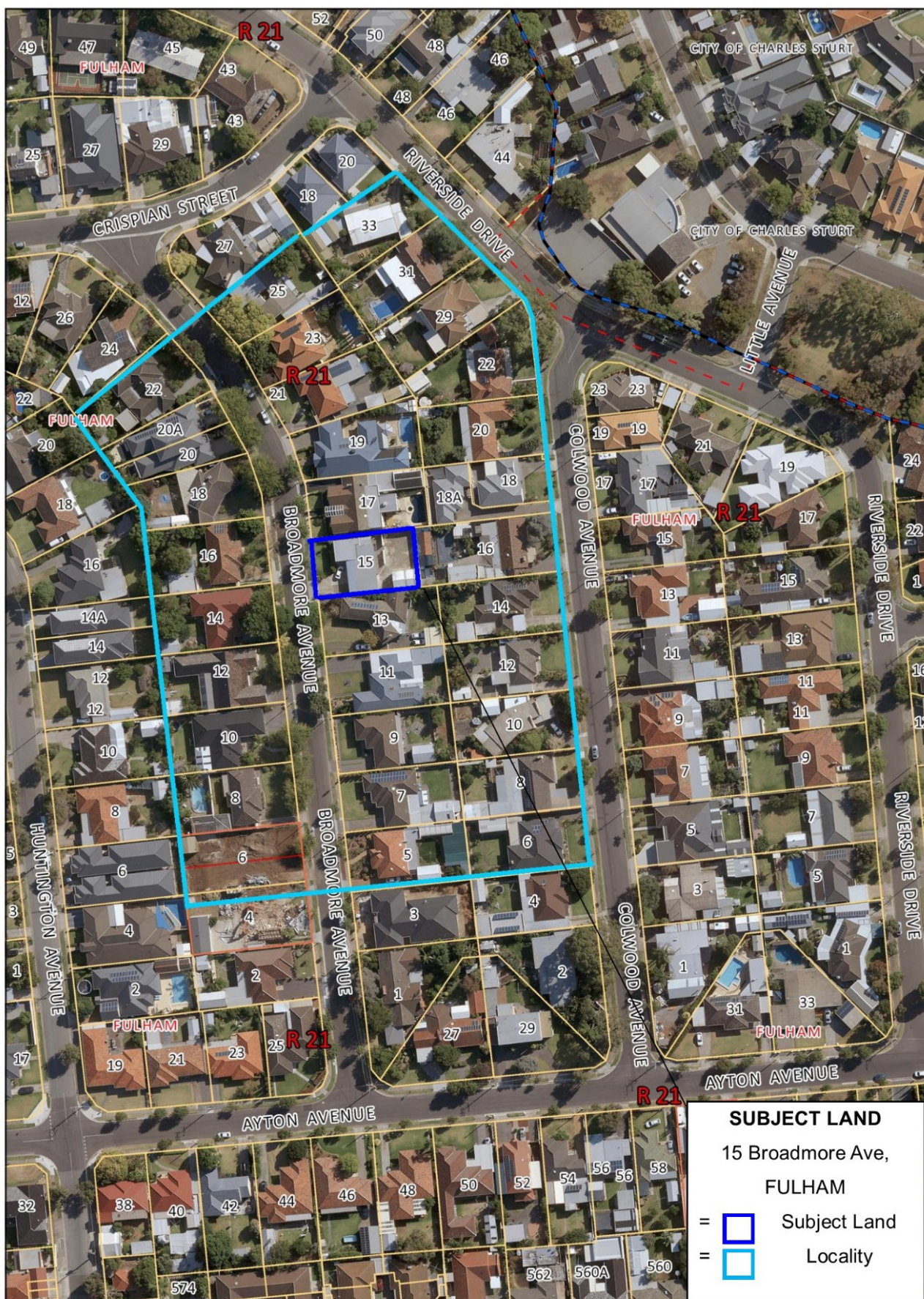
SITE AND LOCALITY

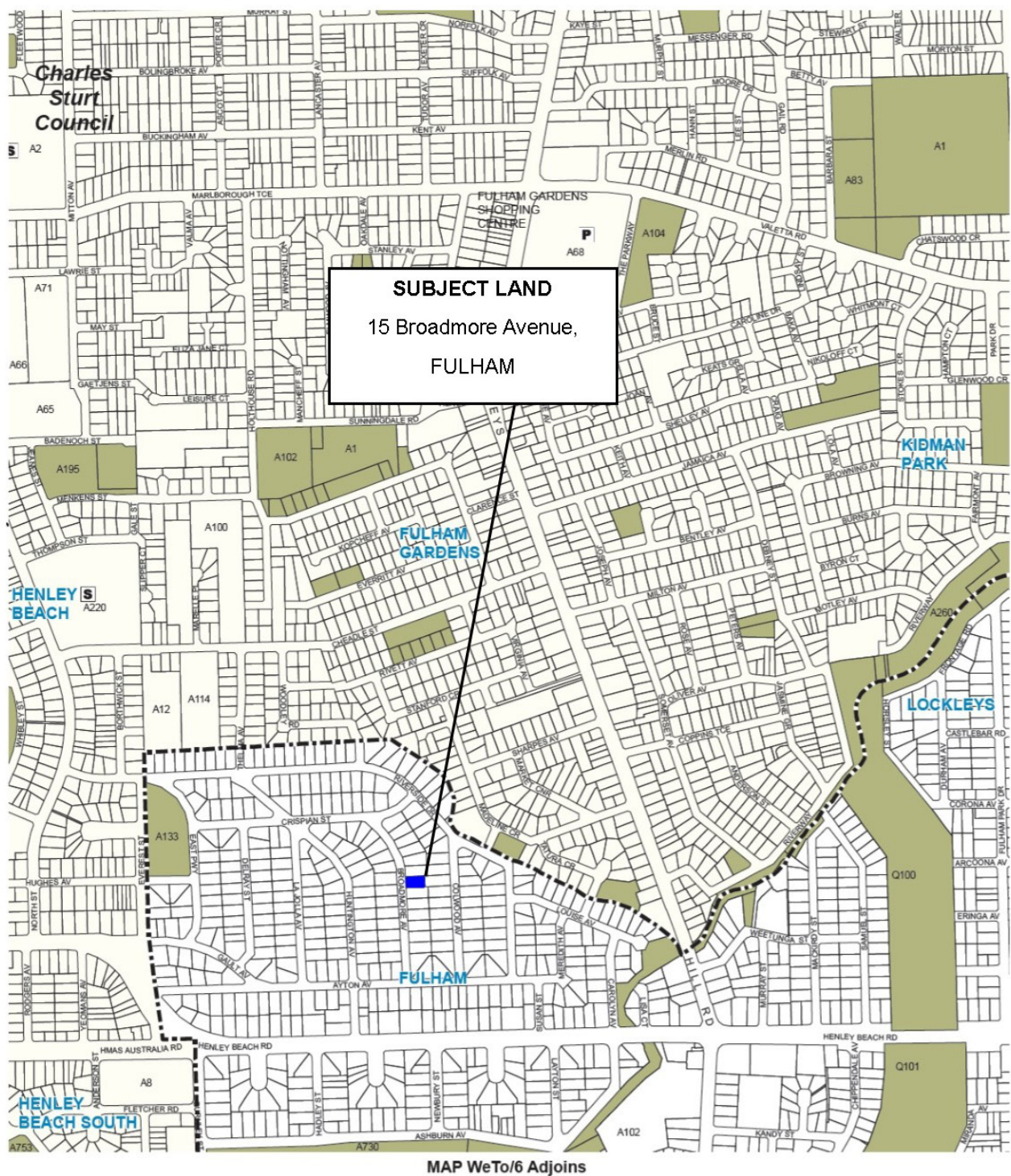
The subject land is formally described as Allotment 92 Filed Plan 119310 in the area named Fulham Hundred of Adelaide, Volume 5737 and Folio 36, or more commonly known as 15 Broadmore Avenue, Fulham. The subject land has a frontage to Broadmore Avenue of 19.81 metres, a depth of 35.05 metres and a site area of 694.3 square metres. Currently existing on the site is a 1960's contemporary style roof dwelling and an outbuilding.

The subject land is located on the eastern side of Broadmore Avenue, and is approximately 260 metres north east of the small Local Centre Zone situated on Henley Beach Road. The streetscape is well vegetated with several street trees at various stages of maturity.

The locality comprises residential development in the form of detached dwellings, with the exception of the semi-detached dwelling on 20 and 20A Broadmore Avenue. The majority of existing dwellings within the locality have considerable sized front yards that are well maintained. Whilst the predominant style of dwellings within the locality consists of 1960's conventional hipped roof dwellings, there are several examples of replacement housing stock or newer dwellings constructed as a result of subdivision.

The site and locality are shown on the following maps.





Location Map WeTo/2

- S** School
- P** Post Office
- Local Reserves
- Development Plan Boundary

WEST TORRENS COUNCIL
Consolidated - 5 May 2016

PROPOSAL

The applicant seeks Development Approval for land division creating one additional allotment. Each of the proposed allotments will be Torrens titled, have a frontage of 9.9 metres to Broadmore Avenue, a depth of 35.05 metres, and an area of 347 square metres.

A copy of the plan of division is contained within **Attachment 1**.

PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations and Residential Zone, Procedural Matters.

REFERRALS

Internal

City Works - Amenity Officer (Street Tree)

There is a street tree in the verge adjacent the subject land. A crossover adjacent proposed Allotment 2 already exists to allow for vehicle access to that site. Feedback obtained from Council's Amenity Officer indicates that a single width crossover could be accommodated adjacent proposed Allotment 1 whilst maintaining a two metre offset from the street tree. A two metre offset from the street tree will enable it to be retained. Given the potential for the street tree to be retained City Works have no objection to the proposal.

External

Development Assessment Commission (DAC) and SA Water

Pursuant to Section 33 and Schedule 29(1) of the Development Act and Regulations, the application was referred to SA Water by the Development Assessment Commission.

Neither DAC nor SA Water had any objections to the proposal, subject to several conditions being added to any consent notice.

Full copies of the relevant reports are contained in **Attachment 2**.

ASSESSMENT

The subject land is located within the Residential Zone and more particularly Low Density Policy Area 21 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Land Division	Objectives	1, 2, 3 & 4
	Principles of Development Control	1, 2, 5, 6, 8 & 12
Orderly and Sustainable Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 3 & 7
Residential Development	Objectives	1, 2, 3 & 4
	Principles of Development Control	1 & 3

Zone: Residential Zone	
Desired Character Statement:	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objectives	1, 2, 3 & 4
Principles of Development Control	1 & 5

Policy Area: Low Density Policy Area 21	
Desired Character Statement:	
<p><i>This policy area will have a low density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).</i></p> <p><i>There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage. In the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park, where the consistent allotment pattern is a significant positive feature of the locality, subdivision will reinforce the existing allotment pattern.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings. Buildings in the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park will be complementary to existing dwellings through the incorporation of design features such as pitched roofs, eaves and variation in the texture of building materials.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.</i></p>	
Objectives	1
Principles of Development Control	1, 2, 4 & 6

QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT	
		Allotment 1	Allotment 2
SITE AREA <i>Low Density Policy Area 21 PDC 4 (within 400m of centre)</i>	350m ² (min.)	347m ² Does Not Satisfy by 0.8%	347m ² Does Not Satisfy by 0.8%
SITE FRONTAGE <i>Low Density Policy Area 21 PDC 4 (within 400m of centre)</i>	9m	9.9m Satisfies	9.9m Satisfies
ALLOTMENT AREA <i>Low Density Policy Area 21 PDC 6</i>	420m ² (min.)	347m ² Does Not Satisfy by 73m² or 17%	347m ² Does Not Satisfy by 73m² or 17%
ALLOTMENT FRONTAGE <i>Low Density Policy Area 21 PDC 6</i>	12m	9.9m Does Not Satisfy by 2.1m or 17.5%	9.9m Does Not Satisfy by 2.1m or 17.5%

QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development satisfies the relevant Development Plan provisions with the exception of the following, as discussed under the following sub headings:

Land Use and Zoning

Dwellings are envisaged within the Residential Zone and detached dwellings are envisaged specifically within the policy area. The proposed allotments will be of a size that supports residential redevelopment.

Land Division

Principle of Development Control (PDC) 6, Residential Zone, Low Density Policy Area 21 provides that land division should create allotments with an area of greater than 420 square metres and a minimum frontage width of 12 metres, other than where land division is combined with an application for dwellings or follows an approval for dwellings on the site.

PDC 4 provides that when a dwelling is located within 400 metres of a Centre Zone it should have a minimum site area of 350 square metres and a frontage to a public road of nine (9) metres.

In this instance the subject land is located within 400 metres of a Centre Zone and if the present application had been combined with an application for dwellings then the requirements of PDC 4 would have been met, with the exception of the site area, as each site has an area three (3) square metres or 0.8% less than prescribed. The site area deficiency of 3 square metres is considered to be minor as this difference will not be discernible from the public realm. The proposed division still results in allotments which have an area and an appropriate frontage width to support envisaged dwelling types of detached dwellings or semi-detached dwellings. The proposal will contribute to the desired character by creating a greater number of housing opportunities located in close proximity to Centre Zones.

It should be noted that, within the Low Density Policy Area 21, the same allotment area and frontage width is prescribed for each envisaged dwelling type. Therefore, a combined application for the purpose of ensuring a specific dwelling type is built on the land is not necessary as the proposed allotments will be a size and width which will accommodate any of the envisaged dwelling types.

SUMMARY


The subject land is located within 400 metres of a Centre Zone and, as such, the site areas and frontage widths of the proposed allotments are considered to be appropriate within this specific area. The development will result in allotments that are suitable to accommodate a detached dwelling or semi-detached dwelling (an envisaged form of development within the zone and policy area) which will be complimentary to the built form within the locality.

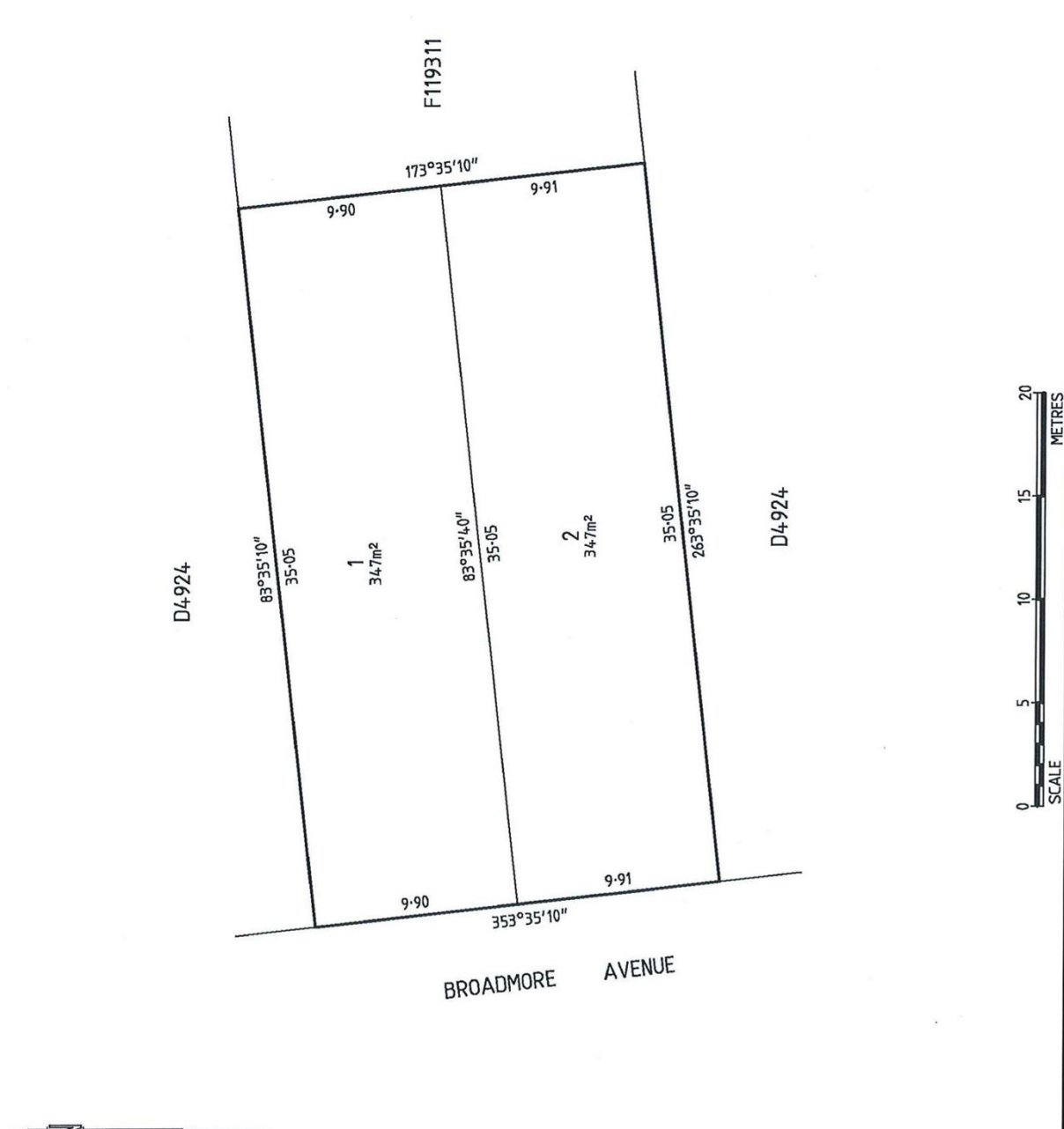
Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance, the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 5 May 2016 and warrants Development Plan Consent.

Attachments

- 1. Plan of Division**
- 2. DAC & SA Water Referral**

PLAN OF PROPOSED DIVISION	
DAC DEVELOPMENT NUMBER	/D /17
COUNCIL..... WEST TORRENS	
ZONE..... LOW DENSITY	
POLICY AREA..... 21	
SUBJECT LAND DETAIL	
ALLOTMENT 92 IN FP 119310	
HUNDRED OF ADELAIDE	
IN THE AREA NAMED	
FULHAM	
15 Broadmore Avenue	
Fulham SA 5024	
TITLE REFERENCES	
C.T. Vol. 5737 Fol. 36	
MAP REFERENCE: 6628/4/H	
TORRENS DIVISION	
TOTAL SITE AREA: 694m ²	
NO. OF EXISTING ALLOTMENTS: 1	
NO. OF PROPOSED ALLOTMENTS: 2	
NO. OF ADDITIONAL ALLOTMENTS: 1	
ANNOTATIONS / EASMENTS	
<p>Please Note: Please instruct your Rates Department to provide street numbering with the Decision Notification Form as per SAPN and NBN Utilities requirements.</p> <p>Land Use Application prepared by Distinctive Homes and to be lodged subsequent to this application.</p> <p>Existing Dwelling to be demolished.</p> <p>All dimensions in metres. Do not scale drawing.</p>	
ALL DATA IS APPROX ONLY	
SUBJECT TO SURVEY AND FINAL PLAN	
 CAVALLO FOREST & ASSOCIATES Surveying & Land Division Consultants 9 George Street, Hindmarsh SA 5007 Ph: (08) 8346 0440 Email: survey@cavalloforest.com.au	
DATE 08/03/17	FIELD ---
REF No 17-057	CHK RC
	DRAWN JC
	REV 01



BROADMORE AVENUE

SCALE
0 5 10 15 20 METRES

Contact Lands Titles Office
Telephone 7109 7018



*Development
Assessment Commission*

27 March 2017

The Chief Executive Officer
City of West Torrens

Dear Sir/Madam

Re: Proposed Application No. 211/D042/17 (ID 57350)
for Land Division by Mr Minh Tran

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 14 March 2017, I advise that the Development Assessment Commission has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the Development Assessment Commission has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.
The alteration of internal drains to the satisfaction of SA Water is required.
Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.
On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.
2. Payment of \$6676 into the Planning and Development Fund (1 allotment(s) @ \$6676/allotment).
Payment may be made by credit card via the internet at www.edala.sa.gov.au or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.

A handwritten signature in black ink, appearing to read 'P. Hodgson'.

Phil Hodgson
Unit Manager
Lands Titles Office



27 March 2017

Our Ref: H0056833

The Chairman
Development Assessment Commission
136 North Terrace
ADELAIDE SA 5000

Dear Sir/Madam

PROPOSED LAND DIVISION APPLICATION NO: 211/D042/17 AT FULHAM

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

The alteration of internal drains to the satisfaction of SA Water is required.

Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.

On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

Yours faithfully

TONY PANNUNZIO

for MANAGER LAND DEVELOPMENT & CONNECTIONS

SA Water
Level 6, 250 Victoria Square
ADELAIDE SA 5000
Ph (08) 7424 1119
Inquiries TONY PANNUNZIO
Telephone 7424 1243

6.2 76 Daly Street, KURRALTA PARK

Application No 211/541/2016

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Variation to land division - Torrens Title; DAC No. 211/D060/16 (Unique ID 54235); Create one(1) additional allotment - Reduce driveway width from 4.5m to 4m and increase depth of Allotment 100 by 600mm
APPLICANT	Damir Ivanovic
APPLICATION NO	211/541/2016
LODGEMENT DATE	28 April 2016
ZONE	Residential
POLICY AREA	Medium Density Policy Area 19
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> • City Assets - Traffic and Manoeuvrability External <ul style="list-style-type: none"> • DAC • SA Water
DEVELOPMENT PLAN VERSION	5 November 2015
MEETING DATE	11 July 2017

RECOMMENDATION(S)

It is recommended that the Panel allow the proposed variation and impose the same conditions of the current approval as shown below, with the exception of Land Division Consent - Council Condition 2, which as a result of the variation will no longer be necessary.

The Development Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the Development Act 1993 resolves to GRANT Development Plan Consent for Application No. 211/541/2016 by Damir Ivanovic to undertake Land division - Torrens Title; DAC No. 211/D060/16 (Unique ID 54235); Create one(1) additional allotment at 76 Daly Street, Kurralta Park (CT 5285/278) subject to the following conditions of consent (and any subsequent or amended condition that may be required as a result of the consideration of reserved matters under Section 33(3) of the Development Act):

DEVELOPMENT PLAN CONSENT **Council Conditions**

1. *Development is to take place in accordance with the plans prepared by Jeanes & Somerville Surveyors relating to Development Application No. 211/541/2016 (DAC 211/D060/16).*

Development Assessment Commission Conditions

Nil

LAND DIVISION CONSENT

Council Conditions

1. *That prior to the issue of clearance to the division approved herein, all existing structures on Lot 101 shall be demolished.*
2. *The existing Class 1a dwelling is required to be fire separated in accordance with Part 3.7.1 of the BCA.*
 - *Evidence must be submitted to Council to verify compliance with the above*
 - *If the fire separation does not exist, an application for Building Rules Consent under the Development Act 1993 shall be lodged with Council (or Private Certifier) and Development Approval shall be issued prior to the commencement of building work. A Schedule 19A – Statement of Compliance from the builder (building supervisor), shall be submitted to Council upon completion of the work. Evidence that fire separation of the units is in place shall be submitted to Council prior to the issue of Section 51 Clearance.*

Development Assessment Commission Conditions

3. *The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.*
The alteration of internal drains to the satisfaction of SA Water is required.
Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.
On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.
4. *Payment of \$6488 into the Planning and Development Fund (1 allotment(s) @ \$6488/allotment).*
Payment may be made by credit card via the internet at www.edala.sa.gov.au or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, Level 5, 50 Flinders Street, Adelaide.
5. *A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.*

BACKGROUND

The proposed variation is presented to the Development Assessment Panel (DAP) for the following reason:

- A condition of the sub-delegation under section 39(7) of the Development Act 1993 only allows for variation applications to be determined by the original assessing officer. In this instance the Development Assessment Panel granted the Development Approval for the land division 211/541/2016 on 13 September 2016.

PREVIOUS OR RELATED APPLICATION(S)

- DA 211/107/2016 - Construction of a single storey group dwelling with garage under main roof - Development Plan Consent granted 28 November 2016.
- DA 211/541/2016 - Land division - Torrens Title; DAC No. 211/D060/16 (Unique ID 54235); Create one(1) additional allotment - Development Approval granted 13 September 2016.

SITE AND LOCALITY

The subject land is formally recognised as Allotment 14 Deposited Plan 2478 in the area named Kurralta Park Hundred of Adelaide as contained in Certificate of Title Volume 5285 Folio 278. The land is more commonly known as 76 Daly Street, Kurralta Park. The subject land is rectangular in shape with a frontage of 19.8 metres, a depth of 51.51 metres and a total site area of 1020sqm.

The subject land is located on the western side of the street, north of the Garfield Avenue intersection. The subject land currently accommodates a 1950's single storey conventional style dwelling and associated structures including outbuildings, carports and verandahs.

The locality is characterised by a vast mixture of residential development. Dwelling styles evident within the locality include, 1950's and 1960's conventional hipped roof, Tudor style, and Spanish mission. Non-residential land uses nearby include the Marlestone industrial area.

The site and locality are shown on the following maps.





Location Map WeTo/13

WEST TORRENS COUNCIL

PROPOSAL

This application is for the variation to an authorisation previously given for land division -Torrens Title; DAC No. 211/D060/16 (Unique ID 54235); Create one (1) additional allotment.

The changes to the proposal are outlined within the following table:

	Original Development Approval	Proposed Change
Driveway width	4.5m	4m (with allowance for stormwater outlet)
Area of allotment 100	522sqm	538sqm
Area of allotment 101	499sqm	482sqm

A copy of the original plan of division is contained within **Attachment 1** and a copy of the new plan of division is contained within **Attachment 2**.

PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations and Residential Zone, Procedural Matters.

REFERRALS

Internal

- **City Assets - traffic and manoeuvrability**
 - No objections - The dwelling as approved within DA 211/107/2016 can remain in the same position and the proposed changes will not impact on the ability for a vehicle to enter and exit the site in a forward direction. The proposed driveway arrangement will still enable sufficient room for a vehicle to pass through and for stormwater to be offset the minimum distances from existing verge features.

External

Pursuant to Section 33(1) (c) and Regulation 29(1) of the Development Act and Regulations, the application was referred to

- **SA Water**
 - No new feedback. Original conditions as per original approval to be applied. No issues were raised.
- **Development Assessment Commission (DAC)**
 - No new feedback. Original conditions as per original approval to be applied. No issues were raised.

DISCUSSION

The purpose of the proposed amendment to the land division is to eliminate the need for the existing dwelling to be fire separated in accordance with *Part 3.7.1 of the Building Code of Australia*.

The proposed changes to the land division will not affect the overall functionality of each proposed allotment nor will it have any undesirable impacts on the surrounding land uses or locality. The allotment sizes that will be a result of the proposed amendment will still be greater than the minimum sought within the Medium Density Policy Area 19.

The dwelling as approved within development application 211/107/2016 will not be impacted by the change to the land division. Factors such as private open space, vehicle parking and manoeuvrability, site coverage, setbacks for both allotments will not be deficient due to the proposed changes to the boundaries.

The reduction of the driveway width to four (4) metres is considered to be sufficient as it is in accordance with General Section, Land Division Principle of Development Control 7, which states that, allotments in the form of a battleaxe configuration should provide for an access onto a public road, with the driveway 'handle' being not less than 4 metres in width to facilitate landscape planting along the driveway.

Overall, the changes will not present any new inconsistencies with the relevant Development Plan provisions the original proposal was assessed against.

SUMMARY

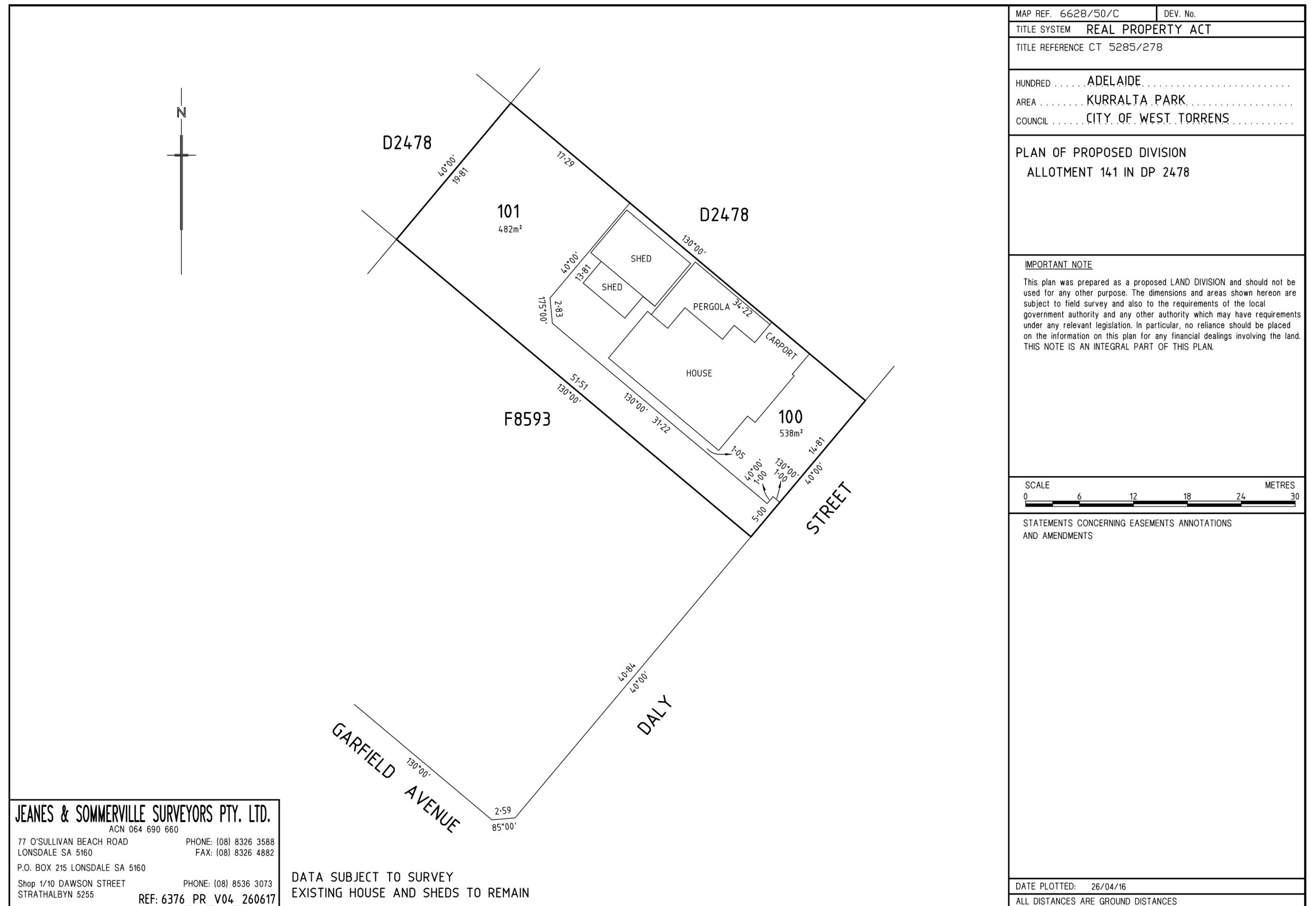
The proposed amendments will result in a development that is consistent with the nature of the existing development approval.

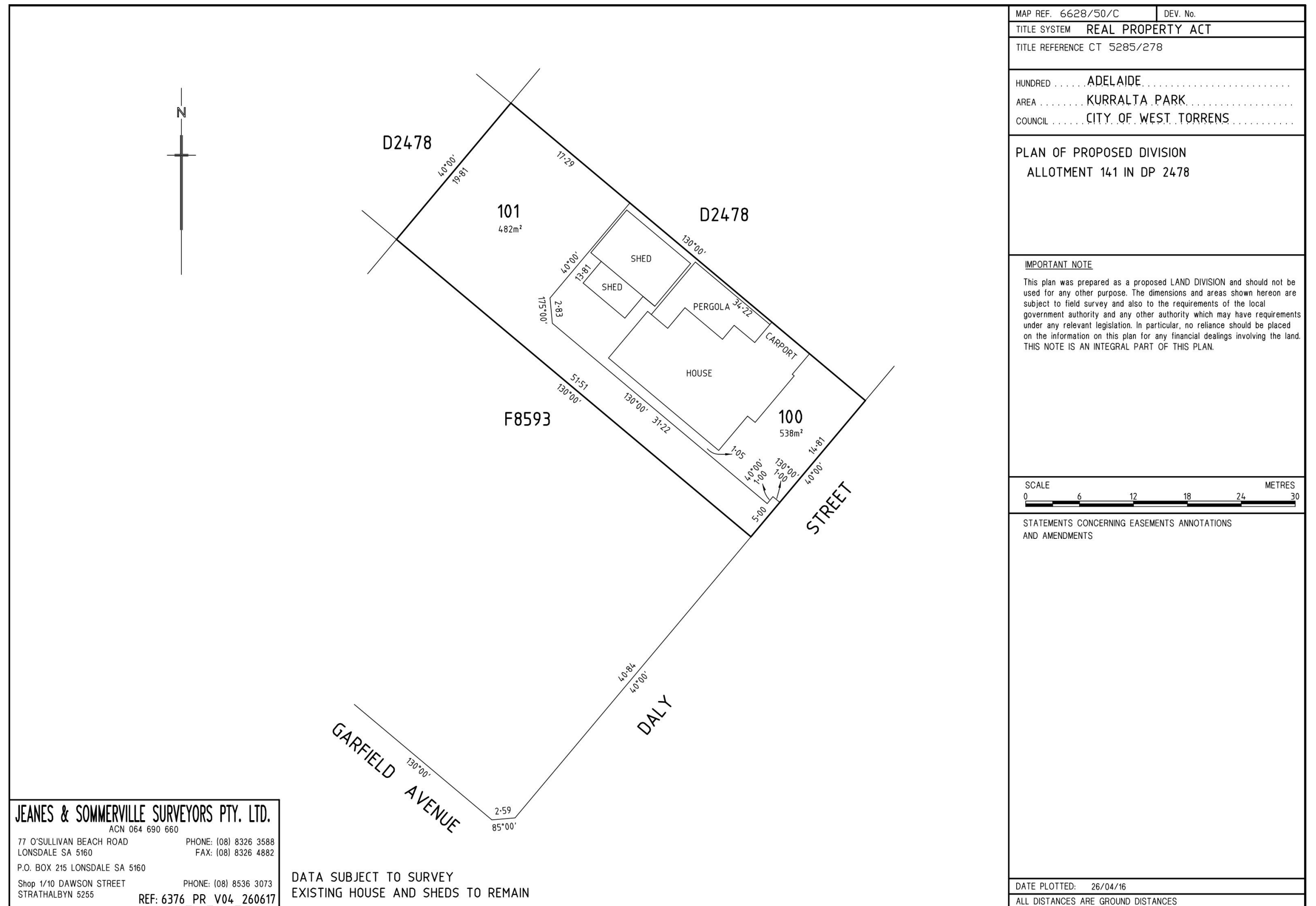
Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance, the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 5 May 2016 and warrants Development Plan Consent.

Attachments

1. Original Plan of Division
2. Amended Plan of Division





6.3 4 Layton Street, FULHAM

Application No 211/413/2017

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Land division - Torrens Title; DAC No. 211/D052/17 (Unique ID 57568); Create one (1) additional allotment
APPLICANT	Rachel Fraser
APPLICATION NO	211/413/2017
LODGEMENT DATE	7 April 2017
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 21
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> ▪ Nil External <ul style="list-style-type: none"> ▪ Development Assessment Commission (DAC) ▪ SA Water
DEVELOPMENT PLAN VERSION	5 May 2016
MEETING DATE	11 July 2017

RECOMMENDATION

The Development Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/413/2017 by Rachel Anne Fraser to undertake Land division - Torrens Title; DAC No. 211/D052/17 (Unique ID 57568); Create one (1) additional allotment at 4 Layton Street (CT5699/210) subject to the following conditions of consent (and any subsequent or amended condition that may be required as a result of the consideration of reserved matters under Section 33(3) of the Development Act):

DEVELOPMENT PLAN CONSENT

Council Conditions

1. Development is to take place in accordance with the plans prepared by Cavallo Forest & Associates Surveying and Land Division Consultants relating to Development Application No. 211/413/2017 (DAC 211/D052/17).

LAND DIVISION CONSENT

Council Conditions

2. Prior to the issue of clearance to this division, existing structures, except for the swimming pool, must be removed from the allotments approved herein. For this purpose, a separate application for demolition shall be submitted for the consideration and determination by Council.
3. Prior to the issue of clearance to this division, work shall be undertaken with regards to ensuring that all access to the swimming pool complies with Australian Standard AS1926.1-2012 Fencing for Swimming Pools, AS1926.2-1995 Swimming Pool Safety – Location of Fencing for Private Swimming Pools and the Building Code of Australia. Alternatively, all of the water shall be drained from the swimming pool.

Development Assessment Commission Conditions

4. Payment of \$6676 into the Planning and Development Fund (1 allotment(s) @ \$6676/allotment). Payment may be made by credit card via the internet at www.edala.sa.gov.au or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, Level 5, 50 Flinders Street, Adelaide.
5. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.
6. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services. The alteration of internal drains to the satisfaction of SA Water is required. Subject to our new process, on receipt of the developer details and site specification an investigation will be carried out to determine if the connections to your development will be standard or non standard fees. On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

BACKGROUND

The development proposal is presented to the Development Assessment Panel (DAP) for the following reason:

- With regard to residential development and land division applications, where all proposed allotments and or sites fail to meet, nor are within 5% of, the minimum frontage widths and site areas designated in respective zones and policy areas within the West Torrens Council Development Plan.

PREVIOUS OR RELATED APPLICATION(S)

DA 211/445/2017 - Construction of a two-storey dwelling with garage under main roof - Under Assessment pending decision on land division (DA 211/413/2017).

SITE AND LOCALITY

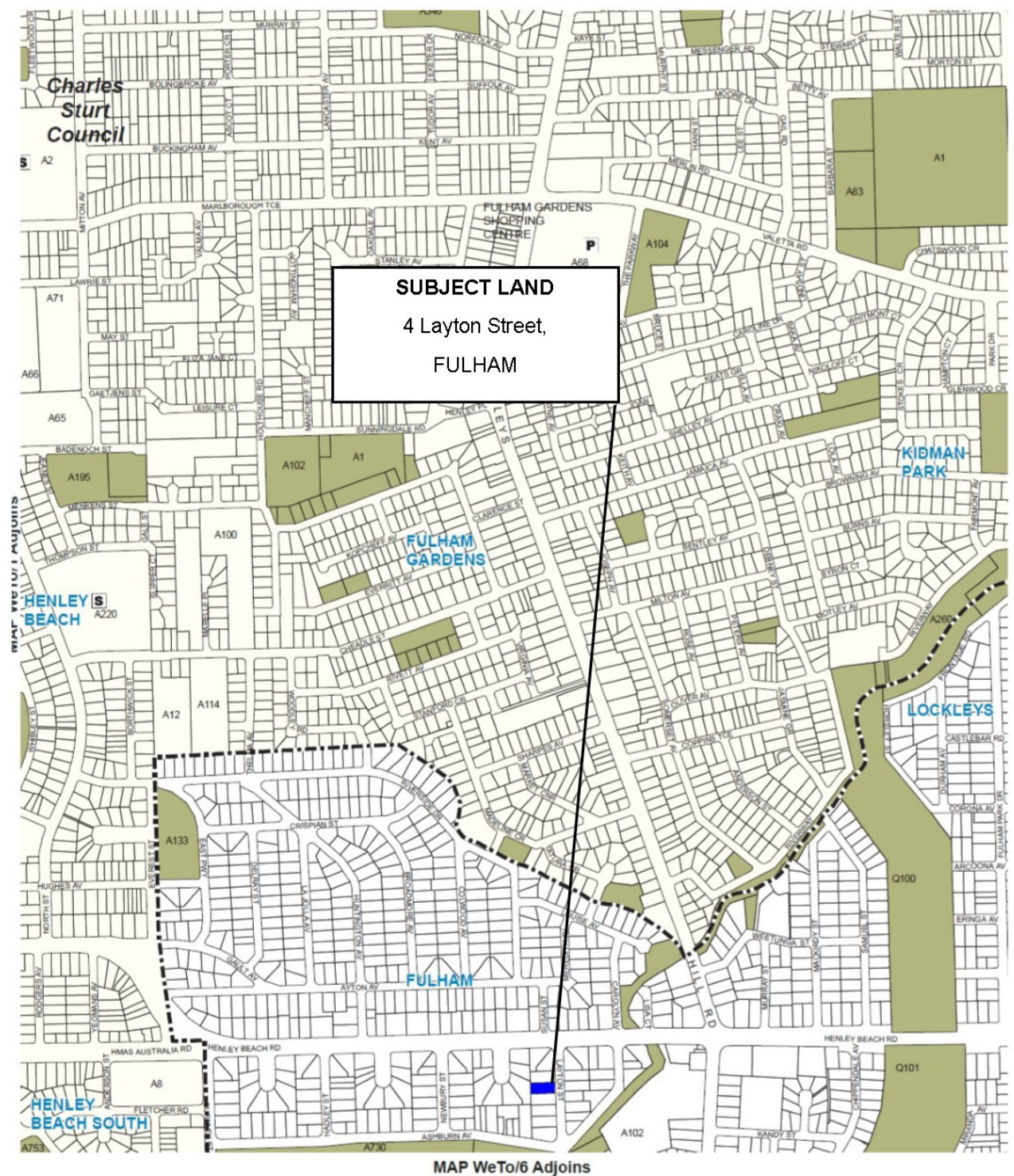
The subject land is formally described as Allotment 72 Deposited Plan 4445 in the area named Fulham Hundred of Adelaide, volume 5699 and folio 210, or more commonly known as 4 Layton Street, Fulham. The subject land has a frontage to Layton Street of 20.42 metres, a depth of 45.72 metres and a site area of 933.6 square metres. Currently existing on the site is a late 1950's conventional hipped roof dwelling with other associated buildings, such as an outbuilding, verandah and carport and a swimming pool.

The subject land is located on the western side of Layton Street and approximately 40.5 metres south of Henley Beach Road. Approximately 200 metres east of the subject land is a Local Centre Zone, which contains non-residential uses such as personal service establishments and shops including eateries.

The locality comprises residential development mainly in the form of single storey detached dwellings. The majority of dwellings along the streetscape were established within the 1950's, however some redevelopment and replacement of older housing stock has occurred on 1, 1A, 2, 2A and 7 Layton Street.

The site and locality are shown on the following maps.





Location Map WeTo/2

- S** School
- P** Post Office
- Local Reserves**
- Development Plan Boundary**

WEST TORRENS COUNCIL
Consolidated - 5 May 2016

PROPOSAL

The applicant seeks development approval for land division creating one additional allotment. Proposed Allotment 1 will have an eleven (11) metre wide frontage to Layton Street, a depth of 45.72 metres, and a site area of 504 square metres. Proposed Allotment 2 will have a 9.42 metre wide frontage to Layton Street, a depth of 45.72 metres, and a site area of 429 square metres.

There is an existing driveway to allow vehicle access to proposed Allotment 2. A single width crossover satisfying the minimum offsets for verge features could be accommodated adjacent proposed Allotment 1 as there is eight (8) metres between the stobie pole and existing street tree.

The swimming pool is proposed to be retained on proposed Allotment 1, and the proposed boundaries of the land division take into account its location so to ensure that it does not encroach into proposed Allotment 2. Given that the swimming pool is located to the rear of the subject land a new dwelling could be sited appropriately and facilitated on the proposed allotment.

A copy of the plan of division is contained within **Attachment 1**.

PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations and Residential Zone, Procedural Matters.

REFERRALS

Internal

No internal referrals were conducted for this application as it is evident that an additional crossover can be accommodated which will not impact on existing verge features. In addition, the subject land is not located in a flood affected area, and therefore no additional stormwater or finished floor level considerations will need to be taken into account.

External

Development Assessment Commission (DAC) and SA Water

Pursuant to Section 33 and Schedule 29(1) of the Development Act and Regulations, the application was referred to SA Water by the Development Assessment Commission.

Neither DAC nor SA Water had any objections to the proposal, subject to several conditions being added to any consent notice.

Full copies of the relevant reports are contained in **Attachment 2**.

ASSESSMENT

The subject land is located within the Residential Zone and more particularly Low Density Policy Area 21 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
<i>Land Division</i>	<i>Objectives</i>	<i>1, 2, 3 & 4</i>
	<i>Principles of Development Control</i>	<i>1, 2, 5, 6, 8 & 12</i>
<i>Orderly and Sustainable Development</i>	<i>Objectives</i>	<i>1, 2, 3, 4 & 5</i>
	<i>Principles of Development Control</i>	<i>1, 3 & 7</i>
<i>Residential Development</i>	<i>Objectives</i>	<i>1, 2, 3 & 4</i>
	<i>Principles of Development Control</i>	<i>1 & 3</i>

Zone: Residential Zone	
Desired Character Statement:	
<p><i>This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer.</i></p>	
Objectives	1, 2, 3 & 4
Principles of Development Control	1 & 5

Policy Area: Low Density Policy Area 21	
Desired Character Statement:	
<p><i>This policy area will have a low density character. In order to preserve this, development will predominantly involve the replacement of detached dwellings with the same (or buildings in the form of detached dwellings).</i></p> <p><i>There will be a denser allotment pattern and some alternative dwelling types, such as semi-detached and row dwellings, close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage. In the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park, where the consistent allotment pattern is a significant positive feature of the locality, subdivision will reinforce the existing allotment pattern.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings. Buildings in the area bounded by Henley Beach Road, Torrens Avenue and the Linear Park will be complementary to existing dwellings through the incorporation of design features such as pitched roofs, eaves and variation in the texture of building materials.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings.</i></p>	
Objectives	1
Principles of Development Control	1, 2, 4 & 6

QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT	
		Allotment 1	Allotment 2
SITE AREA <i>Low Density Policy Area 21</i> <i>PDC 4 (within 400m of centre)</i>	350m ² (min.)	504m ² Satisfies	429 m ² Satisfies
SITE FRONTAGE <i>Low Density Policy Area 21</i> <i>PDC 4 (within 400m of centre)</i>	9m	11m Satisfies	9.42m Satisfies
ALLOTMENT AREA <i>Low Density Policy Area 21</i> <i>PDC 6</i>	420m ² (min.)	504m ² Satisfies	429 m ² Satisfies
ALLOTMENT FRONTAGE <i>Low Density Policy Area 21</i> <i>PDC 6</i>	12m	11m Does Not Satisfy by 8.3%	9.42m Does Not Satisfy by 21.5%

QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development satisfies the relevant Development Plan provisions with the exception of the following, as discussed under the following sub headings:

Land Use and Zoning

Dwellings are envisaged within the Residential Zone and detached dwellings are envisaged specifically within the policy area. The proposed allotments will be of a size that supports residential redevelopment.

Land Division

Principle of Development Control (PDC) 6, Residential Zone, Low Density Policy Area 21 provides that land division should create allotments with an area of greater than 420 square metres and a minimum frontage width of 12 metres, other than where land division is combined with an application for dwellings or follows an approval for dwellings on the site.

PDC 4 Residential Zone, Low Density Policy Area 21 provides that when a dwelling is located within 400 metres of a Centre Zone it should have a minimum site area of 350 square metres and a frontage to a public road of nine (9) metres.

In this instance the subject land is located within 400 metres of two Centre Zones and if the present application had been combined with an application for dwellings then the requirements of PDC 4 would have been met.

It should be noted that within the Low Density Policy Area 21 the same allotment area and frontage width is prescribed for each envisaged dwelling type. Therefore, a combined application for the purpose of ensuring a specific dwelling type is built on the land is not necessary as the proposed allotments will be a size and width which will accommodate any of the envisaged dwelling types listed.

The proposed division still results in allotments which have an area and an appropriate frontage width to support envisaged dwelling types of detached dwellings or semi-detached dwellings. The proposal will contribute to the desired character by creating a greater number of housing opportunities located in close proximity to Centre Zones.

SUMMARY

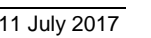
The subject land is located within 400 metres of a Centre Zone and, as such, the site areas and frontage widths of the proposed allotments are considered to be appropriate within this specific area. The development will result in allotments that are suitable to accommodate a detached dwelling or semi-detached dwelling (an envisaged form of development within the zone and policy area) which will be complimentary to the built form within the locality.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 5 May 2016 and warrants Development Plan Consent.

Attachments

- 1. Plan of Division**
- 2. DAC & SA Water Referral**



Contact Lands Titles Office
Telephone 7109 7016



22 April 2017

The Chief Executive Officer
City of West Torrens

Dear Sir/Madam

Re: Proposed Application No. 211/D052/17 (ID 57568)
for Land Division by Ms Rachel Anne Fraser

In accordance with Section 33 of the Development Act 1993 and Regulation 29 (1) of the Development Regulations 2008, and further to my advice dated 03 April 2017, I advise that the Development Assessment Commission has consulted with SA Water Corporation (only) regarding this land division application. A copy of their response has been uploaded in EDALA for your consideration. The Commission has no further comment to make on this application, however there may be local planning issues which Council should consider prior to making its decision.

I further advise that the Development Assessment Commission has the following requirements under Section 33(1)(c) of the Development Act 1993 which must be included as conditions of land division approval on Council's Decision Notification (should such approval be granted).

1. The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.
The alteration of internal drains to the satisfaction of SA Water is required.
Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.
On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.
2. Payment of \$6676 into the Planning and Development Fund (1 allotment(s) @ \$6676/allotment).
Payment may be made by credit card via the internet at www.edala.sa.gov.au or by phone (7109 7018), by cheque payable to the Development Assessment Commission marked "Not Negotiable" and sent to GPO Box 1815, Adelaide 5001 or in person, at Ground Floor, 101 Grenfell Street, Adelaide.
3. A final plan complying with the requirements for plans as set out in the Manual of Survey Practice Volume 1 (Plan Presentation and Guidelines) issued by the Registrar General to be lodged with the Development Assessment Commission for Land Division Certificate purposes.

The SA Water Corporation will, in due course, correspond directly with the applicant/agent regarding this land division proposal.

PLEASE UPLOAD THE DECISION NOTIFICATION FORM (VIA EDALA) FOLLOWING COUNCIL'S DECISION.

A handwritten signature in black ink, appearing to read "P. Hodgson", with a long, sweeping underline.

Phil Hodgson
Unit Manager
Lands Titles Office



22 April 2017

Our Ref: H0057610

The Chairman
Development Assessment Commission
136 North Terrace
ADELAIDE SA 5000

Dear Sir/Madam

PROPOSED LAND DIVISION APPLICATION NO: 211/D052/17 AT FULHAM

In response to the abovementioned proposal, I advise that pursuant to Section 33 of the Development Act it is necessary for the developer to satisfy this Corporation's requirements, which are listed below.

The financial requirements of SA Water shall be met for the provision of water supply and sewerage services.

The alteration of internal drains to the satisfaction of SA Water is required.

Subject to our new process, on receipt of the developer details and site specifications an investigation will be carried out to determine if the connections to your development will be standard or non standard fees.

On approval of the application, all internal water piping that crosses the allotment boundaries must be severed or redirected at the developers/owners cost to ensure that the pipework relating to each allotment is contained within its boundaries.

Yours faithfully

TONY PANNUNZIO

for MANAGER LAND DEVELOPMENT & CONNECTIONS

SA Water
Level 6, 250 Victoria Square
ADELAIDE SA 5000
Ph (08) 7424 1119
Inquiries TONY PANNUNZIO
Telephone 7424 1243

6.4 101-105 Hardys Road, UNDERDALE

Application No 211/1546/2016

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Construction of sixteen (16) three-storey group dwellings all with garages under main roof - Non-Complying
APPLICANT	Township Development Pty Ltd
APPLICATION NO	211/1546/2016
LODGEMENT DATE	19 December 2016
ZONE	Industry
APPLICATION TYPE	Non-Complying
PUBLIC NOTIFICATION	Category 3
REFERRALS	Internal <ul style="list-style-type: none"> City Assets
DEVELOPMENT PLAN VERSION	5 May 2016
MEETING DATE	11 July 2017

RECOMMENDATION

The Development Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993*, resolves to GRANT Development Plan Consent for Application No. 211/1546/2016 by Township Development to undertake the construction of sixteen (16) three-storey group dwellings all with garages under main roof at 101-105 Hardys Road, Underdale (CT6161/341), subject to CONCURRENCE from the Development Assessment Commission and the following conditions of consent:

Council Conditions

1. That the development shall be undertaken and completed in accordance with the plans and information detailed in this application except where varied by any condition(s) listed below.
2. That a stormwater scheme shall be designed in accordance with Australian Standards and recognised engineering best practices to ensure that stormwater does not adversely affect any adjoining property or public road and for this purpose stormwater drainage shall not at any time:
 - a) Result in the entry of water into a building; or
 - b) Affect the stability of a building; or
 - c) Create unhealthy or dangerous conditions on the site or within the building; or
 - d) Flow or discharge onto the land of an adjoining owner; and not flow across footpaths or public ways; or
 - e) Result in a total discharge of water from the entire site of more than 20 litres per second for the site critical 20 year ARI storm event.

For this purpose, final stormwater and stormwater quality management details shall be submitted to and approved by Council prior to, or at the time of, application for Building Rules Consent.

The following table outlines current recommended practice for the targeted improvement of stormwater quality from new development of scale, as outlined in the State Government's Water Sensitive Urban Design Policy - 2013, the targets being:

<i>Parameter</i>	<i>Target</i>
Reduction Litter /Gross Pollutant	90%
Reduction in Average Annual total Suspended Solids (TSS)	80% *1
Reduction in Average Annual Total Phosphorous (TP)	60% *1
Reduction in Average Annual Total Nitrogen (TN)	45% *1

*1 - Reduction as compared to an equivalent catchment with no water quality management controls

3. That the finished floor levels of the proposed dwellings shall be as follows:
 - Dwellings 1-3: 14.05
 - Dwelling 4: 14.10
 - Dwellings 5-7: 14.15
 - Dwelling 8: 14.20
 - Dwellings 9-10: 14.25
 - Dwellings 11-16: 14.15
4. That any stormwater connection through the road verge area shall satisfy the following requirements:
 - 100x50x2mm RHS galvanised steel, or
 - 125x75x2mm RHS galvanised steel, or
 - Multiples of the above.
5. That any retaining walls shall be designed to accepted engineering standards, and not of timber construction if retaining a difference in ground level exceeding 200mm. For this purpose detailed design of the proposed gabion wall shall be submitted to Council to determine and resolve any likely impact the wall may have on existing light poles along the boundary.
6. That all driveways, parking and manoeuvring areas shall be formed, surfaced with concrete, bitumen or paving, and be properly drained, and shall be maintained in reasonable condition at all times.
7. That all dwellings within the site shall have their domestic waste collected by private contractors.
8. That driveway access into the property shall be located at a minimum of 1.0 metre from the existing pram ramp and should be shaped such that they appropriately interact with and accommodate existing verge features in front of the subject and adjacent properties.
9. That any redundant crossover(s) shall be reinstated to the satisfaction of Council at full cost to the developer.

10. That the significant tree (*Eucalyptus camaldulensis*) adjoining the site shall be retained in its entirety and protected with a Tree Protection Zone (TPZ) as follows:
 - The TPZ radius from the centre of the trunk = 15m
 - The TPZ area around the tree = 707m²
 - The Structural Root Zone (SRZ) radius from the centre of the trunk = 4.3m
11. That protective fencing shall be erected between the tree and the development site (along the property boundary) prior to any development activities occurring. The fence shall be erected in accordance with the site plan and fencing guidelines detailed within the Tree Environs report dated 12 July 2010.
12. That site preparation activities for levelling the site, trenching for footings and other services shall remain outside of the TPZ. These activities shall proceed with caution under the supervision of the project arborist, if permitted to occur within the TPZ. Site preparation activities shall adhere to the guidelines detailed within the Tree Environs report dated 12 July 2010.
13. That underground services shall remain outside of the TPZ. If they must pass through the TPZ they must use alternate installation methods such as directional boring or manual excavation and these activities shall adhere to the guidelines detailed within the Tree Environs report dated 12 July 2010.
14. That paving activities within the TPZ shall utilise a no-dig method and incorporate porous materials to allow for the diffusion of gases and moisture between the root system and atmosphere. All paving within the TPZ shall occur in accordance with the guidelines detailed within the Tree Environs report dated 12 July 2010.
15. That all landscaping activities within the TPZ shall avoid disturbance of the tree's root system. All landscaping within the TPZ shall occur in accordance with the guidelines detailed within the Tree Environs report dated 12 July 2010.
16. That all boundary fencing within the TPZ shall use panels on a post and rail system with no continuous trenching.
17. That a detailed landscape plan shall be submitted to and approved by Council prior to or at the time of application for Building Rules Consent.
18. Council requires one business days' notice of the following stages of building work:
 - Commencement of building work on site
 - Commencement of placement of any structural concrete
 - Completion of wall and roof framing prior to the installation of linings
 - Completion of building work.

BACKGROUND

The Development Assessment Panel at its meeting held on 10 April 2012 resolved to GRANT consent to an application submitted by Township Developments Pty Ltd for the demolition of an existing warehouse and construction of five (5) two-storey group dwellings and one (1) two-storey residential flat building containing three (3) dwellings at 101-105 Hardys Road, Underdale, subject to the concurrence of the Development Assessment Commission and a number of conditions.

The Development Assessment Commission's concurrence was received and formal consent was issued on 16 August 2012. Consent was subsequently granted on 3 April 2014 to an application to vary the authorisation previously given, including an additional dwelling to the residential flat building.

As part of the development process Council sought to extend the shared use path known as the River Torrens Linear Park between Hardys Road and Chatswood Grove, Underdale, which required a portion of the subject land.

This was successfully negotiated in March 2015. As part of the proposed development the developer agreed to vest Reserve Land in Council. Works on the extended path were completed towards the latter half of 2014.

Council is advised that the Applicant has not pursued the consent granted as subsequent market feedback, following lack of sales, indicated that the form and configuration of the dwellings was not in line with the market's expectations for the locality.

PREVIOUS OR RELATED APPLICATION(S)

- 211/996/2010 - Demolition of an existing warehouse and construction of five (5) two-storey group dwellings and one (1) two-storey residential flat building containing three (3) dwellings - DPC Granted 16 August 2012
- 211/867/2014 - Torrens Title Land Division, DAC 211/D101/14, Create nine (9) residential allotments - Approval Granted 6 March 2015
- 211/996/2010/A - Demolition of an existing warehouse and construction of five (5) two-storey group dwellings and one (1) two-storey residential flat building containing three (3) dwellings - Variation to an authorisation previously granted - Various minor changes and the inclusion of an additional dwelling to the residential flat building - DPC Granted 3 April 2014
- 211/136/2016 - Construction of four (4) three storey residential flat buildings totalling 20 dwellings all with garages under the main roof - DPC Refused 8 November 2016

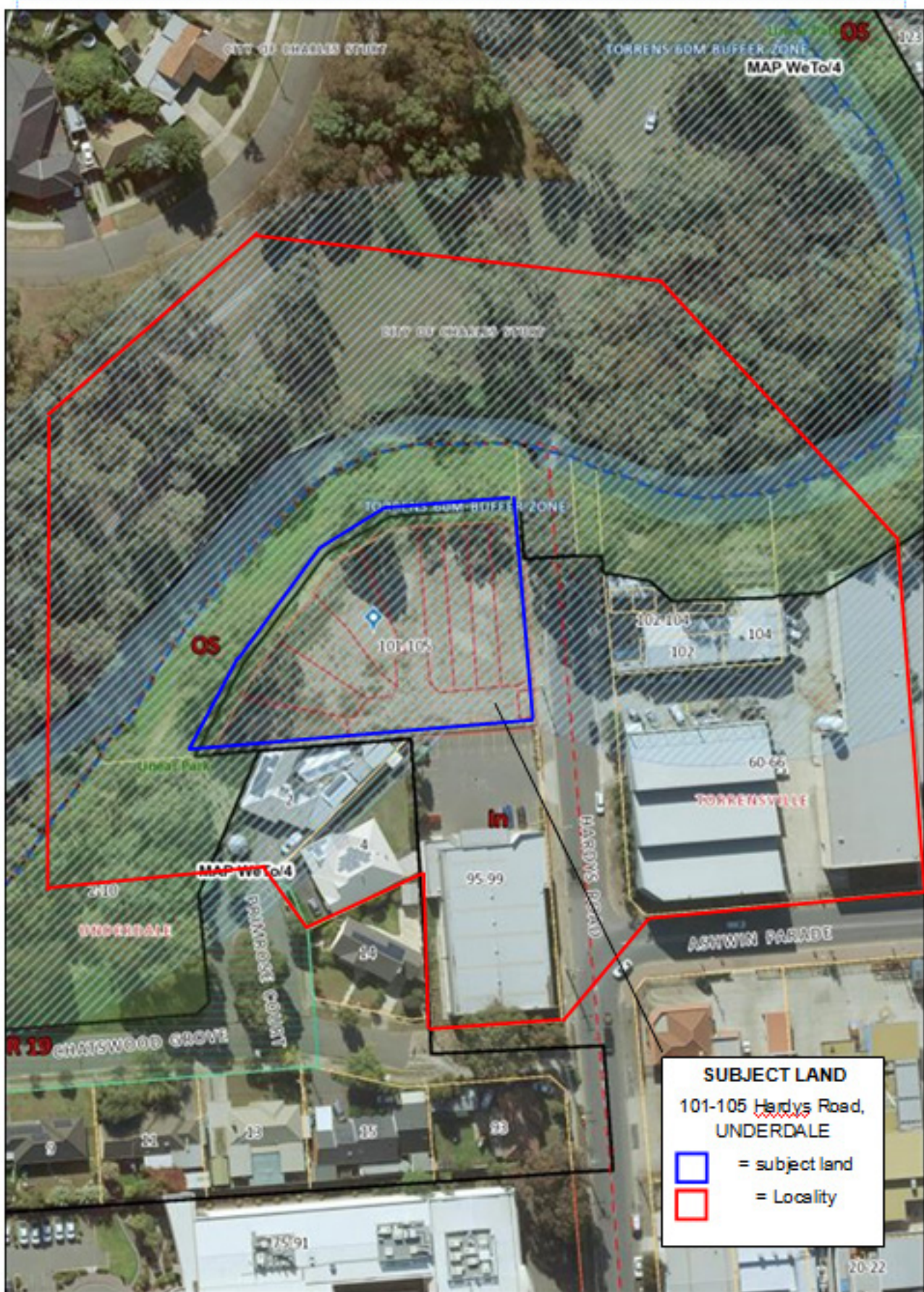
SITE AND LOCALITY

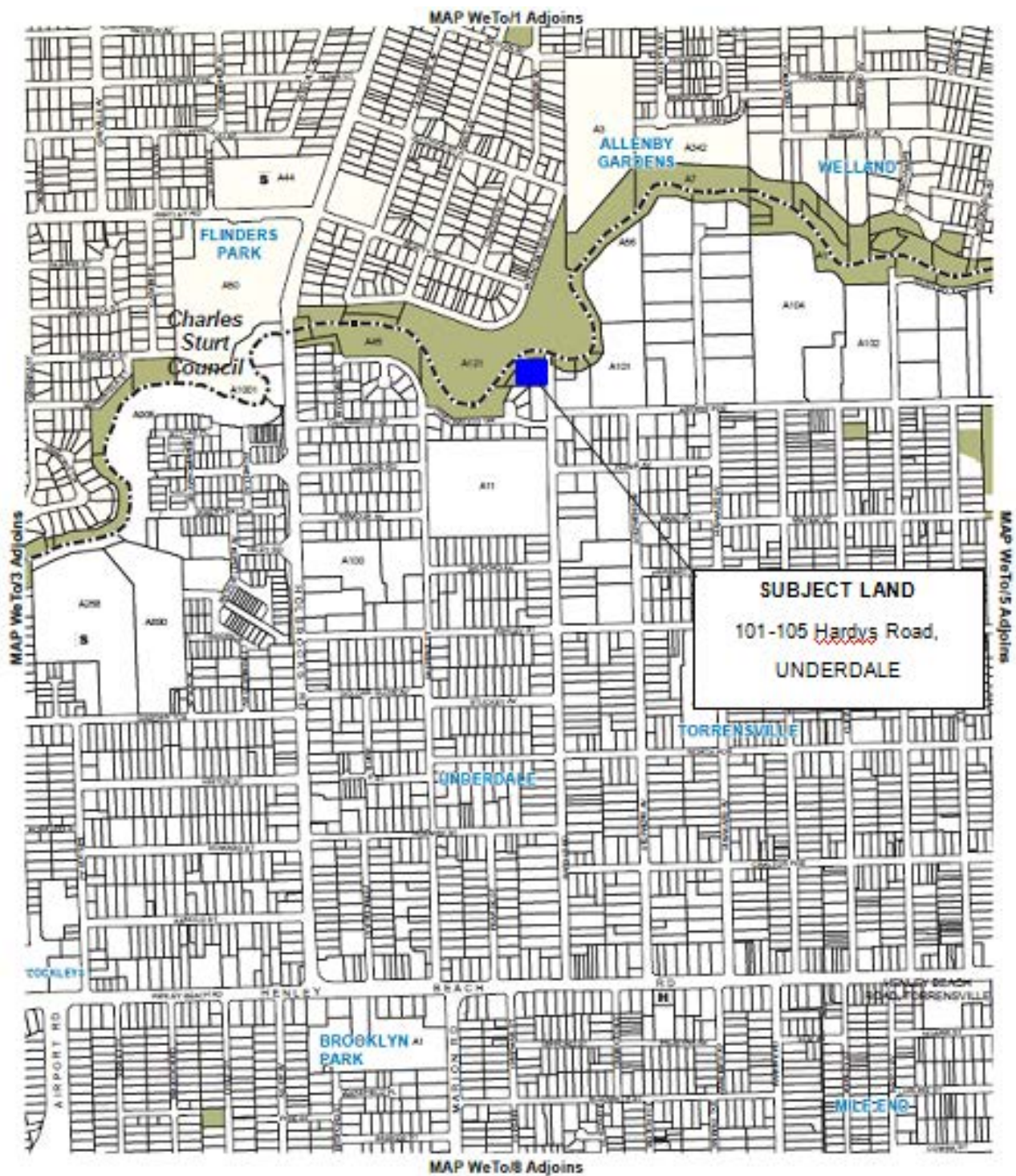
The subject site is irregular shaped vacant land, located at the northwestern extremity of Hardys Road, has a frontage of 49.87 metres to Hardys Road and 98.65 metres to the River Torrens Linear Park and an area of 2722m².

The locality has a mixed character. The land to the north and west of the subject land is zoned Open Space and forms part of the River Torrens Linear Park. The lands on the northern side of the River Torrens, opposite the subject land, are within the City of Charles Sturt, zoned for residential purposes and include single and two-storey dwellings.

The lands to the southwest of the subject land, being Chatswood Grove and Primrose Court, are within the Residential Zone and, more particularly, Medium Density Policy Area 19. These properties are primarily single-storey dwellings on relatively large allotments, capable of redevelopment at medium density.

The lands to the south of Chatswood Grove and opposite the subject land on the eastern side of Hardys Road are zoned Industry and include a mix of light industrial and warehouse uses.





Location Map WeTo/4

- [S] School
- [H] Other Health Services
- [Green] Local Reserves
- [Dashed Line] Development Plan Boundary

WEST TORRENS COUNCIL
Consolidated - 5 May 2016

PROPOSAL

It is proposed to erect and complete sixteen (16) group dwellings, ten (10) fronting the Linear Park and six (6) adjacent to Hardys Road.

The dwellings fronting the Linear Park comprise:

- Double garage, entry foyer, storeroom, laundry and guest room/ office at ground level;
- Open kitchen, lounge and dining area at first floor level;
- Bedrooms and rumpus room at second floor level.

The dwellings adjacent Hardys Road comprise:

- Garage, laundry and guest room/office at ground level;
- Open kitchen, lounge and dining area at first floor level;
- Bedroom/study at second floor level.

The site will be extensively landscaped.

The application documentation forms **Attachment 1**.

NON-COMPLYING

The application is for a non-complying form of development as dwellings are listed as non-complying in the Industry Zone. Should the Panel resolve to approve the application then the concurrence of the Development Assessment commission is required. Alternatively, should the Panel refuse the application, no rights of appeal are afforded to the Applicant.

PUBLIC NOTIFICATION

The application is a Category 3 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Development Regulations and the Procedural Matters, Zone Section, Industry Zone of the West Torrens Council Development Plan.

Properties notified:	Fifteen (15) properties were notified during the public notification process.
Representations:	Two (2) representations were received. NOTE: An out-of-time representation was received from Michael Wohlstadt - Planning Advisory Services, acting on behalf of Plastico, the occupiers of 60-66 Ashwin Parade. Mr Wohlstadt reiterated previous concerns regarding the non-complying nature of the proposed development. As a matter of law, this representation cannot be taken into consideration.
Persons wishing to be heard:	None of the representors identified that they wished to address the Panel.
Summary of Representations:	Concerns were raised regarding the following matters: <ul style="list-style-type: none"> • Future residents will complain about noise from existing commercial/industrial activities; • Traffic and parking implications; • Rubbish collection; • Non-complying nature of the development.

A copy of the representors' concerns and the applicant's response is contained in **Attachment 2**.

REFERRALS

Internal

City Assets

City Assets' concerns have been addressed.

External

No external agency referrals were required.

ASSESSMENT

The subject land is located within the Industry Zone as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Crime Prevention	Objectives	1
	Principles of Development Control	1, 2, 3, 4, 5, 6, 7, 8 & 10
Design and Appearance	Objectives	1 & 2
	Principles of Development Control	1, 2, 3, 4, 5, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23 & 24
Energy Efficiency	Objectives	1 & 2
	Principles of Development Control	1, 2, 3 & 4
Hazards	Objectives	1, 2, 4, 7, 8, 9 & 10
	Principles of Development Control	1, 2, 3, 13, 14 & 15
Interface between Land Uses	Objectives	1 & 2
	Principles of Development Control	1, 2, 4 & 5
Orderly and Sustainable Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 3, 4, 5, 6, 7 & 8
Regulated Trees	Objectives	1 & 2
	Principles of Development Control	1, 2 & 3
Residential Development	Objectives	1, 2, 3 & 4
	Principles of Development Control	1, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 27, 28, 29, 30 & 31
Significant Trees	Objectives	1 & 2
	Principles of Development Control	1, 2, 3 & 4
Transportation and Access	Objectives	2
	Principles of Development Control	1, 2, 4, 8, 9, 10, 11, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42 & 43
Zone: Industry Zone		
Objectives		1 & 2
Principles of Development Control		1, 2, 4, 5, 6, 7, 8, 10 & 11

QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
SITE AREA	No provision	170m ² (average)
SITE FRONTAGE	15m	49.87m Satisfies
SITE COVERAGE	No provision	42.9%
STREET SETBACK	No provision	Between 1.55m and 3.06m
SIDE/REAR SETBACKS	No provision	Dwellings 1-4 and Dwellings 5-10 will be set back 2.62m and 1.48m respectively from the Linear Park. Dwelling 10 will be set back between 0.8m and 1.48m from the Linear Park. Dwellings 11-16 will be set back 5.49m from southern boundary, Dwelling 10 will be set back 3.67m from this boundary.
PRIVATE OPEN SPACE General Section - Residential Development PDC 19	24m ²	Dwellings 1-10: 26m ² - 51m ² Dwelling 11: 51.7m ² Dwellings 12-15: 36.6m ² Dwelling 16: 40m ² Satisfied
CARPARKING SPACES Module: Table WeTo/2 Off-street Vehicle Parking requirements	36 car-parking spaces required	38 provided Satisfied
STORAGE General Section - Residential Development PDC 31	8m ³	Dwellings 1-10: 16m ³ Dwellings 11-15: 10.75m ³ Satisfied

QUALITATIVE ASSESSMENT

In this instance it is considered that the key issues to be considered and determined are:

- Land Use and Zoning
- Interface Between Land Uses
- Design and Appearance
- Regulated/Significant trees
- Transportation and Access
- Land Contamination
- Private Open Space

A detailed and comprehensive Statement of Effect and supporting documentation accompanies the application. It is not necessary to rehearse such detail here but suffice to say that Council's Administration is in support of its general intent.

Land Use and Zoning

It may validly be held that the "die was cast" when the original consent was granted. At the time, the land use question was considered and it was determined that, due to its location, the land was suitable for residential purposes.

In fact, Council has been investigating the potential or otherwise of rezoning the Underdale and Torrensville Industry Zone to facilitate medium density residential development since 2008. To this end, a number of studies have been carried out, including the West Torrens Residential and Industrial Interface Study and Economic and Land Use Analysis, culminating in the preparation of the Underdale and Torrensville Urban Renewal Development Plan Amendment which was endorsed for public consultation by Council at its meeting held on 20 June 2017.

The subject site has been identified as being suitable for medium density residential development.

Interface between Land Uses

The Development Plan seeks to minimise any adverse impact and conflict between land uses.

The surrounding land uses in the industrial zoned land, albeit commercial/industrial in nature, are somewhat benign and low impacting. They do not generate excessive impacts such as noise, dust, fumes, odours or the like that may be expected in more heavily industrialised areas.

The subject land is located at the edge of the Industry Zone adjacent to residential development and open space. The design and siting of the proposed development will minimise any adverse impact and conflict between land uses.

An Environmental Noise Assessment prepared by Sonus Pty Ltd accompanies the application. The report concludes that subject to certain acoustic treatments being met, an adequate level of amenity for the proposed dwellings, whilst protecting the continuation of the lawfully existing industrial land uses, will be attained.

Design and Appearance

The scheme creates a modern development which takes full advantage of the aspect and prospect offered by the site and its setting. It is considered that the completed development will create an appropriate presentation and design response to both Hardys Road and the Linear Park.

Principles of Development Control 25 and 26, General Section, Design and Appearance, provide that:

*PDC 25 Buildings and structures should be set back a minimum of 8 metres from the boundary of the **Open Space Zone** plus an additional 1 metre for every metre of vertical wall height above 5 metres.*

PDC 26 Development should not impair, disfigure, interfere with or detrimentally affect the amenity, aesthetic appearance or scenic beauty of:

(a) the River Torrens

(b) land within 60 metres of either side of the River Torrens

(c) the landscape visible from the River Torrens.

Whilst it is acknowledged that the Open Space Zone quantitative setback provision has not been met it is considered that the proposed development will not impair, disfigure, interfere with or detrimentally affect the amenity, aesthetic appearance or scenic beauty of the River Torrens and the Linear Park.

These are industry standards that recommend orientating medium to high density residential housing to face and front out to linear parks to improve passive surveillance.

The present proposal achieves this standard.

Regulated / Significant trees

There is a large *Eucalyptus camaldulensis* (River Red Gum) adjacent to the site at the culmination of Hardys Road. A number of tree protection conditions are recommended.

Transportation and Access

A Traffic Impact Assessment prepared by GTA Consultants accompanies the application. The report concludes that the traffic management measures proposed are appropriate, in certain respects exceeding the requirements of the Development Plan, and the existing road network is capable of accommodating the proposed development in a safe and convenient manner.

It is also noted that each dwelling will be provided with a wall-mounted bicycle storage device.

Land Contamination

A report by qualified environmental engineers, FMG, formed part of the original proposal. The report concluded that, based on the intrusive soil investigation program carried out, no contamination issues were identified that would pose any limitations for residential use of the land.

Private Open Space

The dwellings are basically designed to provide upper level living. In these circumstances it is considered appropriate that the open space requirements, PDC 20, General Section - Residential Development apply. The proposal meets the recommended private open space requirements.

The proposal provides relatively large areas of communal open space and the benefits and attributes of the River Torrens Linear Park are readily and directly accessible from the subject land.

SUMMARY

The present proposal has addressed all the reasons for refusal of the previous application in that the land use interface issues, lack of private open space, dwelling density and insufficient car parking have all been satisfied.

It is also interesting to note that the proposed development meets the recommended qualitative and quantitative provisions of the Development Plan Amendment endorsed by Council for public consultation.

Having regard to the nature of the site, its historical and factual context and the relevant provisions of the West Torrens Council Development Plan it is considered that the proposed development is not seriously at variance with the provisions of the Development Plan and is an appropriate form of development that has due regard to its neighbours.

On balance, the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 5 May 2016 and warrants Development Plan Consent.

Attachments

- 1. Proposal plans and documentation**
- 2. Representations and Response**



The structure shall be constructed in accordance with current edition of National Construction Code. Figured dimensions shall take preference over scaled drawings. Where site conditions do not reflect the intent shown in the drawings or where site measurements conflict with dimensions, form or notation shown seek clarification before commencement. These plans remain the exclusive property of Mustang Homes and are protected by Copyright laws. Legal action will be taken against any infringement, whether it be in part or in full, unless written permission is given.



REVISIONS		
AMENDMENT	DATE	DESCRIPTION
1. Initial Concept	08 Feb 2018	
2. Final Design	10 Jun 2018	
3. Final Design	18 Jun 2018	
4. Final Design	22 Jun 2018	
5. Final Design	24 Jun 2018	
6. Final Design	26 Jun 2018	
7. Final Design	28 Jun 2018	
8. Final Design	30 Jun 2018	
9. Final Design	02 Jul 2018	
10. Final Design	04 Jul 2018	
11. Final Design	06 Jul 2018	
12. Final Design	08 Jul 2018	
13. Final Design	10 Jul 2018	
14. Final Design	12 Jul 2018	
15. Final Design	14 Jul 2018	
16. Final Design	16 Jul 2018	
17. Final Design	18 Jul 2018	
18. Final Design	20 Jul 2018	
19. Final Design	22 Jul 2018	
20. Final Design	24 Jul 2018	
21. Final Design	26 Jul 2018	
22. Final Design	28 Jul 2018	
23. Final Design	30 Jul 2018	
24. Final Design	01 Aug 2018	
25. Final Design	03 Aug 2018	
26. Final Design	05 Aug 2018	
27. Final Design	07 Aug 2018	
28. Final Design	09 Aug 2018	
29. Final Design	11 Aug 2018	
30. Final Design	13 Aug 2018	
31. Final Design	15 Aug 2018	
32. Final Design	17 Aug 2018	
33. Final Design	19 Aug 2018	
34. Final Design	21 Aug 2018	
35. Final Design	23 Aug 2018	
36. Final Design	25 Aug 2018	
37. Final Design	27 Aug 2018	
38. Final Design	29 Aug 2018	
39. Final Design	31 Aug 2018	
40. Final Design	02 Sep 2018	
41. Final Design	04 Sep 2018	
42. Final Design	06 Sep 2018	
43. Final Design	08 Sep 2018	
44. Final Design	10 Sep 2018	
45. Final Design	12 Sep 2018	
46. Final Design	14 Sep 2018	
47. Final Design	16 Sep 2018	
48. Final Design	18 Sep 2018	
49. Final Design	20 Sep 2018	
50. Final Design	22 Sep 2018	
51. Final Design	24 Sep 2018	
52. Final Design	26 Sep 2018	
53. Final Design	28 Sep 2018	
54. Final Design	30 Sep 2018	
55. Final Design	02 Oct 2018	
56. Final Design	04 Oct 2018	
57. Final Design	06 Oct 2018	
58. Final Design	08 Oct 2018	
59. Final Design	10 Oct 2018	
60. Final Design	12 Oct 2018	
61. Final Design	14 Oct 2018	
62. Final Design	16 Oct 2018	
63. Final Design	18 Oct 2018	
64. Final Design	20 Oct 2018	
65. Final Design	22 Oct 2018	
66. Final Design	24 Oct 2018	
67. Final Design	26 Oct 2018	
68. Final Design	28 Oct 2018	
69. Final Design	30 Oct 2018	
70. Final Design	01 Nov 2018	
71. Final Design	03 Nov 2018	
72. Final Design	05 Nov 2018	
73. Final Design	07 Nov 2018	
74. Final Design	09 Nov 2018	
75. Final Design	11 Nov 2018	
76. Final Design	13 Nov 2018	
77. Final Design	15 Nov 2018	
78. Final Design	17 Nov 2018	
79. Final Design	19 Nov 2018	
80. Final Design	21 Nov 2018	
81. Final Design	23 Nov 2018	
82. Final Design	25 Nov 2018	
83. Final Design	27 Nov 2018	
84. Final Design	29 Nov 2018	
85. Final Design	01 Dec 2018	
86. Final Design	03 Dec 2018	
87. Final Design	05 Dec 2018	
88. Final Design	07 Dec 2018	
89. Final Design	09 Dec 2018	
90. Final Design	11 Dec 2018	
91. Final Design	13 Dec 2018	
92. Final Design	15 Dec 2018	
93. Final Design	17 Dec 2018	
94. Final Design	19 Dec 2018	
95. Final Design	21 Dec 2018	
96. Final Design	23 Dec 2018	
97. Final Design	25 Dec 2018	
98. Final Design	27 Dec 2018	
99. Final Design	29 Dec 2018	
100. Final Design	31 Dec 2018	

rivergum

homes

387-391 South Road, MILE END SOUTH SA 5031

Phone: (08) 8354 7800 Fax: (08) 8234 6099

A.B.N. 70 065 466 337 B.Lic. BLD 113681

Web: rivergumhomes.com.au

Client

Township Development Pty Ltd

Project		
Proposed Development Group Dwellings		
At: Lot 101-105 Hardys Road UNDERDALE		
Drawn	Date	By
1001/150	08 Feb 2018	1001/150
Scale	Sheet No.	3 of 6
Project No.	ISSUE NO.	19
PRE 0401		
The structure shall be constructed in accordance with current edition of National Construction Code. Figure dimensions shall take preference over scaled drawings. Where site conditions do not reflect the intent shown in the drawings or where site measurements conflict with dimensions, levels or notation shown, site clarification shall be sought prior to commencement. These plans remain the exclusive property of Rivergum Homes and are protected by Copyright laws. Legal action will be taken against any infringement whether it be in part or in full, unless written permission is given.		

- SURFMIST

CEMTEL - DESIGNER SERIES - URBAN GREY

BASALT

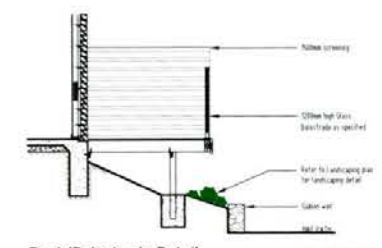
CEMTEL - DESIGNER SERIES - WOODGRAIN TEAK

AUSTRAL - COBALT

MONUMENT



River Torrens Elevation TYPE A SCALE 1:100 @ A1



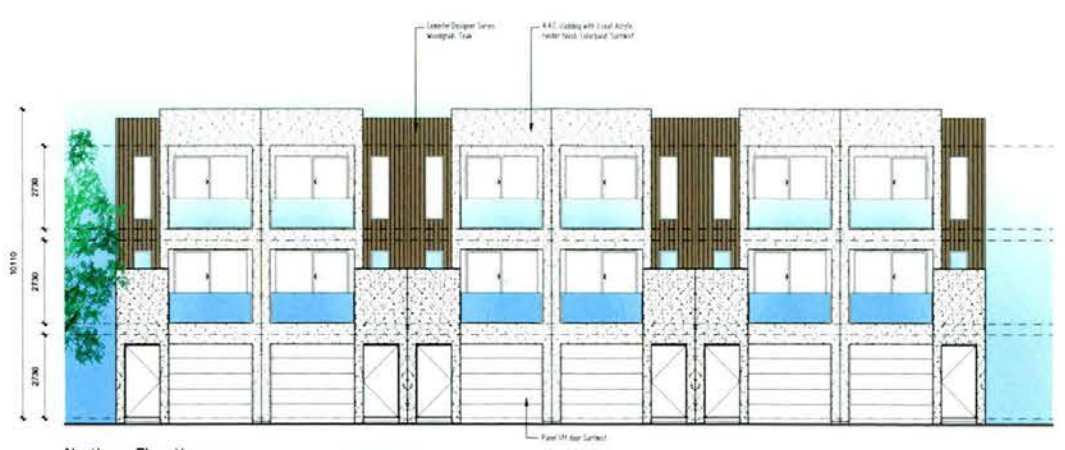
Deck/Balustrade Detail SCALE 1:50 @ A1



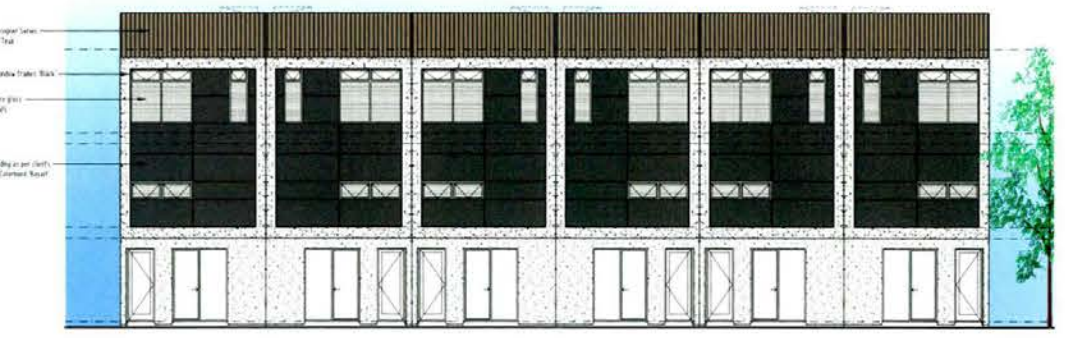
Internal Road Elevation TYPE A SCALE 1:100 @ A1



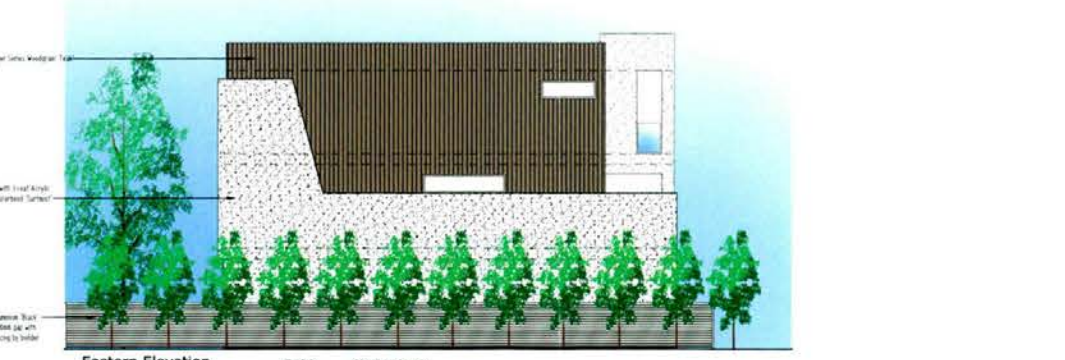
East/West Elevation TYPE A SCALE 1:100 @ A1



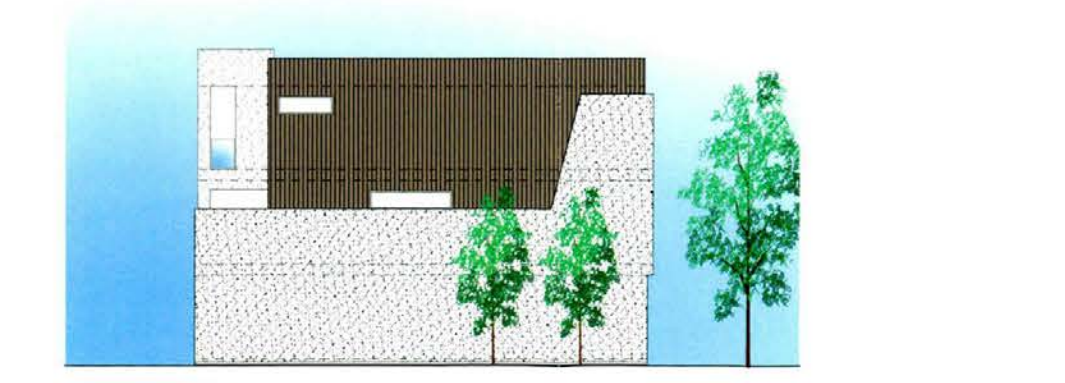
Northern Elevation TYPE B SCALE 1:100 @ A1



Southern Elevation TYPE B SCALE 1:100 @ A1



Eastern Elevation TYPE B SCALE 1:100 @ A1



Western Elevation TYPE B SCALE 1:100 @ A1

The structure shall be constructed in accordance with current edition of National Construction Code. If figured dimensions shall take preference over scaled drawings. Where site conditions do not reflect the intent shown in the drawings or where the measurements conflict with dimensions, levels or notation shown seek clarification before commencement. These plans remain the exclusive property of Virengum Homes and are protected by Copyright laws. Legal action will be taken against any unauthorised whether it be in part or in full, unless written permission is given.





STATEMENT OF EFFECT

Sixteen (16), three-storey group dwellings

At: 101-105 Hardys Road, Underdale
For: Township Developments Pty Ltd



Prepared by
MasterPlan SA Pty Ltd
ABN 30 007 755 277, ISO 9001:2008 Certified
33 Carrington Street, Adelaide SA 5000
Telephone: 8193 5600, masterplan.com.au

March 2017



Contents

1.0	INTRODUCTION	1
2.0	BACKGROUND.....	1
3.0	THE LAND.....	1
4.0	THE LOCALITY.....	3
5.0	THE PROPOSAL.....	8
6.0	THE RELEVANT DEVELOPMENT PLAN.....	12
7.0	DEVELOPMENT PLAN ASSESSMENT	14
8.0	SOCIAL, ECONOMIC AND ENVIRONMENTAL EFFECTS	26
9.0	CONCLUSIONS.....	27



1.0 INTRODUCTION

We have been instructed by Township Development Pty Ltd to prepare a Statement of Effect in relation to their proposal to construct sixteen (16) three-storey group dwellings at 101-105 Hardys Road, Underdale.

This Statement of Effect satisfies the Council's request and Part 4, Regulation 17, Sub-Regulation (5) of the *Development Regulations 2008*. It also describes the land in question, its surroundings and the proposal, before going onto assess the proposal against the relevant provisions of the West Torrens Council Development Plan.

2.0 BACKGROUND

Our client has previously sought and subsequently obtained Development Plan Consent from the City of West Torrens ('the Council') to construct two pairs of two storey semi-detached dwellings and six, two storey detached dwellings on the land.

This consent has not been pursued by our client, primarily on the basis of market feedback that the form and configuration of the dwellings was not in line with the market's expectations for the site.

In addition, our client has voluntarily sold an area of land adjacent the linear reserve to Council to enable the construction of a continuous shared path along the River Torrens Linear Park, adjacent to the land. This path has now been constructed, enabling a high quality connection to be provided for both pedestrians and cyclists.

In November 2016, the Council's Development Assessment Panel considered and ultimately refused an application by our client to construct 20 dwellings on the subject land, despite a recommendation for approval by Council staff. The panel was primarily concerned with the land use interface, the lack of private open space, the dwelling density and the provision of car parking.

Our client has considered these reasons and now seeks consent for an amended proposal, which is contained within this Development Application.

3.0 THE LAND

The land is located at the northern end of Hardys Road and on the southern side of the River Torrens Linear Park.

It consists of one allotment, which is more formally described as Allotment 25 in Deposited Plan 110090, Hundred of Adelaide and is the whole of the land described in Certificate of Title Volume 6161 Folio 341.

The land is irregular in shape. It has a frontage of 49.87 metres to Hardys Road and an area of some 2,722 square metres. It also adjoins a 309 square metre reserve on its northern side (being the land previously sold by our client to Council for integration into the River Torrens Linear Park).



The land, which was previously developed with an industrial warehouse structure which covered the majority of the allotment, has already been cleared and levelled, and the adjoining reserve to the north has been battered down to the level of the now constructed bitumen shared path on the southern side of the River Torrens Linear Park.

The following photos shows the current disposition of the subject land:



Photo 1: Looking south from northern edge of land.



Photo 2: Looking east from south western corner of subject land.



The land remains accessible via an existing crossover to Hardys Road.

The Certificate of Title Register Search attached at **Appendix A** indicates that there are no registered easements or encumbrances which have the ability to impede or avert the proposal altogether.

There are no regulated or significant trees, or other material vegetation, within the confines of the land. There is, however, a large eucalypt located at the end of Hardys Road, to the north east of the land. An assessment of the impact of the previous proposal on this tree was undertaken in 2010. The relationship of the dwellings now proposed is similar to those proposed in the previous development application.

A copy of the arboricultural assessment previously undertaken by Tree Environs is attached at **Appendix B**.

According to Overlay Map WeTo/4 – Development Constraints of the West Torrens Council Development Plan, a portion of the northern and western extents of the land fall within the ambit of an area which is described as being subject to a ‘flood hazard’.

4.0 THE LOCALITY

Whilst inspecting the land and its surroundings, we noted, amongst other things, that:

- the immediate locality and those adjacent allotments fronting Hardys Road are used primarily for light industrial land uses including small scale manufacturing, service trade premises and warehousing with ancillary office/showrooms;
- the non-residential uses in the locality appear to operate at low levels of intensity;
- residential development closely abuts the western portion of the southern boundary of the land, however this is accessed from Chatswood Grove rather than Hardys Road;
- a green belt of vegetation exists on either side of the River Torrens, forming a soft edge to the industrial and residential development in this location, with the extent of this open space and its relationship to adjacent development varying from site-to-site.;
- Hardys Road is a dead end road which terminates as it meets the River Torrens to the north-east of the land with the majority of traffic travelling along Hardys Road diverting to the east along Ashwin Parade, south of the subject site;
- the locality is characterised by the juxtaposition of non-residential and residential land uses; and
- the land is located adjacent to land in another zone, the Open Space Zone to the north and west, and the Residential Zone to the south.



The following photographs depict the character of the locality:



Photo 3 – Looking north along Hardys Road, corner of Ashwin Parade in foreground.

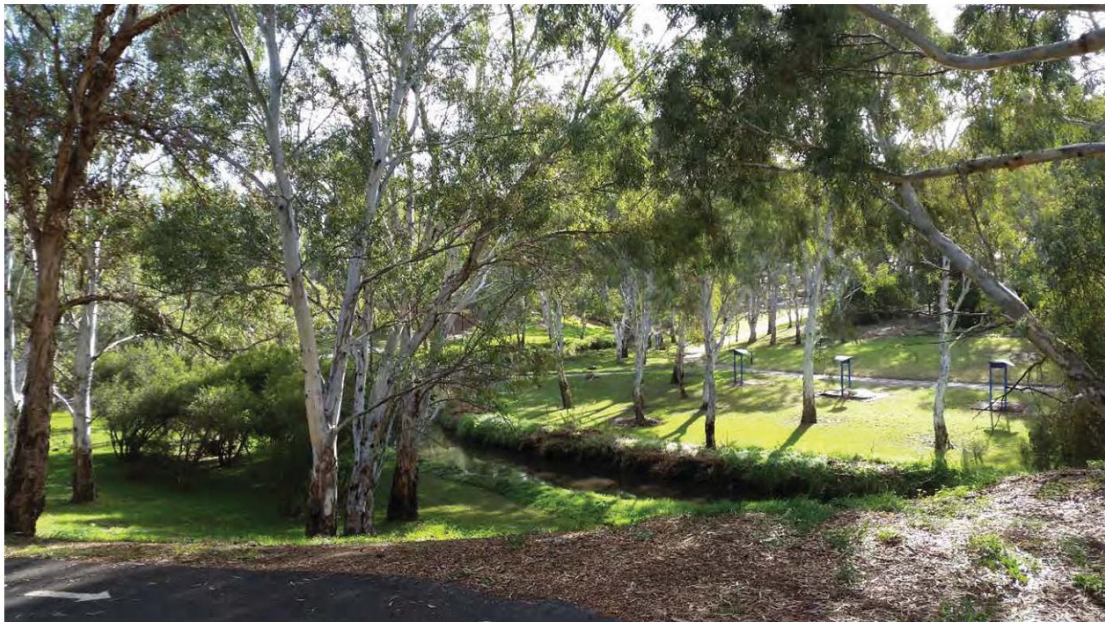


Photo 4 – River Torrens Linear Park, looking north west from western edge of subject land.



Photo 5 – River Torrens Linear Park, looking north east from northern edge of subject land.



Photo 6 – Looking south along Hardys Road, subject land right foreground.



Photo 7 – Industrial development on eastern side of Hardys Road, opposite subject site.



Photo 8 – Industrial development on eastern side of Hardys Road, opposite and immediately south of subject site.



Photo 9 – Relationship of industrial and residential development in locality.



Photo 10 – Relationship of industrial and residential development in locality.

The character of the locality is clearly defined by its transitional nature where residential and non-residential uses are located immediately adjacent each other with an apparent level of relative harmony. Typically, residential development in the locality is comprised of single storey detached dwellings on moderately sized allotments.



The majority of dwellings appear to date from the late 1950s through to the early 1980s. Non-residential development is a mix of uses but is typified by small (relative to industrial uses) allotments housing warehousing and light manufacturing, typically presenting offices and car parking areas to street frontages. Older buildings are typically steel construction with newer developments more typically being constructed of pre-cast concrete panels.

The character of the locality is substantially influenced by the presence of the River Torrens Linear Park. It is an attractively landscaped linear open space which is a visual focus for the overwhelming number of dwellings in the locality, including the exclusively residential development on the northern side of the River.

The level of amenity in the locality is generally moderate, with the visual appearance and operation of non-residential uses serving to reduce the level of amenity, but not substantially so. The impact of the non-residential uses and of traffic, particularly on Ashwin Parade and Hardys Road, to the south of Ashwin Parade, is significantly offset by the positive visual and environmental impact of the presence of the River Torrens Linear Park.

On the occasions the locality has been inspected, the amenity impacts of the non-residential development have been limited, with no noise, odour or perceptible air quality impacts evident to an extent that would indicate significant and substantial potential for land use conflict.

The locality, including the subject land, in relation to its surroundings, is shown on the Locality Plan at **Appendix C**.

5.0 THE PROPOSAL

Our client seeks Development Plan Consent from the City of West Torrens ('the Council') to construct sixteen (16), three-storey group dwellings on the land.

The particulars of the proposal are detailed in the following sub-sections of this Statement of Effect.

They are also shown across the proposal plans, elevations and diagrams at **Appendix D**.

5.1 Land Use

Each building is properly defined in land use terms as a 'group dwelling'.

According to Schedule 1 of the *Development Regulations 2008*, a 'group dwelling' means:

"One of a group of two or more detached buildings, each of which is used as a dwelling and one or more of which has a site without a frontage to a public road or to a road proposed in a plan of land division that is the subject of a current development authorisation."



The nature of the proposed construction is that each dwelling, whilst butted against one another, will be structurally separate.

5.2 Dwelling Density

The gross density of the proposed development equates to 58.8 dwellings per hectare, which has reduced significantly from the gross density of the previous scheme (73.47 dwellings per hectare). Similarly, the average site area per dwelling has increased from 136.1 square metres to 170.1 square metres.

5.3 Dwelling Mix

Dwellings 1 to 10 will contain three rooms capable of being used as bedrooms. The prospective occupants of these dwellings will therefore have the choice of using all three rooms as bedrooms or two rooms as bedrooms and one room as a study. Dwellings 1 to 10 also include a separate rumpus room and a two (2) vehicle garage.

Dwellings 11 to 16 will also contain three rooms capable of being used as bedrooms. The prospective occupants of these dwellings will therefore have the choice of using all three rooms as bedrooms or two rooms as bedrooms and the other room as a study. Dwellings 11 to 16 do not have a separate rumpus room and only contain single width garages.

5.4 Dwelling Size

Dwellings 1 to 10 will have an overall floor area of 236.22 square metres. Dwellings 11 to 16 will have an overall floor area of 208.35 square metres.

5.5 Site Coverage

The dwellings, if measured from the external faces of their external walls, will occupy 1,168.16 square metres, or 42.9 percent of the land. The overall site coverage has reduced significantly from the previous proposal (50.13 percent of the land).

5.6 Setbacks

Dwelling 1 is proposed to be set back 3.06 metres from the Hardys Road property boundary, while the eastern side wall of Dwelling 16 is proposed to be set back between 1.55 metres and 1.77 metres from the Hardys Road boundary.

Dwellings 1 to 4 are proposed to be set back between 1.09 metres and 2.62 metres from the Linear Park reserve boundary. Dwellings 5 to 10 are proposed to be set back between 0.82 metres and 1.43 metres from the Linear Park reserve boundary.



Dwelling 10 is proposed to be set back 3.67 metres from the southern property boundary, adjacent to the dwelling located at 2 Chatswood Grove. Dwellings 11 to 16 are proposed to be set back 5.49 metres from the southern property boundary, which abuts the car parking area for the adjacent business.

5.7 Building Height

All of the dwellings will contain three floor levels and will have an overall building height of 10.11 metres, when measured from the top of the parapets to the finished ground level below.

5.8 External Materials

The palette of materials selected by our client includes, but is not limited to:

- clay bricks;
- aerated concrete panels;
- matrix cladding;
- weatherboard;
- powder-coated aluminium window frames;
- powder-coated steel panel lift doors; and
- clear glass.

5.9 Private Open Space

Dwellings 1 to 10 include between 26.0 square metres and 31.0 square metres of private open space, with 22.0 square metres in the form of balconies and the balance made up of varying sized ground floor decks.

Dwellings 12 to 15 include 36.6 square metres of private open space, comprised of 26.0 square metres of ground level rear yard and 10.6 metres of first floor balcony.

Dwellings 11 and 16 have slightly wider rear yards and have 51.7 square metres and 40.0 square metres of private open space respectively.

All of the proposed dwellings have improved private open space from the previous proposal, which included private open space areas that ranged from 12.2 square metres to 22.0 square metres.



5.10 Access

The dwellings and the visitor car parking area situated between Dwellings 5 to 10 and Dwellings 11 to 16 will be accessible via a new two-way crossover to Hardys Road.

The reversing bay at the southern end of the visitor car park will enable visitors to enter and exit the land in a forward direction, and in a safe and convenient manner.

5.11 Car Parking

The land, based on the current layout, will be able to accommodate up to 38 vehicles.

Dwellings 1 to 10 will come equipped with double garages. As a consequence, they will be able to accommodate two cars each.

Dwellings 11 to 16 will come equipped with single garages which will be set back from the common driveway 5.5 metres, to enable a second vehicle to park within the driveway. As a result, they will each be able to accommodate two vehicles.

The visitor car parking spaces between Dwellings 5 to 10 and Dwellings 11 to 16 will contain six (6) spaces and a reversing bay.

5.12 Bicycle Parking

Each dwelling will come equipped with a wall-mounted device that allows for one bicycle to be stored in a convenient and secure manner.

5.13 Waste

The prospective occupants of each dwelling will be provided with two bins (one for general waste and one for recyclable materials). The site plan at **Appendix D** clearly shows that there will be enough room within the garages to accommodate both bins.

A communal bin for organic matter will be kept within an unobtrusive enclosure which is centrally located for all dwellings, adjacent to the common driveway.

General waste, recycling and green waste will be collected from the site by a private contractor.

5.14 Landscaping

The common property and each community title allotment will be neatly landscaped with a suitable selection of trees, shrubs and grasses. There will also be enough room on the ground floor decks and upper level balconies for an assortment of potted plants.



6.0 THE RELEVANT DEVELOPMENT PLAN

The relevant version of the West Torrens Council Development Plan for procedural and assessment purposes was gazetted and subsequently consolidated on 5 May 2016.

The land, under this version of the West Torrens Council Development Plan, is situated at the periphery of an Industry Zone. It also abuts an Open Space Zone to the north and Medium Density Policy Area 19 of the adjacent Residential Zone to the south.

Accordingly, we consider the following provisions to be relevant to our assessment of the proposal.

Industry Zone

Objectives: 1.

Principles: 1, 2, 6, 7, 8, 10 and 11.

Residential Zone

Objectives: 1, 2 and 3.

Principles: 1.

Medium Density Policy Area 19

Objectives: 1.

Principles: 1.

Open Space Zone

Objectives: 1.

Principles: 1.

'Design and Appearance' Module

Objectives: 1 and 2.

Principles: 1, 2, 3, 4, 5, 9, 10, 11, 12, 13, 14, 15, 21, 22, 25 and 26.

'Energy Efficiency' Module

Objectives: 1 and 2.

Principles: 1, 2 and 3.

**'Hazards' Module**

Objectives: 2 and 4.

Principles: 1, 2, 4, 5, 6 and 7.

'Industrial Development' Module

Objectives: 2.

Principles: Nil.

'Infrastructure' Module

Objectives: 3.

Principles: 1.

'Interface between Land Uses' Module

Objectives: 1.

Principles: 2, 3, 4 and 5.

'Landscaping, Fences and Walls' Module

Objectives: 1.

Principles: 1, 2, 3 and 4.

'Medium and High Rise Development' Module

Objectives: 1 and 4.

Principles: 1, 5 and 12.

'Metropolitan Open Space System' Module

Objectives: Nil.

Principles: 1.

'Natural Resources' Module

Objectives: 5, 6 and 10.



Principles: 5, 7, 8, 9, 10, 11 and 13.

‘Orderly and Sustainable Development’ Module

Objectives: 3.

Principles: 1 and 7.

‘Residential Development’ Module

Objectives: 1, 2 and 3.

Principles: 4, 5, 6, 7, 8, 9, 10, 11, 12, 18, 19, 20, 21, 23, 27, 28, 30 and 31.

‘Transportation and Access’ Module

Objectives: 2.

Principles: 23, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44 and 45.

‘Waste’ Module

Objectives: 1.

Principles: 1, 5 and 6.

7.0 DEVELOPMENT PLAN ASSESSMENT

In order for the Council to grant Development Plan Consent to this development application, it should be satisfied that:

- the intended use of the land is appropriate and will not prejudice the operations of surrounding businesses located within the Industry Zone;
- the density of this development is acceptable;
- the dwelling sizes are acceptable;
- the siting, height and external appearance of each dwelling is acceptable;
- the amount of private open space offered to the prospective occupants of each dwelling is sufficient;
- the prospective occupants will not be unreasonably impacted by the types and levels of noise expected to be generated on an ongoing basis by the surrounding light industrial premises;



- the level of amenity presently experienced by the occupants of the neighbouring residences to the south will not be affected, to an unreasonable degree, by overlooking or overshadowing;
- sufficient car and bicycle parking will be provided on-site;
- stormwater and waste will be managed in an effective and environmentally sound manner; and
- the dwellings will be suitably energy efficient.

Our assessment of the proposal will therefore focus on these key considerations.

7.1 Land Use

Whilst the Objectives of the Industry Zone state the land is 'primarily' for industrial, warehouse, storage and transport land uses, the land is located at the interface between the Residential Zone, the Open Space Zone and Industry Zone. It needs to be recognised that there will be circumstances where the development of a site for a use which is not envisaged within a zone will nonetheless represent the best policy outcomes for the subject site and locality. In the case of the subject site, the policy of these adjacent zones is relevant to an assessment of the proposal, particularly consideration of the aesthetic impact of the proposal on the River Torrens Linear Park.

In addition to this, we note that a previous consent for residential development of the land has been granted which, if enacted, would culminate in the construction of ten dwellings on the land. The Council has therefore already carefully considered the fundamental land use question and determined that this parcel of land is suitable for residential purposes.

This development would not compromise the achievement of Objective 2 of the Industry Zone, as it does not involve 'manufacturing activities' rather it proposes a residential land use adjacent to an existing 'residential area'.

In considering land use, there are two key issues for consideration.

Firstly, the extent to which the development of the subject site could limit the ability for the site and locality to be developed for uses envisaged in the locality.

The proposal will prevent the use of the subject site for industrial or other envisaged uses. This should, however, be viewed in the context of a previous consent for a residential development and what appears to be a reasonable availability of land for non-residential uses in the general area.

In respect of the potential for the introduction of additional 'sensitive' uses to inhibit the operation of non-residential uses in the locality this needs to be considered in the context of existing residential development in the locality and the generally low intensity nature of non-residential uses.



Whilst the introduction of residential uses in the vicinity of uses which have the potential to generate adverse amenity impacts does create the potential for land use conflict, it does not mean that such an outcome is assured.

The proposal should be viewed in the context of a previous consent for residential development of the subject site. Whilst the proposal seeks consent for six (6) additional dwellings, the extent to which this increases the potential for land use conflict is not necessarily proportional to the number of dwellings. The future occupiers of dwellings will commence their occupation in the understanding that non-residential uses are located in the vicinity of the subject site. Additionally, the industrial uses surrounding the subject site appear to generally operate at relatively low levels of intensity. Like all uses, given their proximity to existing residential development, they are subject to policy instruments such as the *Environment Protection (Noise) Policy 2008* under the *Environment Protection Act 1993* which imposes ongoing obligations on their performance. The performance required under such regulatory instruments is unlikely to materially change as a result of the proposal.

Secondly, consideration should be given to the highly prominent nature of the site and its visibility from the River Torrens Linear Park, and from those residential properties located on the northern side of the River Torrens. This highly prominent nature, which is accentuated by the position of the site on a headland, means that uses in the Industry Zone may well present poorly to the north when contrasted with the appearance of the proposed development.

Having regard to these factors, it is considered that the proposal is an acceptable use of the subject land.

7.2 Dwelling Density

Principle 3 of the 'Residential Development' Module provides guidance with respect to dwelling density.

It recommends that:

- 3 Medium and high density residential development in areas close to activity centres, public and community transport and public open spaces.**

The proposal is for a medium density form of development. Medium density is considered to be appropriate in this location as the land abuts a recreation area and open space in the form of the River Torrens to the north and west, is within walking distance of Ashley Street to the south and Holbrooks Road to the west (both roads are presently used by public transport) and the District Centre to the east, which accommodates the Brickworks Market Place shopping centre development.

We note that the overall dwelling density has been reduced from 73.5 dwellings per hectare to 58.8 dwellings per hectare, from the previous proposal on the land. This results in the average site area per dwelling increasing from 136.1 square metres to 170.1 square metres.



The proposed development represents a reduction of four (4) dwellings from the previous proposal, which was refused by the Panel. The Applicant has considered the previous reasons for refusal and has undertaken a significant and meaningful amendment to reduce the overall dwelling density on the site.

7.3 Dwelling Size

Principle 9 of the 'Residential Development' Module provides guidance with respect to the size of dwellings. It recommends that:

- 9 Residential development should provide a high quality living environment by ensuring the following minimum internal floor areas (including internal storage areas but not including balconies and car parking):**
- (a) **studio (where there is no separate bedroom): 37 square metres**
 - (b) **1 bedroom dwelling/apartment: 50 square metres**
 - (c) **2 bedroom dwelling/apartment: 75 square metres**
 - (d) **3+ bedroom dwelling/apartment: 100 square metres.**

In accordance with Principle 9, Dwellings 1 to 10 have an overall floor area of 214.2 square metres (excluding the deck and the balconies) and Dwellings 11 to 16 will have an overall floor area of 193.7 square metres excluding the balconies).

The proposal is therefore consistent with this provision.

7.4 Setbacks

We are of the opinion that the proposed setbacks are satisfactory on the basis that:

- the previous warehouse building, which has been demolished from the site, abutted the road frontage for its entire length;
- Dwelling 1 and Dwelling 16 will be set-back 3.06 metres, and between 1.55 metres and 1.77 metres respectively from Hardys Road, which will provide for an improved setback from the previous building;
- the adjoining commercial building to the south appears to be set-back between 2.0 metres and 2.5 metres from Hardys Road, the proposed dwellings will complement this setback;
- the setback from the River Torrens will allow for the provision of functional ground level open space in the form of a deck; and
- none of the habitable room windows or private open space areas associated with the neighbouring residences to the south will be overshadowed to an unacceptable degree.



7.5 Building Height

Principle 7 of the Industry Zone provides guidance with respect to the height of buildings. It recommends that:

- 7 No building should exceed 12 metres in overall height.**

In accordance with Principle 7, the proposed dwellings will have an overall building height of 10.11 metres, when measured from the top of the parapets to the finished ground level below.

The proposal is therefore consistent with this provision.

7.6 External Design and Appearance

Principles 1, 2, 5, 14 and 15 of the 'Design and Appearance' Module provide guidance with respect to the external design and appearance of dwellings.

Together, they recommend that:

- 1 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:**
 - (a) building height, mass and proportion
 - (b) external materials, patterns, colours and decorative elements
 - (c) roof form and pitch
 - (d) façade articulation and detailing
 - (e) verandas, eaves, parapets and window screens.
- 2 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties or drivers.**
- 5 Balconies should:**
 - (a) be integrated with the overall form and detail of the building
 - (b) include balustrade detailing that enables line of sight to the street
 - (c) be recessed where wind would otherwise make the space unusable (d) be self-draining and plumbed to minimise runoff.
- 14 Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.**
- 15 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.**

The dwellings have been cleverly designed so that all four of their facades are expressive but not overly dominant when viewed from any angle.

The breaking up of the built form into three areas, the provision of integrated balconies, the use of contemporary and robust materials, and the application of complementary colour tones will greatly assist in this regard.



None of the facades will feature large, monotonous or highly reflective surfaces. Instead, they will be broken down into discrete elements to create an appropriate sense of visual relief.

The use of small vertical and horizontal components will also serve to break up the width of the dwelling whilst reducing their apparent height.

The orientation, design and vertical profile of the dwellings will also provide opportunities for passive surveillance over the common driveway, Hardys Road and the River Torrens.

Although the entrance to Dwellings 1 to 10 is situated behind the panel lift doors, this is no different to say a solid masonry fence on a front property boundary which requires an intercom and electronic access. Furthermore, these dwellings do not face Hardys Road and therefore do not have a direct impact on this non-residential streetscape.

7.7 Private Open Space

Given that Dwellings 1 to 10 will essentially rely upon decks and balconies for private open space, we feel it is appropriate to assess this proposal against Principle 22 of the 'Residential Development' Module.

This Principle recommends that:

- 22 Dwellings located above ground level should provide private open space in accordance with the following table:**

Dwelling Type	Minimum Area
Two-bedroom dwelling	11 square metres
Three plus bedroom dwelling	15 square metres

In accordance with Principle 22, Dwellings 1 to 10, which have been designed with three bedrooms, will come equipped with 22.0 square metres of private open space (the decks have not been included in this calculation as they are not sufficiently screened from the public realm).

Dwellings 11 to 16, which incorporate ground level private open space, are more appropriately assessed against Principle 19 of the 'Residential Development' Module.

Principle 19 recommends that:

- 19 ground level should provide private open space in accordance with the following table:**

Dwellings



Site area per dwelling (square metres)	Minimum area excluding any area at ground level at the front of the dwelling (square metres)	Minimum dimension (metres)	Minimum area provided at the rear or side of the dwelling, directly accessible from a habitable room (square metres)
<300	24, of which 8 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres	3 (excluding balconies)	16

Dwellings 11 to 16 have site areas that range between 109.3 square metres and 125.8 square metres, and come equipped with private open space areas that range between 36.7 square metres and 51.7 square metres, consistent with Principle 19.

These private open spaces have been carefully designed to maximise functionality in respect of their location, dimension, relationship to the internal living areas and access to sunlight. They will all be suitably landscaped, shielded from Hardys Road, and supplemented by the adjoining linear park to the north which is accessible to the public via the site itself.

The provision of private open space associated with this proposal represents a significant increase from that of the previous proposal, which was refused by the Panel (the previous scheme included private open space areas that ranged from 12.2 square metres to 22.0 square metres).

7.8 Noise

Principles 4 and 5 of the 'Interface between Land Uses' sub-section provide guidance with respect to noise and amenity more generally.

Together, they recommend that:

- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.**
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.**

In order to lessen the impacts associated with the adjacent light industrial premises to the east and to the south, such as noise and light spill, the main living areas and bedrooms associated with Dwellings 11 to 16 have been located on the first and second floor levels, and the bedrooms on the ground floor level of Dwellings 1 to 10 have been located behind the double garages.



In addition to this, each dwelling will be insulated and glazed in a manner which satisfies the relevant sections of the National Construction Code. Such treatments may include additional glazing thickness and/or additional wall and or ceiling insulation.

The proposed development has been reviewed by Sonus Acoustic Engineers. A summary of the Sonus Acoustic Engineers review is set out below:

- the nearby industrial noise sources have been assessed against the relevant requirements of the *Environment Protection (Noise) Policy 2007* and Australian/New Zealand Standard AS/NZS 2107;
- based on the assessment, the proposed development will achieve relevant environmental noise criteria with the inclusion of acoustic treatment, as proposed; and
- achieving the acoustic criteria will ensure an adequate level of amenity for the residents, while protecting the continuation of the lawfully existing industrial land uses.

A copy of the Sonus Acoustic Engineers report is attached at **Appendix E**.

We believe that the steps taken by our client to ameliorate the impacts associated with this interface are sufficient, particularly as the adjacent premises have an environmental duty and obligation to comply with the relevant criteria set out under the *Environment Protection (Noise) Policy 2007*.

7.9 Overlooking

Principle 27 of the 'Residential Development' Module provides guidance with respect to overlooking.

It recommends that:

- 27 Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.7 metres or permanent screens having a height of 1.7 metres above finished floor level.**

It is clear from this Principle that none of the upper level windows need to be raised or obscured to a height of 1.7 metres above the relevant finished floor level, as all 20 of the dwellings will be three storeys tall.

Notwithstanding, the second floor windows on the southern elevation of Dwellings 11 to 16, will incorporate fixed obscure glazing to a height of 1.7 metres above the floor level, to ameliorate any potential for unreasonable overlooking of the dwellings to the south of the subject land.

7.10 Overshadowing

Principles 11 and 12 of the 'Residential Development' Module provide guidance with respect to overshadowing.



Together, they recommend that:

- 11 Development should ensure that north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 5.00 pm on the 21 June.**
- 12 Development should ensure that ground-level open space of existing buildings receives direct sunlight for a minimum of two hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:**
 - (a) half of the existing ground-level open space**
 - (b) 35 square metres of the existing ground-level open space (with at least one of the area's dimensions measuring 2.5 metres).**

The extent of shadow that is likely to be cast by this development between the hours of 9:00 am and 3:00 pm on the winter solstice is captured across the sun shadow diagrams at **Appendix D**.

It is clear from these diagrams that:

- the north facing-windows belonging to the neighbouring residence to the south of the land will continue to receive direct sunlight from around 10.00 am onwards during the winter solstice; and
- more than half of the private open space belonging to the neighbouring residence to the south of the land will continue to receive direct sunlight for at least two (2) hours (between 10.00am and 12.00pm) during the winter solstice.

Whilst the proposal satisfies both of these Principles, it is important to keep in mind that these diagrams depict the worst-case scenario, as the winter solstice marks the shortest day of the year and is when the sun is at its lowest point in the sky.

A swimming pool occupies approximately 50% of the adjacent dwelling's private open space, which is not likely to be used during winter. During summer, the proposed development will not cause any overshadowing of the adjacent dwelling's private open space.

7.11 Access and Car Parking

Principles 23 and 35 of the 'Transportation and Access' Module provide guidance regarding vehicular access.

Together, they recommend that:

- 23 Development should have direct access from an all-weather public road.**
- 35 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.**



GTA Consultants have undertaken a Traffic Impact Assessment (TIA) for the proposed development. Having considered the proposed development, the site and the locality, GTA have concluded that:

- *"The provision of 38 on-site car parks will exceed the requirements of the Development Plan and there would be a theoretical surplus of 2 car parking spaces for visitors.*
- *The car parking and garage layout dimensions are consistent with dimensional requirements set out in AS/NZS 2890.1:2004 for Off Street Car Parking.*
- *The development is located immediately adjacent the Torrens Linear Park Shared Path, with walking/cycling access to/from the city (approx. 5km). There is reasonable access to public transport with bus services on Holbrooks Road and Ashley Street.*
- *The site can internally accommodate an 8.8m Medium Rigid Vehicle (MRV) for refuse collection, subject to three visitor parking spaces being designated as No Parking during refuse collection times.*
- *The site is anticipated to generate an additional 15 trips during the peak hour and 144 daily trips to Hardys Road, which will not adversely impact on the safety and function of Hardys Road or the adjacent road network."*

A full copy of the TIA, prepared by GTA Consultants, is attached as **Appendix F**.

The TIA undertaken by GTA Consultants considered the access to and from the land via Hardys Road, which is an all-weather public road, and also the manoeuvring of vehicles within the land. GTA consider the access arrangements to and from the land to be appropriate, and have advised that the parking spaces, aisle and turnaround bay comply with the relevant standards.

On this basis, the access and manoeuvring arrangements associated with the proposal are considered to be appropriate and accord with the relevant provisions of the West Torrens Council Development Plan.

According to Table WeTo/2 of the Development Plan, the proposal generates a theoretical demand for 36 spaces (32 for the prospective occupants and four (4) for visitors).

The proposed development includes provision for 2 vehicles to park within the site of each dwelling (including at least 1 undercover), plus an additional six (6) visitor spaces, resulting in a total of 38 car parking spaces.

The provision of on-site car parking therefore exceeds the number of required car parking spaces prescribed within Table WeTo/2.



7.12 Bicycle Parking

Clause (b) pertaining to Principle 30 of the 'Residential Development' Module provides guidance with respect to the provision of bicycle parking.

It recommends that:

- 30 Site facilities for group dwellings, multiple dwellings and residential flat buildings should include:**
- (b) bicycle parking for residents and visitors (for developments containing more than 6 dwellings)**

Each dwelling will come equipped with a wall-mounted device that allows for one bicycle to be stored in a convenient and secure manner. These devices will encourage the prospective occupants to utilise the shared paths which run parallel to the River Torrens.

7.13 Stormwater

Principles 5, 8, 9, 10 and 11 of the 'Natural Resources' Module provide guidance with respect to the management of stormwater.

Together, they recommend that:

- 5 Development should be designed to maximise conservation, minimise consumption and encourage re-use of water resources.**
- 8 Water discharged from a development site should:**
- (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state**
- (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.**
- 9 Development should have adequate provision to control any stormwater over-flow runoff from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.**
- 10 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.**
- 11 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.**

Our clients have engaged FMG Engineering to provide a Stormwater Management Plan for the proposed development. FMG provided a detailed Stormwater Management Plan for the previous proposal, which was endorsed by the Council's Engineers.



We are therefore confident that the site is capable of draining stormwater in an appropriate and acceptable manner.

However, given the outcome of the previous Application, our clients would like the assurance of a Development Plan Consent before having FMG undertake the Stormwater Management Plan.

As such, we request that the Council either impose a condition requiring our client to submit an acceptable Stormwater Management Plan to the Council prior to the granting of Development Approval; or to require the submission of a Stormwater Management Plan as a Reserve Matter pursuant to Section 33 (3) of the *Development Act 1993*.

7.14 Waste

Principle 6 of the 'Waste' Module provides guidance with respect to the management of waste.

It recommends that:

- 6 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:**
- (a) screened and separated from adjoining areas
 - (b) located to avoid impacting on adjoining sensitive environments or land uses
 - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
 - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water
 - (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
 - (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

Clause (c) pertaining to Principle 30 of the 'Residential Development' Module provides guidance with respect to the management of waste as well. It recommends that:

- 30 Site facilities for group dwellings, multiple dwellings and residential flat buildings should include:**
- (c) household waste and recyclable material storage areas away from dwellings.

The prospective occupants of each dwelling will be provided with two bins (one for general waste and one for recyclable materials). The site plan at **Appendix D** clearly shows that there will be enough room within the garages to accommodate both bins.

A communal bin for organic matter will be kept within an unobtrusive enclosure central to the dwellings and the common driveway area.

General waste, recycling and green organics will be collected internally to the site by a private contractor using a rigid vehicle.



7.15 Landscaping

Principle 10 of the 'Landscaping, Fences and Walls' Module provides guidance with respect to landscaping. It recommends that:

- 10 A minimum of 10 percent of the site should be landscaped.**

In accordance with Principle 10, the landscaped areas will account for approximately 430 square metres (excluding any landscaping located within the community title lots), which equates to 15.8 percent of the land. The provision of communal landscaping is therefore considered to be acceptable.

7.16 Energy Efficiency

Principle 3 of the 'Energy Efficiency' Module provides guidance with respect to this particular facet of the overall design. It recommends that:

- 3 Development should facilitate the efficient use of photovoltaic cells and solar hot water systems by:**
- (a) taking into account overshadowing from neighbouring buildings**
 - (b) designing roof orientation and pitches to maximise exposure to direct sunlight.**

The roof atop each dwelling has been designed with a 2.0-degree pitch so that it can eventually accommodate an efficient bank of solar panels.

Whilst the solar panels will be concealed from the public realm by the parapet walls, they will still be able to capture plenty of sunlight during either the morning or the afternoon.

8.0 SOCIAL, ECONOMIC AND ENVIRONMENTAL EFFECTS

8.1 Social

The proposal will provide sixteen (16) dwellings in a location which has direct frontage and access to open space. The subject site is close to services and infrastructure, including public transport, and is in close proximity to the Adelaide CBD.

8.2 Economic

The site has been vacant for a considerable period of time. The redevelopment of the site for residential purposes allows for the highest and best use of the site to prevail. In addition, the dwelling typologies selected will allow for the dwellings to be marketed at reasonable sale prices.



8.3 Environmental

The proposed development is considered to represent a better outcome for the subject site from an environmental perspective than redevelopment for non-residential uses which are envisaged in the zone. The proposal will not materially impact existing vegetation, in particular the significant tree adjacent the north-eastern corner of the subject site. The proposal will allow for the appropriate detention and management of stormwater and will allow for the establishment of vegetation in the open areas of the site and at the interface with the River Torrens Linear Park.

9.0 CONCLUSIONS

We have concluded from our detailed assessment of the proposal that it:

- is not seriously at variance with the relevant provisions of the West Torrens Council Development Plan; and
- warrants the granting of Development Plan Consent and the concurrence of the Development Assessment Commission.

In support of our conclusions, we take this opportunity to reiterate that:

- the intended use of the land is entirely appropriate and will not prejudice the operations of surrounding businesses;
- the density of this development is acceptable (and significantly lower (4 dwellings) than that of the previous proposal);
- the dwelling sizes are acceptable;
- the siting, height and external appearance of each dwelling is acceptable;
- the amount of private open space afforded to the prospective occupants of each dwelling is sufficient (and significantly greater (more than double in some instances) than the previous proposal);
- the prospective occupants will not be unreasonably impacted by the types and levels of noise generated by the surrounding light industrial premises;
- the level of amenity presently experienced by the occupants of the neighbouring residences to the south will not be affected, to an unreasonable degree, by overlooking or overshadowing;
- sufficient car and bicycle parking will be provided on-site;
- stormwater and waste will be managed in an effective and environmentally sound manner; and



- the dwellings will be suitably energy efficient.

Whilst the proposed development is not a use which is envisaged in the Industry Zone, the subject site is highly visible from the River Torrens Linear Park, being sited on a prominent headland. The site is immediately adjacent residential development to the south and only interfaces with non-residential uses for a portion of its southern boundary and its eastern boundary (such uses are on the opposite side of Hardys Road). The non-residential uses in the locality typically appear to be of relatively low scale and intensity and will already be constrained by the interface with existing residential properties.

The site is one where the prominence of the site and its visibility from a key open space asset serving the Adelaide metropolitan area provides substantial impetus for its development in a manner that responds to and appropriately interfaces with the River Torrens Linear Park. On this basis, and for the reasons set out in this Statement of Effect, the proposal is considered deserving of Development Plan Consent.

A handwritten signature in black ink, appearing to read 'Michael Richardson'.

Michael Richardson

Bachelor of Urban and Regional Planning (Hons)

7 March 2017

managing trees in the urban landscape

PO Box 51
Smithfield SA 5114

Ph: 08 8254 2066
Fax: 08 8254 2955

info@tree-environs.com
www.tree-environs.com



12 July 2010

Mark Johnson
Township Development Pty Ltd
PO Box 234
Kent Town SA 5071

**Re: Arboricultural assessment of a *Eucalyptus camaldulensis* in
relation to a proposed development at 101-105 Hardys Rd,
Underdale**

Summary

The *Eucalyptus camaldulensis* is a significant tree with several attributes worthy of preservation. It is located on council land 10m to the east of the proposed development. It is in good health and does not have a short life expectancy. The tree has a good structure, but as a mature specimen, has a history of branch failure. Council should regularly assess the tree for maintenance requirements to manage risk to public safety. The tree requires a tree protection zone with a radius of 15m and an area of 707m². The proposed development does not excessively encroach into the tree protection zone and the proposed development in relation to this tree is considered acceptable.



Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Introduction

Brief

I carried out an arboricultural assessment of a significant tree at the end of a cul-de-sac adjacent to 101-105 Hardys Rd, Underdale on the 28th June 2010 following a request from Mark Johnson of Township Development Pty Ltd.

I was requested to assess the possible impacts of the proposed development activities at the site on the significant tree located on council land outside the property and to recommend strategies to minimise these possible impacts.

Qualifications

I have based this report on my site observations and the information provided to me. I have twelve years experience in the field of arboriculture. A summary of my qualifications includes:

- Bachelor of Science (Botany and Ecology) – University of Sydney
- Diploma of Horticulture (Arboriculture)
- Certified Arborist (#AU – 0007A) – International Society of Arboriculture
- Quantified Tree Risk Assessor (QTRA) – License number 770

Documents and information provided

I was provided with the following documents to assist me in the preparation of this report;

- A copy of a site plan *Torrens River/Hardys Rd* produced by/for the City of West Torrens. This plan outlines the contours of the River Torrens, the location of trees and the property boundaries of the subject land.
- A copy of a *proposed site and ground floor plan* and *proposed first floor plan* of the proposed development at 101-105 Hardys Rd, Underdale

Scope of this report

This report is concerned with the impacts of the proposed development on the larger significant River Red Gum at the end of the cul-de-sac on Hardys Rd.

It does not take into account the possible impacts of the proposed development on the smaller significant and non significant trees along the southern embankment of the River Torrens adjacent to the proposed development.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Observations

Site visit

I carried out the assessment at the site on the 28th June 2010 in the presence of Mark Johnson of Townhouse Development Pty Ltd.

I had full access to the tree in question and observations were from what was visible from public land at the end of Hardys St. All my observations were visual from ground level^a. All dimensions marked (~) are estimates.

The weather at the time of my visit was overcast, dull and raining with average visibility.

Site description

This property is located in a commercial precinct of Underdale adjacent to the River Torrens with the surrounding areas:

- Larger commercial and industrial allotments with few trees to the east and south of the site.
- Residential areas to the west and north of the site with moderate tree cover.
- The River Torrens immediately to the north of the site with a high density of tree cover, predominantly locally indigenous tree species with some remnant vegetation.

The site itself consists of a large shed structure which occupies the majority of the site. There are no notable trees on this site.

Identification of tree

The tree in question is a *Eucalyptus camaldulensis* or **River Red Gum** which is the most widespread species of eucalypt in Australia occurring in every mainland State. It is notably a tree of riverine sites whether of permanent or seasonal water, and often extends to adjoining floodplains and into the slopes of the Mt Lofty Ranges. The variety *camaldulensis* is a medium sized woodland tree (up to 45m) with a short, thick bole, smooth bark in irregular plates of varying colour, rough bark at the base, large open crown and often without a lignotuber. It is distinguished by the opercula which are normally strongly beaked. It dominates the Murray-Darling river systems, but also occurs on lower Eyre Peninsula, Kangaroo Island, Yorke Peninsula, the south-east of South Australia and the adjacent Glenelg River system and intervening plains of western Victoria, and streams as far east as Sale in eastern Victoria. It also naturally occurs in the hills of the Mt Lofty Ranges and Plains around Adelaide and the south east of South Australia. Annual rainfall for the Murray Darling form is between 325-750mm. It is indigenous to many parts of Adelaide^b.

It is located at the end of the cul-de-sac on Hardys Rd, immediately adjacent to the southern embankment of the River Torrens.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

The tree is growing in a mulched garden bed at the end of the roadway as follows:

- Open and undisturbed area to the north of the tree where the steep embankment of the River Torrens is.
- 10.0m from the existing building line to the west of the tree.
- ~6m from the bitumen roadway at the end of Hardys Rd to the south.



Tree dimensions

Height	20m
Crown spread	~22m
Trunk circumference 1m above ground level	6.30m
Diameter at breast height (DBH) °	1860mm
Diameter above root flare	1860mm
Approximate age of tree	150+ years

This is likely to be a naturally occurring remnant tree in the local area.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Legal requirements

This tree qualifies as a significant tree under the *Development Act 1993*^d.

The Principle of Development Control 318 of the City of West Torrens *Development Plan* (consolidated 10th June 2010) states that where a significant tree has one or more of the following attributes that development should preserve these attributes. Below is my opinion of the attributes this tree possesses.

(a) Does the tree make an important contribution to the character or amenity ^e of the local area?	Yes	This tree is growing in a natural riparian zone where native and remnant vegetation dominates the local character. The tree also provides a wide range of social benefits in the local area.
(b) Is the tree indigenous to the local area and its species is listed under the National Parks and Wildlife Act as a rare or endangered native species?	No	This is a locally indigenous tree but is not listed as rare or endangered.
(c) Does the tree represent an important habitat ^f for native fauna?	Yes	As a locally indigenous remnant tree, it provides important habitat value for native fauna.
(d) Is the tree part of a wildlife corridor of a remnant area of native vegetation?	Yes	This remnant tree is growing along the River Torrens which is a natural wildlife corridor containing other remnant vegetation.
(e) Is the tree important to the maintenance of biodiversity in the local environment?	Yes	As a locally indigenous remnant tree, it provides important biodiversity value in the local environment.
(f) Does the tree form a notable visual element to the landscape of the local area? ^g	Yes	This large mature tree is clearly visible for several hundred metres along Hardys Rd and from several vantage points on the northern side of the River Torrens.

In my opinion, this tree possesses attributes worthy of preservation.

Tree health

This tree is in good health.

Foliage colour, distribution and density are normal throughout the crown.

There are a moderate number of dead branches in the crown, some of which are large (up to 200mmØ).

There is evidence of borer activity^h and kino exudateⁱ on the lower trunk, which is normal for a mature specimen like this. This activity is not adversely affecting tree health.

There is also evidence of termite activity, as visible in the various cavities and trunk wounds on the lower trunk and various branches throughout the crown. Again, this is typical for an older specimen like this, and is not adversely affecting tree health.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Tree structure

This tree's structure consists of a single trunk with primary branches from ~4m to form a large, broad spreading, moderately dense and irregular crown.

There is a wound on the lower trunk on the northern side that extends from ground level up to ~7m on the north western side. This wound is related to the failure of several large branches on the north western side of the tree.

The lower trunk was sounded with a mallet^j to determine the extent of internal decay. There was some internal decay noted near ground level. There is sufficient reaction wood^k around the margins of the trunk wound to provide sufficient support for the main trunk.

There have been several large branch failures throughout the crown of this tree, including larger diameter branches (up to 300-400mmØ) in the lower and mid crown. These have predominantly been larger and older branches with a more horizontal orientation in the lower crown. There is evidence of internal hollowing in many of these failure points.

There are several smaller secondary branches that partially overhang the proposed development at ~10m to the west of the tree. These branches are younger, smaller branches with a more upright orientation near the trunk and become more horizontal at their extremities. There have been very few branch failures from this section of the tree.

Proposed development activities

The proposed development activities at the site include:

- Demolition of the existing shed structure 10.0m to the west of the tree.
- Construction of ten new three storey townhouses, the closest of which is 10m to the west of the tree, on the existing building footprint.
- Relocation of the over head powerlines to service the new development. It is proposed that these will be relocated underground further away from the tree than the current stobie pole connection. The final location of the underground electricity supply to the site is unknown at this point in time.
- The location of other underground services is unknown at this point in time.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Appraisal

Tree attributes

This tree possesses several attributes worthy of preservation and offers a lot to the local community and environment. The proposed development should aim to preserve these attributes.

Tree health

This tree is in good health and does not have a short life expectancy. If the growing environment is protected from the potentially adverse impacts of the entire development process and well maintained in the future, this tree will remain an asset at the site for many years.

Tree structure

This is a large and mature tree with a relatively good structure. However, as it is an older specimen, it has a history of larger branch failures. This is typical for a large older specimen like this.

The branch failures that have occurred to date have generally been larger branches in the lower and mid crown with a more horizontal orientation. These branch failures have reduced the length and leverage of many of the larger branches in the lower and mid crown. This has assisted in reducing the risk of larger branch failures occurring.

However, due to the age of the tree and the pattern of branch failure and internal decay that is present, the risk of future branch failures remains moderately high.

The location of this tree at the end of the cul-de-sac indicates that vehicular traffic under the tree is limited. There are also few permanent structures under this tree. Therefore, the risk of property damage or vehicle damage is considered to be relatively low. There is a higher level of pedestrian activity in the vicinity of this tree (along the river walkway). Therefore, it is prudent that the council undertake regular assessments of this tree to determine pruning requirements to manage risk at this site.

In relation to the proposed development, only a small proportion of the crown overhangs the tree. This western part of the tree is considered to be sound with a low risk of failure. Therefore pruning to manage risk in this area is of a lower priority.

Pruning may be required to provide suitable clearances for the proposed structure and to improve safety for the construction workers and the future residents. This pruning must not remove tree parts excessively and must not be performed by building contractors. All pruning must conform to the Australian Standard AS 4373 – 2007 *Pruning of Amenity Trees*. All pruning should be carried out or supervised by appropriately qualified and experienced arborists. This pruning may include the removal of larger dead branches, minor crown lifting

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

and reduction pruning to the west over the proposed dwellings and other pruning as required by Council to manage risk and address clearance requirements.

Tree Protection Zone

To protect a tree from the possible adverse impacts of development activities, a tree protection zone¹ (TPZ) is required.

The tree protection zone for this tree is calculated as follows;

- The TPZ radius^m from the centre of the trunk = 15m (maximum under the Australian Standard)
- The TPZ areaⁿ around tree = 707m²
- The Structural Root Zone^o (SRZ) radius from the centre of the trunk = 4.3m

Trees can tolerate some encroachment into their standard TPZ of between 10-20%^p. The proposed development activities at the site encroach into this standard TPZ area as follows;

- The proposed dwelling that is closest to the tree encroaches into the TPZ by ~69m² (9.8%)
- All works are outside the Structural Root Zone.

The total level of encroachment in this situation can be adequately offset by the remaining area around the tree which will remain undeveloped, and the TPZ area will remain intact with a modified shape. Furthermore, the development activities will be occurring on top of the existing building footprint, so the impacts on the root system are likely to be minimal. The development in relation to this tree is considered acceptable.

To protect this tree in relation to the development at the site, consideration must be given to the various activities that are occurring within the vicinity of the tree. Modifications to the proposed design are not required. Protective fencing and other protective measures are required to protect this tree during the entire development process.

In relation to the proposed development at this site, the following is required.

Protective fencing

Protective fencing must be erected around trees to be retained on site prior to any development activities commencing. Ideally the TPZ fence should be set up at the standard TPZ radius of 15m.

This is not practical at this site as the tree is located on Council land and the majority of the tree protection zone is outside the property boundaries. Fencing cannot be erected around this entire tree. However, some fencing should be erected to separate the development site and the tree and all precautions must be taken around this tree as though the TPZ is in place.

Please refer to the TPZ fencing specifications attached.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Demolition and site clearing activities

The demolition of existing structures and surfaces at the site will require heavy machinery to move about. If they work within the TPZ area, they can potentially compact the soil and damage tree roots, trunks and branches.

Demolition activities must adhere to the guidelines attached.

Site preparation activities

The preparation of the site for the new development requires a range of activities such as levelling, grade changing and trenching for footings or underground services.

These activities will require heavy machinery to move about on the site and can potentially cause harm to the tree and its root system.

All site preparation activities must stay outside of the amended TPZ. If these activities are to occur within the TPZ, the works must be carried out with extreme caution under the supervision of the project arborist.

As the level of encroachment into the tree protection zone for this tree is within acceptable levels, the site preparation activities within the proposed building footprint are acceptable, provided they do not extend past the property boundary.

Site preparation activities must adhere to the guidelines attached.

Construction activities

Construction activities are wide and varied. These activities may include but are not limited to; laying of building foundations, building the frame of the structure, brickwork or other walling materials, scaffolding, roofing, interior fitting etc.

These activities require a range of different contractors accessing the site, receiving and storing materials, generating waste and spoil etc. If these activities occur unchecked within a tree protection zone, the cumulative effects of these activities may cause harm to the tree and its root system.

The tree must be well protected with fencing and other suitable ground protection during all phases of the construction process. Areas for parking, storage, waste disposal, mixing and wash out areas must be clearly defined, well away from the tree protection zone.

Construction activities must adhere to the range of guidelines attached.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Underground services

Several underground services are required to service the property.

It is proposed to re-locate the electricity supply to the site underground. The exact location of this is unknown at this point in time, but is likely to occur outside of the tree protection zone. The same applies for all other underground services at the site.

These services should be routed outside of the TPZ area. If underground services must pass through the TPZ, they must utilise underground boring methods or manually excavated trenches where tree roots are left intact.

Please refer to the guidelines for installing underground services within a TPZ attached.

Paving

Paving works often require excavation works, soil compaction and the installation of impervious surfaces. These can all have an adverse impact on the soil and the trees root system which can adversely affect tree health.

Paving activities at the site appear to be largely outside the tree protection zone and are therefore unlikely to impact on the tree.

For further information on paving within the TPZ refer to the guidelines for installing hard surfaces.

Landscaping

There are a range of landscaping activities that may cause harm to the soil and roots of the tree. These include but are not limited to; grade changes up or down, soil compaction from heavy machinery and stockpiling of materials, damage to tree trunks and branches from machinery, soil contamination from improper chemical use, root cutting from trenching activities for underground services and retaining walls, root damage from soil cultivation and planting and from paving activities. This can have an adverse impact on the long term health of a tree.

Where landscaping activities are to occur within the TPZ, they should follow the attached guidelines.

Fencing

Fencing works on the boundaries of the property within the TPZ must not use continuous trenching, as would be the case for a masonry wall, concrete plinth or other similar fence construction. It is recommended that boundary fencing at the site use lightweight panel fencing on a post and rail support system with concrete pads to support the posts.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

If all of these strategies to minimise the impacts on the tree are observed, there will be a minimal impact on the long term health of the tree. These guidelines should form part of the Conditions of approval for the development of the site.

Conclusion

On the basis of my observations and discussion, I summarise my conclusions as follows:

- The *Eucalyptus camaldulensis* is a significant tree with several attributes worthy of preservation.
- It is located on council land 10m to the east of the proposed development.
- It is in good health and does not have a short life expectancy.
- The tree has a good structure, but as a mature specimen, has a history of branch failure. Council should regularly assess the tree for maintenance requirements to manage risk to public safety.
- The tree requires a tree protection zone with a radius of 15m and an area of 707m².
- The proposed development does not excessively encroach into the tree protection zone and the proposed development in relation to this tree is considered acceptable.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Recommendations

1. I recommend that the *Eucalyptus camaldulensis* be retained on site and protected with a Tree Protection Zone (TPZ) as follows:
 - The TPZ radius from the centre of the trunk = 15m
 - The TPZ area around tree = 707m²
 - The Structural Root Zone (SRZ) radius from the centre of the trunk = 4.3m
2. Protective fencing must be set up between the tree and the development site (along the property boundary) prior to development activities commencing. The fence shall be set up according to the site plan and fencing guidelines attached.
3. Demolition activities must proceed with caution in and adjacent to the TPZ for. Demolition activities must adhere to the guidelines attached.
4. Site preparation activities for levelling the site, trenching for footings and other services must remain outside the TPZ. These activities must proceed with caution under the supervision of the project arborist, if permitted to occur within a TPZ. Site preparation activities must adhere to the guidelines attached.
5. Underground services must remain outside a TPZ. If they must pass through a TPZ they must use alternative installation methods such as directional boring or manual excavation. Refer to guidelines attached for installing underground services.
6. Paving activities within a TPZ must utilise a no dig method and incorporate porous materials to allow for the diffusion of gases and moisture between the root system and atmosphere. Refer to paving guidelines attached.
7. Landscaping activities must avoid disturbance to the root system. Refer to landscaping guidelines attached.
8. Boundary fencing must use panels on a post and rail system with no continuous trenching.

As this tree qualifies as a significant tree, an application must be made to your local council to approve this development in relation to this tree. While I believe the recommendations made above are the most appropriate to minimise the impacts on the tree, Council may take an alternative point of view and refuse consent. Development activities cannot occur until appropriate planning approvals have been granted from your local Council. These approvals must be shown to all contractors prior to commencement of works.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Specifications:

Preservation of trees on development sites

Impacts from development activities

Development activities include a wide range of activities, including, but not limited to:

- Demolition of existing structures, surfaces, underground services and vegetation
- Preparation of the site including earthworks, levelling, battering, trenching and retaining walls
- Construction activities
- Installation of underground services
- Landscaping
- Paving
- Fencing

These activities can affect trees in a number of ways including^a:

- Branch damage from incorrect pruning practices
- Branch and trunk damage from mechanical injury from trucks, cranes, excavators etc.
- Root damage (the most common cause of tree damage on development sites) from:
 - The removal of fine feeder roots reduces the trees ability to assimilate moisture and nutrients for the soil, thereby affecting tree health.
 - Severing roots increases the incidence of fungal, pathogen and insect invasion into the tree, thereby affecting tree health.
 - Lowering of grade, excavation, trenching
 - Mechanically wounded, crushed or torn
 - Soil compaction by machinery, storage of materials, installation of work sheds
 - Soil build up
 - Laying of pavements
 - Chemical contamination from solvents, fuels, oil, diesel, herbicide, cement, waste etc.
 - Changes in air levels through changes in drainage patterns
 - Changes in available water
 - Potential loss of tree stability
- Soil compaction leads to a range of possible problems including:
 - Reduced soil aeration
 - Reduced water infiltration
 - Reduction in soil biodiversity
 - Reduced uptake of water and nutrients

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

A range of strategies are required to minimise these potentially harmful impacts. Certain activities are restricted within the TPZ including, but not limited to:

- Machine excavation and trenching
- Cultivation
- Storage
- Preparation of chemicals and cement products
- Parking of vehicles and plant
- Refuelling
- Dumping of waste
- Wash down and cleaning of equipment
- Placement of fill
- Lighting of fires
- Soil level changes
- Temporary or permanent installation of utilities and signs
- Physical damage to the tree

A range of specifications for the variety of development activities that will occur on site is set out on the following pages. These specifications must be followed. Failure to follow these specifications may result in damage to the tree which could constitute a tree damaging activity^r which is a breach of the *Development Act 1993*

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Tree Protection Zone Fencing

To minimise the possible adverse impacts to a tree from development activities, a tree protection zone (TPZ) is required. All potentially adverse activities must not occur within this zone or must be modified to minimise the impacts.

To ensure all relevant people are aware of the need to protect trees on development sites, protective fencing must be erected around all trees to be retained on site and all access points, storage areas, ground protection measures, signage and other tree protection measures put into place.

These protective measures must be put in place prior to any development activities commencing, must remain in place during these activities and can then be removed at completion of the project.

Protective Fencing Specifications:

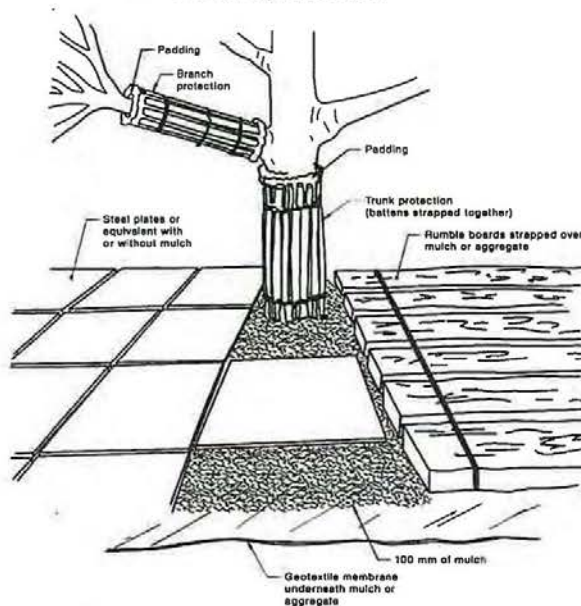
1. Allowances should be made in the project budget and program for tree protection measures and costs. This should include site visits, monitoring and certification by the project arborist.
2. Prior to any works occurring, the project manager and construction supervisors must meet on site with the project arborist to review work procedures, access routes, storage areas, parking and tree protection measures.
3. The TPZs of all significant trees remaining on a site must be marked out and fenced off prior to any works commencing on the site. Some predevelopment work may be undertaken before TPZ fencing is erected after consultation and agreement with the project arborist and local Council.
4. Fences must be installed around trees located on adjacent allotments where the TPZ for those trees crosses the boundary into the allotment being developed. In this case, a fence should be installed at or near the property boundary and at the end of Hardys Rd to separate the Workzone from the tree protection zone.
5. The TPZ fence shall consist of a sturdy 1.8 - 2m high chain mesh, ring-lock, colour bond fence or similar. If mobile panel fences are used, they must be securely fixed in place with steel star pickets so they cannot be moved or the fence easily opened for access. The fencing must be braced to prevent them from being knocked over. The use of orange safety bunting is not acceptable as a TPZ barrier unless it is attached to a more substantial, fully secured fence as described above.
6. Weather-proof signage must be displayed on all sides of the fence visible to the construction work. The sign must show the following information. Access to the area within the TPZ is restricted. It requires Arboricultural and Council approval.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale



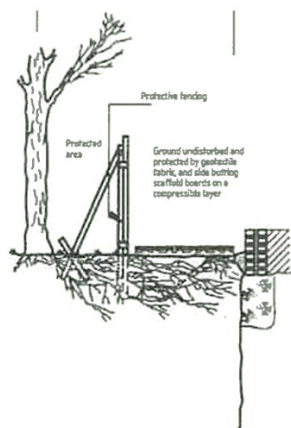
7. The entire area within the TPZ fence shall have a 75mm layer of organic mulch applied. The mulch should have a mixture coarse and fine particle sizes. This must be spread by hand, not machinery. This mulch can be retained on site if not contaminated. Alternatively it can be removed from the site at completion of the project.
8. **Pedestrian Ground Protection.** Sufficient space must be allocated between the TPZ fence and site development to allow for any construction work carried out by pedestrians without shifting the fence or working inside of the protection zone. Where a TPZ fence has been permitted to be reduced to allow pedestrian construction activities, ground protection measures are required between the TPZ fence and building area. This is achieved by placing a layer of geotextile fabric on top of the natural ground. This is then covered with a mulch layer that is 75-100mm thick. Load bearing timber boards (such as scaffold boards) can be placed on top of this mulch to make walking easier if required. This ground protection can then be removed at completion of the project.

Ground protection



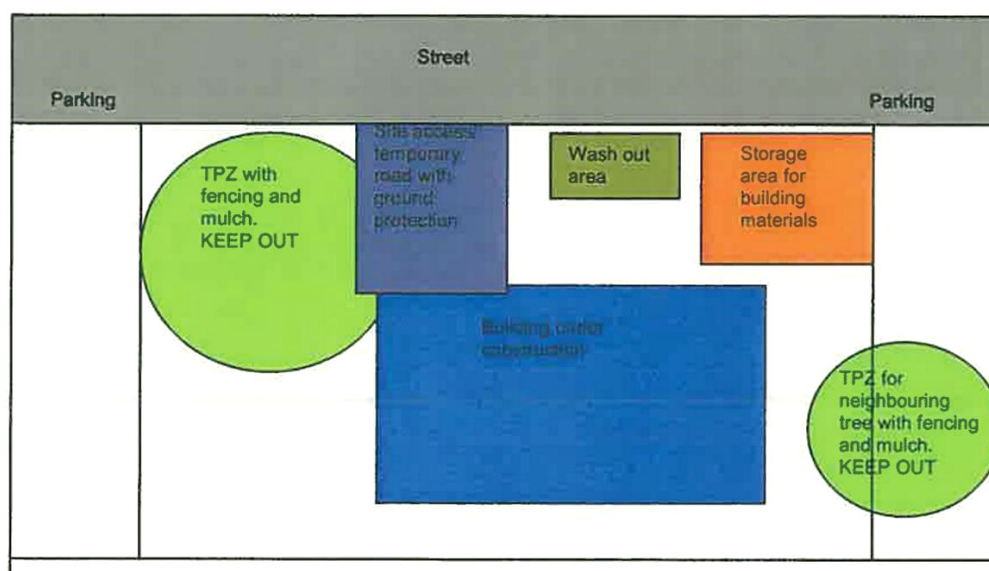
Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

9. A defined storage area for building materials and hazardous chemicals should be marked out well away from the TPZ.
10. A car parking area for contractors should be marked out well away from the TPZ.
11. A defined wash out area should be marked out well away from the TPZ and waste appropriately managed.
12. Any variations to these guidelines must not occur without consulting the project arborist and gaining the appropriate approvals from local Council.



Ground protection and fencing within a Tree Protection Zone

Indicative specification drawings:



Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Suppliers:

Fencing suitable for Tree Protection Zones is available from;

- Australian Temporary Fencing – Ph 131 716
- Geotextile Fabrics and EcoCell – Geofabrics (Adelaide) Edwardstown. 8293 3613 or www.geofabrics.com.au

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Demolition and site clearing activities

Demolition activities generally require large heavy machinery to move about on the site. Damage to tree trunks, branches and the root zone often occurs, even when the machinery operators have the best intentions and take great care.

These activities can cause a range of potential damage to trees as outlined before.

Strategies to minimise impacts

To minimise the impacts of these activities on trees, all trees to be retained on site must have protective fencing, ground protection and other tree protection measures in place prior to demolition activities commencing (refer to fencing guidelines).

Extreme care must be taken when carrying out demolition work and vegetation removal within a TPZ. Failure to protect a tree during development activities may constitute a tree damaging activity under the *Development Act 1993*.

Specifications

1. Allowances should be made in the project budget and program for tree protection measures and costs. This should include site visits, monitoring and certification by the project arborist.
2. The relevant contractors should meet with the project arborist on the site prior to works proceeding to discuss all work procedures, access routes and tree protection measures.
3. Tree Protection Zone fencing and suitable ground protection shall be established at this point in time prior to any further demolition activities occurring. Refer to TPZ fencing guidelines.
4. The demolition of structures within a TPZ shall be carried out by machinery as carefully as possible to avoid damage to surrounding trees. The wheels of the machinery should be placed outside the TPZ to avoid soil compaction. Where possible, machinery should work on existing hard surfaces or be placed inside the building and pulling material in and down, away from the trees. The project arborist should be on site during demolition activities within a TPZ.
5. Existing underground services should not be removed within a TPZ. These should be sealed off and left intact. If they must be removed, please consult with the project arborist.
6. Mini excavators and hand tools (such as Kangas, mini-jackhammers, crowbars, shovels and spades) are to be used to remove hard surfaces within the TPZ.
7. When removing concrete/paved surfaces within a TPZ, it is expected that there will be a proliferation of fine roots on the soil surface. Hand tools should be used to remove these hard surfaces. Exposed roots shall be covered with a 75mm layer of mulch and irrigated by hand as soon as possible after being exposed. Alternatively, exposed roots could be protected from desiccation by placing Hessian cloth on top that is kept moist by periodic wetting. This must remain in place until the new surfaces are put into place.
8. No stockpiling of debris, soil or any other material within the TPZ. Debris to be piled outside the area.
9. No lowering or raising of grade within the TPZ.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Site preparation and earthworks

Earthworks, including excavation, levelling and trenching activities for the preparation of the site generally require large heavy machinery to move around on the site.

Damage to tree trunks, branches and the root zone often occurs, even when the machinery operators and construction staff have the best intentions and take great care.

Strategies to minimise impacts

The most effective time to minimise tree damage on a development site is in the design phase. The location and design of all earthworks, structures, hard surfaces and underground services should be determined with the tree protection zones in mind prior to any works commencing on site.

To minimise the impacts of development process on the trees, all trees to be retained on site must have protective fencing and suitable ground protection measures put into place prior to development activities commencing.

All machinery must be kept outside the TPZ and must not come into contact with the trunk or branches of the tree. Care must be taken when site preparation and construction activities must occur within the TPZ.

Specifications

1. Allowances should be made in the project budget and program for tree protection measures and costs. This should include site visits, monitoring and certification by the project arborist.
2. The building contractor and associated sub contractors should meet with the project arborist on the site prior to works proceeding to discuss all work procedures, access routes and tree protection measures.
3. TPZ fencing and ground protection shall be established prior to any site preparation or construction activities occurring. Refer to TPZ fencing guidelines.
4. Any permitted earthworks activities within and adjacent to the TPZ shall be carried out by machinery as carefully as possible to avoid damage to surrounding trees. The wheels of heavy machinery should be placed outside the TPZ to avoid soil compaction where possible. The project arborist must be on site during these activities within a TPZ.
5. No stockpiling of debris, soil or any other material within the TPZ. These materials are to be stockpiled outside the area or removed from the site immediately.
6. No lowering of grade within a TPZ. The soil surface can be skimmed by removing loose organic matter, turf or old gravel surfaces carefully using hand tools to establish new levels. Skimming of the surface should cease when fine tree roots are encountered and should not exceed 100mm below the original level.
7. If ground levels are to be raised within a TPZ, it should be achieved by using granular material that does not inhibit air and moisture movement (load bearing structural fill). Suitable materials include graded, no-fines gravel, washed aggregate or similar.
8. Continuous trenching activities for footings or underground services should not occur within a TPZ unless the level of encroachment is deemed to be acceptable by the project arborist and

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

approved by Council. Alternative footing designs should be considered such as pier and beam, screw piles, suspended slabs, radial strip footings, or cantilevered sections, all above natural ground level.

9. Where support piers are required to be installed within a TPZ, the smallest diameter hole should be used. Mini piling rigs should be used in these areas to avoid damage to tree branches. Suitable vehicle ground protection is required where piling vehicles enter a TPZ.
10. Underground services should not be installed using a continuous open trench within the TPZ unless the level of encroachment is deemed to be acceptable. Alternative methods for installing underground services should be considered, such as underground directional boring.
11. Any excavations that have to be undertaken within a TPZ should be undertaken carefully by hand, avoiding damage to the protective bark covering larger roots. Roots smaller than 25mmØ may be pruned back (preferably to a side branching root) using sharp pruning tools (such as secateurs or handsaws). Roots larger than 25mmØ should only be pruned after consultation with the project arborist.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Construction activities

Construction activities are wide and varied. These activities may include (but are not limited to); installation of building footings and slabs, plumbing works, building the frame of the structure, installing brickwork or other walling materials, scaffolding, roofing, interior fitting etc.

These activities require a range of different contractors accessing the site, receiving and storing materials, generating waste and spoil etc. If these activities occur unchecked within a tree protection zone, the cumulative effects of these activities may cause harm to the tree and its root system.

The tree must be well protected with fencing and other suitable ground protection during all phases of the construction process. Areas for parking, storage, waste disposal, mixing and wash out areas must be clearly defined, well away from the tree protection zone.

1. Scaffolding for construction activities should not interfere with trees to be retained on site. Minor pruning may be permitted to facilitate the installation of scaffolding. Any pruning must be approved by Council and performed by qualified arborists, not building staff.
2. The tree must be well protected with fencing and other suitable ground protection during all phases of the construction process.
3. Areas for building staff and sub contractors to park vehicles must be clearly defined, well away from the tree protection zone.
4. Areas for the receiving and storage of building materials and supplies must be clearly defined, well away from the tree protection zone.
5. Areas for waste disposal bins must be clearly defined, well away from the tree protection zone.
6. Areas for mixing and wash out areas must be clearly defined, well away from the tree protection zone.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Installation of Underground Services

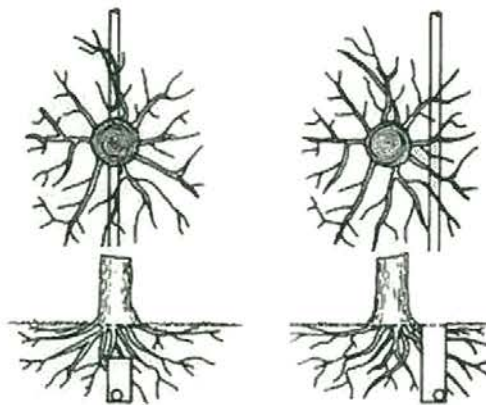
Several underground services are required to service a new development. These may include but are not limited to:

- Gas supply
- Electricity supply
- Water supply
- Sewer drainage
- Septic tank connections
- Stormwater drainage
- Irrigation pipes
- Telephone and fibre optic cables
- Fire mains

The traditional method of installing these services is through an open trench. The digging of an open trench within a tree protection zone is likely to sever tree roots which can have an adverse impact on the trees long term health and stability.

All underground services should be routed outside of all TPZ areas. Where site constraints do not allow underground services to be directed around the TPZ then they can potentially be installed within the TPZ with the assistance of non destructive excavations using an Air Spade™ or by underground directional boring below the main root zone.

The Air Spade™ uses high pressure air to excavate soil away without disturbing tree roots (below left). Excess soil may need to be removed by hand tools and/or a vacuum device.



Less damage is done to tree roots if utilities are tunneled under a tree (left, top and bottom) rather than across the roots (right, top and bottom).

Directional drilling is a method of boring a pathway underground without using an open trench (above right). The pipe/conduit etc. are then attached to the end of the drilling equipment and drawn back through the hole as it retracts. This leaves the majority of tree roots intact.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Air Spade specifications

1. Mark out the intended pathway for the underground services within the TPZ.
2. Mark out any existing underground services. Contact Dial Before You Dig for the relevant site plans. Contact a qualified service locator to determine the exact locations of such existing services. These must be located by hand to confirm their location before proceeding.
3. Excavate the soil using a combination of hand operated tools and an Air Spade™ down to the required depth for the entire length of the service required within the TPZ.
4. Open trenching for an underground service outside of the TPZ is permitted.
5. No tree roots greater than 25mm in diameter shall be cut. Larger roots may be cut in consultation with the project arborist.
6. Any roots that do require cutting must be done so with sharp pruning tools such as secateurs, handsaws or chainsaws.
7. Insert the underground service into the trench by weaving between exposed tree roots.
8. Backfill the trench as soon as possible after the service is installed.
9. If the trench is to remain exposed for more than 2 hours, the exposed roots must be kept moist by hand irrigation. These roots must not be allowed to dry out.
10. If the exposed trench is to remain exposed for more than 12 hours, the exposed roots must be protected from desiccation by covering with Hessian cloth that is kept moist by hand irrigation.

Directional boring specifications

1. Mark out the intended pathway for the underground services within the TPZ.
2. Mark out any existing underground services. Contact Dial Before You Dig (ph 1100) for the relevant site plans. Contact a qualified service locator to determine the exact locations of such existing services. These must be located by hand to confirm their location before proceeding.
3. All entry and exit points for the drilling equipment must be located outside of the TPZ. If site constraints require entry or exit points to be within a TPZ, consultation with the project arborist and Council approval is required before proceeding.
4. All inspection points, sumps, inverts, direction changing elbows etc. must be placed outside the TPZ where possible. If site constraints require these structures to be within a TPZ, consultation with the project arborist and Council approval is required before proceeding.
5. The directional boring is required to be 1m or greater below natural soil level within the TPZ to bypass the majority of the trees roots. If site constraints require shallower depths within a TPZ, consultation with the project arborist and Council approval is required before proceeding.
6. Existing pipes/conduits should remain in place if local guidelines permit. The removal of these structures within the TPZ by open trench retrieval is likely to cause substantial damage to the tree. Directional boring has the capability of 'pipe cracking' which cracks the existing pipe and forces it into the surrounding soil.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Suppliers

- Air Spade excavations by Tree Environs – 8254 2066.
- Underground boring by SA Directional Drilling Services – 8333 1330
- Dial Before You Dig – 1100
- Sure Search Locations – 0418 896 772

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Paving within Tree Protection Zones

Paving, concrete, bitumen and other hard surfaces can adversely affect the health of a tree in several ways.

Grade Changes: Sub base preparation often involves grade changes to obtain a level surface.

- Lowering of the soil level can remove tree roots, including fine non woody feeding roots that may not be clearly visible. This reduces root function which in turn adversely affects tree health.
- Severing of larger tree roots can encourage pathogens to enter a tree which may adversely affect tree health. Pathogens include fungi, bacteria, termites and other insects.
- Severing large roots close to a tree has the potential to reduce tree stability which may result in whole tree failure.
- Increasing the grade can reduce soil oxygen levels in the existing root zone. This can affect root function and may result in root death. This in turn will have an adverse impact on tree health.
- The depth of sub base preparations will vary depending on the intended load. Trafficable areas will require a thicker base while pedestrian areas can be shallower. This must be taken into account when preparing for paved areas.

Soil Compaction: Excessive soil compaction increases soil bulk density, reduces moisture infiltration and reduces gaseous exchange. Tree roots require a soil with adequate pore spaces to allow the free movement of moisture, oxygen and other gasses in and out of the root zone. Therefore excessive soil compaction can affect long term tree health.

Impervious Surfaces: Paving materials can seal the surface above a root system which may reduce moisture and gas exchange between the atmosphere and root zone. This can adversely affect long term tree health.

It is also important to remember that tree roots are likely to contribute to the movement of a non engineered hard surface as the root system enlarges. The design of the paved surface should take this possible root disturbance into consideration and should be appropriately engineered where possible.

Strategies to minimise impacts:

Where it is intended to construct hard surfaces within a tree protection zone, precautions should be taken to maintain the health and condition of the root system. This is achieved by paying attention to the following points;

- Considering the finished paving levels in the design phase of the project. Aim to have the finished paver levels and all sub base preparations installed using the 'no dig' method.
- Preserve the soil structure at a suitable bulk density for root growth and function. Strategies to minimise soil damage must be in place prior to site works commencing.
- Prevent physical damage to the roots during construction of the surface. Wherever possible this is achieved by employing a 'no dig' method by avoiding the lowering of the grade within the TPZ.
- Make provision for water and oxygen to reach the root system. This can be achieved by using porous materials or providing aeration systems.
- Allow for future growth of the root system wherever possible.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

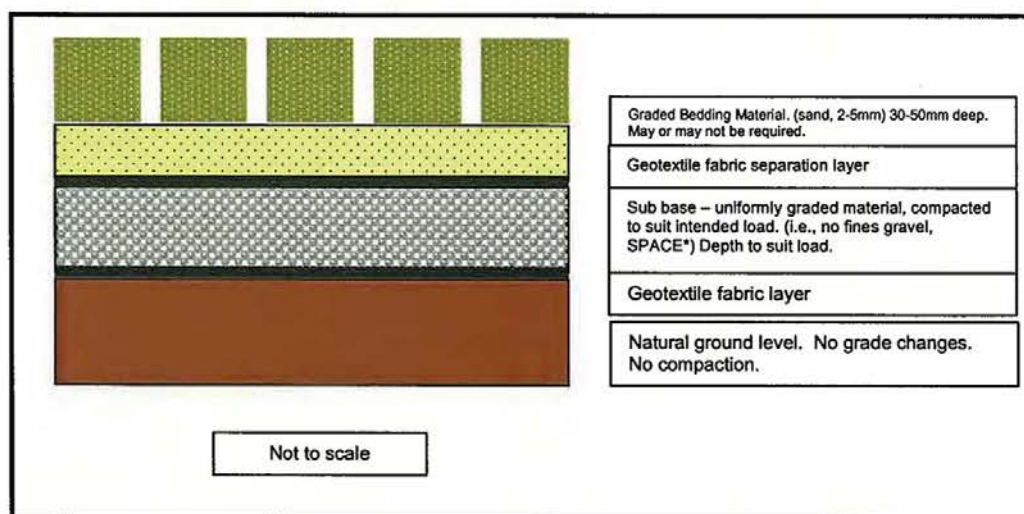
Specifications: Permeable Paving.

1. In some situations, consideration should be given to constructing the final surface prior to the main building works to provide protection for the roots during the construction phase.
2. The soil surface should be carefully skimmed to establish a base for new paved surfaces. Existing surface coverings should be removed carefully to avoid damage to roots just below the surface. Hand assistance/ guidance are essential during this phase of works. Loose organic matter should be removed carefully using hand tools when working inside of the protection zone. The natural soil level should not be lowered any more than 50-80mm. Lowering of grade should cease when tree roots are encountered, including fine diameter non woody roots.
3. Any excavations which must be undertaken within the TPZ should be carried out carefully by hand. Prevent damage to the protective bark on larger roots. Exposed roots must be protected from desiccation by hand irrigation. Roots smaller than 30mm in diameter may be pruned back using sharp pruning tools such as secateurs or handsaws. Roots >50mm diameter should only be pruned in consultation with the project arborist.
4. The natural soil structure within the TPZ should be protected from excessive compaction throughout the construction phase. The natural soils below paved surfaces should not be compacted unless absolutely necessary (e.g. carrying heavy vehicle loads). Compaction of natural soils is not normally necessary for pedestrian or light traffic paving applications.
5. A geotextile fabric layer should be placed on the prepared natural ground level prior to applying the sub-base material. Geotextiles act as a filter between layers and prevent the mixing of sub-base materials with the natural soils. They also assist in preventing tree roots from growing up into the sub base material. A fine grade geo textile fabric is required to limit root growth. Geotextile fabrics also allow the movement of air and moisture to reach the trees root system.
6. Level surfaces should be established above the existing natural ground level (and geotextile fabric layer) using porous sub base materials where required. If ground levels must be raised, this should be by no more than 300mm using porous graded sub base materials.
7. Sub base materials should be uniformly graded to ensure there are adequate pore spaces between the particles to allow for air and moisture movement. There should be no fine particles in the mixture. e.g. no fines gravel, washed aggregate or similar. The sub base material should be selected to suit the anticipated use and load of the paved surface.
8. It may be necessary to install a load suspension layer such as a cellular confinement system (e.g. Ecocell®) in this sub base layer. This contains the sub-base material in a cellular structure to assist in distributing loads evenly and preventing materials moving down a slope.
9. This sub base material should then be compacted to suit the intended load according to the design specifications. Compaction should be to the minimum level required to support the intended load.
10. A geotextile layer should be placed on top of the prepared sub base to prevent the bedding material from moving down into the sub base material and blocking pore spaces. The geo textile will still allow air and moisture movement into the lower soil profile.
11. The bedding layer should use a graded aggregate between 2-5mm to provide good drainage.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

12. Pavers should be of an interlocking type using porous material to allow moisture and gas exchange, such as EcoPave, Permapave or similar.
13. The laid pavers should have a 2-5mm graded aggregate swept in to fill in the gaps between pavers to ensure adequate air and moisture movement.

Indicative specification drawing ONLY: (Final design to be determined by Engineer)



Permapave (Porous paver systems)– Dymon Industries (07) 3284 6841 or www.permapave.com.au

Terrabond - Available from TMH Safety, Klemzig. 8369 1526. For further information go to www.terrabond.com.au

Eco Cell™ cellular confinement systems – Geofabrics (Adelaide) Edwardstown. 8293 3613 or www.geofabrics.com.au

Geotextile Fabrics – Geofabrics (Adelaide) Edwardstown. 8293 3613 or www.geofabrics.com.au

*SPACE. Structurally permeable Aerated Compactable Earth. This material is a by-product of the water filtration industry. The prepared product is available locally through David Lawry on 0418806803. The gravel like material can be compacted to a CBR of 25, yet retains pore space for aeration and water movement. SPACE provides an excellent surface for the laying of unit pavers or other suitable surfaces. It can be used as a growing medium, although the nutrient status and moisture levels of the material require careful management.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Landscaping guidelines around established trees

Established trees may be damaged by a range of landscaping activities that encroach into their tree protection zone. Damage to a significant tree may constitute a tree damaging activity and a breach of the *Development Act 1993*. These activities may include but are not limited to;

- **Soil compaction** from heavy machinery, bob cats and the stockpiling of landscaping materials such as soils, pavers and other materials. Compacted soils impede root growth.
- **Mechanical damage** from machinery and tools working in the vicinity of trees which can damage tree roots, trunk, bark and branches.
- **Soil contamination** from careless use of herbicides, paver cutting etc.
- **Continuous trenching** for footings, retaining walls, fencing, underground services and utilities which may sever an excessive number of tree roots affecting tree health and stability.
- **Grade changes** for levelling garden and lawn areas and paved or concreted surfaces. The removal of roots by reducing grade can adversely affect tree health and tree stability. Adding soil can smother roots, reduce oxygen availability and reduce root function. Grade changes must be avoided.
- **Access drives** in the vicinity of trees can compact and/or contaminate the soil, damage trunks and branches.
- **Damaged branches** result from machinery or people not experienced in tree pruning.

Strategies to minimise impacts

To minimise the possible adverse impacts from these activities during landscaping activities, a tree protection zone (TPZ) is required. The TPZ roughly equates to the drip line of the tree, but is accurately calculated in the body of the tree report. All potentially adverse activities must not occur within this zone or must be modified to minimise the impacts. Below is a range of strategies to minimise the impacts on a tree when landscaping the area within the TPZ.

Landscaping guidelines

1. Ideally, the area beneath a tree should have minimal hard and soft landscaping. The landscape design should aim to provide the trees with ideal growing conditions so the trees remain a valuable asset at the site for many years. The root system requires access to air, moisture organic matter and minimal disturbance.
2. Prior to any works occurring, the landscaping team should meet on site with the project arborist to review the landscape design, work procedures, access routes, storage areas, parking areas and tree protection measures.
3. The removal of existing unwanted vegetation in the vicinity of established trees should be carried out manually with hand tools, not heavy machinery. Removal of larger trees should be carefully dismantled by qualified arborists.
4. The area under each tree should have a 75-100mm layer of coarse structured well composted mulch applied. Mulches should have a mixture of particle sizes such as wood chip and leaf. Fine textured highly processed mulches should be avoided. Mulches generated by tree removals on site could be used. This must be spread by hand, not machinery. Mulches should extend out to the edge of the drip line if possible. If not, consider mulching as large an area as possible. This mulch will add organic matter to the soil, add nutrients, help to improve soil moisture retention, encourage beneficial soil micro-organisms, reduce soil moisture evaporation and help to suppress weeds. Mulch should be topped up every 1-2 years or as required.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

5. Paving works should be kept to a bare minimum within any TPZ. If paving must occur, it must utilize a no dig method, use permeable sub base preparations to minimum soil compaction requirements and utilise permeable unit pavers such as EcoPavers, Permapave or similar.
6. Cultivation of the area under the tree should be kept to a minimum and undertaken with hand tools. Changes in ground level within the root area of established trees should be avoided. Do not build up soil levels by more than 100mm in the vicinity of established trees.
7. Avoid soil compaction within the tree protection zone. Prevent heavy machinery, compactors and vehicles from accessing the area.
8. Take extreme care when using herbicides in the vicinity of established trees. Target the undesirable plants carefully and follow manufacturer's recommendations. Avoid using herbicides where rain or wind is expected.
9. Under-planting of competitive plants should be kept to a minimum. Minimise the use of turf, dense groundcovers etc.
10. The future size of larger plants and trees should be considered carefully. Plants should be well spaced considering their future growth habit and size.
11. The use of locally indigenous plants should be considered for their habitat, biodiversity and wildlife corridor value.
12. A defined storage area for landscaping materials should be marked out well away from any TPZ.
13. A car parking area for contractors should be marked out well away from any TPZ.
14. If an access point passes over the TPZ a temporary protective roadway is required. This is achieved by placing a layer of geotextile fabric on the natural ground, then a 100mm+ thick layer of mulch with load bearing timbers/steel plates placed on top.
15. All tree pruning should be carried out by qualified arborists.
16. Underground services should be installed outside the TPZ. If they must pass through a TPZ, underground directional boring or manual trenching is required. Tree roots larger than 25mmØ should be left intact.
17. Fences and retaining walls must not use continuous strip footings. Pier and beam, post and pad methods or similar must be used to bridge across the root system.

References:

Australian Standards – AS4970-2009 *Protection of Trees on Development Sites*

British Standards – BS 5837:2005 *Trees in relation to construction - Recommendations*

International Society of Arboriculture - Avoiding *Tree Damage During Construction* Fact Sheet available at www.treesaregood.org/treecare

Matheny and Clarke (1998) *Trees and Development, A Technical Guide to the Preservation of Trees During Land Development* (International Society of Arboriculture, Indiana).

Gilman, Edward F. *Trees for Urban & Suburban Landscapes*. University of Florida, Delmar 1997

Costello, L.R. and Jones, K.S. *Reducing Infrastructure Damage by Tree Roots* ISA Western Chapter, California 2003

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

If you have any further queries regarding issues raised in this report please feel free to contact me.

Yours sincerely



Michael Palamouni
B.Sc., Dip. Hort. (Arboriculture)
ISA Certified Arborist (AU007A)
Member ISA, ISAAC, SASA
Tree Environs Pty Ltd.

(m) 0412 174 507
(e) michael@tree-environs.com

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

Endnotes

^a A visual tree assessment (VTA) is an analytical process undertaken by a qualified Arborist or other suitably trained person to determine the structural soundness of a tree. Biological and mechanical components of trees are assessed, including tree health; presence of pests and diseases, die-back, foliage density and distribution, and vitality; growth rate, wound wood development, capacity to respond to improved conditions. Mechanical components include trunk lean, crown bias, bark inclusions, wounds, hollowing, trunk bulges, ribs, cracks, branch form, failure history, pruning history, condition of trunk flare, and other existing defects. All these factors are examined to determine if internal weaknesses may be present. If abnormalities are detected, we may conduct further investigations using a range of tools. These include sounding mallets, long thin drill bits, Resistograph, Sonic Tomograph, Air spade and other tools as required. Ref: Mattheck, Claus & Breloer, Helga. *The Body Language of Trees. A Handbook for Failure Analysis*. Department of the Environment. London 1997.

^b Boland, D., Brooker, M., Chippendale, G., Hall, N., Hyland, B., Johnston, R., Kleinig, D., McDonald, M. and Turner, J *Forest Trees of Australia - Fifth Edition* CSIRO Publishing 2006 and Centre for Plant Biodiversity Research (2006)- Contributors: AV Slee, MIH Brooker, SM Duffy, JG West *EUCLID – Eucalypts of Australia* Interactive CD ROM. Nicolle, D (1997). *Eucalypts of South Australia*.

^c Diameter at Breast Height (DBH) is the diameter of the trunk measured at breast height. This measurement is taken at 1.40m above ground level. This is the nominal point measured to determine Tree Protection Zones using the Australia Standard method AS 4970-2009 *Protection of trees on development sites*. When calculating a DBH for a tree with multiple trunks, the combined DBH do not accurately represent the root volume or area and the TPZ becomes exaggerated. Combining DBH in the following formula results in a revised total DBH that better represents the total stem cross sectional area as if it were 1 stem. From this a more proportional TPZ can then be calculated.

$$\text{Combined DBH} = \sqrt{A^2 + B^2 + C^2 \text{ etc.}}$$

(A, B and C etc. are the DBH of each individual stem)

^d The Development Act 1993 and Development Regulations 2008 defines significant trees as "trees within the designated area that have a trunk with a circumference of 2 metres or more or, in the case of trees with multiple trunks, that have trunks with a total circumference of 2 metres or more and an average circumference of 625 millimetres or more, measured at a point 1 metre above natural ground level."

This legislation controls certain activities on or near significant trees by declaring it to be 'Development.' This may include such activities as tree removal, tree pruning, development near significant trees or any other 'tree-damaging activity' as defined in the Act. "tree-damaging activity means— (a) the killing or destruction of a tree; or (b) the removal of a tree; or (c) the severing of branches, limbs, stems or trunk of a tree; or (d) the ringbarking, topping or lopping of a tree; or (e) any other substantial damage to a tree, and includes any other act or activity that causes any of the foregoing to occur but does not include maintenance pruning that is not likely to affect adversely the general health and appearance of a tree;"

Any of these activities requires a development application to be submitted to your local Council. Council approval is required prior to any of these activities occurring. Breaches of this Act are subject to fines of up to \$120,000.

^e Trees and shrubs are proven to provide a range of social, environmental, economic and psychological benefits that improve the pleasantness of a local area that positively affect human wellbeing. The amenity value of trees include gaseous and particulate pollution mitigation, amelioration of climatic extremes (shading, cooling and wind speed reduction), mitigation of heat islands, attenuation of noise pollution, store and sequester carbon (reducing greenhouse gasses), improve air quality, improve water quality, stormwater mitigation and erosion control, visual screening of undesirable views, aesthetically enhance local areas, aesthetically enhance urban structures, improve property values, reduce urban glare, improve human health, wellbeing and relaxation, reduce stress and anxiety, reduce crime and improve healing rates of patients. In addition, locally indigenous plants provide further benefits including; provide important habitat for local fauna, maintain biodiversity in the local environment, provide wildlife corridor links with areas of native and indigenous vegetation.

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

^f Important habitat and biodiversity value is considered to be present when the tree is indigenous to the local area and provides an opportunity for native animals to perch, nest, breed, feed and shelter in the tree. Animals that may use the tree include native birds, mammals, insects and other invertebrates, lizards and other reptiles. Australian native trees will also provide some of these benefits, but are not considered to be as important as locally indigenous trees. Exotic trees can also provide some of these benefits, but are considered to provide limited habitat and biodiversity value.

^g This opinion may need to be verified by a qualified landscape architect.

^h Eucalypt Longicorn Beetle - *Phoracantha* spp., commonly known as Borers, are host specific beetles that feed on living or dead gum trees. The adult beetles have very long segmented antennae that are directed backwards over their elongated and cylindrical body. The larvae are white tapering grubs with large dark brown head with strong mandibles. The larvae bore mainly under the bark of the lower trunk feeding on the cambium layer. In severe attacks the trunk may be ring barked causing the death of the tree. They pupate inside the wood. When they hatch, the adults cut emergence oval shaped holes through the bark. Naturally, they may occur in small numbers in healthy trees. They can increase in numbers in trees that are stressed or unhealthy for a wide variety of reasons (incompatible with climate, poor pruning history, root disturbance, poisoning etc.).

ⁱ Kino is a reddish brown gummy substance, formed by *Eucalyptus* species in response to injury, such as insect damage, mechanical wounding, bark expansion cracks, fungi, fire, etc. It is formed in veins or pockets in the wood or bark. Kino plays a role in defence, helping to provide protection against insect attack and pathogenic fungi. (Ref. Tippet, J. 1986. Formation and Fate of Kino Veins in *Eucalyptus*. International Association Wood Anatomy Bulletin #7)

^j Sounding describes the use of impact tones when striking a soft faced mallet on sections of the trunk, branches or exposed structural roots to determine their condition. The mallet impact gives various tones depending upon the density of the wood beneath the target area. This technique is used to assist Arborists when determining the structural condition of trees. 'Sounding' can give experienced Arborists valuable information regarding the internal structural condition of the tree section being considered. The information is used in conjunction with a visual tree assessment or other testing apparatus where necessary to determine the structural status of subject trees or parts of trees.

^k **Reaction wood** is a type of wood that is usually laid down in wider annual increments than ordinary wood, commonly in an asymmetric or elliptical shaped cross-section. In broad leaved trees a type of reaction wood known as **tension wood** develops. It has high tensile strength and resists elongation far more than ordinary wood. Tension wood develops along the tops of branches in response to gravity in most broad leaved species, as well as at the sides of vertical trunks and branches of broad leaved and coniferous trees in response to swaying movement or bending stress. **Compression wood** forms on the underside of branches in Coniferous trees and on the compression side of vertical trunks and branches in any tree in response to tree movement and stresses. Both 'Tension' and 'Compression' wood provide the tree with increased structural strength. The development of these tissues can also indicate structural instability when combined with defects, decay or wounding. Whether or not trees with such tissues are considered to indicate weakness or strength depends upon many factors, including species characteristics, tree form, growing conditions, growth rates and tree health generally.

- Mattheck, Claus & Breloer, Helga. *The Body Language of Trees. A Handbook for Failure Analysis*. Department of the Environment. London 1997.
- Debenham, C. *The Language of Botany Society for Growing Australian Plants*.
- Shigo, Alex. *Modern Arboriculture, a systems approach to the care of trees*. Shigo and Trees, Associates 1991.
- Harris, R. Clark J. R. Matheny N. P. *Arboriculture: Integrated management of landscape trees, shrubs and vines*. ISBN 0-13-044280-1 4th Edition 2004 Prentice Hall, Inc.

^l Tree Protection Zones are areas designated by Arborists for the preservation trees on development sites. TPZs preserve tree root systems and the immediate soil environs as well as protecting the above ground parts of the tree from inadvertent crown or trunk damage. The zone within the TPZ must be monitored and managed by qualified Arborists to avoid the many potentially adverse consequences of uncontrolled development. Management of the crown and improvements to the growing conditions within this zone should occur for several

Tree Environs Pty Ltd - Arboricultural assessment of a *Eucalyptus camaldulensis* in relation to a proposed development at 101-105 Hardys Rd, Underdale

reasons, including compensation for root loss, to reduce plant stress, improve tree form and safety, to improve the growing conditions generally and to extend tree longevity.

^m The Tree Protection Zone (TPZ) radius is calculated by multiplying the trunk diameter at 1.4m by a factor of 12. The radius is measured from the centre of the trunk at ground level. A TPZ should not be less than 2m nor greater than 15m (except where crown protection is required). This method is outlined in the Australian Standard AS 4970 – 2009 *Protection of trees on development sites*.

ⁿ TPZ area = πr^2

^o The Structural Root Zone (SRZ) is the area around the base of a tree required for the tree's stability in the ground. The woody root growth and soil cohesion in this area are necessary to hold a tree upright. The SRZ is nominally circular with the trunk at its centre and is expressed as a radius in metres. This zone considers the tree's structural stability only, not the root zone required for the tree's vigour and long-term viability, which will usually be a much larger area. There are many factors that affect the size of the SRZ (e.g. tree height, crown area, soil type, soil moisture). The SRZ may also be influenced by natural or built structures, such as rocks and footings. An indicative SRZ radius can be determined from the following formula. Root investigations may provide more information on the extent of these roots. From AS 4970-2009 *Protection of Trees on Development Sites*.

$$\text{SRZ radius} = (D \times 50)^{0.42} \times 0.64$$

(D= trunk diameter in metres when measured above the root buttress)

Any work within the SRZ should be avoided. Where no alternative exists, the work must be supervised by a qualified Arborist and approved by Local Council. Tree removal may be required depending upon the size and number of roots affected.

^p It may be possible to encroach into or make variations to the standard TPZ. Encroachment includes excavation, compacted fill and machine trenching. If the encroachment is less than 10% of the TPZ area and is outside the SRZ, detailed root investigations should not be required. The area lost to this encroachment should be compensated for elsewhere and contiguous with the TPZ. If the proposed encroachment is greater than 10% of the TPZ area or inside the SRZ, the project arborist must demonstrate that the tree would remain viable. The area lost to this encroachment should be compensated for elsewhere and contiguous with the TPZ. This may require root investigation by non-destructive methods and consideration of relevant factors including: location and distribution of roots, the potential number and size of root loss, tree species and tolerance to root disturbance, age, vigour and size of tree, lean and stability of the tree, soil characteristics, volume, topography and drainage, the presence of existing or past structures or obstacles and design factors. From Australian Standard AS 4970 – 2009 *Protection of trees on development sites*.

^q The Australian Standard: AS 4970 – 2009, *Protection of trees on development sites*

^r **tree-damaging activity** means—

- (a) the killing or destruction of a tree; or
- (b) the removal of a tree; or
- (c) the severing of branches, limbs, stems or trunk of a tree; or
- (d) the ringbarking, topping or lopping of a tree; or
- (e) any other substantial damage to a tree,

and includes any other act or activity that causes any of the foregoing to occur but does not include maintenance pruning that is not likely to affect adversely the general health and appearance of a tree.

(As defined in section 4 - Interpretation of the *Development Act 1993*.)

GORMACK GRAPH PAPERS

CO22Y 5 mm cross section

TOWNSHIP DEVELOPMENT P/L
101-105 HAZARDS AS
UNDERLAYS

Trees
Eucalyptus Camphylloides

TC = 6.30 m = 516 m² ACANT

D911 = 1860 mm

TP2 radius = 15m

TP2 area = 707 m²

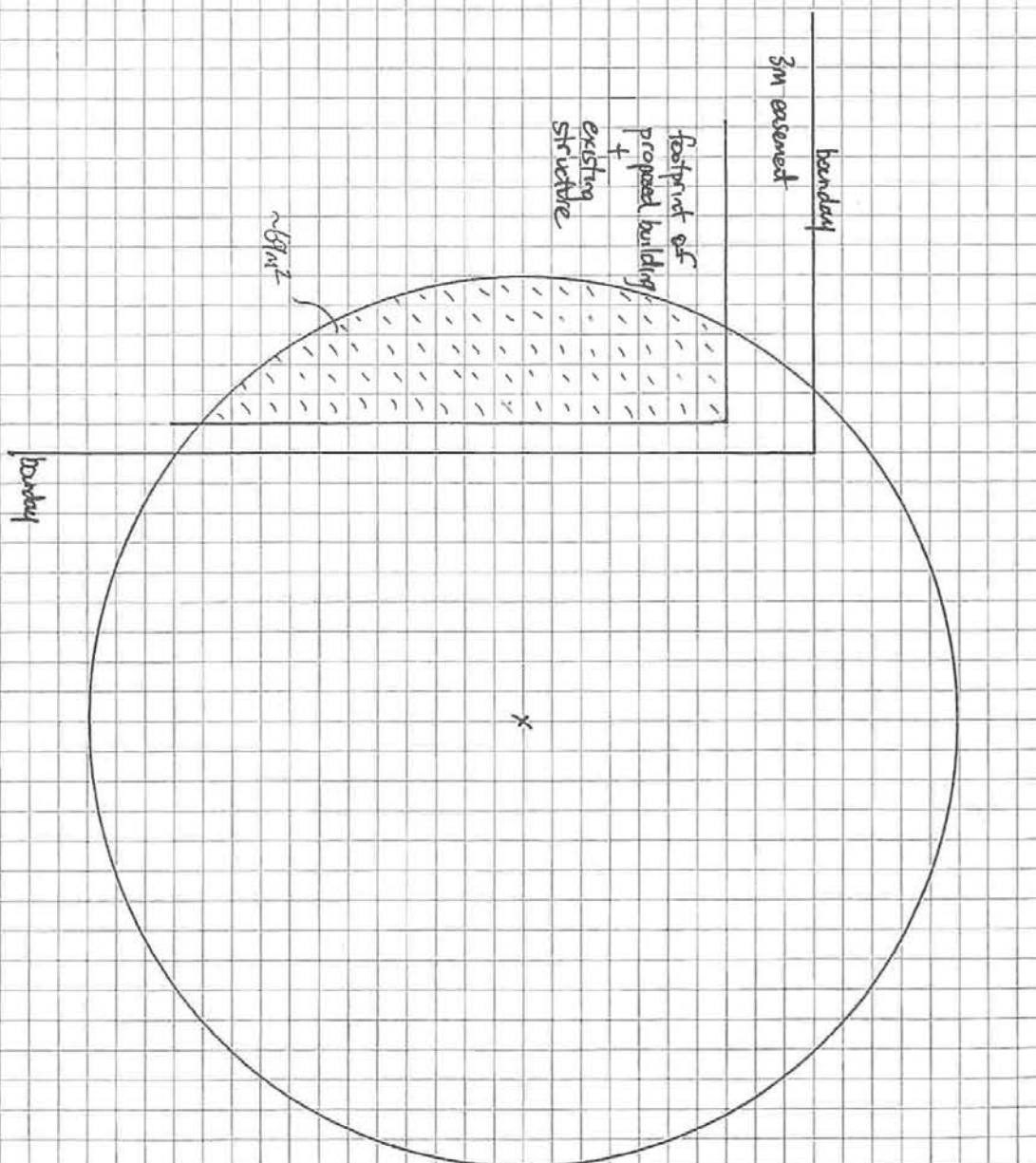
DSC = 2000 mm

SQ2 = 444 m

ENCROACHMENT

Impacts @ 10m

~67 m² = 9.8%



Hardys Road

Residential Development

Environmental Noise Assessment

February 2017

S4968C3

sonus.

Chris Turnbull
Principal
Phone: +61 (0) 417 845 720
Email: ct@sonus.com.au
www.sonus.com.au

Prepared by
Chris Turnbull (MAAS)

Reviewed by
Jason Turner (MAAS)

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

INTRODUCTION

A residential development is proposed on Hardys Road, Underdale. The extent of the subject land is marked in red on Figure 1 below.



Figure 1: Subject Land

The proposed site is bound by:

- residential land to the south west of the site, and;
- industrial land to the south east of the site and on the opposite side of Hardys Road.

This assessment considers the noise impact on the proposed residential development based on the following:

- observations conducted on and around the subject land;
- attended and unattended noise measurements conducted at the site;
- noise data previously measured from a range of industrial land uses similar to those that currently exist around the subject land, and;
- Rivergum Homes drawings of the proposal with project name "Proposed Development Group Dwellings" and amendment date 07 December 2016.

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

DEVELOPMENT PLAN

The subject land is located within an Industry Zone of the City of West Torrens Council Development Plan. The West Torrens Council Development Plan (consolidated 5 May 2016) has been reviewed, and the relevant provisions summarised below:

Interface between Land Uses

PRINCIPLES OF DEVELOPMENT CONTROL

1. Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
...
(b) noise
...
2. Development should be sited and designed to minimise negative impacts on existing and potential future land uses desired in the locality.
4. Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
5. Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

ASSESSMENT CRITERIA

To achieve the requirements of the City of West Torrens Council Development Plan, it should be ensured that the proposed residential development is designed such that it:

- minimises negative impacts on existing and potential future land uses desired in the locality, and;
- protects residents from potential adverse impacts.

Impact on Industry

To provide an objective assessment to ensure that the proposed residential development minimises negative impacts on industry in the vicinity, reference is made to the *Environment Protection (Noise) Policy 2007* (the Policy).

The Policy provides a means to objectively assess the noise from for land uses based on the Development Plan zoning of the land. The Policy provides indoor and outdoor criteria. In the circumstances of a new residence, which is specifically designed to reduce internal noise levels, the indoor criteria of the Policy are the appropriate criteria. Achieving the requirements of the Policy protects existing land uses from any action under the *Environment Protection Act 1993* in the event of a noise complaint.

For an industry zone, the relevant indoor noise level is 45 dB(A) within the habitable rooms of the proposed the residential development.

Under the Policy, the goal noise levels are effectively “adjusted” for each characteristic of tone, impulse, low frequency and modulation that the noise source exhibits. Based on the observations and noise measurements conducted at the subject land, and at other similar sites, the industrial noise sources surrounding the subject land would typically attract an adjustment of 5 dB(A).

Based on the above, the Policy recommends a noise level of up to 40 dB(A) within the habitable rooms of the proposed the residential development.

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

Residential Amenity

To provide an objective assessment to protect residents from potential adverse impacts, reference is made to the Australian/New Zealand Standard AS/NZS2107¹ (the Australian Standard).

The Australian Standard provides recommended design sound levels for different types of building occupancy or activity. For a “houses and apartments near minor roads”, the Standard provides “maximum” recommended design sound levels of 40 dB(A) for “living areas” and “work areas”, and 35 dB(A) for “sleeping areas”.

Project Criteria

Based on the objective requirements of the Policy and the Australian Standard, it is considered that achieving a noise level of 35 dB(A) in bedrooms and 40 dB(A) in all other habitable spaces will satisfy all of the requirements of the City of West Torrens Council Development Plan.

ASSESSMENT

Observations and a combination of attended and unattended noise measurements have been used to assess the existing acoustic environment at the proposed site.

The continuous unattended noise measurements were made at the south eastern corner of the site between 18 and 25 July 2016. The noise logging included the noise from all noise sources in the vicinity of the site which included the industrial land uses, traffic on public roads, birds, aircraft and periods of wind and rain.

To determine the contribution of noise from the industry, audio recorded during the measurement period has been assessed. From the audio recordings and attended measurements, it was determined that the noise from industry in the environment is less than 60 dB(A) and often at a level of 50 dB(A).

The level of 60 dB(A) equates to truck and forklift activity on the closest industrial land uses to the proposed development. This type of activity was observed to be typical for the type of facilities in the vicinity of the site.

¹ Australian/New Zealand Standard AS/NZS2107 “Acoustics – Recommended design sound levels and reverberation times for building interiors”

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

Based on the above, it is considered that designing the proposed residential development (to achieve the internal noise criteria) based on an external noise level of 60 dB(A) at the boundary of the proposed site will not constrain the land uses desired in the locality.

Recommended Acoustic Treatment

The noise within the proposed residences has been predicted using noise data collected from the site and from a range of industrial land uses. Based on the predictions, the following recommendations are made.

Facades marked as red in Appendix B on all levels should incorporate the following acoustic treatment measures:

- Windows and sliding doors in bedrooms glazed with minimum 10.38mm thick laminated glass;
- Windows and sliding doors in habitable rooms (other than bedrooms) glazed with minimum 6.38mm thick laminated glass;
- Seals fitted to all external doors and openable windows such that the door or window is sealed airtight when closed;
- "A.A.C" used for the external cladding to be a minimum of 75mm thick;
- Where "Cemintel" or "Matrix" cladding is used for the external walls of bedrooms, install 2 layers of 13mm thick fire rated plasterboard (or equivalent on the external side of the studwork (between studwork and external cladding). It is noted that 10mm thick plasterboard can be used for the internal lining wall and ceiling lining;
- External walls to incorporate a minimum 75mm thick insulation with a minimum density of 14kg/m³.
- 75mm thick insulation with a minimum density of 30kg/m³ to be installed over the ceilings on the top level.

Facades marked as blue in Appendix A on all levels should incorporate the following acoustic treatment measures:

- Windows and sliding doors in bedrooms glazed with minimum 6.38mm thick laminated glass;
- Seals fitted to all external doors and openable windows such that the door or window is sealed airtight when closed;
- "A.A.C" used for the external cladding to be a minimum of 75mm thick;
- External walls to incorporate a minimum 75mm thick insulation with a minimum density of 14kg/m³.
- 75mm thick insulation with a minimum density of 30kg/m³ to be installed over the ceilings on the top level.

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

CONCLUSION

An assessment has been made of the noise from industry to the proposed residential subdivision in Underdale.

The industrial noise sources have been assessed against the relevant requirements the *Environment Protection (Noise) Policy 2007* and Australian/New Zealand Standard AS/NZS2107.

Based on the assessment, the proposed subdivision will achieve relevant environmental noise criteria with the inclusion of the acoustic treatment described above.

Achieving the acoustic criteria will ensure an adequate level of amenity for the residences while protecting the continuation of the lawfully existing industrial land uses; thereby satisfying the provisions of the City of West Torrens Council Development Plan.

Hardys Road – Residential Development
Environmental Noise Assessment
S4968C3
February 2017

sonus.

APPENDIX A: Extent of Acoustic Treatment Measures



Page 8

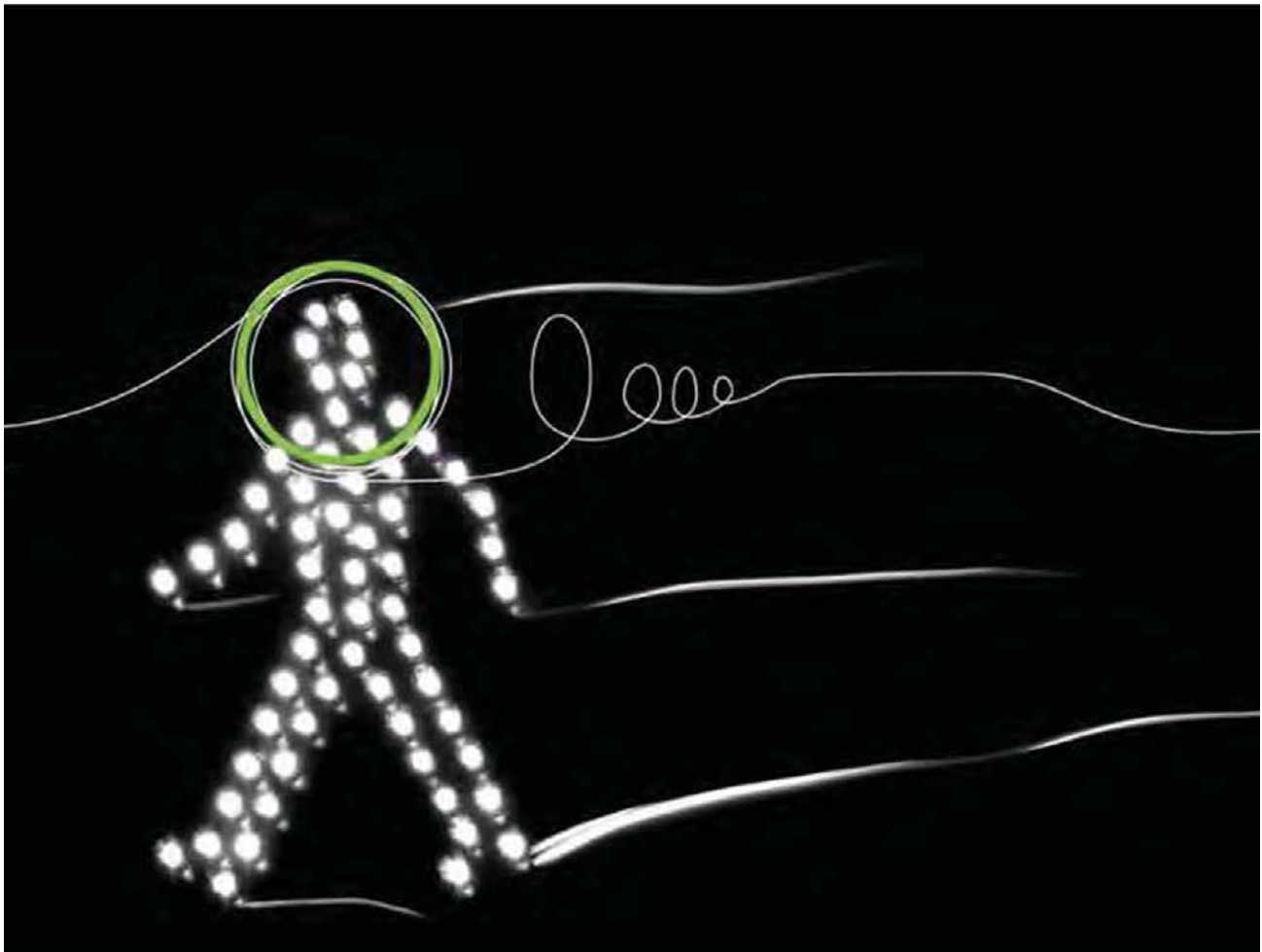


APPENDIX F

Traffic Impact Assessment

14559SOE02





101-105 Hardys Road, Underdale Proposed Residential Subdivision Transport Impact Assessment

Client // Township Development Pty Ltd
c/- Masterplan
Office // SA
Reference // S121670
Date // 17/02/2017

101-105 Hardys Road, Underdale

Proposed Residential Subdivision

Transport Impact Assessment

Issue: A 17/02/2017

Client: Township Development Pty Ltd
 c/- Masterplan
 Reference: 16A1092000
 GTA Consultants Office: SA

Quality Record


Issue	Date	Description	Prepared By	Checked By	Approved By	Signed
A	17/02/17	Final	Richard Frimpong	Paul Froggatt	Paul Froggatt	

Table of Contents

1. Introduction	1
1.1 Background	1
1.2 Purpose of this Report	1
1.3 References	1
2. Existing Conditions	2
2.1 Subject Site	2
2.2 Road Network	2
2.3 Sustainable Transport Infrastructure	3
3. Development Proposal	4
3.1 Land Uses	4
3.2 Car Parking	4
3.3 Vehicle Access	4
3.4 Refuse Collection	4
4. Car Parking	5
4.1 Development Plan Requirements	5
4.2 Adequacy of Parking Supply	5
4.3 Car Parking Layout	5
5. Refuse Collection	6
6. Traffic Impact Assessment	7
6.1 Traffic Generation	7
6.2 Traffic Impact	7
7. Conclusion	8
Figures	
Figure 2.1: Subject Site and its Environs	2
Figure 2.2: Public Transport Map	3
Figure 5.1: Reversing Area Access for Refuse Collection	6
Tables	
Table 2.1: Road Based Public Transport Provision	3
Table 4.1: Development Plan Car Parking Assessment	5
Table 6.1: Traffic Generation Estimates	7

16A1092000 // 17/02/2017

Transport Impact Assessment // Issue: A

101-105 Hardys Road, Underdale, Proposed Residential Subdivision



1. Introduction

1

1.1 Background

A development application is to be submitted for a proposed residential development to be located at 101–105 Hardys Road in Underdale.

GTA Consultants (GTA) has previously completed a Transport Impact Assessment for a similar proposal for the site. GTA has now been engaged to complete a Transport Impact Assessment for the current revised scheme.

1.2 Purpose of this Report

This report sets out an assessment of the anticipated transport implications of the proposed development, including consideration of the following:

- i existing traffic and parking conditions surrounding the site;
- ii parking demand likely to be generated by the proposed development;
- iii suitability of the proposed parking in terms of supply (quantum) and layout;
- iv traffic generation characteristics of the proposed development;
- v proposed access arrangements for the site;
- vi transport impact of the development proposal on the surrounding road network.

1.3 References

In preparing this report, reference has been made to the following:

- o West Torrens Council Development Plan (Consolidated 5 May 2016)
- o Australian Standard/ New Zealand Standard, Parking Facilities, Part 1: Off-Street Car Parking AS/NZS 2890.1:2004
- o Australian Standard / New Zealand Standard, Parking Facilities, Part 6: Off-Street Parking for People with Disabilities AS/NZS 2890.6:2009
- o plans for the proposed development prepared by Rivergum Homes
- o traffic and car parking surveys undertaken by GTA Consultants as referenced in the context of this report
- o various technical data as referenced in this report
- o an inspection of the site and its surrounds
- o other documents as nominated.

2. Existing Conditions

2

2.1 Subject Site

The subject site of approximately 2,750sq.m is located at the end of Hardys Road in Underdale with a 35 metre frontage.

The site is currently located in an Industry Zone of the City of West Torrens. However, the location of the site immediately adjacent to the linear park and the proximity of adjoining existing residential development to the south and west indicate that a residential redevelopment is appropriate and has previously been supported by Council through an approval for eight residential dwellings.

The location of the subject site and the surrounding environs is shown in Figure 2.1.

Figure 2.1: Subject Site and its Environs



(PhotoMap courtesy of NearMap Pty Ltd)

2.2 Road Network

2.2.1 Adjoining Roads

Hardys Road

Hardys Road functions as a local road and is under the maintenance and control of the City of West Torrens Council. It is a two-way road configured with a 10.0 metre wide carriageway set within an 18.5 metre wide road reserve (approx.). Kerbside parking is permitted on both sides adjacent the site.

To the south of the site Hardys Road intersects with Ashwin Parade at a simple priority intersection.

2.3 Sustainable Transport Infrastructure

2.3.1 Public Transport

Figure 2.2 shows the subject site in relation to existing public transport routes within its vicinity whilst Table 2.1 summarises the road based routes and major destinations that can be reached using these services.

Figure 2.2: Public Transport Map

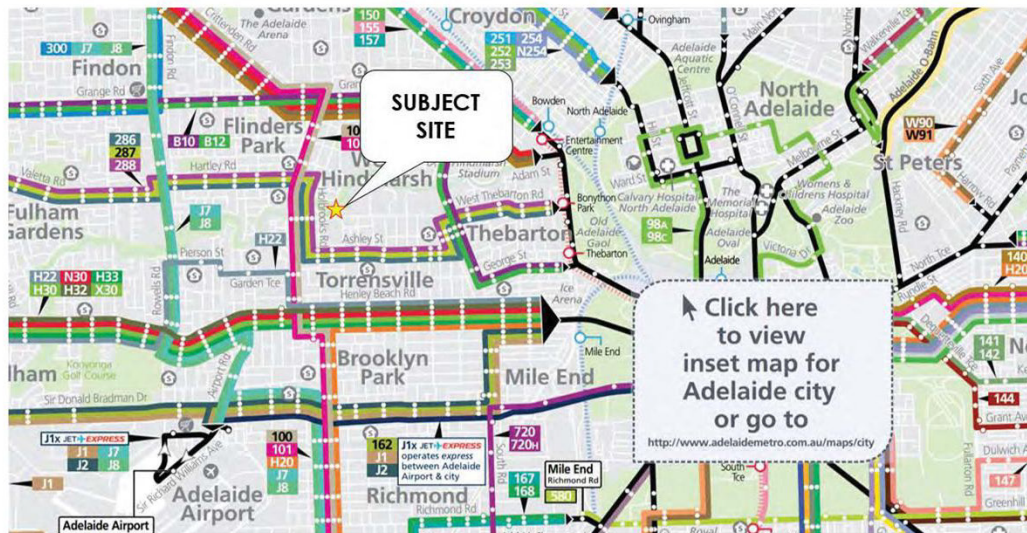


Table 2.1: Road Based Public Transport Provision

Service	Route Number	Route Description	Distance to Nearest Stop
Bus	286	Henley Beach to City	620 m
	287	Henley Beach to City	620 m
	288	West Lakes Interchange to City	620 m
	100	Arndale Interchange to Glen Osmond	620 m
	101	Arndale Interchange to Flinders University	620 m

While GTA acknowledges that public transport is not within the immediate vicinity of the subject site, the development is located adjacent Torrens Linear Park and within an established urban area.

2.3.2 Pedestrian and Cycling Infrastructure

There are no sealed footpaths on Hardys Road to the north of Ashwin Parade or bicycle lanes. However, the River Torrens Linear Park is located immediately adjacent the site to the north with shared paths providing a link between the city and Henley Beach.

3. Development Proposal

3

3.1 Land Uses

The proposed development comprises 16 townhouse dwellings each containing 3 bedrooms.

3.2 Car Parking

Car parking for the development is to be provided as follows;

- 10 dwellings will be provided with 2 car parking spaces in the form of an enclosed double garage
- 6 dwellings will be provided with 2 car parking spaces in the form of a single enclosed garage and an additional open car parking space in front of the garage
- 6 visitor car parking spaces will be provided within the site.

Based on the above, a total of 38 car parks have been provided within the site.

3.3 Vehicle Access

Vehicle access is proposed from Hardys Road via a two-way crossover.

3.4 Refuse Collection

GTA understands that refuse collection will be carried out within the site by private waste collection vehicles. A bin store has been located near the visitor parking spaces.

4. Car Parking

4

4.1 Development Plan Requirements

Recommended car parking rates for new development with the City of West Torrens are contained in Table WeTo/2 of the West Torrens Council Development Plan.

For group dwellings such as those being proposed, the Development Plan identifies a car parking rate of 2 car parking spaces per dwellings, one of which is to be covered, plus an additional 0.25 car parking spaces per dwelling.

Based on the above, Table 4.1 summarised the Development Plan car parking requirements.

Table 4.1: Development Plan Car Parking Assessment

Description	Use	Quantity	Development Plan Parking Rate	Parking Requirement
Group Dwellings	Townhouse	16 dwellings	2 spaces per dwelling	32 spaces
	Visitor	16 dwellings	0.25 spaces per dwelling	4 spaces
	Total			36 spaces

The above assessment indicates that the development proposal generates a parking requirement of 36 spaces, consisting of 32 spaces for residents and 4 spaces for visitors.

4.2 Adequacy of Parking Supply

The proposed development will provide 32 spaces for residents and 6 spaces for visitors. The provision of parking will, therefore, exceed the Development Plan requirements and is considered appropriate.

4.3 Car Parking Layout

The internal layout has been designed in accordance with Australian Standard/New Zealand Standard for Off-Street Car Parking (AS/NZS 2890.1:2004).

Some of the key design features have been detailed below:

- The single garages have a doorway width of 2.7 metres and will generally be a minimum 3m wide and 5.9m long. The garages reduce in width to 2.5m near the southern end, however, will comply with the parked vehicle design envelope requirements of the Australian Standard.
- The double garages have a minimum doorway width of 4.8 metres, with internal dimensions 5.5 metres wide x 5.5 metres long.
- A minimum turning apron width of 6.8 metres has been provided for entry to garages which exceeds the minimum requirements of the Australian Standard.
- 2.4 metres wide x 5.5 metre long visitor car parking spaces provided, set within a minimum 5.8 metre aisle as per the requirements of the Australian Standard.

5. Refuse Collection

It is understood that refuse collection will occur within the site using a private waste collection vehicle. GTA has therefore assessed the ability for a typical MRV refuse truck to turn within the site to avoid the need for reversing movements along the driveway.

The results of the turn path assessment are shown in Figure 5.1.

Figure 5.1: Reversing Area Access for Refuse Collection

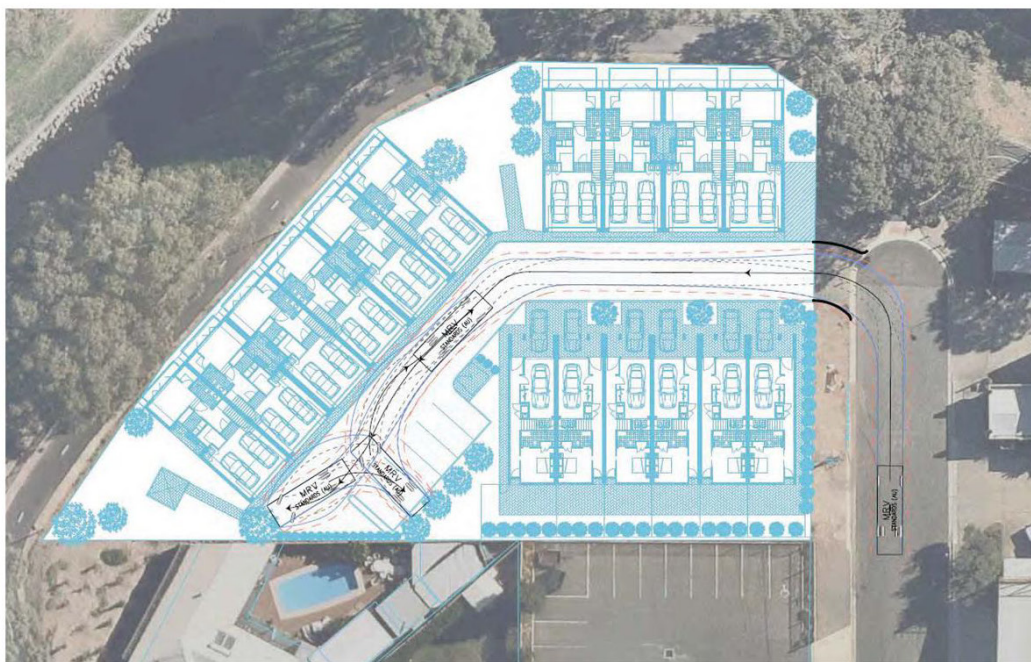


Figure 5.1 shows that an MRV would need to utilise three of the visitor parking spaces as a turnaround area. It is therefore recommended that these three spaces are designated as No Parking during waste collection times to ensure that the area remains clear for the vehicle to turn.

Refuse collection would typically occur during the AM on weekdays when demand for visitor parking would be low. On this basis, the temporary occupation of the three visitor parking spaces is considered appropriate and would generally have no impact on visitor parking availability.

It is also noted that the crossover to Hardys Road would be located close to an existing pram ramp. To ensure that the refuse vehicle (and other light vehicles) do not encroach over the pram ramp when exiting the site, it is recommended that the crossover is angled slightly to create some separation. This matter could be addressed through detailed design.

6. Traffic Impact Assessment

6

6.1 Traffic Generation

6.1.1 Design Rates

Traffic generation estimates for the proposed development have been sourced from the 'Guide to Traffic Generation Developments', Road and Traffic Authority New South Wales RTA NSW Guide – 2002.

On this basis, an estimate of the peak hour and daily traffic volumes resulting from the proposal is set out below in Table 6.1.

Table 6.1: Traffic Generation Estimates

Description	Quantity	Design Generated Rate		Traffic Generation Estimates	
		Peak Hour	Daily	Peak Hour	Daily
Residential	16 dwellings	0.9 trips/dwelling	9 trips/dwelling	15	144

Table 6.1 indicates that the site could potentially generate 15 vehicle movements in a peak hour with 144 vehicle movements over the entire day.

6.2 Traffic Impact

The anticipated traffic generation of 15 trips in a peak hour and 144 trips daily could not be expected to have an adverse impact on the safety or operation of the adjacent road network.

It should also be noted that the traffic generation estimates above are based on typical low-density residential traffic generation rates. The medium density nature of the development with good access to cycling and public transport is likely to result in a lower traffic generation than the above estimates and therefore the assessment is considered conservative and the actual traffic impact is likely to be lower.

7. Conclusion

7

Based on the analysis and discussions presented within this report, the following conclusions are made:

- i The provision of 38 on-site car parks will exceed the requirements of the Development Plan and there would be a theoretical surplus of 2 car parking spaces for visitors.
- ii The car parking and garage layout dimensions are consistent with dimensional requirements set out in AS/NZS 2890.1:2004 for Off Street Car Parking.
- iii The development is located immediately adjacent the Torrens Linear Park Shared Path, with walking/cycling access to/from the city (approx. 5km). There is reasonable access to public transport with bus services on Holbrooks Road and Ashley Street.
- iv The site can internally accommodate an 8.8m Medium Rigid Vehicle (MRV) for refuse collection, subject to three visitor parking spaces being designated as No Parking during refuse collection times.
- v The site is anticipated to generate an additional 15 trips during the peak hour and 144 daily trips to Hardys Road, which will not adversely impact on the safety and function of Hardys Road or the adjacent road network.

STATEMENT OF REPRESENTATION
Pursuant to Section 38 of the Development Act, 1993

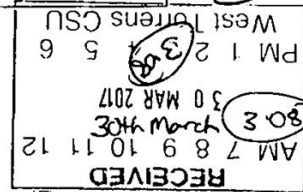
TO

Chief Executive Officer
 City of West Torrens
 165 Sir Donald Bradman Drive
 HILTON 5033

City of West Torrens

31 MAR 2017

City Development



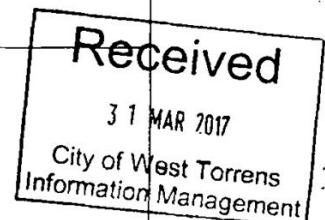
DEVELOPMENT No.

211/1546/2016

PROPERTY ADDRESS:

101-105 Hardys Road, UNDERDALE SA 5032

YOUR FULL NAME	PAUL DAY - DAYCO PTY LTD
YOUR ADDRESS	104 HALSEY ROAD TORRENSVILLE SA 5031
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	FACTORY OPPOSITE DEVELOPMENT <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION	
DEVELOPMENT WILL INTERFERE WITH OUR ABILITY TO CARRY OUT WORK AT OUR WORKSHOP	
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought)	
SEE ATTACHMENT	



Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO NOT WISH TO BE HEARD

☒

I DESIRE TO BE HEARD PERSONALLY

☐

I DESIRE TO BE REPRESENTED BY _____

☐

(PLEASE SPECIFY)

SIGNED

DATE 29/3/17

Responsible Officer: Tony Kelly

Ends: Thursday 30 March 2017

If space insufficient, please attach sheets



Dayco Pty Ltd
104 Hardys Road
Torrensville, SA, 5031

29/3/17

Chief Executive Officer
165 Sir Donald Bradman Drive
Hilton, SA, 5033

**OBJECTION TO Dev. No. 211/1546/2016 FOR:
101-105 HARDYS ROAD, UNDERDALE, SA, 5032**

Dayco Pty Ltd leases a workshop at 104 Hardys Road (directly opposite the proposed development). As a Director I object to the proposed development because the manufacturing and commercial work we perform will be adversely affected by resident and visitor vehicles and pedestrians, and our work may adversely affect residents.

Dayco Pty Ltd has designed, manufactured and serviced wastewater treatment equipment from this site since 2003 and has no intention of relocating in the foreseeable future (at least 5 years).

Our work on occasions generates noise from steel fabrication (welding, cutting, grinding machining etc) and forklift use. We comply with all requirements for the Industrial Zone in which we work but it is obvious residents at 101-105 Hardys Road (on land recently rezoned by Council from Light Industrial to allow a residential development) will find our work irritating.

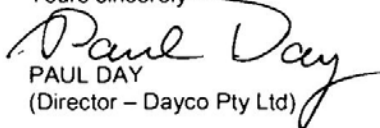
We need trucks to deliver materials and dispatch goods (several times a week in busy periods). These are usually on rigid or tautliner trucks (typically up to 12 tonne but can be articulated vehicles on occasions). Obviously couriers, employees and visitors need clear access to our site. The timing and duration of work depends on our sales and workload and although usually between 7:30am – 5pm we can and do operate outside those hours when workloads are busy (typically 6am – 8pm) and up to 7 days/week. We intend to make full use of our right to manufacture.

The proposed development is an intensive residential build with 20 dwellings (and only 5 visitor car parks). Obviously vehicle and pedestrian access to the dwellings will be busy and all adjacent streets will be crowded with parked cars. Those on Hardys Road could severely restrict heavy vehicle access to my workshop. These heavy vehicles need to reverse into our property or reverse out. The high residential density and inadequate off-street-parking make this dangerous.

This development makes serious industrial activity impossible for adjacent properties. The following actions would reduce but not eliminate problems:

1. Make the proposed development have all access via Primrose Court with no access via Hardys Road and a high noise wall constructed adjacent Hardys Road. This is the best option and acceptable to Dayco Pty Ltd.
2. There is almost certainly going to be a traffic and/or pedestrian accident because of vehicle and pedestrian traffic at the end of Hardys Road. Making Hardys Road a No Parking area on both sides for the length of the development will significantly reduce the risk (but not eliminate it). This will also allow rubbish collection. No Parking on one side of the road would still create high risk of injury and make it difficult for large vehicles to enter our workshop.
3. Should the development be approved in its proposed form it is inevitable Dayco Pty Ltd will be forced to relocate to other suitable premises. This will be expensive because we have heavy machinery and need an air conditioned workshop for some manufacturing processes.

Yours sincerely


PAUL DAY
(Director – Dayco Pty Ltd)

DAYCO PTY LTD (ABN 14 058 455 557)

Office/Warehouse Address: 104 Hardys Road, Torrensville, South Australia, 5031

Tel: (08) 8338 1823 • **Fax:** (08) 8338 3532 • **Email:** sales@vfold.com.au

Slurry Dewatering Made Simple | www.vfold.com.au

STATEMENT OF REPRESENTATION
Pursuant to Section 38 of the Development Act, 1993

TO Chief Executive Officer
 City of West Torrens
 165 Sir Donald Bradman Drive
 HILTON 5033

DEVELOPMENT No. 211/1546/2016
 PROPERTY ADDRESS: 101-105 Hardys Road, UNDERDALE SA 5032

YOUR FULL NAME	RICHARD & MARY REBECCO
YOUR ADDRESS	104 HARDYS RD TORRENSVILLE
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	OWNER 104 HARDYS RD <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION	
MY REPRESENTATIONS WOULD BE OVERCOME BY <small>(state action sought)</small>	

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO NOT WISH TO BE HEARD

I DESIRE TO BE HEARD PERSONALLY

I DESIRE TO BE REPRESENTED BY

(PLEASE SPECIFY)



SIGNED

DATE 28.3.17

Responsible Officer: Tony Kelly
 Ends: Thursday 30 March 2017

If space insufficient, please attach sheets

REBELLATO NOMINEES PTY LTD & MARY REBELLATO

60 Main Street Lockleys SA 5032

Owners 102 / 104 Hardys Road Torrensville S.A. 5031

Re: Dev. No. 211/1546/2016

Property Address 101-105 Hardys Road Underdale SA 5032

Chief Executive Officer

I am disappointed that this application has surfaced again without any consideration for our previous concerns and objection.

It is the responsibility of the council to approve applications that comply and reject the ones that don't.

This application clearly does not comply .. we as adjoining owners or tenants, have previously objected and will continue to object.

About 5 years ago the Council shortened Hardys Road to allow the walkway path to continue on the southern side of the huge tree that sits at the end of the road.

This has made it impossible to reverse out of our driveway and continue in a forward manner onto Hardys Road.

It is very difficult to negotiate transport into our premises as it is, let alone with extra vehicles parked on Hardys Road.

We currently have a tenant (Dayco P/L), who has been here since 2003.

They need the current status in egress and no restrictions on noise or business hours to continue, in order to operate, or they will need to relocate.

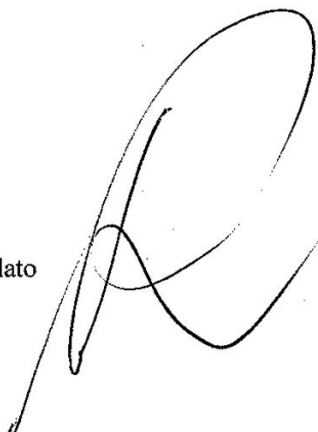
Is it the Councils' agenda to drive manufacturing business away from the area?

From Hardys Road to South Road there are NO residential dwellings from Ashwin Parade to the river, so why does the Council want to make this site an exception?

We are totally against such a development for the following reasons.

- Car parking from the residential development on Hardys Road will seriously impact entry and exit to our premises.
- Rubbish collection not only will be a horrendous eyesore, but also a logistical nightmare.
- Residents will complain about noise.

Richard & Mary Rebellato

A large, stylized handwritten signature in black ink, appearing to be a combination of 'R' and 'M' intertwined.

STATEMENT OF REPRESENTATION
Pursuant to Section 38 of the Development Act, 1993

TO Chief Executive Officer
 City of West Torrens
 165 Sir Donald Bradman Drive
 HILTON 5033

DEVELOPMENT No. 211/1546/2016
 PROPERTY ADDRESS: 101-105 Hardys Road, UNDERDALE SA 5032

YOUR FULL NAME	RICHARD REBELATO
YOUR ADDRESS	102 HARDYS RD BREENSVILLE
YOUR PHONE No	
YOUR EMAIL	
NATURE OF INTEREST	OWNER / OCCUPIER OF 102 HARDYS <small>(eg. Adjoining resident, owner of land in the vicinity etc.)</small>
REASON/S FOR REPRESENTATION	
MY REPRESENTATIONS WOULD BE OVERCOME BY (state action sought)	

Please indicate in the appropriate box below whether or not you wish to be heard by Council in respect to this submission: -

I DO NOT WISH TO BE HEARD

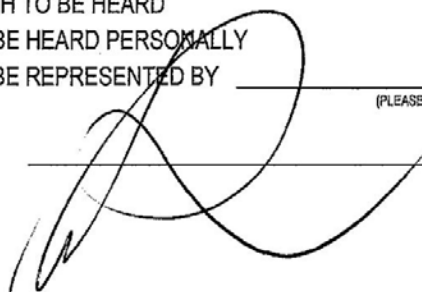
I DESIRE TO BE HEARD PERSONALLY

I DESIRE TO BE REPRESENTED BY _____

(PLEASE SPECIFY)

☒
☐
☐

SIGNED



DATE 28.3.17

Responsible Officer: Tony Kelly
 Ends: Thursday 30 March 2017

If space insufficient, please attach sheets

Richard Rebellato

102 Hardys Road Torrensville S.A. 5031
Owner / Business Operator

Re: Dev. No. 211/1546/2016

Property Address 101-105 Hardys Road Underdale SA 5032

Chief Executive Officer

I am very disappointed that this application has surfaced again without any consideration for my previous concerns and objection.

It is the responsibility of the council to approve applications that comply and reject the ones that don't.

This application clearly does not comply .. we as adjoining owners or tenants, have previously objected and NOW you want our opinion again.

The reasons we gave previously have not been addressed.

I, as owner occupier oppose this application.

This area is zoned "Light Industry", and by allowing residential dwellings is only going to cause chaos, anger and frustrations between residents and business owners.

The proposed development has an enormous 16 residential dwellings with a probable of 50 - 70 residents, and only 6 visitor's car parks, which I will guarantee will be taken up by the residents and overflow to Hardys Road.

Parking is a problem now, with people parking their cars here before going for a walk along the river.

About 5 years ago the Council shortened Hardys Road to allow the walkway path to continue on the southern side of the huge tree that sits at the end of the road.

This has made it impossible to reverse out of our driveway and continue in a forward manner onto Hardys Road.

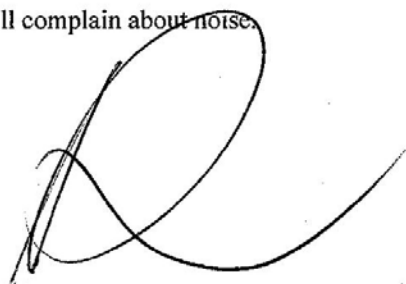
It is very difficult to negotiate transport into my premises as it is, let alone with extra vehicles parked on Hardys Road.

As a business owner at 102 Hardys Road I have no restrictions on noise or business hours and need to continue this in order to operate.

I am totally against such a development for the following reasons.

- Car parking from the residential development on Hardys Road will seriously impact entry and exit to our premises.
- Rubbish collection not only will be a horrendous eyesore, but also a logistical nightmare.
- Residents will complain about noise.

Richard Rebellato





13 April 2017

City of West Torrens
165 Sir Donald Bradman Drive
HILTON SA 5033

Attention: Mr Tony Kelly

Dear Mr Kelly

**Re: DA 211/1546/2016 – 101-105 Hardys Road, Underdale
Response to Representations**

On behalf of Township Development Pty Ltd, we refer to Council correspondence dated 4 April 2017, which provided copies of the three (3) representations received during the public notification of the above-mentioned development application.

We note that all three (3) of the representors are existing owners or occupiers of the same land (102-104 Hardys Road, Torrensville). Rebellato Nominees Pty Ltd and its directors, Richard and Mary Rebellato, own the land, while Dayco Pty Ltd occupies the premises.

Accordingly, many of the issues raised in these representations are similar and, as such, this response addresses the issues raised rather than each representation separately.

The key issues raised are as follows:

- land use interface – the representors are concerned with the impact that existing industrial land uses on their site may have on future residents of the proposed development. Conversely, they are also concerned that the establishment of residential development adjacent to their site may provide unreasonable limitations on future development of their site for industrial purposes;
- traffic, car parking and access – the representors assert that there is insufficient on-site car parking to cater for the likely demands generated by the proposed development and, as such, that there will be a significant demand for on-street car parking, which could conflict with heavy vehicle movements to and from their site; and

33 Carrington Street
Adelaide, 5000
P (08) 8193 5600
masterplan.com.au

Offices in SA | NT | QLD
ISO 9001:2015 Certified
ABN 30 007 755 277
plan@masterplan.com.au



14559LET07



- management of waste collection – the representors assert that the placement of bins along the verge will create an eyesore and that collection vehicles will conflict with trucks that service existing industry.

These issues are discussed in the following sub sections.

Prior to addressing the areas of concern, we would like to correct some misinformation. Firstly, we note that the Dayco representation refers to the construction of twenty (20) dwellings. This application is for sixteen (16) dwellings, not 20 as referenced in the Dayco representation. Secondly, the Dayco representation refers to five (5) on-site visitor car parking spaces, whereas this proposal clearly includes six (6) on-site visitor car parking spaces.

We question whether this representor has even viewed the proposed plans, as the representation seems to be based on our client's previous proposal for the land.

We raise this as our client has worked very hard to overcome the Panel's concerns with the previous proposal and has made meaningful amendments (including reducing the number of dwellings by four (4), such that adequate on-site car parking can be provided, setbacks can be improved, site coverage lessened, vehicle manoeuvring improved and other associated improvements on the previous proposal).

We believe that our client's amended proposal is a far better outcome for adjacent property owners and occupiers (both industrial and residential), as evidenced by the reduced number of representations received in relation to the current proposal.

To have three (3) representations submitted from the same adjacent property, one of which appears to still be commenting on the previous application, is disappointing and, in our view, misleading.

LAND USE INTERFACE

Zoning and land use considerations of a proposal are, by their nature, generic. It can be the case that the circumstances of a particular site and proposal will render a proposal which is wholly consistent with the intent of the zone, inappropriate. We would suggest that this is clearly the case in respect of the subject site where its high level of visibility from the linear park would render the development of a commercial or industrial use as, at best, a significant lost opportunity.

Testing the proposal against more specific interface questions such as acoustic and air quality performance, and against traffic and access impacts is a far more logical approach.

The purpose of seeking primarily commercial and industrial development in industry zones is to ensure sufficient land is available for employment uses and to protect such uses, which have the potential to generate adverse amenity impacts from the encroachment of inappropriate uses.



In the case of this proposal, the loss of the site for employment lands will not make a significant impact on the supply of employment lands in the area more generally, as is primarily justified by its location immediately adjacent the boundary of the zone.

The significance of the policy in respect of the proposal is, in our submission, lessened by the location of the site on the fringe of the zone, where a significant amount of residential development is already present and where there is a clear expectation that interface issues will need to be adequately managed. It is significant that the proposal is generally consistent with those provisions within the Industry Zone which relate to amenity for nearby non-residential development.

The policy within the zone clearly seeks degrees of compliance as to the impact of envisaged development on nearby residential development, with a higher focus on amenity at the periphery of the Industry Zone.

Relevant policy includes that which seeks setbacks to adjacent residential development (PDC 6), height limits (PDC 7), avoidance of noise impacts upon residential development (PDC 8), avoidance of environmental nuisance (PDC 9), minimum amounts of landscaping (PDC 10 and 11) and advertising signage (PDC 12).

As we have outlined, we consider that the subject site is unique in respect of its location on the very fringe of the Industry Zone and its position on a headland, making it highly visible from the River Torrens Linear Park.

The creation of the linear park as a high value recreational and environmental asset for the metropolitan area has occurred incrementally through the acquisition and redevelopment of areas of land adjacent the river. Our client recently sold a portion of the subject land to the Council to allow for the redevelopment of the shared path in a form that meets contemporary standards, significantly enhancing the amenity of the park.

Redevelopment of the subject land for commercial or industrial uses envisaged within the zone would be a significant lost opportunity in respect of interface with this regionally significant public open space.

The Development Plan against which the proposal is being assessed is relatively recent, however the policy in the Industry Zone is somewhat older. We note that the Council is considering rezoning the subject land for residential development, although this does not have a bearing on the assessment of the application currently before the Council.

We also note that the Council has, in the recent past, approved a residential development proposal on the subject land. In doing so, the Council determined that the site has characteristics which make a residential development appropriate, despite the industrial zoning of the land.



Whilst the proposed development could be utilised as a reference for other non-complying residential development within the Industry Zone, we would submit that the unique nature of the subject land and the relatively confined locality would substantially limit both the quantum of sites to which such a reference could be made and the weight that would be placed upon such a reference in a future hypothetical assessment.

Our conclusion, based on this approach, is that subject to the impact of potential externalities on the subject land being appropriately managed, the land use proposed sufficiently accords with the Development Plan.

The Statement of Effect submitted to the Council addressed the issue of land use in detail and concluded that the use of the subject land for residential purposes is appropriate and acceptable in the circumstances.

ACOUSTIC PERFORMANCE

A key issue to the representors is the extent to which the proposed development could impact upon their ability to continue the lawful uses of their site. These concerns are understood and appreciated by our client.

In particular, the extent to which noise generated on industrial sites impacts upon the occupiers of the proposed dwellings is of concern to the representors. To address this issue, our client has engaged Sonus, a qualified and experienced acoustic engineering consultancy, to review the subject land, locality and proposal, and to report upon its acoustic performance.

The report prepared by Sonus is **enclosed** with this correspondence. It concludes that subject to the implementation of some design changes, relating to thickened glazing, modified materials and additional insulation, the impact of existing development in the locality upon the proposed dwellings will be consistent with the requirements of the *Environmental Protection (Noise) Policy 2007* ('the Policy').

Our client has indicated that they are prepared to adopt all the recommendations of the Sonus report in this regard.

Achieving the requirements of the Policy is significant in that it provides a clear quantitative test to determine acceptable acoustic impacts. By doing so, it provides effective protection for industrial development against complaints relating to acoustic performance.

We also note that this proposal does not include any dwellings with a frontage to Hardys Road. In addition to reducing the number of dwellings on the land by four (4), our client has reoriented the dwellings that previously faced Hardys Road, to reduce the perceived overlooking and, more importantly,



to reduce the window openings to the potential noise source. This is an amendment that was made in direct response to an issue raised by the representors when the previous proposal was notified.

It is concluded that, subject to the recommendations of Sonus being adopted, the proposed development is appropriate in respect of its acoustic performance, particularly having regard to the nature of existing non-residential uses in the locality of the subject land.

TRAFFIC, CAR PARKING AND ACCESS

Concern has also been expressed by the representors regarding the provision of on-site car parking, the resulting impact on on-street car parking and the impact that the proposed development will have on vehicular access to their site on the opposite side of Hardys Road.

To address this issue, our client has sought advice from GTA Consultants, a firm of qualified and experienced Traffic Engineers, as to the demand for car parking resulting from the proposal and the potential impact on on-street car parking and vehicle manoeuvring.

GTA concluded that this revised proposal, with sixteen (16) group dwellings (reduced from 20 in the previous application), would result in a demand for 32 resident car parking spaces and four (4) on-site visitor car parking spaces (36 car parking spaces in total), consistent with the relevant car parking rates prescribed in the Development Plan (Table WeTo/2 – Off Street Vehicle Parking Requirements).

The proposed development includes 32 resident car parking spaces (two (2) per dwelling) and six (6) visitor spaces, which exceeds the prescribed provision of on-site car parking by two (2) spaces. As such, the theoretical demand for car parking associated with the proposal will be exceeded on-site, such that no overflow car parking will be required in the street.

GTA have also concluded that all on-site car parking spaces meet the relevant Australian Standards and that vehicle manoeuvring for all types of vehicles up to an 8.8m medium rigid vehicle (such as a removalist truck or a refuse collection truck) can be accommodated on-site.

A copy of the GTA Transport Impact Assessment is **enclosed**.

We also note that one of the representors suggested vehicular access via Primrose Court. The subject land does not have any frontage to Primrose Court and, as such, vehicular access is not possible in this location.

Having considered the GTA advice and the relevant provisions of the Development Plan, we are of the view that the provision of on-site car parking exceeds the minimum requirement prescribed within the Development Plan and it has been demonstrated that vehicle manoeuvring will be both safe and convenient.



MANAGEMENT OF WASTE COLLECTION

In terms of waste collection, the representors have conflicting opinions. The tenant at 104 Hardys Road, Dayco Pty Ltd, has suggested that making both sides of Hardys Road a 'no-parking' zone would alleviate conflicts with trucks entering and exiting their premises, and would enable waste collection from the proposed development to be collected in a convenient manner. Conversely, the owners of 102-104 Hardys Road are of the view that waste collection within Hardys Road would be an eyesore.

Our client is proposing that waste collection occur within the site, via a private waste collection vehicle. GTA has reviewed the turn paths required for an 8.8m medium rigid vehicle and have concluded that such a vehicle can enter and exit the land in a forward direction, by utilising three (3) of the visitor car parking spaces for manoeuvring. To facilitate this manoeuvre, GTA recommend that these spaces be designated as a 'no-parking' area during waste collection times (which would typically be one morning per week when visits are less likely to occur).

Our client would be accepting of conditions placed upon any consent to give effect to the amended waste collection as now proposed.

The proposed solution clearly resolves one of the representors' concerns regarding the on-street collection of waste being an eyesore, without the need for any on-street car parking controls (which is outside the scope of this application). That said, if the Council considers it appropriate to control parking within Hardys Road, adjacent to the site, in the future, then this proposal stands on its merit via the provision of adequate car parking within the site.

SUMMARY

The main issues raised by representors during the public notification period included land use interface conflicts, impacts on on-street car parking and associated vehicle manoeuvring, and conflicting issues regarding waste collection.

The proposed development has been appropriately designed to reduce impacts associated with the industrial interface, with Sonus recommending a range of acoustic measures which would enable the development to meet the relevant environmental noise criteria contained within the *Environment Protection (Noise) Policy 2007* for residential development at the interface with industrial activities.

The proposed development exceeds the number of on-site car parking spaces prescribed within Table WeTo/2 and GTA has also concluded that adequate on-site car parking is proposed to cater for the likely demands generated. As such, there will not be any (theoretical) demand for on-street car parking resulting from the proposed development.



We note that there are currently no car parking controls in place within Hardys Road, adjacent to the subject land. Given the proximity of the street to the newly constructed extension of the Linear Park cycling and walking trail, there is likely to be a relatively high demand for on-street car parking in this location already. The conflicts between trucks and vehicles parked in the street is likely an existing issue, rather than one that will be caused by the proposed development.

Waste collection will occur on-site via a private waste contractor. GTA has confirmed that all vehicle movements can occur in a safe and convenient manner. As such, there will be no visual amenity impacts of bins within the Council verge, which was listed as an area of concern by two (2) of the representors.

We trust this response enables the Council to complete its assessment of the proposal and present it to the Development Assessment Panel for a decision. We note that none of the representors have indicated a desire to be heard at the meeting. Notwithstanding, we will be present at the meeting should the Panel require any additional information or wish to ask any questions of our client.

Would you please advise us of the time and date of the meeting when this matter will be considered. Should anything further be required, please contact the writer.

Yours sincerely

Matt Atkinson MPIA
MasterPlan SA Pty Ltd

enc: Sonus Environmental Noise Assessment.
GTA Transport Impact Assessment.
cc: Township Development Pty Ltd, Att: Mr Mark Johnston (by email).

6.5 5 Ramsey Street, NETLEY

Application No 211/275/2017

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Construction of a single storey detached dwelling with verandah and double garage under main roof
APPLICANT	Taj Homes
APPLICATION NO	211/275/2017
LODGEMENT DATE	9 March 2017
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 20
APPLICATION TYPE	Merit
PUBLIC NOTIFICATION	Category 1
REFERRALS	Internal <ul style="list-style-type: none"> City Works (Amenity Officer) - Street tree External <ul style="list-style-type: none"> Nil
DEVELOPMENT PLAN VERSION	5 May 2016
MEETING DATE	11 July 2017

RECOMMENDATION

The Development Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to REFUSE Development Approval for Application No. 211/275/2017 by Taj Homes for the construction of a single storey detached dwelling with verandah and double garage under main roof at 5 Ramsey Street, Netley (CT 6041/38) for the following reasons:

- The proposed development is contrary to
 - Design and Appearance Council Wide Objectives 1
Reason: The proposal does not exhibit a high design standard that reinforces the positive aspects of the local environment
 - Residential Development Principle of Development Control 16
Reason: The proposal will largely be dominated by garaging, which will not have a positive visual impact on the streetscape.
 - Crime Prevention Principle of Development Control 2 & Residential Development Principle of Development Control 7
Reason: The dwelling design does not include a living area or habitable room with an external outlook to the public street, reducing casual surveillance.

BACKGROUND

The development proposal is presented to the Development Assessment Panel (DAP) for the following reason:

- All applications where the assessing officer recommends refusal, shall be assessed and determined by the DAP.

PREVIOUS OR RELATED APPLICATION(S)

Nil

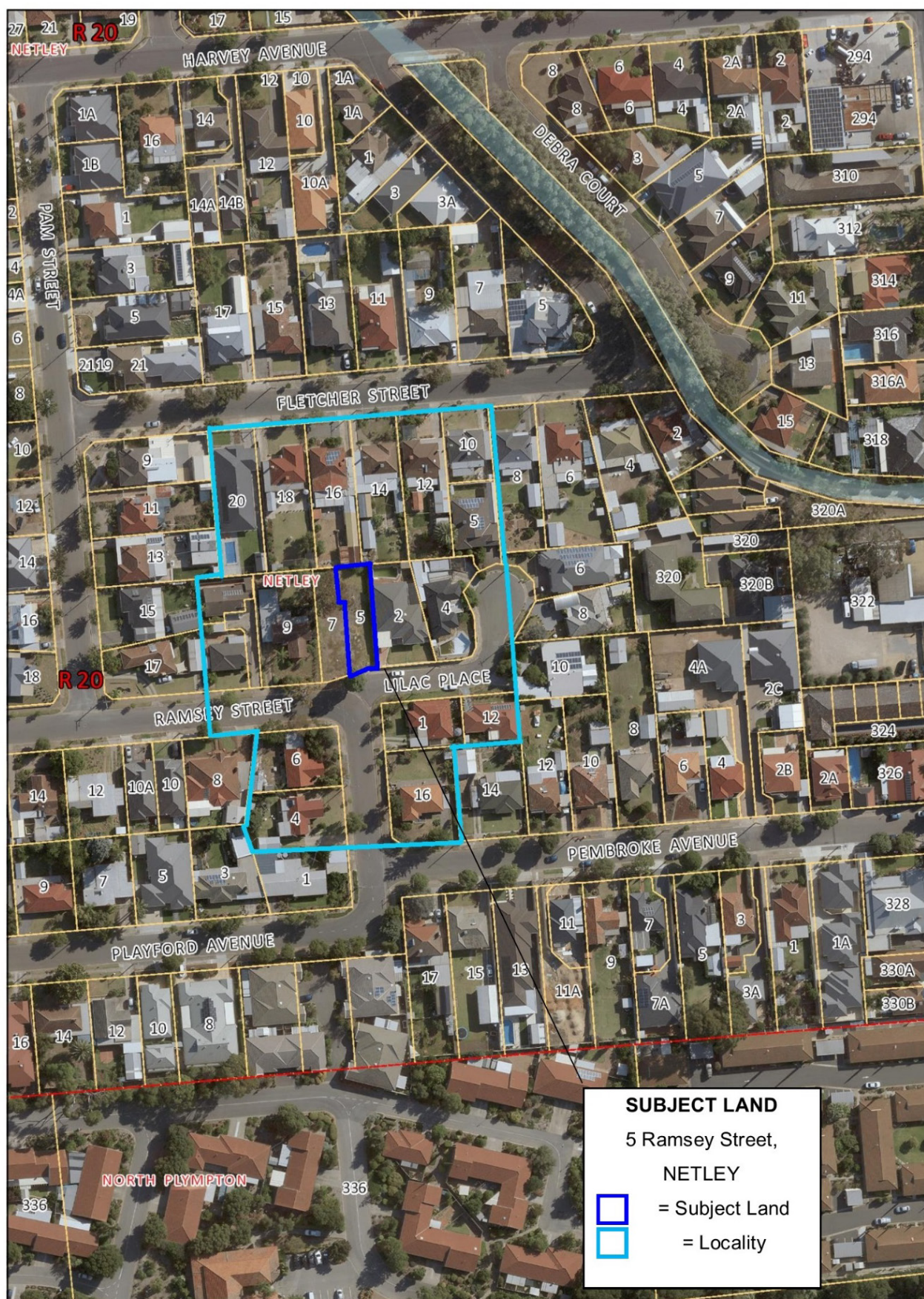
SITE AND LOCALITY

The subject land is formally known as Allotment 23 Deposited Plan 81612 in the area named Netley, Hundred of Adelaide, Certificate of Title Volume 6041 and Folio 38, but is more commonly known as 5 Ramsey Street, Netley. The subject land is an irregular shaped allotment with a 9.41 metre street frontage and a total site area of 344 square metres.

Whilst the address of the subject land suggests the allotment fronts Ramsey Street it is actually located with more of a frontage to Lilac Place. The allotment is on the northern side of Ramsey Street and is located just off of the intersection of Ramsey Street and Lilac Place. The subject land is approximately 251 metres walking distance north-west from the arterial road, Marion Road. Currently the subject land is vacant and its creation was a result of the subdivision of the parent title (CT 5208/992) approved in 2008 (Development Application 211/1066/2008). There is one mature Queensland Brush Box street tree in the verge area adjacent the subject land.

The locality is comprised of residential development in the form of single storey detached dwellings. There is no consistent dwelling style established within the locality.

The locality and site is shown on the maps below.





Item 6.5

PROPOSAL

The proposed development involves the construction of a single storey detached dwelling, with a double garage and two verandahs under the main roof. The dwelling contains an open plan living/dining and kitchen area, three bedrooms, one with an ensuite and walk in wardrobe, a separate bathroom, study and a laundry.

The design and appearance of the building includes a mixture of different modern elements such as rendered parapet, porch over the entrance with skillion roof, and a combination of different textures, materials and finishes, for example stone cladding, render, timber slats.

A copy of the plans and details are contained in **Attachment 1**.

PUBLIC NOTIFICATION

The application is a Category 1 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations and Residential Zone, Procedural Matters.

REFERRALS

Internal

- City Works (Amenity Officer) - Street Tree Impact

City Works would support the application only if the crossover and stormwater connection through the street would meet the minimum two (2) metre offset from the street tree. Initially the proposal did not satisfy the minimum offsets, however, the proposal has since been amended and there are now no outstanding concerns with the current proposal.

External

Nil

ASSESSMENT

The subject land is located within the Residential Zone, and more specifically within the Low Density Policy Area 20 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Crime Prevention	Objectives	1
	Principles of Development Control	1, 2, 3 & 8
Design and Appearance	Objectives	1
	Principles of Development Control	1, 2, 3, 4, 9, 12, 13, 14, 15, 21 & 22
Energy Efficiency	Objectives	1 & 2
	Principles of Development Control	1, 2 & 3
Infrastructure	Objectives	3
	Principles of Development Control	1, 4, 5, 6 & 8
Landscaping, Fences and Walls	Objectives	1
	Principles of Development Control	1, 2, 3 & 4
Orderly and Sustainable Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1, 3 & 7

Residential Development	Objectives	1, 2 & 4
	Principles of Development Control	1, 3, 4, 5, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 18, 19, 20, 21, 28, 29 & 31
Transportation and Access	Objectives	2
	Principles of Development Control	23, 24, 25, 30, 34, 35 & 44

Zone: Residential Zone	
<p><i>Desired Character Statement:</i></p> <p><i>"This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer".</i></p>	
Objectives	1, 2, 3 & 4
Principles of Development Control	1, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13 & 14

Policy Area: Low Density Policy Area 20	
<p><i>Desired Character Statement:</i></p> <p><i>"Allotments in the policy area will be at low density, accommodating predominantly detached dwellings and some other dwellings types such as semi-detached and group dwellings. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings".</i></p>	
Objectives	1
Principles of Development Control	1, 2, 3 & 4

QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
SITE AREA <i>Low Density Policy Area 20</i> <i>PDC 4</i>	300sqm	349sqm Satisfies
SITE FRONTAGE <i>Low Density Policy Area 20</i> <i>PDC 4</i>	9m	9.41m Satisfies
PRIMARY STREET SETBACK <i>Residential Zone</i> <i>PDC 8</i>	Same as one of the adjacent buildings (6m)	6m Satisfies
SIDE/REAR SETBACKS <i>Residential Zone</i> <i>PDC 11</i>	Side 0/1m Rear 3m	East - 1.1m - 1.9m West - garage on boundary - 1.1m Satisfies 4m Satisfies
BUILDING HEIGHT <i>Residential Zone</i> <i>PDC 6</i>	2 storeys or 6m	Single storey Satisfies
INTERNAL FLOOR AREA <i>Residential Development</i> <i>PDC 9</i>	3+ Bedroom, 100m ² (min.)	145m ² Satisfies
PRIVATE OPEN SPACE <i>Residential Development</i> <i>PDC 19</i>	300-500m ² - 60m ² (min.), of which 10m ² may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2m. - Minimum dimension 4m. - 16m ² (min.) at the rear of side of dwelling, directly accessible from a habitable room.	66m ² (total) 2.8m (min. dimension) 66m ² (accessed from habitable room) Partially Satisfies

CARPARKING SPACES <i>Transportation and Access</i> PDC 34	2 car-parking spaces required, 1 of which is covered	4 spaces provided (2 covered) Satisfies
STORAGE <i>Residential Development</i> PDC 30	8m ³ (min)	Not detailed
LANDSCAPING <i>Landscaping, Fences and Walls</i> PDC4	Min 10%	Not detailed

QUALITATIVE ASSESSMENT

In assessing the merits or otherwise of the application, the proposed development satisfies the relevant Development Plan provisions with the exception of the following, as discussed under the following sub headings:

Design and Appearance

The dwelling style within the locality is not homogenous and therefore the proposed dwelling will not present any incompatibility with the established built form with regards to this aspect of the development (Residential Development, Principle of Development Control (PDC) 4). However, an element of the proposed dwelling that is not compatible with the dwellings along the streetscape and the Development Plan is the garage.

The garage element of the dwelling is contrary to the following provision of the Development Plan:

- Residential Development, PDC 16,

"Garages, carports and residential outbuildings should not dominate the streetscape and not adversely impact on the safety of road users and pedestrians, and be designed within the following parameters:

Parameter	Value
Maximum floor area	60 square metres
Maximum wall height	3 metres
Maximum building height	5 metres
Minimum setback from a primary road frontage	Garages and carports sited no closer to the primary road frontage than any part of its associated dwelling Outbuildings should not protrude forward of any part of its associated dwelling
Minimum setback from a secondary road frontage	0.9 metres or in line with the existing dwelling frontage
Maximum length along the boundary	8 metres or 50 per cent of the length along that boundary (whichever is the lesser)
Maximum frontage width of garage or carport with an opening facing a rear access lane	No maximum
Maximum frontage width of garage or carport opening facing the street	6 metres or 50 per cent of the allotment frontage, whichever is less

The proposed dwelling will have a garage which has an opening width that is greater than 50% of the allotment frontage width. The total door width for each allotment is 4.8 metres and half of the allotment frontage width is 4.7 metres. Whilst this deficiency is not large and the expanse of the garage is broken up by a pillar between the doors, it is still considered to be a dominant feature given that the total width of the garage component (not just the panel lift doors) comprises 61% of the entire width of the allotment and the remainder of the front façade only comprises 26%. The garage also appears as the dominant feature as the design highlights this component by separating it from the entrance portion, having two different roof forms over the garage and entrance areas and including a wide parapet around the edge of the garage component.

Whilst the style of dwellings within the locality is varied, the majority of the dwellings have wide facades given that the respective allotments are also wide, so although there are some dwellings within the locality that have double garages, for example, 5 Lilac Place, and 20 Fletcher Street, there is no garage dominance given that the dwelling façade and garage widths are well balanced. In this instance subject land is narrow so this balance is difficult to achieve. The proposed dwelling would be the first within the locality to have a double garage on a narrow allotment.

In addition, to accommodate the double garage only an entranceway and not a habitable room facing the public street could be established. The Floor Plan indicates that this area is a Lounge Room, however, the dimensions of the room may suggest that this area would more than likely simply be used as an entranceway. The Development Plan encourages residential development to be designed to ensure that living rooms have an external outlook, which helps discourage crime by providing casual surveillance (Residential Development PDC 7 & Crime Prevention, PDC 2). In this instance the inclusion of the double garage does not enable these PDC's to be adequately facilitated.

Private Open Space

The proposed development presents a partial deficiency of Residential Development PDC 19 with regards to private open space. The total area of private open space (66 square metres) is greater than the minimum amount required for the allotment, however, the minimum dimension of the western portion is less than 4 metres. The deficiency of the minimum dimension is not considered to be detrimental to the proposal for the following reasons:

- The minimum dimension of 2.8 metres still results in an area that is useable;
- The portion of private open space that has the minimum dimension of 2.8 metres makes up less than 35% of the total private open space;
- Both areas of private open space are joined and are overall a useable size and shape;
- It is located at the side and rear of the subject land to maintain privacy;
- It is accessible from a main living area;
- It is exclusive of areas of driveways, outbuildings, bin storage areas and the like.

In addition, the subject land is located approximately 154 metres east of Beare Avenue Reserve, a large area of public open space, which will be useable for the occupiers for leisure (Residential Development, PDC18).

Landscaping and Domestic Storage

The application is absent of any landscaping and domestic storage detail. It is evident that at least 10% of the site could be landscaped, as for example if the rear section of the private open space was landscaped that would amount to 12% of the site (Landscaping, Fences and Walls PDC 4). If approval for the application was granted, a condition of consent to provide a landscaping plan could be established.

Whilst the site does not specifically label areas of domestic storage, the site includes a sufficient amount. Given that the site provides 4 off street parking spaces when only 2 is required, potentially one half of the garage could be utilised as domestic storage without resulting in a parking deficiency.

SUMMARY

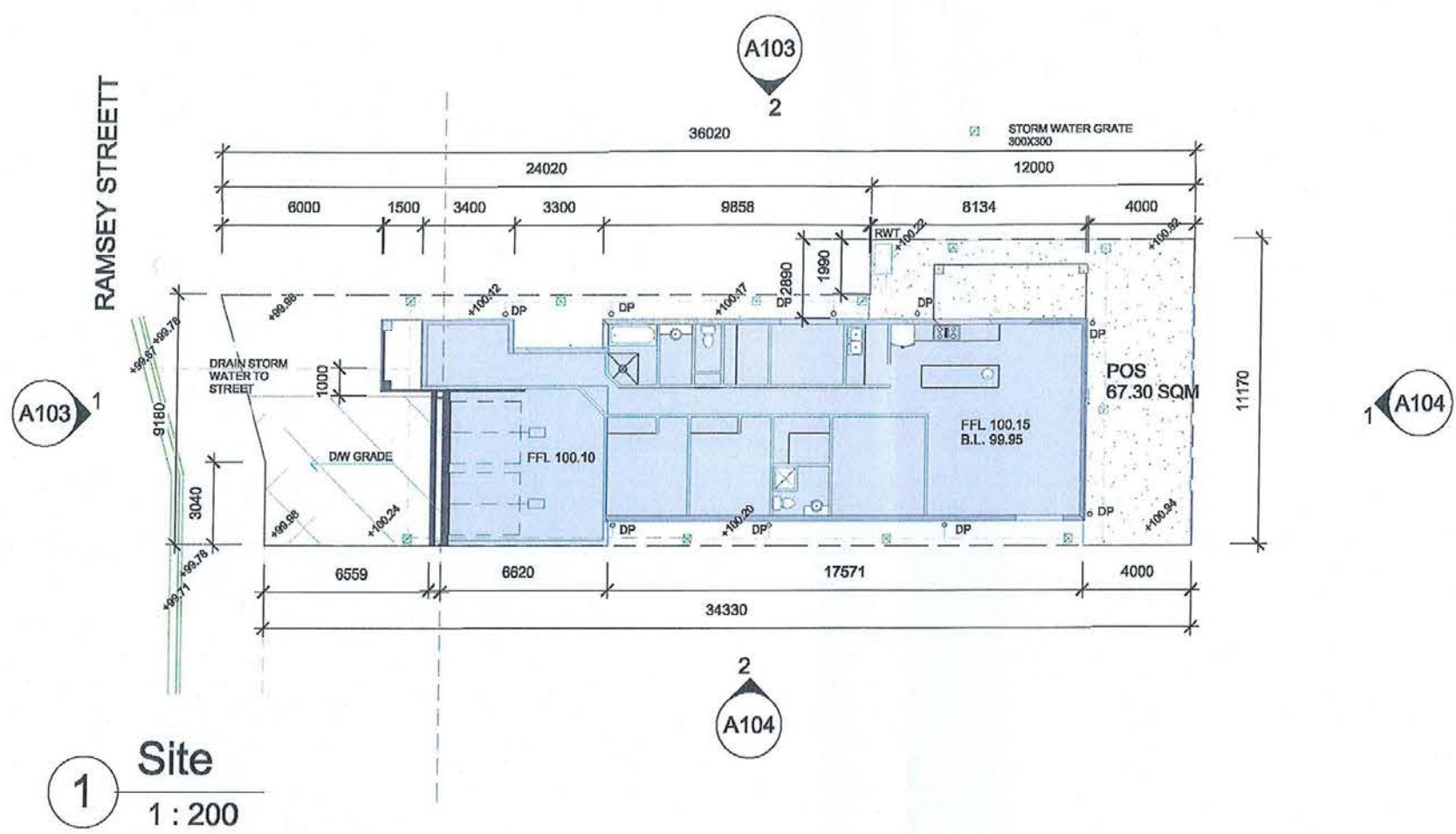
The proposed development is an envisaged form of development within the Residential Zone and Low Density Policy Area 20, and is mostly an orderly and functional development. However, garage width and dominance is considered to be contrary to the Development Plan policy with regard to visual amenity. Furthermore, the inclusion of a double garage reduces the opportunity for casual surveillance of the public street.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be at variance with the Development Plan.

On balance the proposed development does not sufficiently accord with the relevant provisions contained within the West Torrens (City) Development Plan Consolidated 5 May 2016 and does not warrant Development Plan Consent.

Attachments

1. Plans & Details



GENERAL NOTES
THIS DRAWING IS PROTECTED BY COPYRIGHT. IT MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE WRITTEN AUTHORITY OF AHS PROJECT SERVICES.
DO NOT SCALE FROM THIS DRAWING. IF IN DOUBT, ASK.
PRIOR TO COMMENCING SHOP DRAWINGS OR THE MANUFACTURE OF ANY ITEM, THE CONTRACTOR IS TO VERIFY ALL RELEVANT DIMENSIONS ON THE PROJECT SITE.
PRIOR TO COMMENCING CONSTRUCTION THE CONTRACTOR IS TO BRING TO THE NOTICE OF AHS PROJECT SERVICES ANY DISCREPANCIES CONTAINED IN THIS DRAWING.
THE SUBSTITUTION OF ANY STRUCTURAL MEMBERS AND SPECIFIED PRODUCTS, AND ANY VARIATIONS TO ANY PART OF THE DESIGN, WILL VOID ANY RESPONSIBILITIES OF AHS PROJECT SERVICES FOR THE STRUCTURAL INTEGRITY AND PERFORMANCE OF THE BUILDING.
ALL CONSTRUCTION METHODS AND MATERIALS ARE TO BE IN ACCORDANCE WITH:-
THE BUILDING CODE OF AUSTRALIA, THE QUEENSLAND DEVELOPMENT CODE, THE STANDARD BUILDING REGULATIONS AND ANY RELEVANT BUILDING CODE AND AUSTRALIAN STANDARD.



PROJECT
RESIDENCE AT NETLEY

FOR
HARVINDER SINGH

AT
LOT 23
RAMSEY STREET
NETLEY

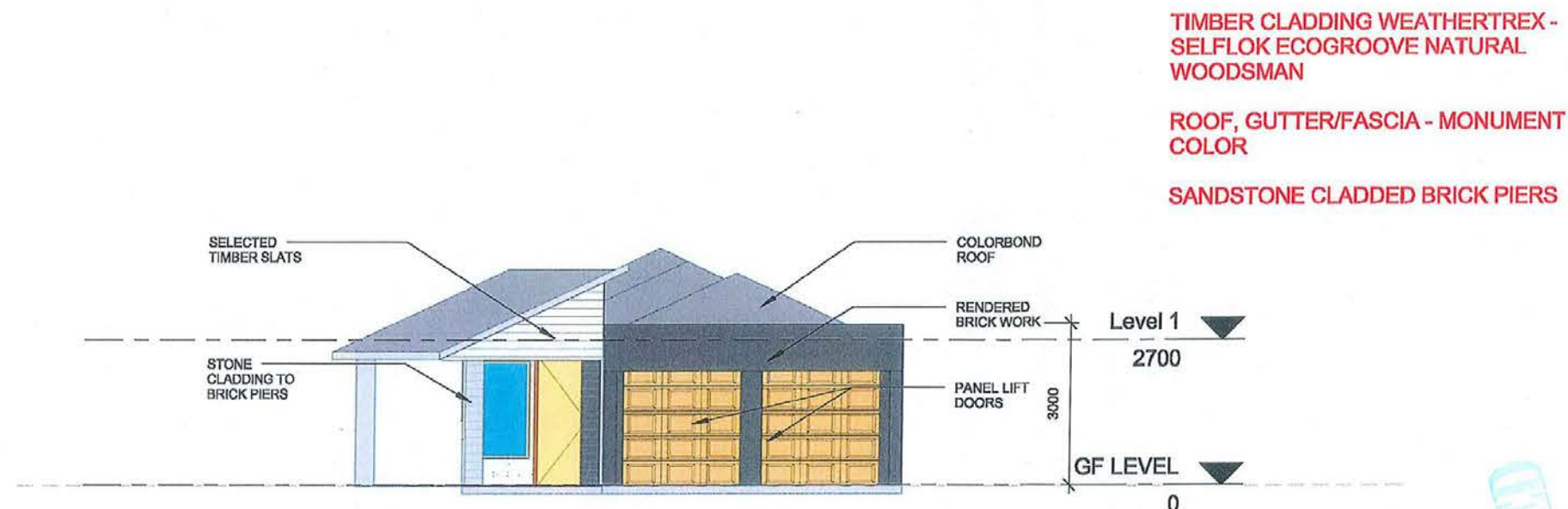
DATE : 23/01/2017	DRAWN : NG
SCALE : @ A3 1 : 200	CHECKED : NG



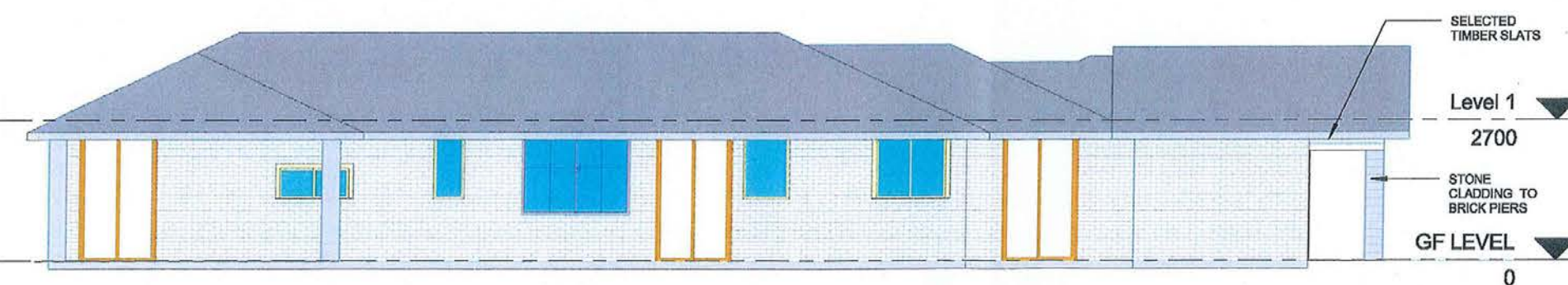
JOB No.	DRAWING No.	REV.	OF
TH-01	A101		

DESIGN DRAWINGS
PLANNING

REVISIONS		
No.	DATE	DETAILS



1 Elevation 1 - a
1 : 100



2 Elevation 2 - a
1 : 100

TIMBER CLADDING WEATHERTREX -
SELFLOK ECOGROOVE NATURAL
WOODSMAN

ROOF, GUTTER/FASCIA - MONUMENT
COLOR

SANDSTONE CLADDED BRICK PIERS

GENERAL NOTES
THIS DRAWING IS PROTECTED BY COPYRIGHT. IT MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE WRITTEN AUTHORITY OF ANS PROJECT SERVICES. DO NOT SCALE FROM THE DRAWING. IF IN DOUBT, ASK. PRIOR TO COMMENCING SHOP DRAWINGS OR THE MANUFACTURE OF ANY ITEM, THE CONTRACTOR IS TO VERIFY ALL RELEVANT DIMENSIONS ON THE PROJECT SITE. PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR IS TO BRING TO THE NOTICE OF ANS PROJECT SERVICES ANY DISCREPANCIES CONTAINED IN THIS DRAWING. THE SUBSTITUTION OF ANY STRUCTURAL MEMBERS AND SPECIFIED PRODUCTS, AND ANY VARIATIONS TO ANY PART OF THE DESIGN, WILL VOID ANY RESPONSIBILITIES OF ANS PROJECT SERVICES FOR THE STRUCTURAL INTEGRITY AND PERFORMANCE OF THE BUILDING. ALL CONSTRUCTION METHODS AND MATERIALS ARE TO BE IN ACCORDANCE WITH :- THE BUILDING CODE OF AUSTRALIA, THE QUEENSLAND DEVELOPMENT CODE, THE STANDARD BUILDING REGULATIONS AND ANY RELEVANT BUILDING CODE AND AUSTRALIAN STANDARD.

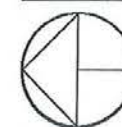


PROJECT
RESIDENCE AT NETLEY

FOR
HARVINDER SINGH

AT
LOT 23
RAMSEY STREET
NETLEY

DATE : 23/01/2017 DRAWN : NG
SCALE : @ A3 1 : 100 CHECKED : NG



JOB No. TH-01 DRAWING No. A103 REV. OF

DESIGN DRAWINGS

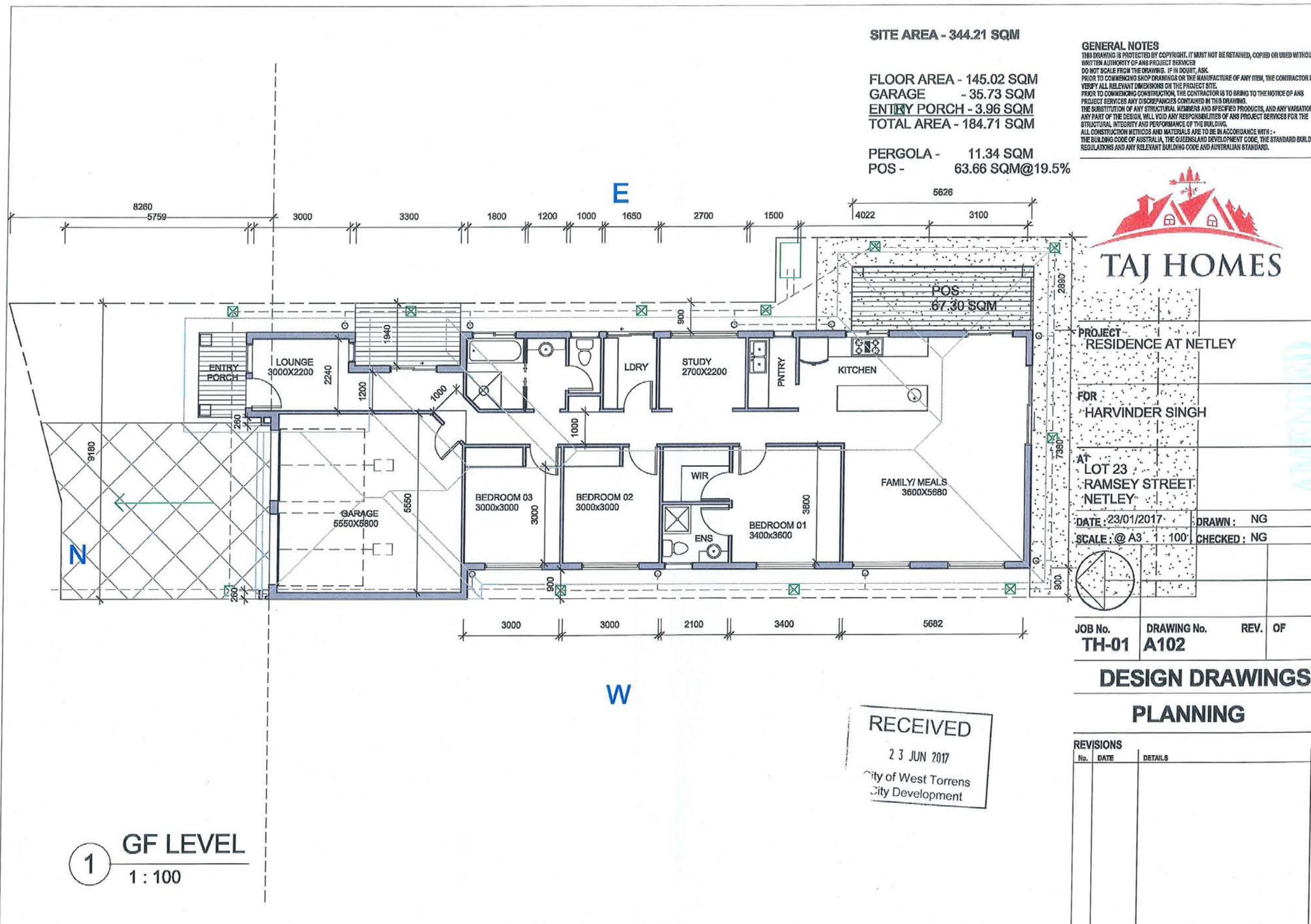
PLANNING

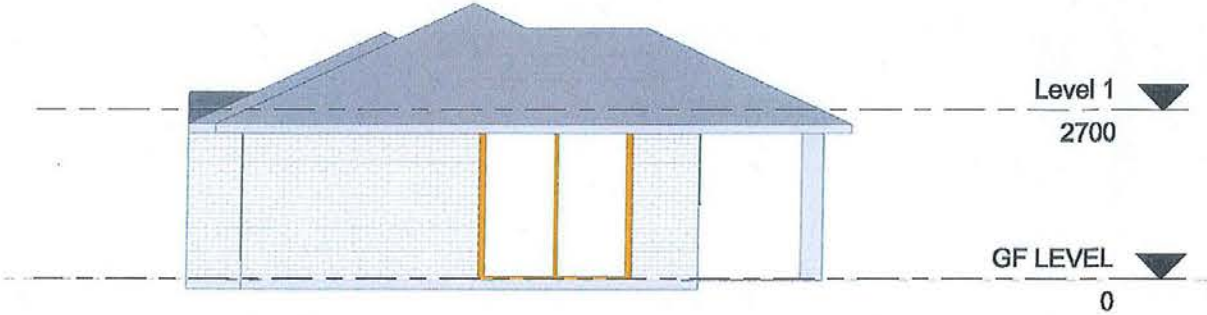
RECEIVED

23 JUN 2017
City of West Torrens
City Development

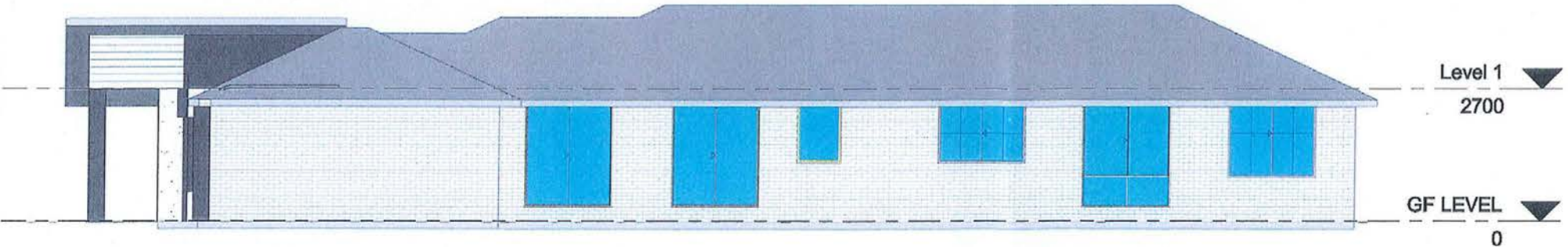
REVISIONS

No.	DATE	DETAILS





1 Elevation 3 - a
1 : 100



2 Elevation 4 - a
1 : 100

RECEIVED
23 JUN 2017
City of West Torrens
Development

GENERAL NOTES
THIS DRAWING IS PROTECTED BY COPYRIGHT. IT MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE WRITTEN AUTHORITY OF AHS PROJECT SERVICES.
DO NOT SCALE FROM THIS DRAWING. IF IN DOUBT, ASK.
PRIOR TO COMMENCING SHOP DRAWINGS OR THE MANUFACTURE OF ANY ITEM, THE CONTRACTOR IS TO VERIFY ALL RELEVANT DIMENSIONS ON THE PROJECT SITE.
PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR IS TO BRING TO THE NOTICE OF AHS PROJECT SERVICES ANY DISCREPANCIES CONTAINED IN THIS DRAWING.
THE SUBSTITUTION OF ANY STRUCTURAL MEMBERS AND SPECIFIED PRODUCTS, AND ANY VARIATIONS TO ANY PART OF THE DESIGN, WILL VOID ANY RESPONSIBILITIES OF AHS PROJECT SERVICES FOR THE STRUCTURAL INTEGRITY AND PERFORMANCE OF THE BUILDING.
ALL CONSTRUCTION METHODS AND MATERIALS ARE TO BE IN ACCORDANCE WITH : -
THE BUILDING CODE OF AUSTRALIA, THE QUEENSLAND DEVELOPMENT CODE, THE STANDARD BUILDING REGULATIONS AND ANY RELEVANT BUILDING CODE AND AUSTRALIAN STANDARD.

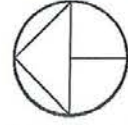


PROJECT
RESIDENCE AT NETLEY

FOR
HARVINDER SINGH

AT
LOT 23
RAMSEY STREET
NETLEY

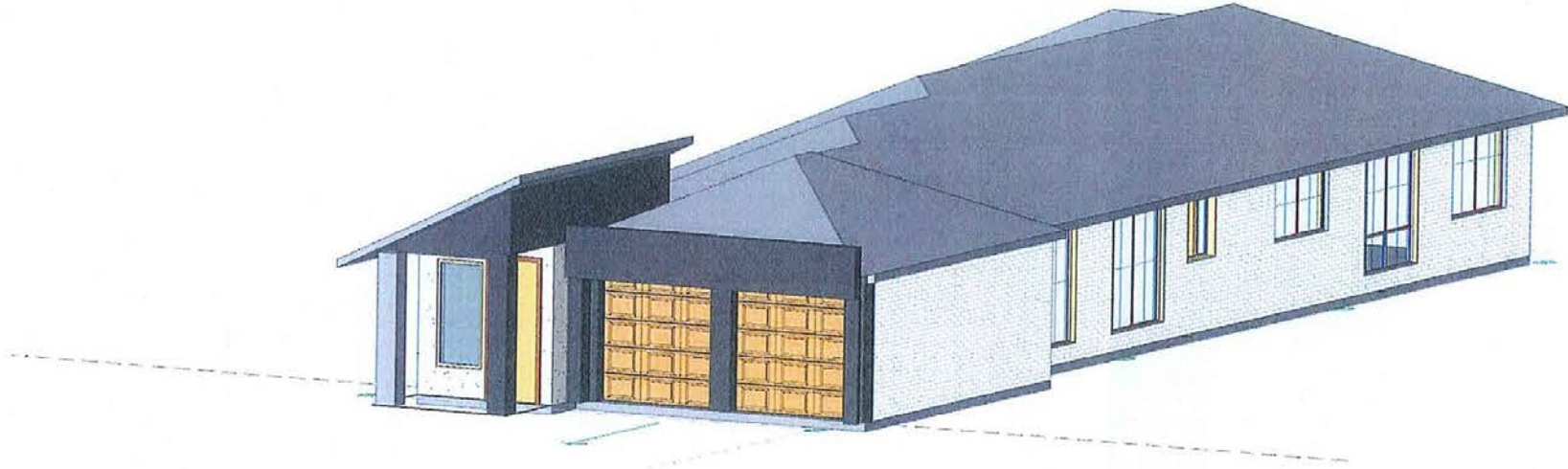
DATE : 23/01/2017 DRAWN : NG
SCALE : @ A3 1 : 100 CHECKED : NG



JOB No. TH-01	DRAWING No. A104	REV.	OF
------------------	---------------------	------	----

DESIGN DRAWINGS
PLANNING

REVISIONS		
No.	DATE	DETAILS



1 {3D}

RECEIVED
23 JUN 2017
City of West Torrens
City Development

GENERAL NOTES
THIS DRAWING IS PROTECTED BY COPYRIGHT. IT MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE WRITTEN AUTHORITY OF ANS PROJECT SERVICES.
DO NOT SCALE FROM THIS DRAWING. IF IN DOUBT, ASK.
PRIOR TO COMMENCING SHOP DRAWINGS OR THE MANUFACTURE OF ANY ITEM, THE CONTRACTOR IS TO VERIFY ALL RELEVANT DIMENSIONS ON THE PROJECT SITE.
PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR IS TO BRING TO THE NOTICE OF ANS PROJECT SERVICES ANY DISCREPANCIES CONTAINED IN THIS DRAWING.
THE SUBSTITUTION OF ANY STRUCTURAL MEMBERS AND SPECIFIED PRODUCTS, AND ANY VARIATIONS TO ANY PART OF THE DESIGN, WILL VOID ANY RESPONSIBILITIES OF ANS PROJECT SERVICES FOR THE STRUCTURAL INTEGRITY AND PERFORMANCE OF THE BUILDING.
ALL CONSTRUCTION METHODS AND MATERIALS ARE TO BE IN ACCORDANCE WITH : -
THE BUILDING CODE OF AUSTRALIA, THE QUEENSLAND DEVELOPMENT CODE, THE STANDARD BUILDING REGULATIONS AND ANY RELEVANT BUILDING CODE AND AUSTRALIAN STANDARD.



PROJECT
RESIDENCE AT NETLEY

FOR
HARVINDER SINGH

AT
LOT 23
RAMSEY STREET
NETLEY

DATE : 23/01/2017	DRAWN : NG
SCALE : @ A3	CHECKED : NG



JOB No. TH-01	DRAWING No. A105	REV.	OF
------------------	---------------------	------	----

DESIGN DRAWINGS
PLANNING

REVISIONS		
No.	DATE	DETAILS

6.6 294 Marion Road, NETLEY

Application No 211/557/2016

DEVELOPMENT APPLICATION DETAILS

DEVELOPMENT PROPOSAL	Installation of non-illuminated signage attached to the existing building and canopy and a freestanding illuminated pylon sign - NON-COMPLYING
APPLICANT	Andrash Pty Ltd
APPLICATION NO	211/557/2016
LODGEMENT DATE	23 May 2016
ZONE	Residential Zone
POLICY AREA	Low Density Policy Area 20
APPLICATION TYPE	Non-complying
PUBLIC NOTIFICATION	Category 3
REFERRALS	Internal <ul style="list-style-type: none"> ▪ Nil External <ul style="list-style-type: none"> ▪ Nil
DEVELOPMENT PLAN VERSION	5 May 2016
MEETING DATE	11 July 2017

RECOMMENDATION

The Development Assessment Panel, having considered all aspects of the report, the application for consent to carry out development of land and pursuant to the provisions of the *Development Act 1993* resolves to GRANT Development Plan Consent for Application No. 211/557/2016 by Andrash Pty Ltd to undertake the installation of non-illuminated signage attached to the existing building and canopy and a freestanding illuminated pylon sign at 294 Marion Road, Netley (CT5105/144) subject to the following conditions of consent and concurrence from the Development Assessment Commission (and any subsequent or amended condition that may be required as a result of the consideration of reserved matters under Section 33(3) of the Development Act):

1. That the development shall be undertaken and completed in accordance with the plans and information stamped with Development Plan Consent on 11 July 2016 as detailed in this application except where varied by any condition(s) listed below.
2. That the signs, herein approved, shall be maintained in good repair with all words and symbols being clearly visible at all times.
3. That the signs, herein approved, shall not move, flash, blink or rotate in any manner.
4. That any illumination of the signs shall be such that no hazard, difficulty or discomfort is caused to either approaching drivers on adjacent public roads or nuisance to adjoining residents.

5. Council requires one business day's notice of the following stages of building work:

- Commencement of building work on site
- The commencement of placement of any structural concrete
- The completion of wall and roof framing prior to the installation of linings
- Completion of building work

Note:

When a building-owner gives notice for the commencement of building work, they shall advise Council of the relevant person, (name, address and telephone number) who will provide the Statement of Compliance required under regulation 83AB. The relevant person must be:

- The licensed building contractor who performed the work, or
- A registered building work supervisor, private certifier or registered architect.

BACKGROUND

The development proposal is presented to the Development Assessment Panel (DAP) for the following reasons:

- All applications for non-complying forms of development shall be assessed and determined by the DAP.

PREVIOUS OR RELATED APPLICATION(S)

DA 211/1261/2016 - Construction of a canopy to existing petrol filling station - Granted Development Approval 7 March 2017.

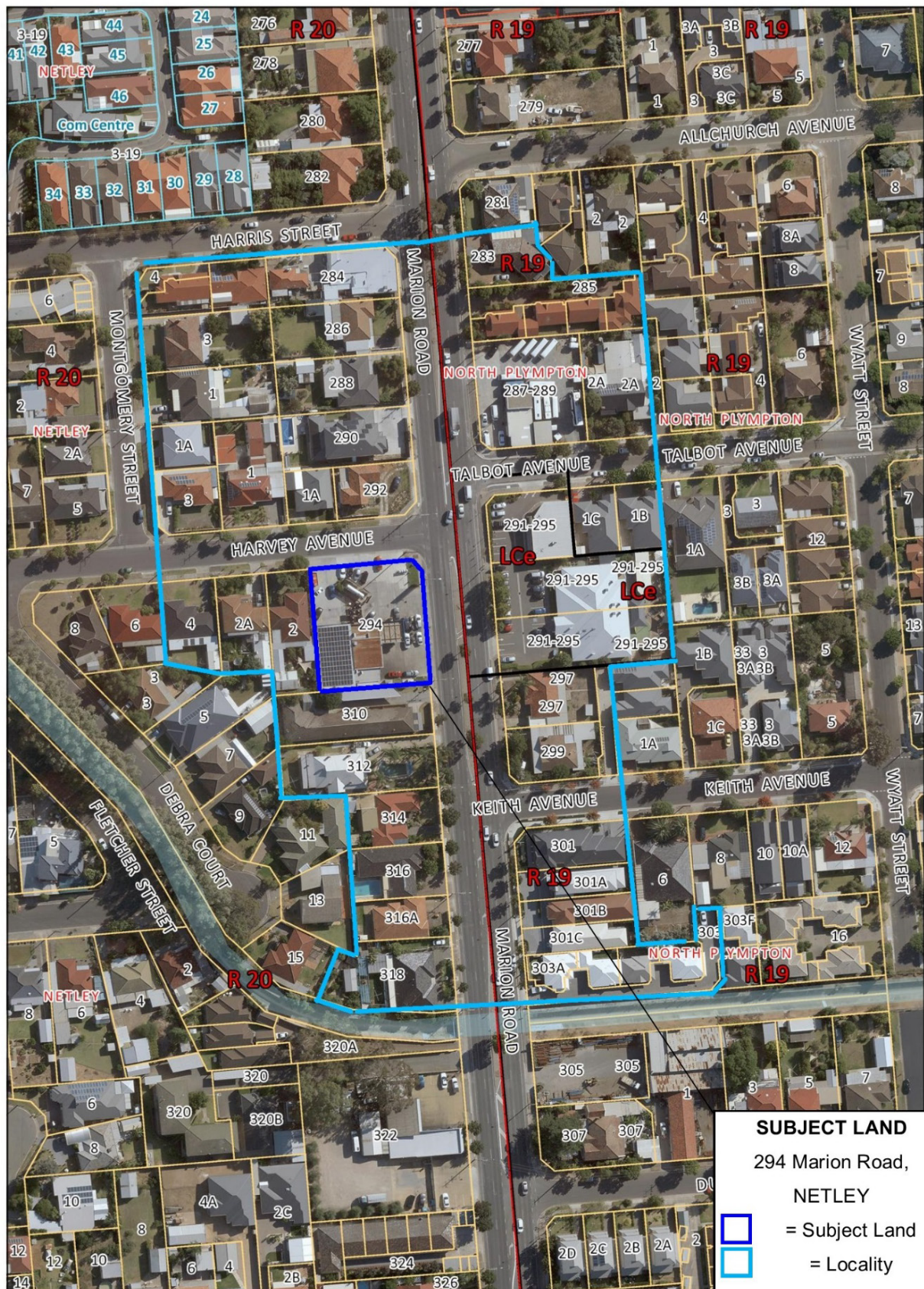
SITE AND LOCALITY

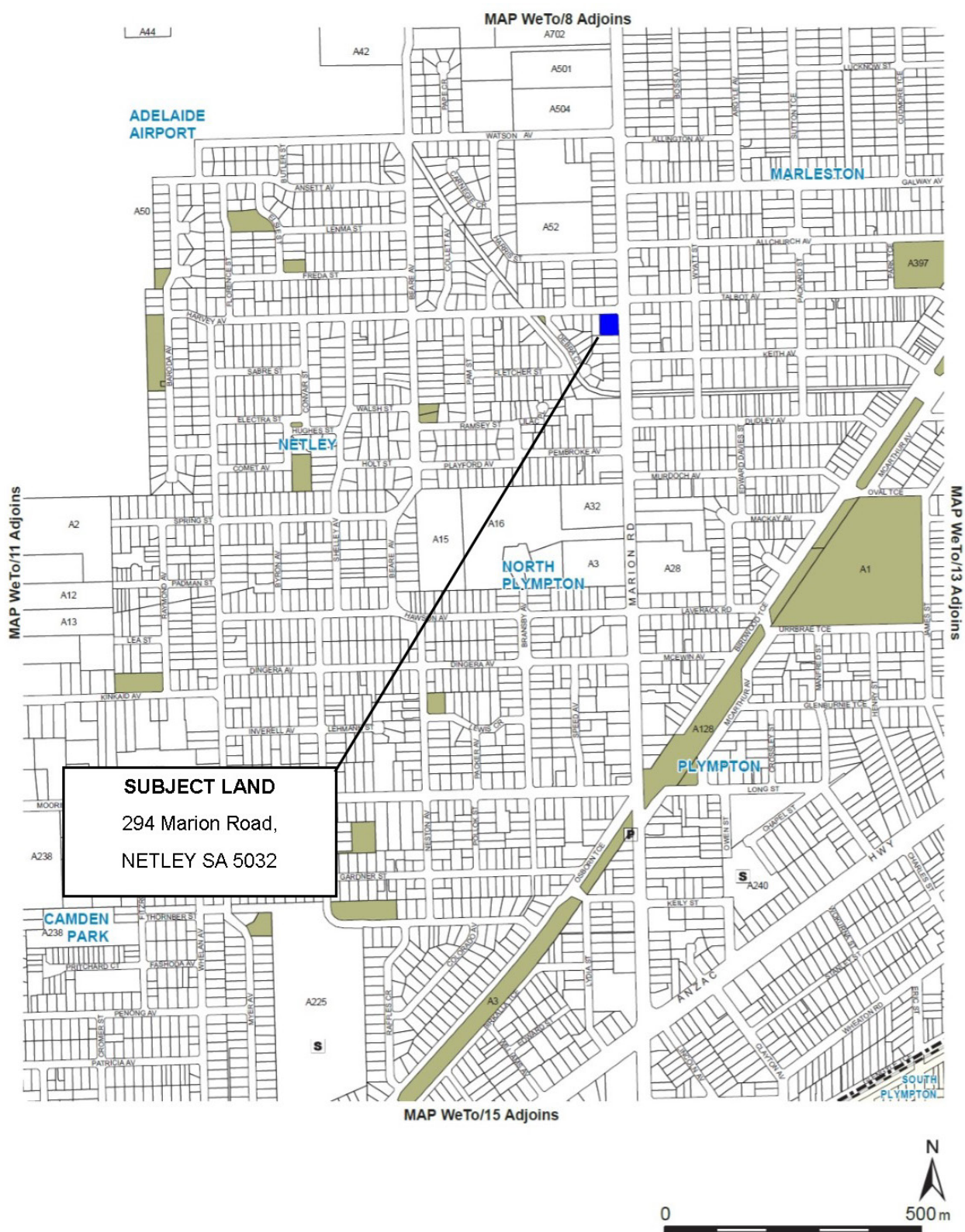
The subject land is formally described as Allotment 40 Filed Plan 2039 in the area named Netley Hundred of Adelaide, but it is more commonly known as 294 Marion Road, Netley.

The subject site is located on the southwest corner of the intersection of Harvey Avenue and Marion Road, directly adjacent the North Plympton Local Centre Zone. The Marion Road frontage, which is the primary frontage, has a width of 38.10 metres, and the secondary street (Harvey Avenue) frontage width is 34.44 metres. The total site area is 1533.42 square metres.

The locality includes both residential and commercial land uses in a residential policy area. The properties within the locality that are on the western side of Marion Road as located within Residential Zone, Low Density Policy Area 20 and the properties on the eastern side of Marion Road are either within the Local Centre Zone or Residential Zone, Medium Density Policy Area 19. Given that there are different zone and policy areas in close proximity to each other it makes for a highly varied streetscape with no consistent built form. Some of the commercial uses within the locality include a shop, office, pre-school and a service trade premises. Some of the dwelling types within the locality include, detached dwellings (two storey and single storey), group dwellings and residential flat buildings.

The site and locality are shown on the following maps.





Location Map WeTo/12

WEST TORRENS COUNCIL

Consolidated - 5 May 2016

PROPOSAL

The applicant seeks Development Plan Consent for the following:

- Remove two (2) existing pylon signs, one located in the northeast corner of the site, and the other in the south east corner of the site. The sign north east of the site advertised the prices of petrol sold on site and the name of the petrol filling station and associated business, Coffee Station. The sign located south east of the site details the business names of services offered on the site, such as Coffee Station and Pitstop Hand Wash. Each of the signs measured approximately 4 meters in height from the natural ground level and had a total area of 9.6 metres. The content was displayed on both sides of each pylon sign;
- Installation of one (1) pylon sign with partial internal illumination. The applicant seeks retrospective approval for this sign as it has already been erected. This sign will advertise the prices of petrol sold on the site and the business name of services offered on site, for example, Coffee Station. The top of the sign will be internally illuminated and the numbers of the petrol prices will be illuminated. The total height of the sign will be 6.7 metres from the natural ground level and the total area will be 16.08 square metres. Advertising content will be visible on both sides of the pylon sign;
- Change of message painted on the parapet on the existing building, on the Harvey Street and Marion Road elevation. Harvey Street elevation message changed from "Carwash and Detailing" to "Eat + Drink". Marion Road elevation message changed from, "Eat + Drink + Relax" to "Convenience".
- Install three (3) attached advertisements to the existing building, which will display the name of the petrol filling station. The size of each sign is approximately 1.8 square metres (0.8m X 2.25m).

A copy of the proposal is contained in **Attachment 1**.

NON-COMPLYING

The application is a non-complying form of development, as advertising is listed as non-complying within the Procedural Matters of the Residential Zone. The applicant has provided a Statement of Effect pursuant to Regulation 17 of the *Development Regulations 2008*, refer **Attachment 2**. Should the Panel resolve to approve the application, the concurrence of the Development Assessment Commission is required. Alternatively, should the Panel refuse the application, no appeal rights are afforded to the applicant.

Administration resolved, under delegation, to proceed with an assessment of the proposal. The application is now presented for a decision.

PUBLIC NOTIFICATION

The application is a Category 3 form of development pursuant to Section 38 and Schedule 9 of the Development Act and Regulations. Given that the proposed development is not listed as a form of Category 1 or 2 development in Council's Development Plan or the Development Regulations 2008, it is then considered a Category 3 by default.

Properties notified:	40 properties were sent letters and an advertisement was included in the newspaper (The Advertiser) to advise the public of the application during the public notification process.
Representations:	0 representations were received.

REFERRALS

Nil

ASSESSMENT

The subject land is located within the Residential Zone and more specifically within Low Density Policy Area 20 as described in the West Torrens Council Development Plan. The main provisions of the Development Plan which relate to the proposed development are as follows:

<u>General Section</u>		
Advertisements	Objectives	1, 2 & 3
	Principles of Development Control	1, 2, 4, 5, 6, 7, 8, 9, 10, 11, 14, 15, 16 & 22
Design and Appearance	Objectives	1
	Principles of Development Control	13
Orderly and Sustainable Development	Objectives	1, 2, 3, 4 & 5
	Principles of Development Control	1 & 3

Zone: Residential Zone	
<p><i>Desired Character Statement:</i></p> <p><i>"This zone will contain predominantly residential development. There may also be some small-scale non-residential activities such as offices, shops, consulting rooms and educational establishments in certain locations. Non-residential activities will be complementary to surrounding dwellings.</i></p> <p><i>Allotments will be at very low, low and medium densities to provide a diversity of housing options in different parts of the zone. The range of allotment sizes will support the desired dwelling types anticipated in each policy area, and the minimum allotment sizes shall be treated as such in order to achieve the Desired Character for each policy area and, in turn, reinforce distinction between policy areas. Row dwellings and residential flat buildings will be common near centres and in policy areas where the desired density is higher, in contrast to the predominance of detached dwellings in policy areas where the distinct established character is identified for protection and enhancement. There will also be potential for semi-detached dwellings and group dwellings in other policy areas.</i></p> <p><i>Residential development in the form of a multiple dwelling, residential flat building or group dwelling will not be undertaken in a Historic Conservation Area.</i></p> <p><i>Landscaping will be provided throughout the zone to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer".</i></p>	
Objectives	4
Principles of Development Control	1, 2 & 5

Policy Area: Low Density Policy Area 20	
Desired Character Statement:	
<p><i>"Allotments in the policy area will be at low density, accommodating predominantly detached dwellings and some other dwellings types such as semi-detached and group dwellings. There will be a denser allotment pattern close to centre zones where it is desirable for more residents to live and take advantage of the variety of facilities focused on centre zones. Battleaxe subdivision will not occur in the policy area to preserve a pattern of rectangular allotments developed with buildings that have a direct street frontage.</i></p> <p><i>Buildings will be up to 2 storeys in height. Garages and carports will be located behind the front façade of buildings.</i></p> <p><i>Development will be interspersed with landscaping, particularly behind the main road frontage, to enhance the appearance of buildings from the street as viewed by pedestrians, provide an appropriate transition between the public and private realm and reduce heat loads in summer. Low and open-style front fencing will contribute to a sense of space between buildings".</i></p>	
Objectives	1
Principles of Development Control	1 & 2

QUANTITATIVE ASSESSMENT

The proposal is assessed for consistency with the prescriptive requirements of the Development Plan as outlined in the table below:

DEVELOPMENT PLAN PROVISIONS	STANDARD	ASSESSMENT
ADVERTISEMENT AREA ADVERTISEMENT PDC 8	<p>The total advertisement area on the fascias, parapets, gable ends, windows and other surfaces of buildings should not exceed:</p> <p>(a) 20 per cent of the sides of the building</p> <p>(b) in relation to the front wall of a building, 20 per cent of the area above 3.7 metres or above a canopy.</p>	<p>Total amount of proposed advertising on building Approx. 13sqm = 19%</p> <p>Satisfies</p> <p>* Please note that this is an approximate figure only as the signage is comprised of individual letters and symbols and not a rectangular board as is typical for most signage.</p>

QUALITATIVE ASSESSMENT

Visual Impact

In assessing the merits or otherwise of the application the proposed advertisement is considered to be appropriate for the site, considering the existing land use of the site and the fact that it relates to the use of the site, the locality, the total amount of advertising on site and the design and appearance of the proposed advertising.

The existing land use is a petrol filling station with other amenities such as a car wash and shop. As explained within the Proposal section of this report, there were previously two pylon signs that existed on the site, and in addition to that, prior to the reconstruction of the canopy (as approved in DA 211/1261/2016), the previous canopy included advertising on all sides, which displayed the name of the petrol filling station. Therefore, the application will predominantly be for the replacement of signage that previously existed on the site, and in fact reducing the amount by one. The reduction of advertising on the site is considered to be favourable as it helps to avoid the site becoming cluttered by advertising. (Advertisements Principle of Development Control (PDC) 2 & 16).

All of the advertisement proposed will contain content that is related to the existing use of the site and no third party advertisements. The appearance will be coordinated in terms of colour and messages, and the message will be simplified and clear, as for example each advertisement or section of the advertisement in regards to the pylon sign will only contain one or two words (Advertisements PDC 1 & 11).

The subject land being located adjacent an arterial road and in close proximity to another non-residential zone is within a locality containing eclectic land uses, which signage can reasonably be expected. In this locality, the proportions of the pylon sign are not considered to be dominating, and will be complementary to the commercial and residential nature of the locality (Advertisements PDC 1 & 16).

Safety

The design and placement of the advertising will not impair the safety of road uses, pedestrians or diminish the amenity of surrounding residential properties.

The design does not include any component that will not move, flash, blink or rotate, which may be seen to cause driver distraction or be disturbing to pedestrians and residents. The proposal will not include the installation of any advertisement on or over a road or footpath, and the subject land is not in close proximity to any traffic signals. Furthermore, the subject land is not adjacent to an arterial road that is permitted the speed 80km/h or more. The speed limit of Marion Road is 60km/h (Advertisements PDC 14, 15 & 22).

As only a moderate component of the proposed signage will incorporate an illuminated display it is considered that any light spill resulting from the sign will not be such that it will detrimentally affect the amenity of nearby residential premises which are located a minimum of 20 metres from the location of the proposed signage. In addition, the LED display on the proposed freestanding sign is intended only to display the current fuel price, and the internally illuminated portion will be for highlighting the business name, both components which will not be frequently changed (Advertisements PDC 14).

SUMMARY

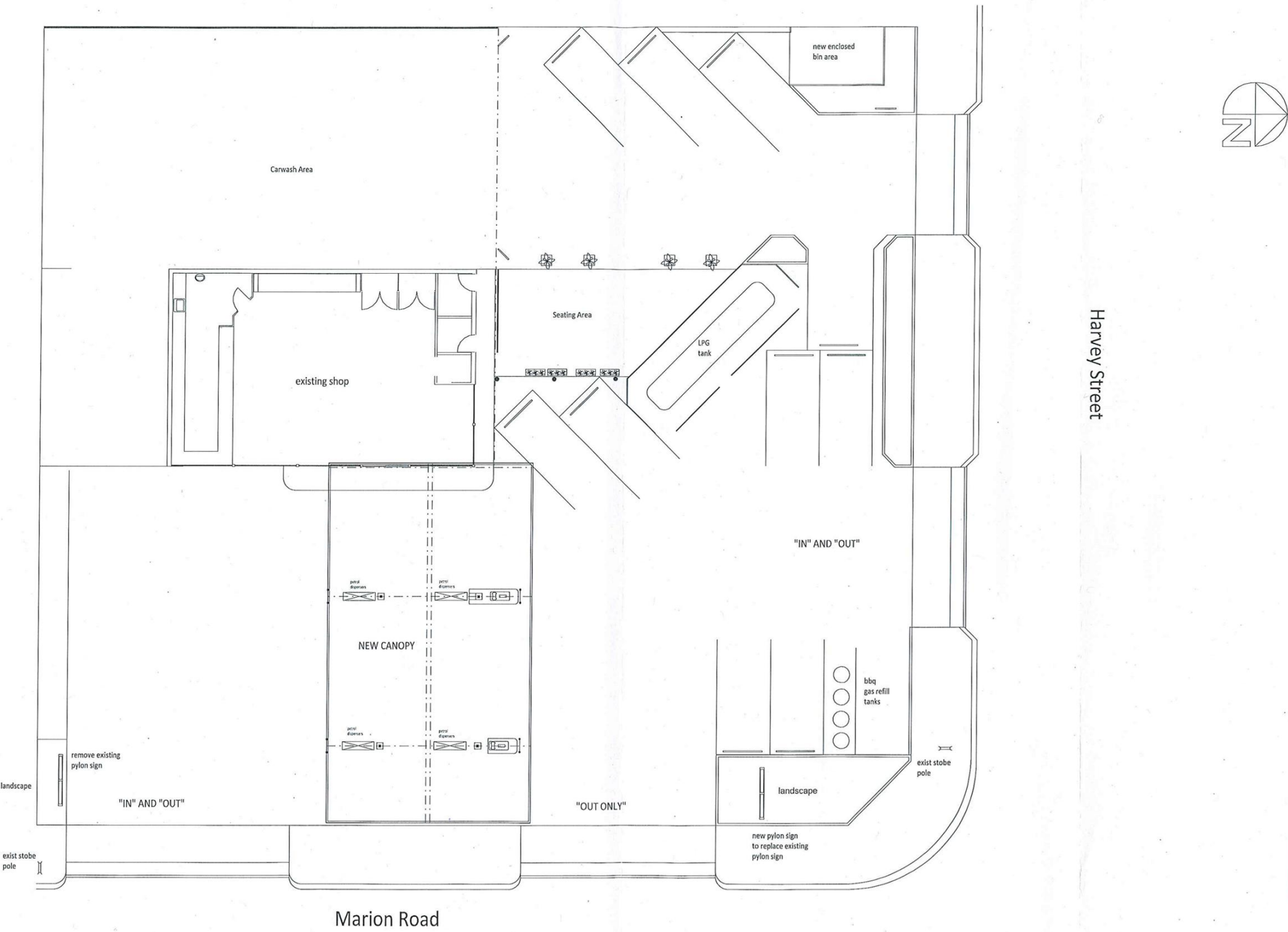
While advertisements are generally discouraged within the Residential Zone, the proposed signage is ancillary to an existing non-residential use and will not result in any unreasonable impacts upon the owners or occupiers of residential premises within the locality. The proposed sign is of a size that is compatible with the site of the development and is not entirely at odds with the streetscape character given that Marion Road is a secondary arterial road and within close proximity to existing commercial land uses.

Having considered all the relevant Objectives and Principles of the Development Plan, the proposal is considered to be not seriously at variance with the Development Plan.

On balance the proposed development sufficiently accords with the relevant provisions contained within the West Torrens Council Development Plan Consolidated 5 May 2016 and warrants Development Plan Consent.

Attachments

- 1. Plans and Details**
- 2. Statement of Effect**

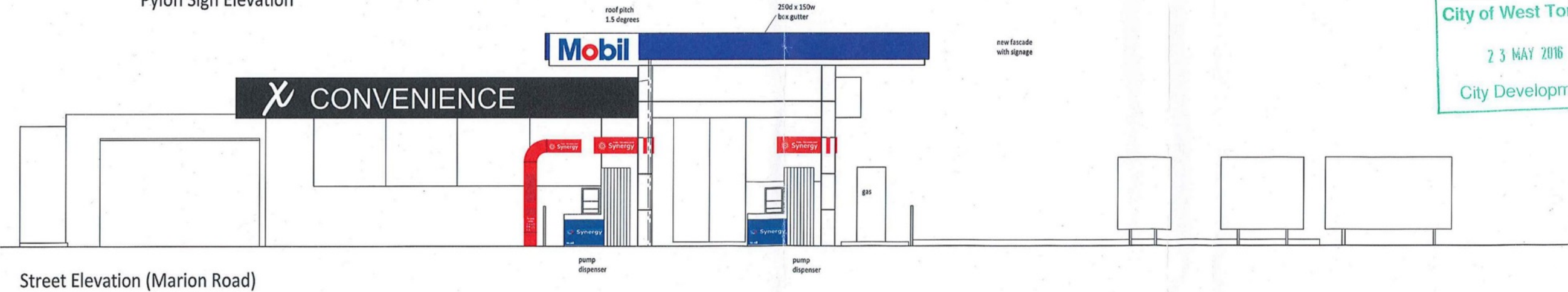
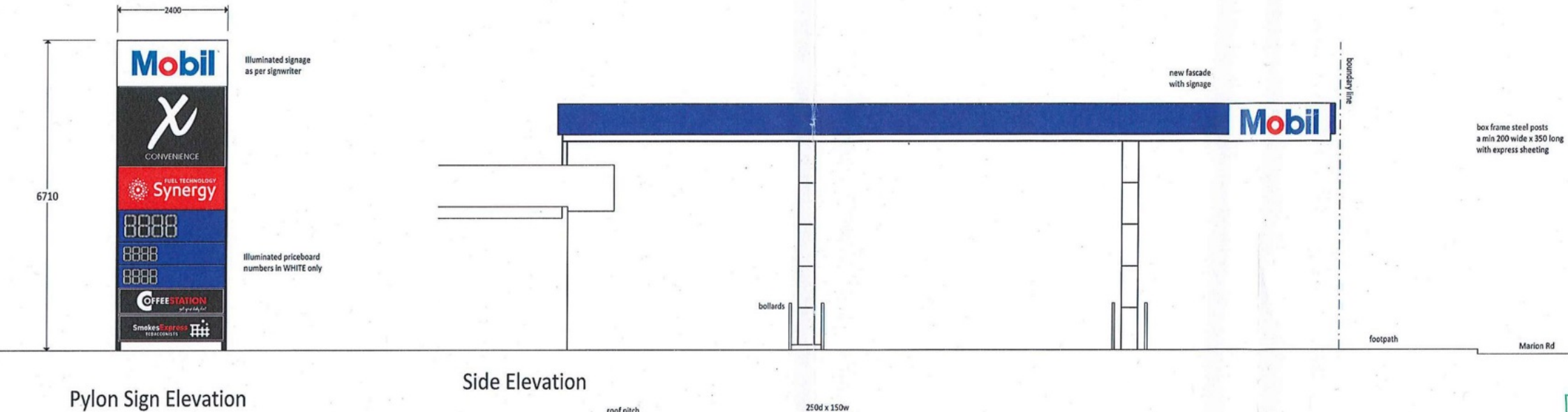
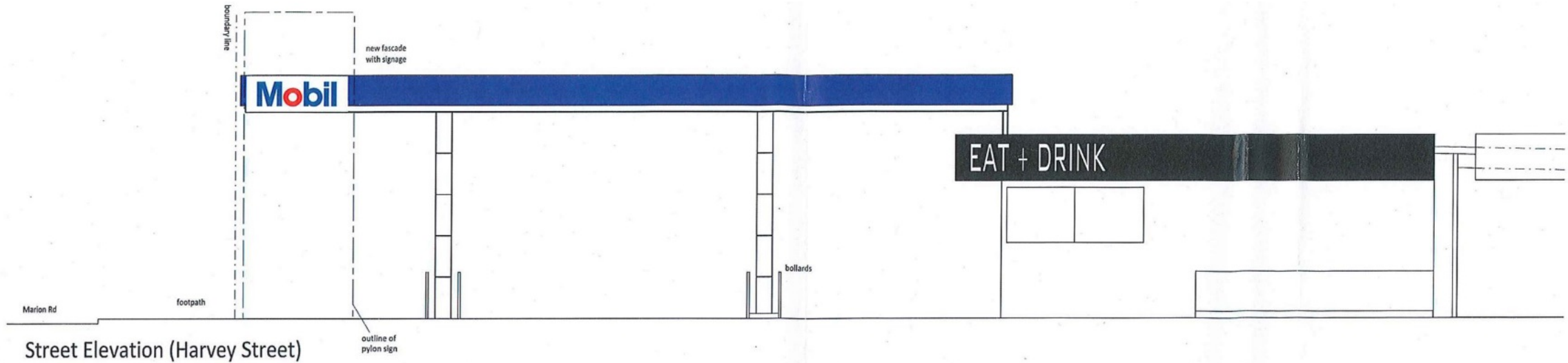


SCANNED
23 MAY 2016
REF No 3988140

City of West Torrens
23 MAY 2016
City Development

Site Plan

<p>Notes:</p> <p>No changes are to be made to this drawing without approval from the owner and/or designer.</p> <p>Assess under the National Construction Codes of Australia</p> <p>All measurements to be checked on site before any construction work commence or are undertaken</p> <p>All fence to be 1800 high colorbond (unless stated).</p> <p>Stormwater to be directed via underground sealed 90mm pvc pipe to proposed rainwater tank (min 1000 litres) then to street water table via underground sealed 90mm pvc stormwater pipe with flexible connections where required.</p> <p>Downpipes to be 12m(max) spacing. Rainwater tank to be plumbed via automated.</p> <p>Reticulation system to wc's and/or laundry cold water taps.</p> <p>There are no brush fences within 3m of proposed dwelling.</p>	<p>When building to the boundary it is the owners responsibility to correctly identify property boundaries. Ground are to be confirmed on site or by engineer in conjunction with any survey plans available.</p> <p>Subject site has connection to sewer, potable water supply, gas and electricity</p> <p>To the best of my knowledge, the site has not been used for purposes in the past or had materials stored upon it that may have caused site/soil or groundwater contamination.</p> <p>Vehicle access to be provided via existing driveway invert and crossover, and in accordance with Council requirements.</p> <p>Buildings/site to be used as private residence</p> <p>No regulated or significant trees existing on the subject site or adjoining land that may be affected by proposed development.</p> <p>No referrals required to any State Agency under Section 37 of the Act, pursuant to Schedule 8 of the Regulations.</p> <p>There are no easements, encumbrances, caveats or land management agreements affecting the subject land.</p>	<p>JOB</p> <p>new canopy and pylon sign to replace existing</p> <p>AT</p> <p>294 Mario Road Netley</p> <p>FOR</p> <p>Andrash P/L</p>	<p>Area Notes:</p> <p>site: 1534.56 sq m</p> <p>shop: 133.89 sq m</p> <p>car wash: 298.63 sq m</p> <p>new canopy: 161.59 sq m</p> <p>pylon sign: 6 sq m</p> <p>old canopy: 62 sq m</p>	<p>amendments:</p> <p>PAGE: 1 of 3</p> <p>JOB NO: 06/16</p> <p>SCALE: 1:200</p> <p>DATE: 22/02/16</p> <p>Copyright</p>	<p>Angelyn Building Designs</p> <p>ABN 48 756 683 508</p> <p>55 Waterhouse Rd.,</p> <p>South Plympton SA 5038</p> <p>Ph: 0438 345 946</p> <p>email: angelyn@internode.on.net</p>
---	---	--	--	--	--



City of West Torrens
23 MAY 2016
City Development

<p>Notes:</p> <p>No changes are to be made to this drawing without approval from the owner and/or designer.</p> <p>Assess under the National Construction Codes of Australia</p> <p>All measurements to be checked on site before any construction work commence or are undertaken</p> <p>Provision for the corrosion protection for the structure to comply with BCA Table 3.4.4.2 for buildings within 1 km of breaking surf - BCA-P2.1</p> <p>Confirmation that the glazing and glazing assemblies will be selected and installed in accordance with AS1288-2006 and for AS2047-2014 as appropriate - BCA-P2.1.</p> <p>Downpipes to be 12m(max) spacing. Rainwater tank to be plumbed via automated.</p> <p>Smoke detectors to be interconnected and hardwired 240v with battery backup as required (AS 3786).</p>	<p>Underfloor area chemical spray barrier termite treated with a durable notice affixed to the premises stating method and date of treatment in accordance with AS3660.1 and minimum 75mm exposed slab edge to finished perimeter paver/concrete path level.</p> <p>All window frames and glazing in accordance with manufacturer's specification and to AS1170, AS1288 and AS2047 and energy rating assessment.</p> <p>All construction, timber framing, tie downs and fixings to be in accordance with the Building Code of Australia, the timber framing code AS1684.2 2010.</p> <p>All wet area details to be in accordance with AS3740-2004, BCA - table 3.8.1.1 with flexible connections to sewer and drainage pipes where required.</p> <p>WC door to be removable (or sliding cavity door).</p> <p>Reticulation system to wc's and/or laundry cold water taps.</p> <p>Waterproofing of external balcony to be in accordance with AS4654.1 & 2 - BCA-P2.2.2</p>	<p>0 1.5 4.5 10.0</p> <p>JOB new canopy and pylon sign to replace existing AT 294 Mario Road Netley FOR Andrash P/L</p>	<p>Amendments AS/NZS 4455.1 changes as per design request</p>	<p>PAGE: 3 of 3 JOB NO: 05/16 SCALE: 1:100 DATE: 17/4/16 Copyright</p>	<p>Angelyn Building Designs ABN 48 756 683 508 55 Waterhouse Rd., South Plympton SA 5038 Ph: 0438 345 946 email: angelyn@internode.on.net</p>
---	---	---	---	--	---

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

**AcroPLAN[SA] Pty Ltd**

ACN: 605 956 232

ABN: 69 605 956 232

PO Box 1146 GLENELG SOUTH SA 5045

Ph: 0416839459

E: acroplansa@gmail.com

STATEMENT OF EFFECT

PROPOSED:

REMOVAL OF TWO INTERNALLY ILLUMINATED PYLON ACRYLIC ADVERTISEMENT SIGNS WITH HOARDINGS AND REPLACEMENT WITH A SINGLE PYLON INTERNALLY ILLUMINATED ACRYLIC ADVERTISEMENT SIGN WITH CONCEALED HOARDING TO 6.5m IN HEIGHT ASSOCIATED WITH EXISTING INTEGRATED SERVICE STATION COMPLEX AND CAR WASH.

At: Allotment 40 (No. 294) MARION ROAD, NETLEY

Date: 20 March 2017

Author: Andrew Cronin MPA – Director, AcroPLAN[SA]

Prepared for Andrash Pty Ltd trading as "X Convenience"

Council Reference:

© AcroPLAN[SA]

All rights reserved; these materials are copyright. No part may be reproduced or copied in any way, form or by any means without prior permission other than by the relevant authority and relevant State agencies for legitimate purposes associated with the assessment of the proposal herein described.

The Ideas, concepts and methodology set out and described in this document are and remain the property of AcroPLAN[SA] and are provided to the client in confidence. They may not be used or applied by the recipient or disclosed to any other person without the prior written consent of AcroPLAN[SA].

CONTENTS

	Page
Title Page	1
Contents	2
Introduction	3
 1. Subject Site & Locality (inc. site photos)	 4-10
 2. Background	 10-11
 3. Description of Proposed Development	 12
 4. Relevant Provisions of the Development Plan	 12-13
 5. Assessment against the Relevant Development Plan Provisions	 13-20
(1) Land Use & Form of Development	
(2) Visual Appearance of Built Form	
(3) Traffic Safety	
(4) Advertisements	
(5) Interface with Adjoining Uses	
 6. Assessment of the Social, Environmental and Economic Impacts	 20-21
 7. Procedural Matters & Referrals	 21
 8. Conclusion	 21-22

INTRODUCTION

AcroPLAN[SA] provides this report on behalf of Andrash Pty Ltd ("The Applicant"), in relation to the proposed advertising signage at their existing integrated service station complex and car wash at 294 Marion Road, Netley. Development Application No 211/557/2016.

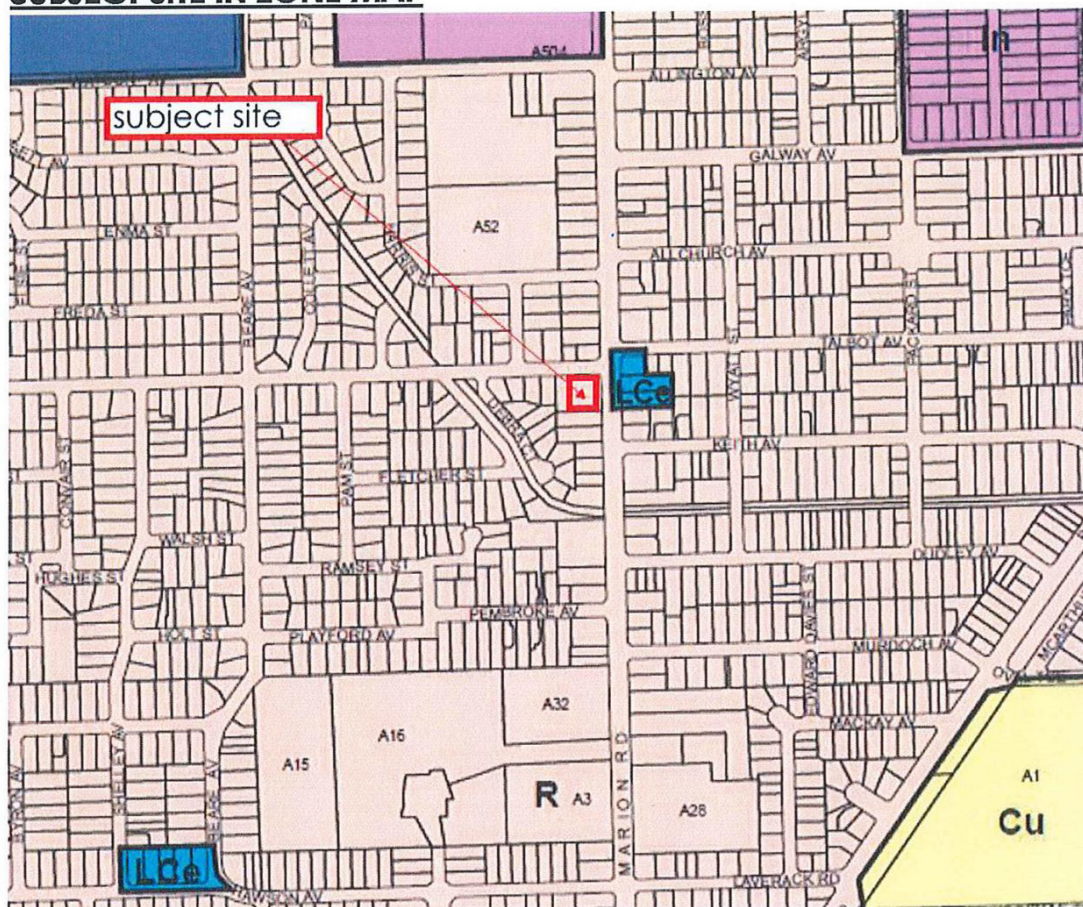
This report is prepared following a review of the following documentation:

- Certificate of Title Volume 5105 Folio 144.
- Proposal plans prepared for the applicant by Angelyn Building Designs dated July 2016
- *Metropolitan Adelaide Road Widening Plan* maps
- Aerial imagery and Google streetview imagery of the site dated back to 2008
- DPTI 24 hour average daily traffic counts contained in Map MT/7 www.dpti.sa.gov.au/traffic_volumes#metro

In preparing this planning report, I confirm that I have carried out all of the necessary investigations including a review of the above information and documentation; an inspection of the site and the locality, and consideration to the pertinent Development Plan provisions of the City of West Torrens Development Plan consolidated 5 May 2016 (the relevant Development Plan against which this proposal is to be assessed), the *Development Act 1993* and its *Regulations 2008* and any other matters relevant to the proposal.

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

SUBJECT SITE IN ZONE MAP

City of West Torrens Map WeTo/12 - Zoning

1.0 SUBJECT SITE AND LOCALITY

The subject site is entirely located within land known as lot 40 (No. 294) Marion Road, Netley. The subject site comprises approximately 1540 square metres (m²) of land contained in a single regular shaped allotment with an arterial road frontage and a frontage to a side street, Harvey Avenue. The land has historically been used for commercial purposes, and has been a service station and car wash for a long period of time, with the petrol filling station existing on site for at least 30 years.

The primary frontage of the site is to Marion Road, and this has a width of some 42 metres. Two access points are contained on the Marion Road frontage and there are another two access points on the frontage to Harvey Avenue. While not formally line-marked as such, the access arrangements are generally entry via the southern access point and egress via the northern access point on Marion Road.

The site is predominantly flat and hard sealed with bitumen or concrete, and has a control building of some 140m², forecourt canopy of about 60m² area, a car wash with associated canopies at the rear and southern side comprising approximately 280m²

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

roofed area wrapping around the control building, a series of fuel bowzers in two rows, an LPG storage tank, raised deck outdoor seating area north of the control building, a number of line-marked and informal car parking spaces and most pertinent for this application, two main internally illuminated pylon signs advertising the fuel pricing and facilities and services available on site, both located on the Marion Road frontage.

The site can be seen in imagery from April 2016 as below:

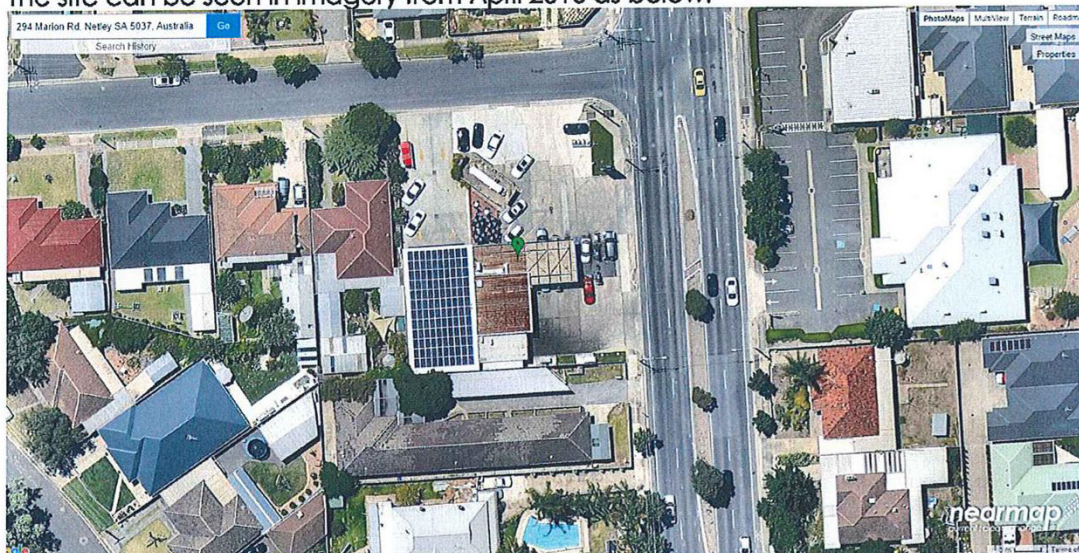


Figure 1: Nearmap Aerial Photograph 21 January 2017 (source: nearmap.com)

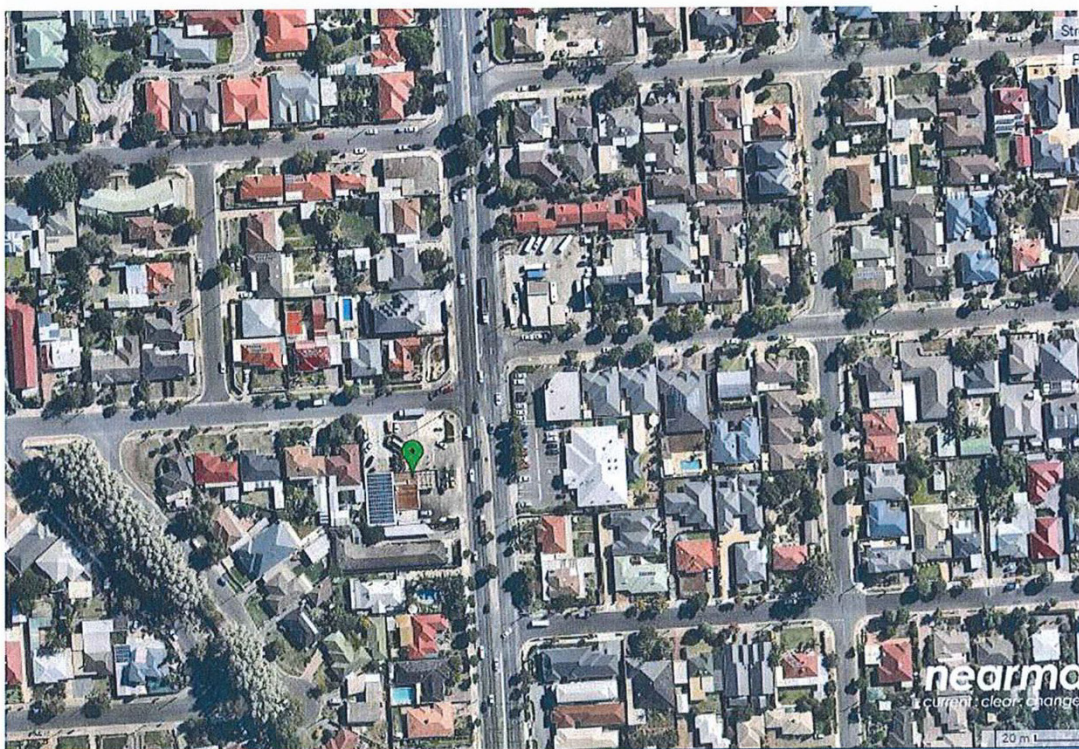


Figure 2: Nearmap Aerial Photograph March 2016 (source: nearmap.com)

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

The site is located in a context of varying uses, which includes residential uses each side of Harvey Street in the locality and on Marion Road to the south of the site, as well as dwellings on the eastern side of Marion Road south of the site and further north from Talbot Road. Other non-residential uses are also notable in the locality, with a bus depot being on the northern corner of Talbot Road, Marion Road intersection in North Plympton diagonally opposite, an office containing a real estate agent on the southern side of the same T junction and directly opposite, an early learning centre on the eastern side of Marion Road. The site in the context of the locality can be seen on the aerial image below from nearmap.com, dated 15 March 2016 and a close-up image more recently taken in January 2017.

Some non-residential features of the locality are located in a small Local Centre Zone, which is directly opposite the site, which can be seen in Map WeTo/12 in the current Development Plan. This includes the real estate agent's office and the childcare centre. However, like the subject site, the bus depot diagonally opposite is located in the Residential Zone.

The site is largely predominated by hard sealed areas, and advertising signage is contained in two pylon signs located on the Marion Road frontage of the site. These can be seen in Google Streetview from July 2016, as follows:



Figure 3: Google Streetview Photograph October 2015 (source: maps.google.com)

The site contains very little landscaping at present and no trees. There are no regulated or significant trees on the site or adjacent the site.

The verge in Marion Road in front of the site is paved with bitumen and there are no street trees or other Council infrastructure within the verge. There are however two 11kV reinforced concrete "Stobie" poles for electricity supply, being approximately adjacent to the site's north-eastern and south-eastern corners respectively.

The site is located in the Residential Zone of the City of West Torrens Development Plan consolidated 5 May 2016 (the relevant Plan for this application), and within the Low Density Policy Area 20. It is at the edge of the Policy Area, having direct interface with another Zone, the Local Centre Zone, and the Medium Density Policy Area 19 of the Residential Zone.

The section of Marion Road between Mooringe Avenue, Plympton (to the south) and Galway Avenue, Marlestone (north) carries on average 33,300 vehicles per day (vpd) in a 24 hour period, according to the Department for Planning, Transport and Infrastructure (DPTI) Annual Average 24 hour two way traffic counts updated 14/9/15 (source

2016

The closest signalised junctions on Marion Road are the junctions with the abovementioned roads, and there are no pedestrian actuated signalised crossings in the locality. There are no traffic lights within 100m of the site.

The relevant part of Marion Road, Netley has not been identified for future road widening purposes in accordance with the *Metropolitan Adelaide Road Widening Plan [MARWP] Act 1972*.



Nearby residential development comprises mostly post war housing stock interspersed with piecemeal infill developments generally in the form of 'two for one' land divisions and construction of brick veneer dwellings with integral garaging and higher than traditional site coverage. The dwellings to the south are comprised of 5 single storey units in a residential flat building with common driveway and carport along the northern side, adjacent to the subject site.

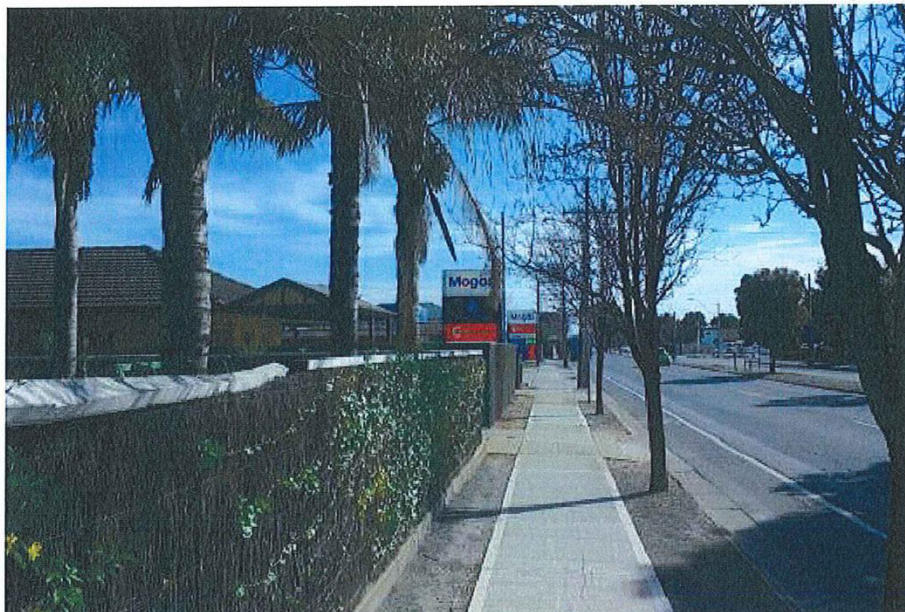
It is noted that the site is approximately 6km west of the Adelaide CBD and there are no major shopping facilities in the surrounding area. The site and locality is serviced with public transport, including bus services along Marion Road and along Harvey Street.

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

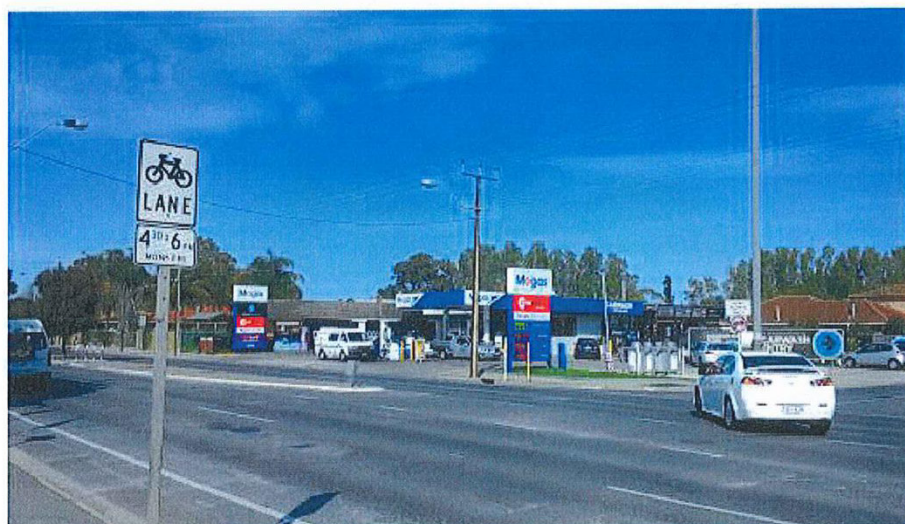
2016

The amenity level of the locality is considered to be quite low principally due to the arterial road environs with high traffic volumes, the lack of softening vegetation on the subject site and in the locality and the commercial nature of the site and nearby sites, which are mostly dated and lack street appeal. The childcare centre on the opposite side of Marion Road is an exception, with a relatively well vegetated car park providing some softening element to the landscape.

SITE PHOTOS



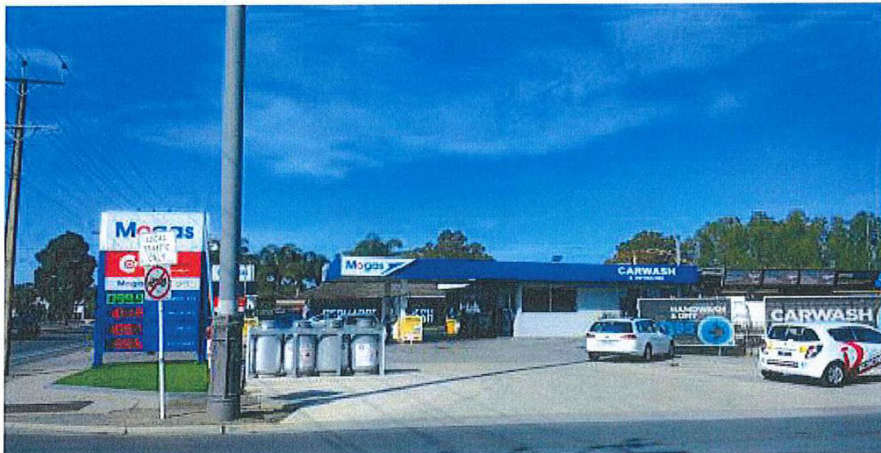
Pedestrian approach towards the site from the south - Marion Rd



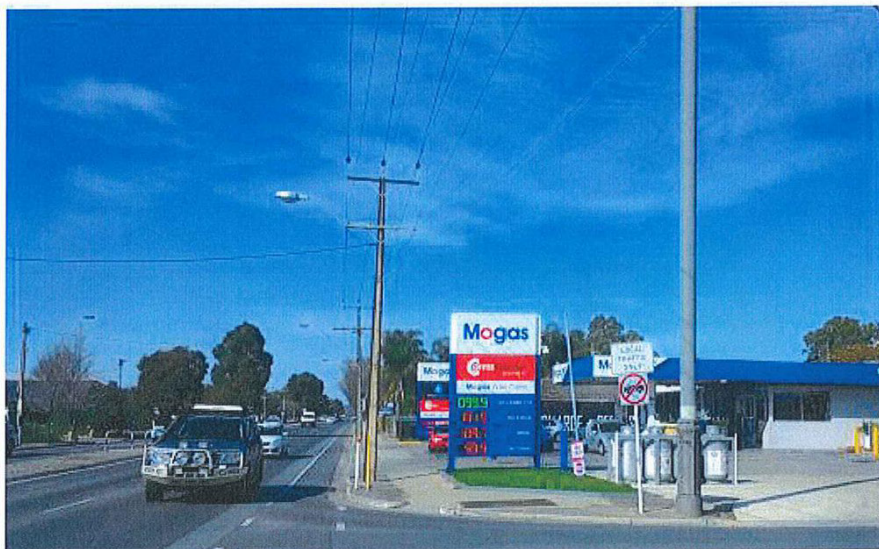
Subject site as viewed from eastern side of Marion Rd

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

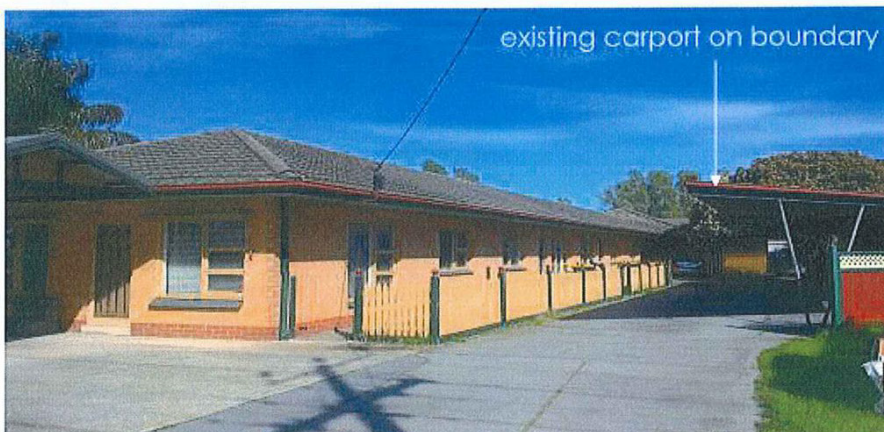
2016



View of Site from the north - Harvey Avenue



View of site from the north - Cnr Harvey Ave and Marion Rd



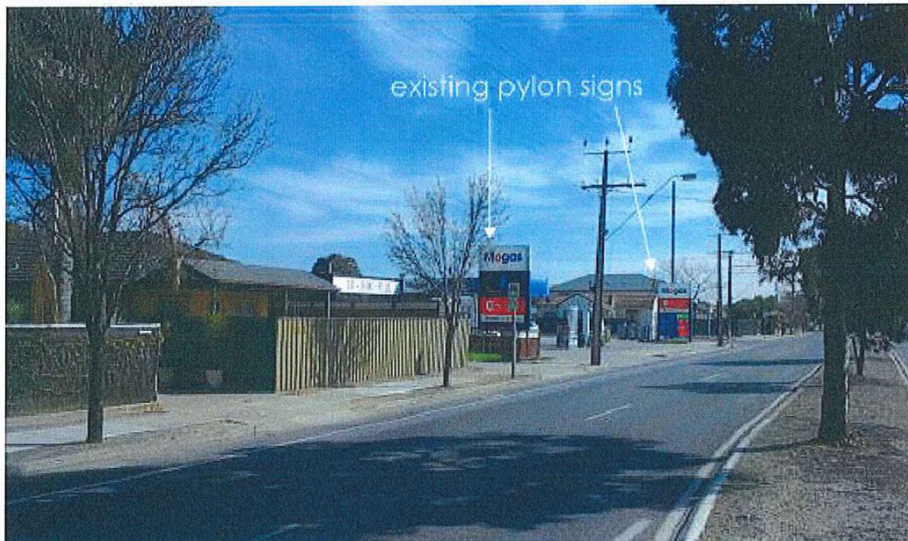
View of adjacent units - south of site on Marion Rd

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016



View of existing Illuminated sign on southern boundary adj. units



Approach to site from the south - Marion Road



Bus Depot Adjacent the site - Cnr Marion Rd and Talbot Rd

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016



Local Centre with RE Agent and Childcare Centre - Marion Rd



X Convenience Cross Road – shows similar signage to what is proposed

2.0 BACKGROUND

Andrash Pty Ltd now operate over 10 similar outlets within South Australia in metropolitan Adelaide and in regional South Australia (SA). Adelaide outlets include Croydon Park, South Plympton, Netley, Brooklyn Park, Angle Vale, Semaphore and Elizabeth Downs, two of which are within the West Torrens Council region. Andrash integrated service station developments do not include fast food franchises, dog washes, gambling facilities. Aside from selling of fuel, their retail focus is on general convenience grocery items, essential automotive products (such as oils, batteries and coolants), pre-made food items (such as baguettes, rolls, sandwiches as well as pastry items, pies, pasties and sausage rolls), frozen yoghurt as well as good quality coffee.

Despite the Zoning, the use of the site upon which this proposal is located, has not been residential in nature for at least 30 years but likely to be much longer. The land use since at least the 1980s has been as a petrol filling station and car wash and in 2009 the former Liberty service station was taken over by Andrash, under the "Mogas" branding. Andrash has now parted ways with Mogas and this has resulted in the rebranding of a number of outlets already. This proposal comprises rebranding the service station in the "X Convenience" and "Mobil" fuels livery as well as consolidating the pylon advertising signage to a single pylon sign at the north-eastern corner of the property, in the same place as the higher of the two existing pylon signs.

The site is also subject to a separate application for a new forecourt canopy, which has been granted Development Plan Consent by Council. Two of the illuminated signs are proposed to be erected on this canopy. The application reference for that application is 211/1261/2016, which was granted consent 9th January 2017.

3.0 DESCRIPTION OF PROPOSED DEVELOPMENT

The proposed development comprises an internally illuminated pylon sign of approximately 1.5m higher than the existing sign towards the north-eastern corner of the site, with dimensions of 6.7m high x 2.4m width and removal altogether of the 4m high pylon sign adjacent to the southern side boundary near the site frontage and adjacent to residential units, as well as illuminated signage on the approved replacement canopy, yet to be constructed. Some incidental acrylic signage is also proposed on the control building, in white lettering on black background.

There is no change to the existing operations of the site proposed. The site use would not neatly fit the definition of petrol filling station and car wash. Signage as proposed will only advertise fuel pricing, and products or brands directly affiliated with the operations on the site. There is no third party advertising proposed.

4.0 RELEVANT PROVISIONS OF THE DEVELOPMENT PLAN

The land is situated within the City of West Torrens and therefore requires assessment against the provisions of the City of West Torrens Development Plan consolidated 5 May 2016.

The following provisions are considered to be most relevant to an assessment of the proposed development:

Residential Zone

- Objectives: 1, 4
- Principles: 1, 2, 3
-

Residential Low Density Policy Area [P.A. 20]

- Objectives: 1

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

- Principles: 1, 2

General*Advertisements*

- Objectives: 1, 2, 3
- Principles: 1, 2, 3, 4, 5, 7, 8, 9, 10, 11, 12, 14, 15, 16, 17

Design and Appearance

- Objective: 1
- Principles: 1, 13, 14

Interface between Land Uses

- Objectives: 1 & 2
- Principles: 1 & 2

Orderly and Sustainable Development

- Objectives: 1, 3
- Principles: 1

5.0 ASSESSMENT OF PROPOSAL AGAINST RELEVANT PROVISIONS

For the purposes of this section, only what I consider the most relevant provisions are referred to at the beginning of each sub-heading. Where there are omissions, the provisions are either considered irrelevant for the assessment, or that the issues surrounding the provisions are not significant matters in terms of the assessment for the development proposed, within its context.

5.1 Use of Land and Form of Development

The subject land is located within the Residential Zone and Low Density Policy Area 20, hereafter referred to as PA 20. The zone generally contemplates typical residential uses and single to two storeyed built form as espoused in Residential Zone Principle of Development Control (PDC) 1 and the Desired Character Statement. The infill objectives of the Policy Area consider that infill development of low to medium density be concentrated in areas within close proximity to centres, while otherwise the continuation of low density forms of residential development is envisaged.

It is noted that the land use considerations of the proposal are largely irrelevant for the purposes of assessment of this proposal. The site has a long history of being used for non-residential purposes. At least for the last 30 years the site has had some form of petrol filling station use. Naturally, signage in some form has always existed on the site in conjunction with the rightful existing use.

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

While Procedural Matters of the Residential Zone stipulate all forms of advertisement and/or advertisement hoardings as non-complying development, the proposal does not seek to establish additional signage, but rather consolidate material shown on two pylon signs onto a single, larger pylon sign at the north-eastern corner of the site, away from the nearest residences. Additionally, two internally illuminated signs are proposed on the new canopy to be constructed. These are purely in the Mobil livery, akin to what can be seen on the Cross Road outlet, pictured earlier in this report. They more or less take the place of similar signs on the control building, which will be removed in the construction of the canopy. Therefore, notwithstanding the non-complying classification, it is considered that the proposal has merit and warrants strong consideration.

In terms of advertising signage, it is considered that the signage is commensurate with the commercial character of the primary use of the site, without being detrimental to the amenity of nearby residential property. Removal of the southern illuminated pylon sign and hoarding will improve the amenity of the nearest neighbours.

The fact that the advertising signage supports a lawfully existing use, with use rights that predate the existence of planning controls in this State must be weighted accordingly.

The proposal includes consolidation of the material contained on two separate pylon signs onto one single pylon sign located at the furthest point on the site from the nearest residential occupants. This is considered to be an overall improvement to the presentation of the site in its context and represents a better outcome for neighbouring occupants than presently exists. The sign on the southern boundary of the site has existed since at least 2009 and will be removed as part of this application, thus reducing the overall impact of advertising signage on adjacent neighbouring properties.

While ordinarily a proposal including such advertising signage may be considered inappropriate in a Residential Zone, it should be noted that the context is far from purely residential, and the section of Marion Road carries over 33'000 vehicles per day, making the baseline conditions of the environment relatively poor.

The subject land is located within the Residential Zone. This zone encourages varying types of residential development as well as small scale non-residential uses that benefit the local community, such as community centres, consulting rooms and small scale shops, in accordance with Principle 1 of the Residential Zone. The existing service station benefits the local community by providing easy access to day to day convenience and essential items and provides a small scale café seating area, which caters primarily as a waiting area for those having their car washed.

RZ Principle 4 states:

Non-residential development such as shops, schools and consulting rooms should be of a nature and scale that:

- (a) serves the needs of the local community
- (b) is consistent with the character of the locality
- (c) does not detrimentally impact on the amenity of nearby residents.

Signage plays an important economic role for businesses in capturing passing trade and making any would-be customers aware of the services available on the site. It is reasonable to expect that a long established commercial use will have advertising

signage, particularly in an arterial road environment, whether or not it is contained in the Residential Zone under current Development Plan policy. A degree of acknowledgement must be given to the historical arrangements on the site and existing use rights that the site enjoys, when considering the level of tolerance for advertising signage in the Residential Zone.

The proposed development seeks primarily to replace existing signs with new signage, which is consistent with the new corporate branding and purely advertises goods and services available on the subject site. While there are a number of signs proposed, careful consideration has been given to the interface with the nearest receivers of potential impact, neighbouring residential property. The signs primarily face the arterial road where these are illuminated, while canopy signage is integrated into the fascia of the new canopy, without being excessive. The signs are considered to be consistent with the architectural theme and branding of the subject site, and will not appreciably impact on the amenity of the locality, which is characterised by non-residential uses despite the Zoning.

5.2 Visual Appearance of Built Form

The subject land is highly visible, in that it has extensive road frontage and exposure to Marion Road and Harvey Avenue. The proposed advertising signage primarily consolidates two pylon signs into a single taller pylon sign located towards the north-eastern corner of the site. There are fascia signs on the forecourt canopy proposed, commensurate with the "Mobil" corporate branding seen at other outlets.

The canopy as approved, is not located close to the boundary of residential property and the signs contained on the canopy are internally illuminated light box signs. The lighting emitted from these is of relatively low intensity and does not cause appreciable overspill from the site, though is designed such that the signs can be seen from around 200m on approach from each direction on the arterial road.

It is considered that the proposed advertising signs will complement and enhance the existing buildings, and will be in accord with the overall intent of the Development Plan in that it does not represent a proliferation of signage, does not cause unreasonable light overspill on adjacent residential properties, is of a scale that is commensurate with buildings on the site and conceals supporting structure. The contemporary signage design is furthermore of a high design standard, befitting its location on a reasonably prominent corner site with good exposure to passing traffic.

5.3 Traffic Safety

The development is sited approximately 240 metres from the nearest signalised traffic junction to the north, being the junction of Galway Avenue and Marion Road. Most of the illuminated signs proposed on site are relatively small and only visible generally from directly in front of the site. The pylon sign located at the front of the site has a mixture of illuminated display, with fuel pricing to consist of LED numbering in white and red, while other components on the sign will be in the form of internally illuminated translucent box signage, with low intensity fluorescent or LED globes.

The pylon sign is sited sufficiently far from the nearest signalised traffic junction to not have any perceivable impact on the safety of road users. The sign does not incorporate

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

any flashing, moving, scrolling, or otherwise moving display, and the lighting contained in the sign is not of an intensity that would cause confusion or distraction to road users.

The location and positioning of signs in relation to access points does not jeopardise safe and convenient access to the site, and is consistent in all respects with the relevant provisions of the Development Plan, in that sightlines for vehicles entering and existing the land, and also turning onto Marion from Harvey Avenue, are not affected by the proposed signage on site. In fact the existing pylon sign is in the same location and it is noted that there has been no issue in this regard, with traffic safety at the junction.

5.4 Advertisements

General, Advertisements

OBJECTIVES

- 1 Urban landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
 - (a) consistent with the predominant character of the urban landscape
 - (c) coordinated with and complement the architectural form and design of the building they are to be located on.
- 2 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
 - (a) clutter
 - (b) disorder
 - (c) untidiness of buildings and their surrounds
 - (d) driver distraction
- 4 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 5 Advertisements and/or advertising hoardings should:
 - (a) be completely contained within the boundaries of the subject allotment
 - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
 - (c) not obscure views to vistas or objects of high amenity value

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY2016

- 7 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building.
- 8 The total advertisement area on the fascias, parapets, gable ends, windows and other surfaces of buildings should not exceed:
 - (a) 20 per cent of the sides of the building
 - (b) in relation to the front wall of a building, 20 per cent of the area above 3.7 metres or above a canopy.
- 9 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 10 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 12 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.
- 14 Advertisements and/or advertising hoardings should not create a hazard by:
 - (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
 - (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
 - (c) distracting drivers from the primary driving task at a location where the demands on driver concentration are high
 - (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).
- 15 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.
- 16 Free standing advertisements and/or advertising hoardings should be:
 - (a) limited to only one primary advertisement per site or complex (except where a site has multiple road frontages, in which case, one freestanding advertising hoarding per road frontage is appropriate)
 - (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

- 17 Except where otherwise specified in a particular zone, policy area or precinct, free standing advertisements should be designed within the following parameters:

Zone/Policy Area	Advertisement Area	Additional Ad. Area/m of site frontage with a public road (m2)	Maximum Height
District or Neighbourhood Centre Zone	6	0.15	9
Arterial Roads Policy Area 1 Local Centre Zone	4	0.1	7
Other Non-residential zones	2	0.1	6

- 18 Free standing advertisements and/or advertising hoardings for multiple-business tenancy complexes should:

- incorporate the name or nature of each business or activity within the site or complex in a single advertisement
- be integrally designed and mounted below the more predominant main complex or site identity advertisement.

An internally illuminated 6.7m high monolith pylon sign is proposed to be located towards the north-eastern corner of the Marion Road frontage of the site, to face north and south to address passing traffic. The sign has supporting hoardings concealed by the external cladding. The sign is higher than the highest pylon signage envisaged in non-residential Zones outside of District, Neighbourhood Centre and Local Centre Zones, Policy Area 1 Arterial Roads, being some 700mm higher than the 6m considered appropriate by Principle of Development Control 17 of the General, Advertisements section of the Development Plan.

While the sign is indeed higher than the associated control building and at variance with PDC 17 above, the proposal replaces two existing pylon signs, both of which are illuminated and one of which is directly abutting the closest residential neighbour, almost on the southern boundary and with its largest face directed such that it is easily seen from the adjacent residential properties and could perceivably create undesirable light spill. The overall combined height of the two existing signs is over 9 metres, and therefore there is a net reduction in surface area of signage with the consolidation to one pylon sign. In this context and that the sign is located away from the boundaries to residential property the proposal is considered appropriate, notwithstanding being at variance to the quantitative provisions in PDC 17.

While the "X Convenience" livery is within a colour lightbox that includes red colour sections, it is not considered that this would be in any manner confused with traffic signals.

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY2016

The critical element is that no red, green, blue or amber LED text or lights will be accommodated on the signage, which can be confusing for approaching motorists approaching signalised intersections. The sign will also feature no flashing, moving, scrolling or other types of displays potentially distracting to drivers. The sign features only white LED text for fuel pricing on a black background, other than the discounted fuel pricing being green or red lettering, however the distance to the nearest signalised junction is over 200 metres and therefore it is highly unlikely to cause any confusion with traffic signals.

The pylon sign for the site is located quite close to the egress driveway on Marion Road, however does not obscure sightlines as it is north of the driveway and traffic goes north along Marion Road adjacent the site, requiring drivers of existing vehicles to look south upon egress.

The proposed pylon sign is setback from the front boundary to be outside of the prescribed clearances of power lines. This ensures also the sign does not obstruct vehicle sightlines for vehicles exiting Harvey Avenue onto Marion Road. It is therefore considered that the signage would not represent a hazard or be confused for traffic or directional signals.

Aside from the illumination of the pylon signs and two fascia mounted signs on the canopy, the remainder of the signage will comprise flat non-illuminated acrylic type signs fixed to either the building or the awning in front of the building. The number of signs is relatively limited, with a single pylon sign, several small signs attached to the canopy, and several small acrylic signs fixed to the building and painted text signage on the side fencing, comprising "Eat & Drink", "Refresh and Revive" in the same font as "Convenience" as seen on the building at present. It is noted that the amount of signage proposed is appropriate in the context of an arterial road environment where there is quite a proliferation of signage already. It is considered that the signage and livery proposed is of a co-ordinated appearance, does not predominate the appearance of the site, or cause visual clutter or disorder.

It is considered that the proposed signs generally accord with the relevant provisions of the Development Plan as stated in "Advertisements" and do not jeopardise the intent of the Residential Zone or prejudice the achievement of the Zone provisions on adjacent parcels of land.

5.5 Interface with Adjacent Uses

OBJECTIVES

- 1 Development located and designed to prevent adverse impact and conflict between land uses.
- 2 Protect community health and amenity and support the operation of all desired land uses.

PRINCIPLES OF DEVELOPMENT CONTROL

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
 - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
 - (b) noise
 - (c) vibration
 - (d) electrical interference
 - (e) light spill
 - (f) glare
 - (g) hours of operation
 - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impact on existing and potential future land uses considered appropriate in the locality.

The site is within the context of a range of different land uses and nearby to the Local Centre Zone. Many nearby uses on Marion Road to the north of the site are of a commercial or retail nature, while to the west is a residential suburban setting that is screened from the proposed signs by virtue of how existing buildings are located on the site. Potential impacts arising from signs are therefore limited to the units to the south and the dwelling immediately to the north to a lesser extent. Properties directly opposite the site are a sufficient distance from the site not to be appreciably impacted by potential light spill. Additionally, some of these are commercial properties.

Irrespective, lighting associated with signage is such that it will have limited overspill to adjacent residential properties given the positioning of the forecourt canopy between the adjacent residences and the main control building, and the land incorporates a 2.1m fence along the southern boundary to obscure direct lines of sight between the common driveway of the adjacent units and the subject site.

The control building sits towards the centre of the site with a car wash behind it and to the south on the same site. No new signs are proposed near the boundaries to adjacent residential properties.

Other aspects relating to interface are covered under separate headings. Overall, it is considered that the proposal reasonably meets the provisions of the Development Plan that speak to interface.

6.0 ASSESSMENT OF EXPECTED SOCIAL, ECONOMIC AND ENVIRONMENTAL EFFECTS

It is not expected that there will be any negative social, economic or environmental effects on the locality as a result of the proposed development; especially considering the signage proposed primarily replaces signs historically located on the site.

The subject land occupies a prominent location at the intersection of a primary arterial road and a local street, providing a high level of exposure for an integrated service station complex and a high degree of accessibility for customers via access from both

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY2016

roads. Accordingly, the proposed development is considered to represent a positive economic driver for the locality and the Council area in general.

6.1 Social

The proposal is not anticipated to result in a serious negative social impact in terms of the continuing amenity of the residential area as illuminated signage is generally sited and designed such that it does not face residential property, or cause unreasonable light overspill.

6.2 Environmental

The proposal is not anticipated to have any serious environmental effects in terms of natural biological systems, nor loss of vegetation. In fact, the proposal seeks to erect signage in predominantly the same location as signs associated with the previous tenant.

Signage will comprise professionally designed advertising signs erected to industry standards and all waste will be disposed of by licensed waste contractors to an appropriate facility, during construction.

It is not considered that the signs would have any perceivable environmental impact once erected; given the baseline environment is a populated, somewhat polluted, heavily trafficked, metropolitan area with no semblance of natural biological systems or processes, or native fauna that could be impacted.

6.3 Economic

The proposal represents an economic use of land in a manner that assists in realising its commercial potential given the frontage to the major metropolitan arterial road. Signage is strictly advertising products and services available on the site and is such that it would not have any significantly detrimental effect on the economic function and performance of existing centre and commercial zones (as it has never had an impact in the past given a similar development has existed on the same site with a similar amount of signage).

7.0 REFERRALS AND PROCEDURAL MATTERS

Procedural Matters of the Residential Zone list all forms of advertisement and/or advertisement hoarding as Non Complying development in the Zone.

As such the proposal is required to undergo a Category 3 public notification process.

While the site is located on an arterial road, there are no State Agency referrals required as the proposal is not located within 100m of a signalised junction or pedestrian actuated crossing.

8.0 CONCLUSION

Having regard to the relevant provisions of the City of West Torrens Development Plan, the condition of the site and the nature of the locality, it is considered on balance that the proposed development sufficiently accords with the provisions of the Development

ANDRASH PTY LTD REPLACEMENT SIGNAGE – 294 MARION ROAD, NETLEY

2016

Plan, and more particularly the General Sections of Advertising Signage, Design and Appearance & Transport Safety (contained in Transportation and Access).

In summary, the proposal is appropriate for the following reasons:

- the proposal is not for the *introduction* of a new or additional non-complying development of the land (i.e. Advertising signs of similar size and within much the same locations have lawfully existed on the site previously, other than that proposed on the new canopy);
- Signs are of a theme and colour scheme commensurate with that of the service station complex, which lawfully exists on the site, and are appropriate for the locality;
- Signs do not cause unreasonable overspill beyond boundaries;
- The signage does not scroll, move, flash or otherwise provide a distraction to motorists on the adjacent road network, or impair the amenity of the locality;
- The signs are consistent with the sizing and location of signs typically anticipated on commercial sites in an arterial road environment, and the number of and scale of signs compliments the remainder of the built form on the site and locality;
- The proposal will improve the form and character of the site and deliver enhancements to the amenity of the site and neighbouring properties by consolidating signage into a signal pylon sign and having signs that are located away from sensitive receivers of impact; &
- The amenity and baseline environment of the immediate locality is somewhat compromised or affected by the large volumes of traffic along this major metropolitan arterial road, and there is a predominance of 'out of zone' commercial and retail uses within the locality. This ultimately means there is less impact associated with the proposal in the context.

On balance it is considered that the proposal is deserving of consent, and it is respectfully requested that the Development Assessment Panel resolve to issue Development Plan Consent subject to the Development Assessment Commission's concurrence and with conditions of approval as appropriate for this type of development.



ANDREW CRONIN MPiA
Director, AcroPLAN[SA] Pty Ltd
B.Des.St. MURP

18 March 2017

17/10/16

Ms Jessica Grima
Development Officer, Planning
City of West Torrens
Via email: jgrima@wtcc.sa.gov.au



AcroPLAN[SA] Pty Ltd

PO Box 1146
GLENELG SOUTH SA 5045
Ph: 0416839459
E: acroplansa@gmail.com

Dear Jessica

RE: Removal of Two Internally Illuminated Pylon Acrylic Advertisement Signs with Hoardings and Replacement with a Single Pylon Internally Illuminated Acrylic Advertisement Sign with Concealed Hoarding to 6.7M in Height associated with Existing Integrated Service Station Complex and Car Wash; and Replacement Of Existing Forecourt Canopy Mounted Internally Illuminated Signage (associated with replacement Forecourt Canopy on separate application). [Non-Complying]

At: Allotment 40 (No. 294) MARION ROAD, NETLEY

I have been engaged by the applicant to provide a brief Statement of Support in relation to the application above pursuant to Regulation 17(1) of the *Development Regulations 2008*, which will be followed by a Statement of Effect should Council elect to proceed with the assessment of this application.

To give a background, the service station premises at the above site has been operational for a number of decades. It has changed ownership over time and been re-branded previously (having been a Liberty Service Station before being changed to "Mogas" in 2009) and the proposal in this case seeks to again, re-brand the service station to "X Convenience" with "Mobil" livery akin to similar Andrash owned service station outlets that have undergone recent re-branding, such as that at Cross Road, South Plympton as an example.

The proposal includes consolidating two existing pylon signs with advertisement hoardings to a single, taller pylon sign in the same location as the current larger pylon sign at the north-eastern corner of the site. This will include advertising of the fuel pricing, petrol branding and consumables available on site.

The smaller of the two pylon signs is currently adjacent to a group of residential flats to the south, close to the boundary, which is approximately 3.5m in height. This would be removed altogether as part of the proposal.

The replacement forecourt canopy signage is proposed to be affixed to a new forecourt canopy that has been lodged as a separate application. This canopy is proposed to be extended to cover the fuel bowers closest to Marion Road, making the fuel dispensing to vehicles filling up on the eastern side of the bowers safer (significantly reducing the likelihood of contaminated water runoff from the forecourt in heavy rainfall) and more pleasant for customers. The signage on the new forecourt canopy will be of a similar nature to existing illuminated signage on the existing forecourt canopy, in the "Mobil" livery instead of the current "Mogas" arrangements.

The site is located in the Residential Zone, Policy Area 20 (Low Density) of the City of West Torrens Development Plan, Consolidated 16 May 2016, which is the relevant Development Plan for this application. The Zone generally contemplates typical suburban infill development as well as continuance of a predominantly low density residential character, and small scale non-residential uses that complement the residential character and amenity expected within a residential zone.

However, it is to be noted that the amenity of the locality is not typical of a normal residential setting. The locality is heavily influenced by the substantial traffic volumes of the Marion Road arterial road environment, which detracts from the amenity of the locality, primarily acoustically and through pollution associated with high traffic volumes, but also the visual and thermal character, comprising a wide expanse of bitumen susceptible to heat island effect and quite narrow footpaths without significant shading vegetation or buffering elements separating vehicles from pedestrians.

Other than the subject site there are other non-residential uses on Marion Road in the locality, including within the Residential Zone. This includes a bus depot diagonally opposite, a small delicatessen to the north and a mechanical repair station further south of the site. Other non-residential uses in the locality, located in a Local Centre Zone include a real estate office and childcare centre, directly opposite the site, on the eastern side of Marion Road. Overall the land use character of the locality could be said to be quite typical of an arterial road environment where residential development exists alongside a number of non-residential uses often with direct interface.

The proposal comprises building work only. There are no changes to the operational aspects of the development that exists on the site. The building work seeks to improve the built form character of the existing service station complex and provide a single consolidated pylon advertising sign for the site on the Marion Road frontage, which will reduce the overall amount of advertising signage on the site.

There is an argument to suggest that building work in pursuance of improvement to an existing non-complying use should not be treated as non-complying development. However it is noted that the Non Complying development list of the Procedural Matters section of the

Residential Zone appears quite deliberately to be 'catch all' with its wording, making any improvements to existing 'out of zone' uses difficult to be able to be assessed as 'merit' applications. A legal opinion has not been sought to consider the specific circumstances of this application in terms of whether the proposal could indeed be considered as 'merit' development, however the applicant wishes to proceed with the assessment on a 'without prejudice' basis, reserving the right to challenge the Council classification of the development as 'non-complying' should this be necessary, pursuant to Section 86(1)(f) of the *Development Act 1993*.


Notwithstanding the above, I am of the opinion that there is sufficient planning merit for Council to proceed with assessment of what is proposed in this application for the following reasons:

- The proposal consolidates two existing advertisement pylon signs to a single pylon sign for the site;
- The canopy signage to be mounted on a new canopy (separate application), while marginally higher due to the increased canopy height, is substantially the same as existing signage and sufficiently setback from adjacent residential units to not impinge on their amenity;
- The environs of the site and locality are atypical of a residential setting, reducing the potential planning impacts arising;
- The proposal seeks to improve the visual appearance of an existing 'out of zone' use, which would not readily be able to be converted to residential use; &
- The proposal does not alter the existing operational aspects of the use and site.

I trust this provides sufficient grounds for Council to proceed with its assessment of this application, which has been deemed to be non-complying in nature. It is acknowledged that a Statement of Effect is required prior to public notification, and there are additional fees payable. For payment requests and invoicing of any outstanding fees please email these to headoffice@andrash.com.au and c/c: angelyn@internode.on.net (Angelyn Building Designs) who is coordinating this application.

Thanks in advance for consideration of this application, and I will await your subsequent request for a Statement of Effect and any other information as may be necessary. If you have any queries please contact me on 041 6839 459 or by email acroplansa@gmail.com

Yours sincerely,



Director, AcroPLAN[SA] Pty Ltd

Urban Planner/ Designer

MURP BDesSt

7 CONFIDENTIAL REPORTS OF THE CHIEF EXECUTIVE OFFICER

7.1 23A Rowells Road, LOCKLEYS

Application No. 211/1008/2015/C

Reason for Confidentiality

It is recommended that this Report be considered in CONFIDENCE in accordance with Section 56A(12)(a) (vii) of the *Development Act 1993*, which permits the meeting to be closed to the public for business relating to the following:

- (vii) matters that must be considered in confidence in order to ensure that the council does not breach any law, order or direction of a court or tribunal constituted by law, any duty of confidence, or other legal obligation or duty.

as this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.

RECOMMENDATION

It is recommended to Development Assessment Panel that:

1. On the basis that this matter is before the Environment Resources and Development Court so any disclosure would prejudice the position of Council, the Development Assessment Panel orders pursuant to Section 56A(12)(a) of the *Development Act 1993*, that the public, with the exception of the Chief Executive Officer, members of the Executive and Management Teams, City Development staff in attendance at the meeting, and meeting secretariat staff, and other staff so determined, be excluded from attendance at so much of the meeting as is necessary to receive, discuss and consider in confidence, information contained within the confidential reports submitted by the Chief Executive Officer.
2. At the completion of the confidential session the meeting be re-opened to the public.

7.2 57 McArthur Avenue, PLYMPTON

Application No. 211/1420/2016

Reason for Confidentiality

It is recommended that this Report be considered in CONFIDENCE in accordance with Section 56A(12)(a) (vii) of the *Development Act 1993*, which permits the meeting to be closed to the public for business relating to the following:

- (vii) matters that must be considered in confidence in order to ensure that the council does not breach any law, order or direction of a court or tribunal constituted by law, any duty of confidence, or other legal obligation or duty.

as this matter is before the Environment Resources and Development Court and it is a requirement of the Court that matters are kept confidential until such time as a compromise is reached or the matter proceeds to a hearing.

RECOMMENDATION

It is recommended to Development Assessment Panel that:

1. On the basis that this matter is before the Environment Resources and Development Court so any disclosure would prejudice the position of Council, the Development Assessment Panel orders pursuant to Section 56A(12)(a) of the *Development Act 1993*, that the public, with the exception of the Chief Executive Officer, members of the Executive and Management Teams, City Development staff in attendance at the meeting, and meeting secretariat staff, and other staff so determined, be excluded from attendance at so much of the meeting as is necessary to receive, discuss and consider in confidence, information contained within the confidential reports submitted by the Chief Executive Officer.
2. At the completion of the confidential session the meeting be re-opened to the public.

8 SUMMARY OF COURT APPEALS

8.1 Summary of Court Appeals

Brief

Monthly statistics are provided for the information of the Panel in relation to:

1. any matters being referred to the Development Assessment Commission (DAC); and
2. any planning appeals before the Environment, Resources and Development Court (ERDC) and their status.

RECOMMENDATION

The Development Assessment Panel receive and note the information.

The current status is listed as follows:

Matters pending determination by DAC

Reason for referral	DA number	Address	Description of development
Section 49	211/722/2016	Lot 2 West Beach Road, WEST BEACH	Change of use to function room
Schedule 10	211/136/2015	134-136 Anzac Highway, GLANDORE	On The Run redevelopment
Section 49	211/1155/2012/A	Lot 2 West Beach Road, WEST BEACH	Amendment to condition regarding lighting
Section 49	211/347/2017	Lot 52 Military Road, WEST BEACH	Addition to tourist accommodation
Section 49	211/54/2017	370 South Road, RICHMOND	Permanent storage of five shipping containers
Section 49	211/370/2017	11-13 Desmond Avenue, MARLESTON	Change of use to distribution facility
Section 49	211/504/2017	Lot 2 West Beach Road, WEST BEACH	Installation of two grandstands and one viewing platform
Section 49	211/507/2017	Lot 2 West Beach Road, WEST BEACH	Installation of two transportable cabins
Section 49	211/586/2017	Lot 14 Richmond Road, KESWICK TERMINAL	Removal of two regulated trees
Section 49	211/698/2017	Lot 12 Richmond Road, KESWICK TERMINAL	Construction of an outbuilding for storage

Development Application appeals before the ERDC

DA Number	Address	Reason for Appeal	Description of Development	Status
211/476/2015/2 & 211/476/2015/3	452 Henley Beach Road, LOCKLEYS	Applicant appealed DAP refusal	Retirement village	Hearing 15-17 May 2017
211/1500/2015	8 Packard Street, NORTH PLYMPTON	Applicant appealed DAP refusal	Construct a residential flat building containing six dwellings	Conciliation Plan pending
211/486/2016	24 Garfield Avenue, KURRALTA PARK	Applicant appealed DAP refusal	Construct a residential flat building containing seven dwellings	Finalisation of compromise pending
211/1166/2016	12 Lowry Street, FULHAM	Applicant appealed DAP refusal	Land division & two dwellings	Finalised
211/1215/2016	21 Bickford Street, RICHMOND	Applicant appealed DAP refusal	Reg Tree removal	Withdrawn
211/1079/2016 211/1295/2016	6 Kimber Terrace, KURRALTA PARK	Applicant appealed DAP refusal	Construct a residential flat building containing FOUR dwellings	Finalisation of compromise pending
211/1008/2015/C	23A Rowells Road, LOCKLEYS	Applicant appealed DAP refusal	Amendment to semi-detached dwelling	Compromise plans presented to July DAP Meeting
211/1420/2016	57 McArthur Avenue, KURRALTA PARK	Applicant appealed DAP refusal	Construct a residential flat building containing four dwellings	Compromise plans presented to July DAP Meeting
211/271/2017	23 White Avenue, LOCKLEYS	Applicant appealed DAP refusal	Carport, alfresco, swimming pool, safety fence, masonry front fence	Preliminary Conference Pending

SUMMARY

The information requested by the Panel has been provided for information purposes.

Attachments

Nil

9 MEETING CLOSE